

Port Phillip Planning Scheme

Amendment C219port

Explanatory Report

Overview

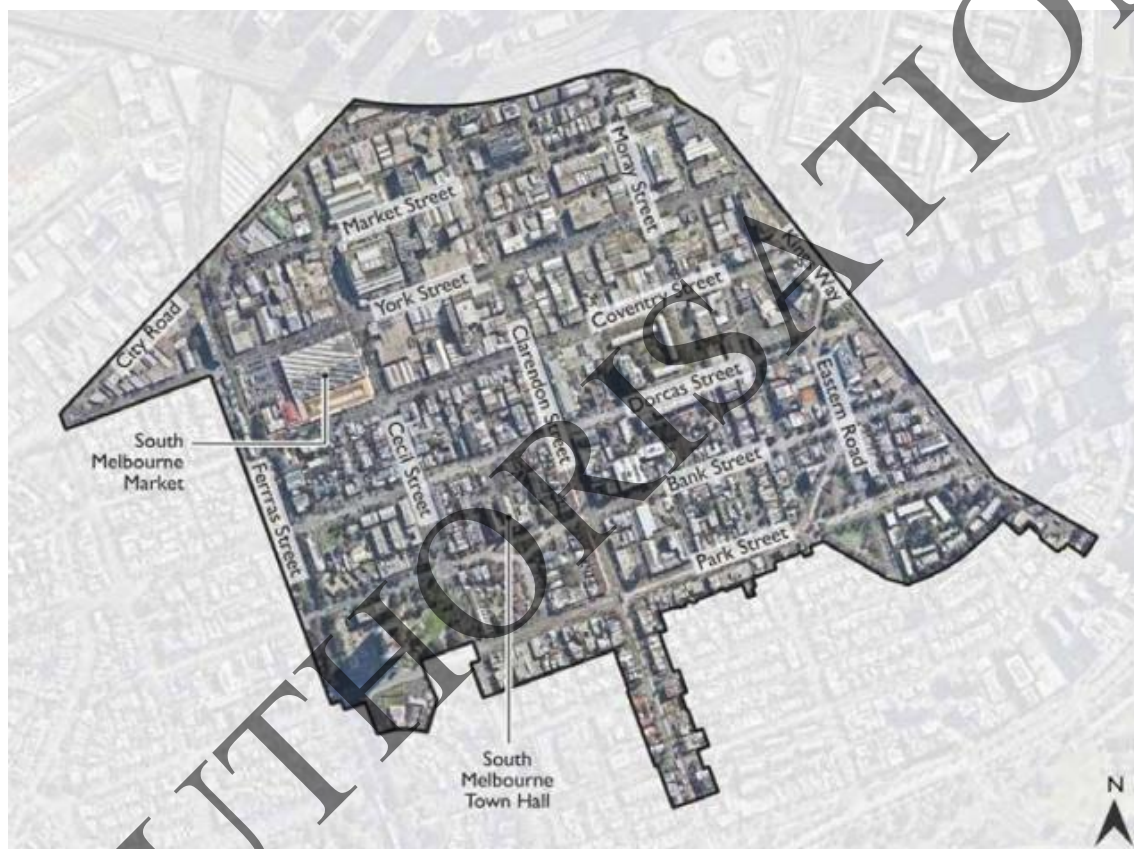


Figure 1: Map of South Melbourne Structure Plan Area

The amendment applies to all land within the South Melbourne Structure Plan Area shown in Figure 1.

The amendment implements the recommendations set out in the South Melbourne Structure Plan which provides a long-term strategic vision for South Melbourne. New planning provisions seek to guide the management of activities and uses, built form, movement and transport, and public realm in South Melbourne.

The amendment introduces new place-based policies for the South Melbourne Structure Plan Area in the Municipal Planning Strategy and Planning Policy Framework. In addition, the amendment replaces Design and Development Overlay (DDO) Schedules 8 and 16 with four new DDO schedules. Together, they will provide a contemporary planning framework to

guide land use, built form, movement and transport, and public realm matters in South Melbourne.

Further, the amendment rezones the 'City Road Industrial Triangle' bound by City Road, Ferrars Street and York Street from the Industrial 1 Zone (IN1Z) to the Commercial 2 Zone (C2Z) and applies the Environmental Audit Overlay (EAO) to the land. It also applies the Heritage Overlay to 9 individual heritage places assessed to be of local significance and 4 places assessed to be of contributory significance to be included as an extension to HO440 (Emerald Hill Residential Precinct).

Finally, the amendment makes consequential changes to the Planning Policy Framework and Operational Provisions.

Where you may inspect this amendment

The amendment can be inspected free of charge at the City of Port Phillip website at <https://haveyoursay.portphillip.vic.gov.au/future-south-melbourne>.

The amendment is also available for public inspection, free of charge, during office hours at the following places:

- St Kilda Town Hall, Corner Carlisle Street and Brighton Road, St Kilda
- Emerald Hill Library and Heritage Centre, 195 Bank Street, South Melbourne
- Port Melbourne Town Hall and Library, 333 Bay Street, Port Melbourne
- Albert Park Library, 319 Montague Street, Albert Park
- Middle Park Library and Community Centre, 254-256 Richardson Street, Middle Park
- St Kilda Library, 150 Carlisle Street, St Kilda

The amendment can also be inspected free of charge at the Department of Transport and Planning website at <http://www.planning.vic.gov.au/public-inspection> or by contacting the office on 1800 789 386 to arrange a time to view the amendment documentation.

Submissions

Any person may make a submission to the planning authority about the amendment. Submissions about the amendment must be received by [insert submissions due date].

A submission must be made:

- Via email to: FutureSouthMelbourne@portphillip.vic.gov.au;
- Via the project *Have Your Say* website: <https://haveyoursay.portphillip.vic.gov.au/future-south-melbourne>; or
- By post mailed to: City of Port Phillip, Private Bag 3 St Kilda, Victoria, Australia 3182

Panel hearing dates

In accordance with clause 4(2) of Ministerial Direction No.15 the following panel hearing dates have been set for this amendment:

- Directions hearing: [insert directions hearing date]
- Panel hearing: [insert panel hearing date]

Details of the amendment

Who is the planning authority?

This amendment has been prepared by the City of Port Phillip, who is the planning authority for this amendment.

Land affected by the amendment

The amendment applies to all land within the South Melbourne Structure Plan Area shown in Figure 1.

A mapping reference table is attached at Attachment 1 to this Explanatory Report to provide further information about land affected by the amendment.

What the amendment does

The amendment implements the recommendations set out in the South Melbourne Structure Plan which provides a long-term strategic vision for South Melbourne. New planning provisions seek to guide the management of activities and uses, built form, movement and transport, and the public realm in South Melbourne.

Specifically, the amendment makes the following changes to the Port Phillip Planning Scheme:

1. Updates Clause 02 (Municipal Planning Strategy) to reflect the strategic vision set out by the South Melbourne Structure Plan.
2. Deletes Clause 11.03-1L-05 (South Melbourne Central) and introduces new place-based policies to the Planning Policy Framework at:
 - a. Clause 11.03-6L-08 (South Melbourne)
 - b. Clause 17.01-1L (Diversified economy - South Melbourne Enterprise Precinct)
 - c. Clause 17.01-2L (Innovation and research – South Melbourne Enterprise Precinct).
3. Amends the following clauses in the Planning Policy Framework to update policy references and make consequential changes:
 - a. Clause 11.03-1L-01 Activity centres
 - b. Clause 15.01-1L-02 Urban design
 - c. Clause 15.01-5L Neighbourhood character
 - d. Clause 15.03-1L Heritage policy
 - e. Clause 17.02-1L Mixed use and office areas
 - f. Clause 17.03-2L Sustainable industry
 - g. Clause 18.01-3L-01 Sustainable and safe transport in Port Phillip
 - h. Clause 18.02-3L-01 Public transport
 - i. Clause 19.02-4L Community facilities.
4. Deletes Design and Development Overlay Schedule 8 (South Melbourne Central) and Design and Development Overlay Schedule 16 (Cnr York, Cecil & Market Streets, South Melbourne).
5. Introduces four new Design and Development Overlay schedules:
 - a. Design and Development Overlay Schedule 37 (Clarendon Street Precinct)

- b. Design and Development Overlay Schedule 38 (South Melbourne Market Precinct)
 - c. Design and Development Overlay Schedule 39 (South Melbourne Enterprise Precinct East and Kings Way Mixed Use Corridor)
 - d. Design and Development Overlay Schedule 40 (South Melbourne Enterprise Precinct West).
6. Rezones the 'City Road Industrial Triangle' bound by City Road, Ferrars Street and York Street from the IN1Z to the C2Z and applies the EAO to this land.
7. Corrects the zoning of seven sites including:
- a. The block of land bound by York, Cecil, Market and Northumberland streets, South Melbourne
 - b. 176-188 Bank Street, South Melbourne
 - c. 383-385 Clarendon Street, South Melbourne
 - d. 68-72 York Street, 191-199 Clarendon Street and 31 Ross Street, South Melbourne
 - e. 120-142 Clarendon Street, Southbank
 - f. 102-106 Park Street, South Melbourne
 - g. 77 Park Street and 286-294 Kings Way, South Melbourne
8. Applies a Heritage Overlay to and inserts new Statements of Significance for nine individual heritage places, including:
- a. HO561: 89 and 99 Eastern Road, and 36A and 58A Napier Street, South Melbourne
 - b. HO563: 110 Bank Street, South Melbourne
 - c. HO564: 151, 153 & 155 Dorcas Street, South Melbourne
 - d. HO565: 108 Bank Street, South Melbourne
 - e. HO566: 167-173 Coventry, South Melbourne
 - f. HO567: 82-86 Clarke Street, South Melbourne
 - g. HO568: 49-55 York Street, South Melbourne
 - h. HO569: 152 Clarendon Street, South Melbourne
 - i. HO570: 68 and 70 Market Street, Southbank
9. Extends the boundary of HO440 to apply to an additional four heritage places:
- a. 2 Alfred Place, South Melbourne,
 - b. 214-216 and 220 Clarendon Street, South Melbourne
 - c. 31 Ross Street, South Melbourne
 - d. 22 Moray Street, South Melbourne.
10. Makes consequential changes to the following:
- a. *Port Phillip Heritage Review - Volumes 1-6* (Adoption Version Amendment C161port Part 2, December 2021)
 - b. *City of Port Phillip Heritage Policy Map* (Adoption Version Amendment C161port Part 2, December 2021) (Part of Port Phillip Heritage Review)
 - c. *City of Port Phillip Neighbourhood Character Map* (Adoption Version Amendment C161port Part 2, December 2021) (Part of Port Phillip Heritage Review).
11. Amends the Operational Provisions at the schedules to:
- a. Clause 72.04 Incorporated Documents
 - b. Clause 72.08 Background Documents
 - c. Clause 74.01 Application Of Zones, Overlays And Provisions
 - d. Clause 72.04 (Further Strategic Work).

Strategic assessment of the amendment

Why is the amendment required?

The amendment is required to implement the recommendations of the *South Melbourne Structure Plan* (City of Port Phillip, June 2024).

South Melbourne is identified as a Major Activity Centre (MAC) in *Plan Melbourne 2017-2050* and as an Enterprise Precinct comprising regionally significant employment land in the *Melbourne Industrial and Commercial Land Use Plan* as “Regionally Significant Industrial Land”. Therefore, South Melbourne is required to play a significant role in achieving the directions of State Policy contained in these documents, along with the directions of State Policy contained in the Planning Policy Framework of the Port Phillip Planning Scheme, particularly regarding housing and employment.

Given its crucial role, the South Melbourne Structure Plan (SMSP) comprises an integrated planning framework to guide change in South Melbourne over a 20-year period. This framework will provide a high level of clarity and certainty for Council, applicants and the wider community on the preferred vision sought for the area.

The South Melbourne Structure Plan is underpinned by a robust suite of technical documents, including the:

- *South Melbourne Structure Plan Background Report 2024* (City of Port Phillip)
- *South Melbourne Built Form Review 2024* (Hodyl & Co)
- *South Melbourne Major Activity Centre & Employment Precincts: Heritage Built Form Analysis & Recommendations 2023* (GJM Heritage)
- *South Melbourne Heritage Gaps Analysis 2023* (Trethowan)
- *South Melbourne Public Realm Framework 2024* (City of Port Phillip)
- *South Melbourne Employment, Economic and Land Use Study 2023* (Urban Enterprise with Echelon Planning)
- *South Melbourne Transport and Movement Study 2023* (Ratio Consultants)
- *South Melbourne Activity Centre Boundary Report 2024* (City of Port Phillip)
- *South Melbourne Zoning Anomalies Report 2024* (City of Port Phillip).

Updated Planning Policy Framework and Municipal Planning Strategy

Policy at clauses 11.03-6L-08 (South Melbourne), Clause 17.01-1L (Diversified economy - South Melbourne Enterprise Precinct) and Clause 17.01-2L (Innovation and research – South Melbourne Enterprise Precinct) are place-based policies that will support South Melbourne’s vibrancy, liveability and diversity. Together, these policies seek to strengthen and consolidate the MAC’s retail, commercial and residential role, expand the Enterprise Precinct’s role and ensure the viability of the Kings Way Mixed Use Corridor. Specific policy is included in Clause 11.03-6L-08 (South Melbourne) on activities and uses, built form and building design, access and movement, and the public realm.

Updated policy in other parts of the PPF recognises the role of the South Melbourne Structure Plan as a background document in informing discretion on specific development matters as they relate to the Structure Plan area.

Updated strategic directions for South Melbourne in the Municipal Planning Strategy at

Clauses 02.03-1 (Settlement) and 02.03-6 (Economic development) more strongly reflect the role of the South Melbourne MAC, South Melbourne Enterprise Precinct and the Kings Way Mixed Use Corridor as an anchor for a sustainable and well-designed mixed use precinct.

Design and Development Overlay Schedules 37, 38, 39 and 40

The amendment implements the built form recommendations of the *South Melbourne Structure Plan, June 2024* (City of Port Phillip) through the introduction of four new schedules to the DDO, including:

- Design and Development Overlay Schedule 37 (Clarendon Street Precinct)
- Design and Development Overlay Schedule 38 (South Melbourne Market Precinct)
- Design and Development Overlay Schedule 39 (South Melbourne Enterprise Precinct East and Kings Way Mixed Use Corridor)
- Design and Development Overlay Schedule 40 (South Melbourne Enterprise Precinct West).

These new DDO schedules replace the existing DDO8 and DDO16, containing precinct specific controls, which seek to strengthen the role of South Melbourne MAC, South Melbourne Enterprise Precinct and the Kings Way Mixed Use Corridor. In turn, the controls provide a tailored response for South Melbourne to ensure forecast growth occurs in a way that successfully balances the valued attributes of the activity centre and delivers a high-quality living and working environment, complemented by an attractive public realm and accessible movement network.

The amendment proposes to implement the following key controls:

Control	Rationale
Mandatory Floor Area Ratios (FARs)	Ensures new development provides a contextual response to the site considering the overall vision for South Melbourne and the specific precinct objectives. The key benefits of using FAR controls include: <ul style="list-style-type: none"> ▪ Provides greater clarity and certainty of future development yield that can be delivered on the site whilst ensuring that amenity outcomes are appropriately managed ▪ Provides design flexibility to respond to the specific conditions of a site and the surrounding context, enabling more site responsive design.
Discretionary maximum building heights	Mandatory FARs are applied with discretionary building heights (metres) to reflect the highly varied character of the Structure Plan area and have been determined through considering factors such as the role and width of streets, lot sizes, interfaces and land uses.
Mandatory maximum building height – DDO37 - Clarendon Street Precinct	Applies only to the Clarendon Street Precinct and is consistent with <i>Planning Practice Note: 59: Role of Mandatory Provisions in Planning Schemes</i> due to the Clarendon Street Precinct's high heritage value, including heritage places included on the Victorian Heritage Register.
Discretionary street wall heights and building	Street wall heights have been determined through considering the role and width of streets. The relationship or ratio between

setbacks (above street wall).	the street wall height and width of the street therefore varies across the Structure Plan area.
Mandatory street wall heights and setbacks	Applies to the Clarendon Street Precinct DDO37 and specific locations in other DDOs. This approach is consistent with <i>Planning Practice Note: 59: Role of Mandatory Provisions in Planning Schemes</i> due to the Clarendon Street Precinct and other location's high heritage value, including heritage places included on the Victorian Heritage Register.
Adaptable building requirements	Enable development to provide adequate floor-to-floor heights that support good internal amenity outcomes and are adaptable to changes in future land use over time, including the floor-to-floor heights of car parking.
Overshadowing	Ensure solar access controls are more targeted to maintain sunlight at certain times of the year to key streets and open spaces. Generally, there is a shift from winter solstice controls to spring equinox controls. Changes to these controls are recommended to enable development outcomes that better reflect the role and function of South Melbourne as a Major Activity Centre and Enterprise Precinct.
Frontage requirements	Ensure frontages should be designed to enhance passive surveillance and contribute to a public realm that is attractive, engaging and safe.
Building separation requirements	Provide equitable development opportunities, avoid visual bulk and provide equitable access to an outlook and good daylight.
Light well requirements	Provide guidance on the use of light wells to provide high-amenity housing and workplaces.
Wind effects on the public realm – DDO38, DDO39, DDO40	Applies to South Melbourne Market Precinct DDO38, South Melbourne Enterprise Precinct East and Kings Way Mixed Use DDO39 and South Melbourne Enterprise Precinct West DDO40 for buildings and works higher than 40 metres. This ensures wind conditions in publicly accessible areas are safe. Does not apply to Clarendon Street Precinct DDO37, given the maximum building height specified is lower than 40 metres.

Heritage Overlay

The amendment gives effect to the heritage recommendations contained in the *South Melbourne Heritage Gaps Analysis 2023* (Trethowan), including applying the Heritage Overlay to 9 individual heritage places assessed to be of local significance and 4 places assessed to be of contributory significance to be included as an extension to HO440.

Rezoning

The amendment proposes to rezone the 'City Road Industrial Triangle' bound by City Road, Ferrars Street and York Street from the Industrial 1 Zone (IN1Z) to the C2Z. The nature of these land uses aligns with those within the broader Enterprise Precinct, providing greater certainty around development outcomes for this area. Retaining the C2Z will protect the Enterprise Precinct's employment growth. No other land is proposed to be rezoned, other than correcting zoning anomalies, generally where either two zones apply to one site.

Environmental Audit Overlay

The rezoning of the former 'City Road Industrial Triangle' from INZ1 to C2Z will allow for land uses such as a child care centre, kindergarten, primary or pre-school, secondary school, children's playground and caretaker house, which are defined as sensitive uses. Hence, the Environmental Audit Overlay (EAO) will apply to this land, which is assessed as having high potential for contamination. The application of an EAO must be undertaken in accordance with relevant policy including *Planning Practice Note 30: Potentially contaminated land*, *Ministerial Direction No.1 Potentially Contaminated Land* and *Ministerial direction 19 – Amendments that may result in impacts on the environment, amenity and human health*. Consistent with these documents, Council undertook an assessment to determine the risk of contamination on a site-by-site basis.

The application of the EAO was based on:

- The *South Melbourne Employment, Economic and Land Use Study 2023* (Urban Enterprise with Echelon Planning) and site inspections to determine current land uses and activities.
- The current zone that applies to the land.
- Sands & McDougall's directory of Victoria - Melbourne & suburban profession & trade directory:
 - Years 1896, 1905, 1915, 1925, 1935, 1945, 1955, 1965, 1974.
- Review of the EPA Priority Site Register.
- Review of photographs held by the State Library of Victoria.

Importantly, the City Road Industrial Triangle is proximate to the Fishermans Bend Urban Renewal Area, where the EAO has been applied to all precincts where sensitive uses are envisaged. Studies undertaken to inform the *Fishermans Bend Framework* (Victorian Government, 2018) suggest that heavy metals and solvents may be widespread across Fishermans Bend and confirm issues with ground water contamination due to past land use activities.

The application of the Environmental Audit Overlay (EAO) to the nominated sites is required to address any potential site contamination issues to meet the requirements of *Ministerial Direction No. 1 – Potentially Contaminated Land*. The potential for contamination results from past and current activities on these sites.

The EAO requires an environmental audit to be undertaken on the land and for a certificate or statement to be obtained from an environmental auditor in accordance with the Environment Protection Act 1970, or a preliminary risk screen assessment or environmental audit statement under the *Environment Protection Act 2017* to be issued. The audit is carried out prior to the construction of the buildings and works in association with a dwelling or other sensitive use.

Correction of mapping errors

The amendment corrects several historical mapping errors where two zones apply to a site. Dual-zoned sites will be corrected to "provide for the fair, orderly, economic and sustainable use and development of land" in accordance with the objectives (in part) of the *Planning and Environment Act 1987*. Now that the anomalies have been corrected, the land can be developed and used in accordance with the zone provisions.

How does the amendment implement the objectives of planning in Victoria?

The amendment implements the objectives of planning in sections 4 and 12(1)(a) of the *Planning and Environment Act 1987*, by updating the planning and policy framework that applies to South Melbourne. New place-based policy, new DDO schedules and addressing gaps in the Heritage Overlay will support appropriate integrated development outcomes, along with investments in infrastructure and the public realm to support the ongoing vibrancy and growth of the South Melbourne MAC, South Melbourne Enterprise Precinct and Kings Way Mixed Use Corridor. Such future outcomes and investment will in turn support the implementation of the objectives of planning in Victoria.

How does the amendment address any environmental, social and economic effects?

By introducing a new planning framework for South Melbourne, the amendment adequately addresses environmental, social and economic effects pursuant to sections 12(2)(b) and (c) of the *Planning and Environment Act 1987*, in turn resulting in a net community benefit. An assessment of environmental, social and economic effects follows.

Environmental

A key pillar of the amendment is to introduce new built form controls and policy to South Melbourne, which are underpinned by extensive built-form analysis. Implementing a mandatory FAR with other predominately discretionary controls, will result in more sustainable design outcomes in terms of design and function. Importantly, new planning provisions will address shortcomings with existing planning controls, enabling built form outcomes that better respond to South Melbourne's heritage context and that are more consistent with the features that give South Melbourne its distinct character, such as consistent streetwalls.

The amendment implements policy seeking to ensure public realm improvements contribute sustainable solutions to addressing climate change. Similarly, the amendment implements policy to encourage a shift to sustainable modes of transport, namely walking and cycling.

Rezoning the 'City Road Industrial Wedge' from IN1Z to C2Z and applying the EAO will recognise the transition of this remnant industrial land to uses generally aligned with the broader South Melbourne Enterprise Precinct.

Social

The amendment increases the development capacity of South Melbourne, ensuring that future development demand for residential and non-residential uses can be met across the South Melbourne MAC, Enterprise Precinct and Kings Way Mixed Use Corridor. New policy and controls seek to improve the internal and external amenity of development, to improve the design quality of new development for residents, workers and visitors. Policy encouraging public realm improvements seeks to reinforce civic pride and enhance a sense of place.

Economic

South Melbourne plays a crucial role in supporting Greater Melbourne's economic activity,

confirmed by technical analysis contained in the supported by the *South Melbourne Employment, Economic and Land Use Study* (Urban Enterprise, November 2023), Therefore, this amendment largely maintains the existing zoning framework that has facilitated its success as a MAC and state-recognised Enterprise Precinct. In turn, this will support the growth of South Melbourne's economic specialisations, especially its unique specialisation in creative industries, along with other economic specialisations in professional and financial services, and retail, hospitality and personal services. Critically, new planning controls and policy will help continue to guide development outcomes and public realm improvements that ensure South Melbourne remains attractive to these businesses and encourage sustained investment in South Melbourne.

Does the amendment address relevant bushfire risk?

The land affected by the amendment is not located within an area of identified bushfire risk.

Does the amendment comply with the requirements of any other Minister's Direction applicable to the amendment?

The amendment is consistent with the Ministerial direction on the form and content of planning schemes.

Ministerial Direction: The form and content of planning schemes

- The amendment complies with the requirements set out in the Ministerial Direction: The form and content of planning schemes.

Direction No. 1: Potentially contaminated land

- The amendment complies with Ministerial Direction No. 1 (Potentially contaminated land) by applying an EAO over land that is being rezoned from INZ1 to C2Z as the change in zoning will allow for a child care centre, which is defined as a sensitive use.

Direction No. 9: Metropolitan strategy

- The amendment is affected by Ministerial Direction No. 9 – Metropolitan strategy under Section 12 of the *Planning and Environment Act 1987*. *Plan Melbourne: Metropolitan Planning Strategy 2017-2050* is the adopted Metropolitan Strategy for the Melbourne metropolitan region including the City of Port Phillip.
- South Melbourne is identified as a Major Activity Centre in Plan Melbourne. The amendment is consistent with outcomes sought by Plan Melbourne by introducing new planning provisions that facilitate housing, employment and other future needs, while considering climate change and sustainability. Doing so will ensure South Melbourne can fulfil its role as a MAC.

Direction No. 11: Strategic assessment of amendments

- The amendment complies with Ministerial Direction No. 11 (Strategic Assessment of Amendments) under section 12 of the *Planning and Environment Act 1987*. The amendment is consistent with this direction which ensures a comprehensive strategic evaluation of a planning scheme amendment and the outcomes it produces has been undertaken.

Direction No. 15: The planning scheme amendment process

- The amendment complies with the requirements set out in Ministerial Direction No. 15 (The planning scheme amendment process).

Direction No. 19: (Amendments that may result in impacts on the environment, amenity and human health)

- The amendment complies with Ministerial Direction no. 19 (Amendments that may result in impacts on the environment, amenity and human health) by applying an EAO over potentially contaminated land in respect of the rezoning of land from I1Z to C2Z which will enable new land uses which require this assessment.
- The City of Port Phillip in its capacity as a Planning Authority sought the written views of the EPA as part of the consultation on the Draft South Melbourne Structure Plan. How the amendment responds to the views of the EPA is addressed in this Explanatory Report under the question 'How does the amendment address the views of any relevant agency?'

How does the amendment support or implement the Planning Policy Framework and any adopted State policy?

State policy

The amendment supports the implementation of State policies and strategies as follows.

Victoria's Housing Statement: The decade ahead 2024-2034

The amendment implements *Victoria's Housing Statement* by introducing DDO schedules that increase South Melbourne's development capacity, including residential development capacity.

Melbourne Industrial and Commercial Land Use Plan 2020

The amendment provides land use and built form guidance for parts of the South Melbourne Structure Plan Area that are identified in this document as both 'Regionally Significant Industrial Land' and as a 'Regionally Significant Commercial Area'.

Unlocking Enterprise in a Changing Economy Strategy 2018

The amendment provides land use and built form guidance for the South Melbourne Enterprise Precinct, one of three key areas outside Melbourne's CBD to attract new businesses and investment.

Urban Design Guidelines 2023

The amendment contains policy consistent with the aim to create neighbourhoods that foster community interaction and make it easy for people of all ages and abilities to live healthy lifestyles and engage in regular physical activity.

Victoria's Climate Change Strategy 2021

The amendment contains policy to contribute to minimising emissions and achieving a climate resilient Victoria by 2050, as articulated by this strategy.

Planning Policy Framework

The amendment supports the implementation of the policy contained in the Planning Policy Framework (PPF) as follows. The amendment balances competing objectives in favour of net community benefit and sustainable development as required by Clause 71.02-3 of the Port Phillip Planning Scheme).

Clause 11.03 Planning for Places

The amendment implements the objectives and strategies at Clause 11.03-6S (Regional and local places) by introducing an integrated place-based planning framework for the South Melbourne Major Activity Centre, South Melbourne Enterprise Precinct and Kings Way Mixed Use Corridor. The amendment contains policy for the South Melbourne Major Activity Centre supporting the outcomes sought by Clause 11.03-1S (Activity centres) and 11.03-1L-01 (Activity centres). The new Clause 11.03-6L-08 (South Melbourne) contains policy for activities and uses, built form, access and movement, and the public realm, seeking to provide guidance on the use of discretion to support decision-making on statutory and strategic planning matters.

Clause 13 Environmental Risks and Amenity

Generally retaining South Melbourne's existing zoning framework and rezoning the 'City Road Industrial Wedge' from IN1Z to C2Z and the application of the EAO supports policy seeking to ensure that land uses are compatible with one another and address their context. Built form guidance contained in the new Clause 11.03-6L-08 (South Melbourne) and DDO schedules considers policy in Clause 13.01-1S (Natural hazards and climate change).

Clause 15 Built Environment and Heritage

Built form guidance contained in the new Clause 11.03-6L-08 (South Melbourne) and new DDO schedules reinforce policy that recognises the role of urban design, building design, heritage and energy and resource efficiency in delivering liveable and sustainable cities, towns and neighbourhoods. Applying the Heritage Overlay supports policy seeking to protect places and sites with significant heritage, architectural, aesthetic, natural, scientific and cultural value.

Clause 16 Housing

The new DDO schedules will increase development capacity in South Melbourne, supporting the provision of diverse and sustainable housing, and leveraging South Melbourne's excellent provision of supporting infrastructure.

Clause 17 Economic Development

The amendment introduces policy to encourage and stimulate ongoing investment in and enhance the growth and vibrancy of the South Melbourne Enterprise Precinct at Clause 17.01-1L (Diversified economy - South Melbourne Enterprise Precinct) and Clause 17.01-2L (Innovation and research – South Melbourne Enterprise Precinct). In turn, this will implement policy at Clause 17 (Economic Development) to provide for a strong and innovative economy, where all sectors are critical to economic prosperity, along with contributing to the economic wellbeing of the state and foster economic growth.

Clause 18 Movement Networks

The amendment reinforces the need for the transport system to be safe, integrated and sustainable.

Clause 19 Infrastructure

The amendment reinforces the need for the development of social and physical infrastructure to be provided in a way that is efficient, equitable, accessible and timely.

How does the amendment support or implement the Municipal Planning Strategy?

The amendment supports the implementation of the strategic directions contained in the Municipal Planning Strategy (MPS), by introducing an updated place-based planning framework to South Melbourne.

Changes to the MPS seek to reinforce the need to:

- Ensure South Melbourne's function as a Major Activity Centre and state-recognised Enterprise Precinct is enhanced and expanded
- Address the shortage of public open space and improve the quality of the public realm
- Support ongoing investment in movement networks to improve accessibility within and outside the neighbourhood, along with encouraging investment and visitation.

The amendment will support the strategic direction to create 10-minute walkable neighbourhoods by integrating land use and transport planning to direct housing and employment growth close to high quality pedestrian routes and public transport services.

Critically, changes to the MPS recognise that the South Melbourne Enterprise Precinct is of state significance and is the largest remaining area of land in the City of Port Phillip exclusively dedicated to employment uses. Importantly, while the amendment will retain employment uses in the Enterprise Precinct, it will increase the development capacity of South Melbourne MAC and Kings Way Mixed Use Corridor enabling it to deliver future housing need. The application of the Heritage Overlay will not unduly restrict the development capacity of the South Melbourne Structure Plan Area.

Does the amendment make proper use of the Victoria Planning Provisions?

The amendment makes suitable use of the Victoria Planning Provisions by selecting the most appropriate provisions to guide land use and development in South Melbourne, in turn supporting the strategic objectives of the Port Phillip Planning Scheme and the South Melbourne Structure Plan.

Zones

Zones have been selected to appropriately guide the use and development of the land.

The Commercial 1 Zone (C1Z) has been selected to support the South Melbourne MAC as a mixed use commercial centre for retail, office, business, residential, entertainment and community uses. Analysis contained in the *South Melbourne Employment, Economic and Land Use Study 2023* (Urban Enterprise with Echelon Planning) finds that the C1Z is appropriately supporting land use and development outcomes in the South Melbourne MAC and that no rezoning of land is warranted, other than the 'City Road Industrial Triangle' to Commercial 2 Zone and the sites identified in the *South Melbourne Zoning Anomalies*

Report 2024 (City of Port Phillip).

The Commercial 2 Zone (C2Z) has been selected to support the South Melbourne Enterprise Precinct as an area that provides for offices and associated business and commercial services together with appropriate industry and retailing. Analysis contained in the *South Melbourne Employment, Economic and Land Use Study 2023* (Urban Enterprise with Echelon Planning) finds that the C2Z is appropriately supporting land use and development outcomes in the South Melbourne Enterprise Precinct and that no rezoning of land is warranted, other than the 'City Road Industrial Triangle' to C2Z and the sites identified in the *South Melbourne Zoning Anomalies Report 2024* (City of Port Phillip). Sites in the 'City Road Industrial Triangle' accord with the purpose of the C2Z and the function of the Enterprise Precinct. The continued application of the Industrial 1 Zone is no longer appropriate, in light of surrounding zones and existing land uses within this area.

Further, retaining the Commercial 2 Zone will support the South Melbourne Enterprise Precinct achieve the strategic direction of the *Melbourne Industrial and Commercial Land Use Plan 2020*, where this area is identified as containing regionally significant industrial land. Given small scale supermarkets are allowed in the Commercial 2 Zone without a permit, policy will encourage supermarkets to locate adjacent to the South Melbourne MAC.

The Mixed Use Zone (MUZ) applies to the Kings Way Mixed Use Corridor to support this area's mixed-use function. Analysis contained in the *South Melbourne Employment, Economic and Land Use Study 2023* (Urban Enterprise with Echelon Planning) finds that the MUZ is appropriately supporting land use and development outcomes in the South Melbourne Kings Way Mixed Use Corridor and that no rezoning of land is warranted, other than the 'City Road Industrial Triangle' to C2Z and the sites identified in the *South Melbourne Zoning Anomalies Report 2024* (City of Port Phillip).

Overlays

Design and Development Overlay (DDO) schedules set out specific requirements relating to the design and built form of new development based on the content in the SMSP. New DDO schedules will provide clear guidelines for built form outcomes, replacing the existing DDO8 and DDO16, which are outdated and do not accord with the *Ministerial Direction: Form and Content of Planning Schemes*.

The Environmental Audit Overlay has been applied to land identified, known or reasonably suspected of being contaminated or potentially contaminated, as set out above.

The Heritage Overlay will ensure that heritage places deemed to meet the threshold for local significance are conserved and enhanced.

How does the amendment address the views of any relevant agency?

During consultation on the Draft South Melbourne Structure Plan, the City of Port Phillip sought comments from a broad range of agencies. Feedback was received from Department of Transport and Planning – Transport Network Planning Integration (DTP), Environment Protection Authority (EPA) and Melbourne Water. The matters raised by agencies have been considered in the preparation of this amendment. Views of agencies will be sought during the exhibition of this amendment.

Department of Transport and Planning – Transport Network Planning Integration (DTP)

With respect to DTP – Transport Network Planning Integration's feedback, the South Melbourne Structure Plan has been updated to better reflect the intention of the State Government's Movement and Place Framework to South Melbourne. These matters have been considered in the preparation of this amendment.

Environment Protection Authority (EPA)

With respect to the EPA's submission, the South Melbourne Structure Plan has been updated to better acknowledge risk of air pollution and noise pollution. Rezoning the remnant 'City Road Industrial Triangle' will support the implementation of Clause 13.05-1S (Noise management) and Clause 13.06-1S (Air quality management), given the direct abuttal of this land with land that allows residential uses in all directions.

Melbourne Water

Melbourne Water raised matters relating to flood risk, increasing densities in flood prone areas. These matters have been considered in the preparation of this amendment.

The South Melbourne Structure Plan acknowledges that much of the City is particularly vulnerable to the impacts of climate change, including rising sea levels, increased storm severity and frequency, and more extreme rainfall. These effects are expected to significantly impact low-lying areas within South Melbourne. Many of these flood prone areas are covered by the Special Building Overlay 1 (SBO1). Conditions that apply to development in these locations can include ground floor levels being set above the flood level or limitations on the design of basement parking and access.

Changes introduced by Amendment VC171 require councils to plan for a 0.2 sea level rise by 2040 with a new requirement to plan for a sea level rise of at least 0.8 metres by 2100. Further, State policy provides that at risk areas should be identified using the best available data.

To assist with the City's consideration of this new benchmark, Melbourne Water has provided the City with flood data and mapping, highlighting areas of Port Phillip, including South Melbourne, most susceptible to sea level rise. Council will continue to work with Melbourne Water, including through future amendments to the Port Phillip Planning Scheme to identify land subject to flooding, and to ensure appropriate referrals to Melbourne Water. These amendment/s are scheduled to commence in 2024/25.

The South Melbourne Structure Plan boundary is consistent with *Planning Practice Note 58: Structure planning for activity centres*. South Melbourne is recognised in State Policy as a Major Activity Centre and Enterprise Precinct and requires a contemporary planning framework to guide decision making. Therefore, the identification of flood prone land in South Melbourne does not preclude the need to undertake strategic work to inform statutory and strategic planning decisions, especially when such work can result in built form outcomes that better respond to flood risk.

Does the amendment address relevant requirements of the Transport Integration Act 2010?

Pursuant to the requirements of the *Transport Integration Act 2010*, the amendment is unlikely to have significant impacts on the transport network. The amendment is consistent with the requirements of the *Transport Integration Act 2010* with actions based on the findings from *South Melbourne Transport and Movement Study 2023* (prepared by Ratio) recommending and identifying opportunities to enhance South Melbourne's transport system. The recommendations to improve the public realm will not likely have significant impacts on the transport system and will be subject to further investigation in the implementation of the South Melbourne Structure Plan.

Resource and administrative costs

What impact will the new planning provisions have on the resource and administrative costs of the responsible authority?

The amendment is not expected to impose any significant additional resource or administrative costs on the responsible authority.

Introducing a revised local policy and new DDOs for South Melbourne is expected to create a clearer and more navigable policy framework for South Melbourne. After an initial familiarisation phase, the new planning provisions should have the effect of reducing any resource or administrative burden on the responsible authority, by introducing a clearer framework that more clearly articulates the responsible authority's vision for South Melbourne.

The policies provide a greater level of certainty to the community and other stakeholders within the municipality and make clear how council will use its discretion when deciding planning permit applications within South Melbourne.

Attachment 1 – Mapping reference table

Location	Land/Area Affected or Address	Mapping Reference	Proposed Zone changes	Proposed Overlay changes	Proposed deletion changes
South Melbourne and Southbank	Land shown in Figure 1 to this explanatory report	Port Phillip C219port 002d-ddo Maps03_04 Exhibition Port Phillip C219port 003ddo Maps03_04 Exhibition		DDO37 DDO38 DDO39 DDO40	D-DDO8 D-DDO16
South Melbourne	'City Road Industrial Triangle' bound by City Road, Ferrars Street and York Street	Port Phillip C219port 001zn Maps03_04 Exhibition Port Phillip C219port 004eao Map03 Exhibition	C2Z	EAO	
South Melbourne and Southbank	The block of land bound by York, Cecil, Market and Northumberland streets, South Melbourne 176-188 Bank Street, South Melbourne 383-385 Clarendon Street, South Melbourne 68-72 York Street, 191-199 Clarendon Street and 31 Ross Street, South Melbourne 120-142 Clarendon Street, Southbank	Port Phillip C219port 001zn Maps03_04 Exhibition	C1Z		

Location	Land/Area Affected or Address	Mapping Reference	Proposed Zone changes	Proposed Overlay changes	Proposed deletion changes
South Melbourne	102-106 Park Street, South Melbourne 77 Park Street and 286-294 Kings Way, South Melbourne	Port Phillip C219port 001zn Maps03_04 Exhibition	MUZ		
South Melbourne	2 Alfred Place, South Melbourne, 214-216 and 220 Clarendon Street, South Melbourne 31 Ross Street, South Melbourne 22 Moray Street, South Melbourne.	Port Phillip C219port 005hoMaps 03_04 Exhibition		HO440	
South Melbourne	89 and 99 Eastern Road, and 36A and 58A Napier Street, South Melbourne	Port Phillip C219port 005hoMaps 03_04 Exhibition		HO561	
South Melbourne	110 Bank Street, South Melbourne	Port Phillip C219port 005hoMaps 03_04 Exhibition		HO563	
South Melbourne	151, 153 & 155 Dorcas Street, South Melbourne	Port Phillip C219port 005hoMaps 03_04 Exhibition		HO564	
South Melbourne	108 Bank Street, South Melbourne	Port Phillip C219port 005hoMaps 03_04 Exhibition		HO565	
South Melbourne	167-173 Coventry, South Melbourne	Port Phillip C219port 005hoMaps 03_04 Exhibition		HO566	

Location	Land/Area Affected or Address	Mapping Reference	Proposed Zone changes	Proposed Overlay changes	Proposed deletion changes
South Melbourne	82-86 Clarke Street, South Melbourne	Port Phillip C219port 005hoMaps 03_04 Exhibition		HO567	
South Melbourne	49-55 York Street, South Melbourne	Port Phillip C219port 005hoMaps 03_04 Exhibition		HO568	
South Melbourne	152 Clarendon Street, South Melbourne	Port Phillip C219port 005hoMaps 03_04 Exhibition		HO569	
Southbank	68 and 70 Market Street, Southbank	Port Phillip C219port 005hoMaps 03_04 Exhibition		HO570	

City of Port Phillip Planning Scheme

Amendment C219port

Instruction sheet

The planning authority for this amendment is the City of Port Phillip.

The City of Port Phillip Planning Scheme is amended as follows:

Planning Scheme Maps

The Planning Scheme Maps are amended by a total of 5 attached map sheets.

Zoning Maps

1. Amend Planning Scheme Map Nos 3 and 4 in the manner shown on the 1 attached map marked Port Phillip Planning Scheme, Amendment C219port.

Overlay Maps

2. Insert new Planning Scheme Map No 3HO and 4HO in the manner shown on the 1 attached map marked Port Phillip Planning Scheme, Amendment C219port.
3. Insert new Planning Scheme Map No 3DDO and 4DDO in the manner shown on the 2 attached maps marked Port Phillip Planning Scheme, Amendment C219port.
4. Insert new Planning Scheme Map No 3EAO in the manner shown on the 1 attached map marked Port Phillip Planning Scheme, Amendment C219port.

Planning Scheme Ordinance

The Planning Scheme Ordinance is amended as follows:

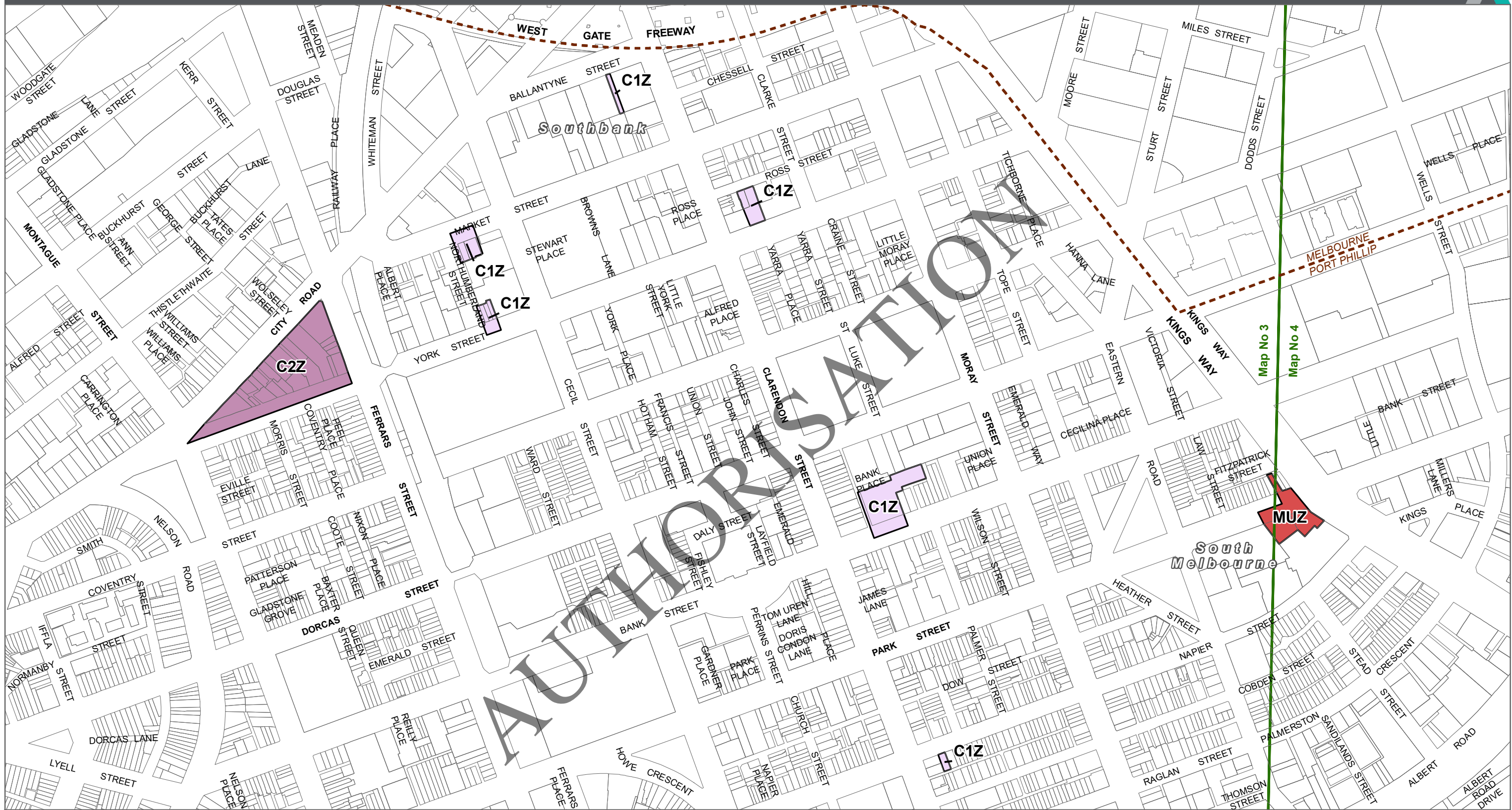
5. In **Purpose and Vision** – replace Clause 02.01-1 in the form of the attached document.
6. In **Purpose and Vision** – replace Clause 02.01-2 in the form of the attached document.
7. In **Purpose and Vision** – replace Clause 02.03-1 in the form of the attached document.

8. In **Purpose and Vision** – replace Clause 02.03-6 in the form of the attached document.
9. In **Purpose and Vision** – replace Clause 02.04-1 in the form of the attached document.
10. In **Purpose and Vision** – replace Clause 02.04-2 in the form of the attached document.
11. In **Purpose and Vision** – replace Clause 02.04-3 in the form of the attached document.
12. In **Purpose and Vision** – replace Clause 02.04-4 in the form of the attached document.
13. In **Purpose and Vision** – replace Clause 02.04-5 in the form of the attached document.
14. In **Planning Policy Framework** - replace Clause 11.03-1L-01 with a new Clause 11.03-1L-01 in the form of the attached document.
15. In **Planning Policy Framework** - delete Clause 11.03-1L-05.
16. In **Planning Policy Framework** – insert a new Clause 11.03-6L-08.
17. In **Planning Policy Framework** – replace Clause 15.01-1L-02 with a new Clause 15.01-1L-02 in the form of the attached document.
18. In **Planning Policy Framework** – replace Clause 15.01-5L with a new Clause 15.01-5L in the form of the attached document.
19. In **Planning Policy Framework** – replace Clause 15.03-1L with a new Clause 15.03-1L in the form of the attached document.
20. In **Planning Policy Framework** – insert Clause 17.01-1L with a new Clause 17.01-1L in the form of the attached document.
21. In **Planning Policy Framework** – insert Clause 17.01-2L with a new Clause 17.01-2L in the form of the attached document.
22. In **Planning Policy Framework** – replace Clause 17.02-1L with a new Clause 17.02-1L in the form of the attached document.
23. In **Planning Policy Framework** – replace Clause 17.03-2L with a new Clause 17.03-2L in the form of the attached document.
24. In **Planning Policy Framework** – replace Clause 18.01-3L-01 with a new Clause 18.01-3L-01 in the form of the attached document.
25. In **Planning Policy Framework** – replace Clause 18.02-3L-01 with a new Clause 18.02-3L-01 in the form of the attached document.





26. In **Planning Policy Framework** – replace Clause 19.02-4L with a new Clause 19.02-4L in the form of the attached document.
27. In **Overlays** – Clause 43.01, replace the Schedule with a new Schedule in the form of the attached document.
28. In **Overlays** – Clause 43.02, insert Schedule 37 in the form of the attached document.
29. In **Overlays** – Clause 43.02, insert Schedule 38 in the form of the attached document.
30. In **Overlays** – Clause 43.02, insert Schedule 39 in the form of the attached document.
31. In **Overlays** – Clause 43.02, insert Schedule 40 in the form of the attached document.
32. In **Overlays** – Clause 43.02, delete Schedule 8.
33. In **Overlays** – Clause 43.02, delete Schedule 16.
34. In **Operational Provisions** – Clause 72.04, replace the Schedule with a new Schedule in the form of the attached document.
35. In **Operational Provisions** – Clause 72.08, replace the Schedule with a new Schedule in the form of the attached document.
36. In **Operational Provisions** – Clause 74.01, replace the Schedule with a new Schedule in the form of the attached document.

End of document

PORT PHILLIP PLANNING SCHEME - LOCAL PROVISION AMENDMENT C219port



LEGEND

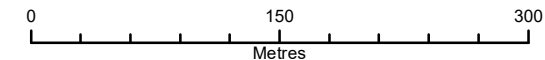
-  C1Z - Commercial 1 Zone
 C2Z - Commercial 2 Zone
 MUZ - Mixed Use Zone
 Local Government Area

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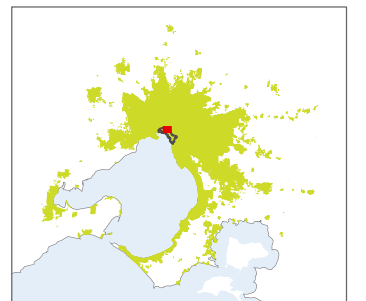
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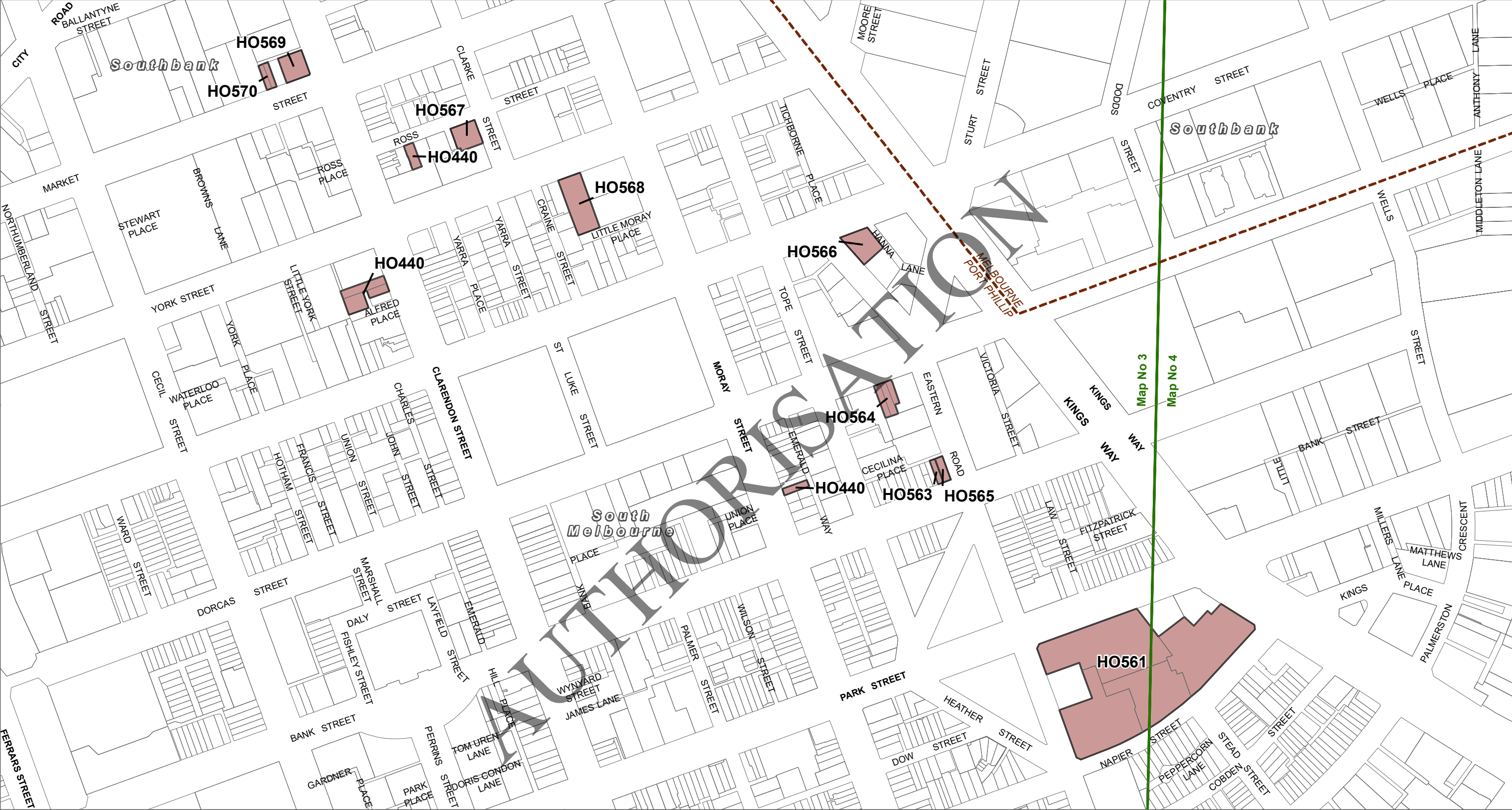
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Print Date: 26/06/2024
Amendment Version: 1

Department
of Transport
and Planning


Part of Planning Scheme Maps 3 & 4




PORT PHILLIP PLANNING SCHEME - LOCAL PROVISION
AMENDMENT C219port



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 HO - Heritage Overlay

 Local Government Area

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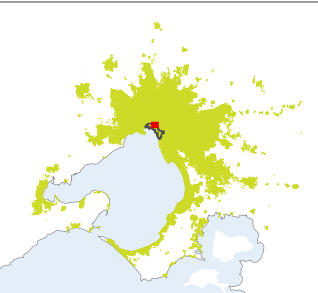
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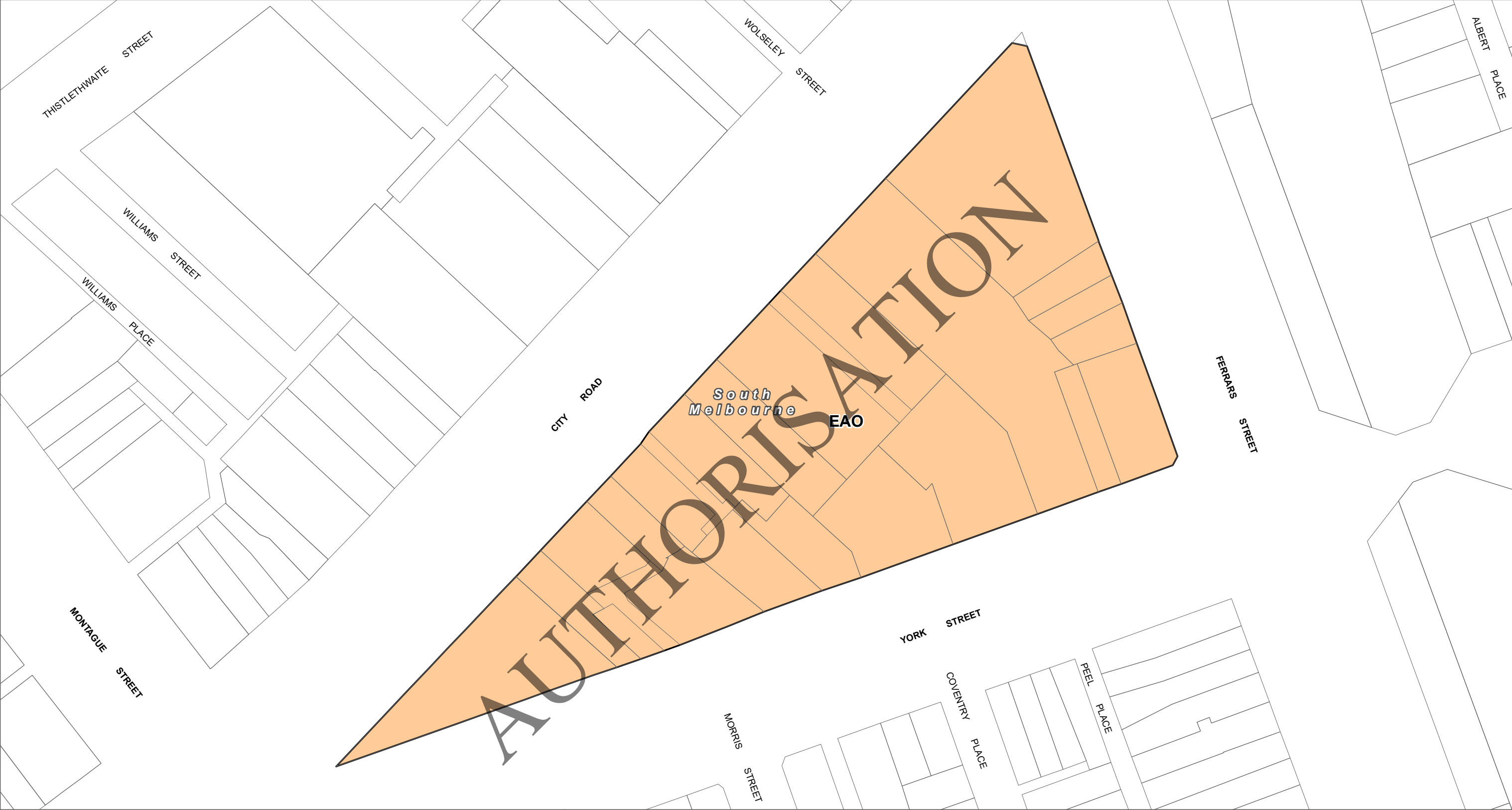






 Department of Transport and Planning

Part of Planning Scheme Maps 3HO & 4HO






LEGEND


-  EAO - Environmental Audit Overlay
-  Local Government Area

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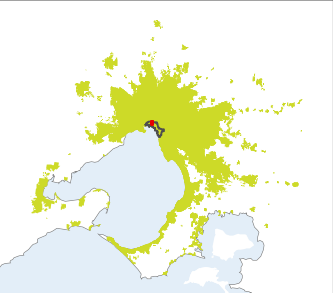
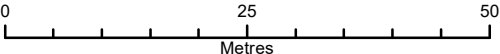


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Print Date: 12/06/2024
Amendment Version: 1

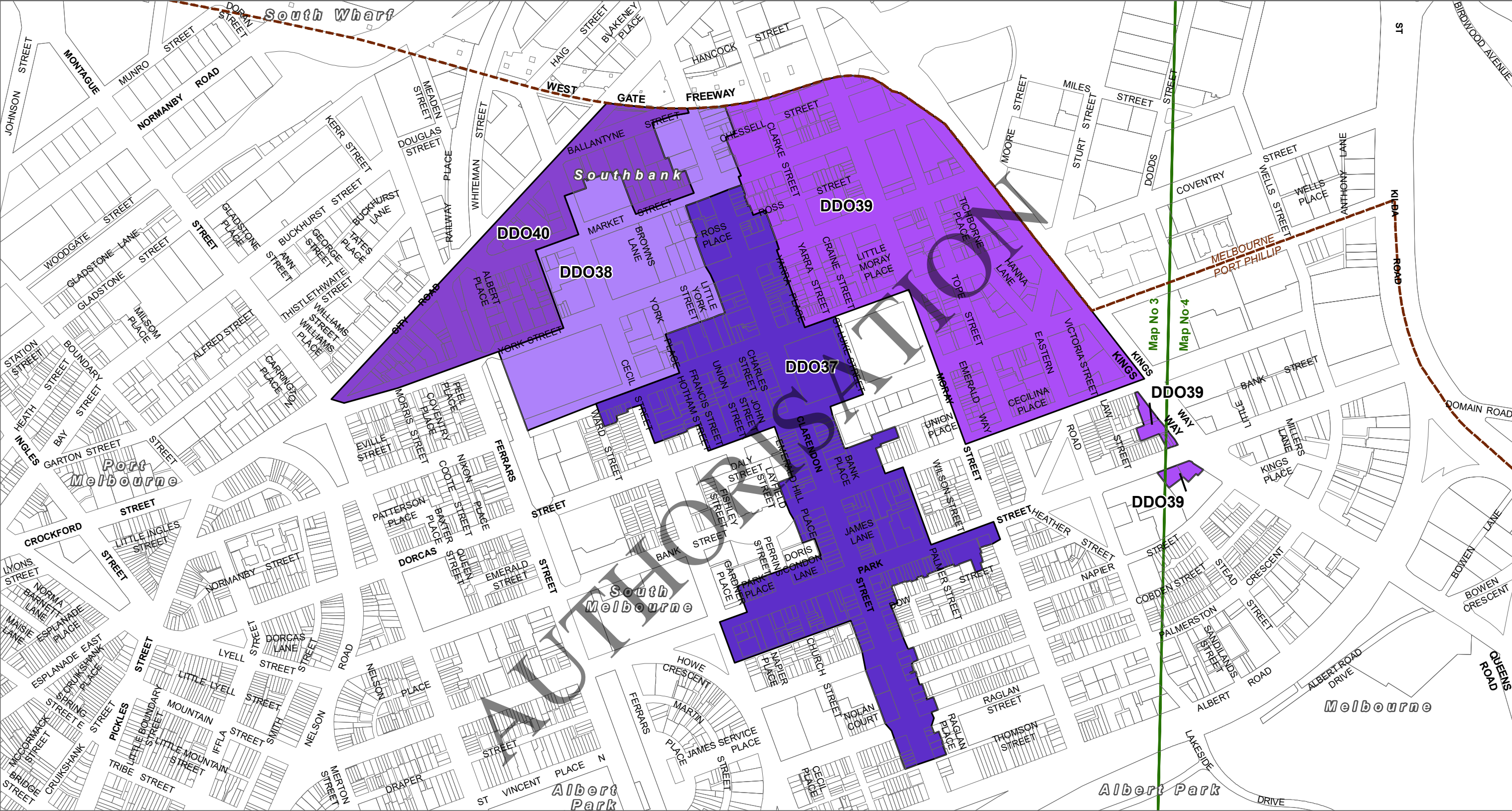


Department of Transport and Planning

Part of Planning Scheme Map 3EAO



PORT PHILLIP PLANNING SCHEME - LOCAL PROVISION
AMENDMENT C219port



- LEGEND**
- DDO37 - Design and Development Overlay - Schedule 37
 - DDO38 - Design and Development Overlay - Schedule 38
 - DDO39 - Design and Development Overlay - Schedule 39
 - DDO40 - Design and Development Overlay - Schedule 40
 - Local Government Area

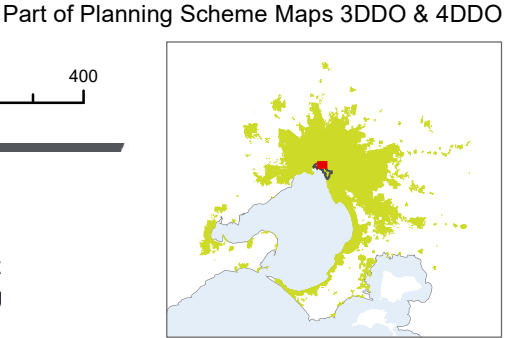
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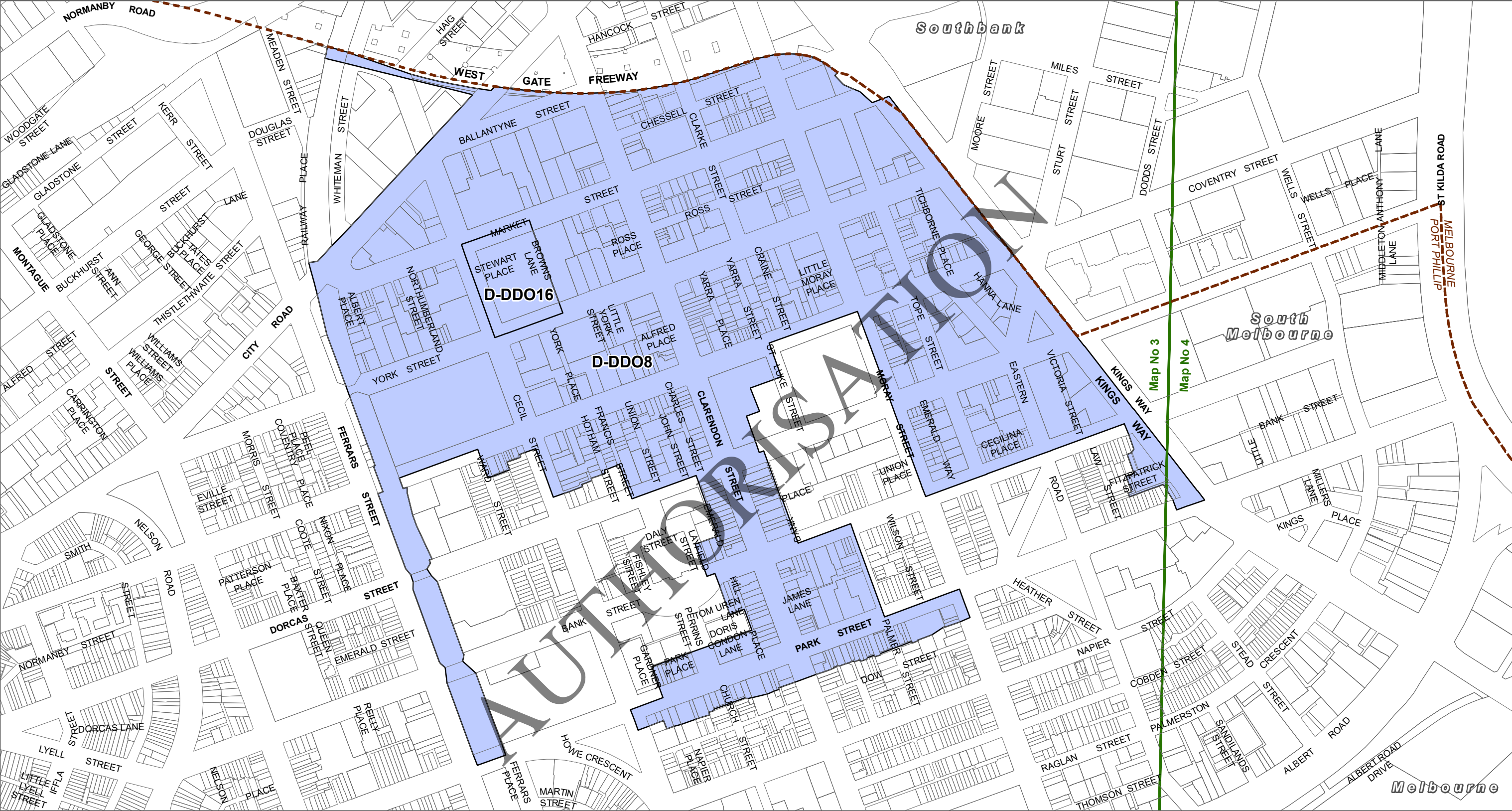
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PORT PHILLIP PLANNING SCHEME - LOCAL PROVISION
AMENDMENT C219port



LEGEND

D-DDO - Area to be deleted from a Design and Development Overlay

Local Government Area

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Part of Planning Scheme Maps 3DDO & 4DDO

02.03
14/04/2023
C203port

STRATEGIC DIRECTIONS

02.03-1
14/04/2023
C203port

Settlement

Activity centres

Port Phillip has an established network of distinct and diverse activity centres:

- Four Major Activity Centres in Port Melbourne (Bay Street), South Melbourne (~~South Melbourne Central~~), Balaclava (Carlisle Street), and St Kilda (Fitzroy/Acland Streets). These provide a wide range of goods, community facilities and services, some serving regional catchments.
- Six Neighbourhood Activity Centres in Port Melbourne (Centre Avenue), Albert Park (Bridport Street/Victoria Avenue), Middle Park (Armstrong Street), Ripponlea (Glen Eira Road), and Elwood (Ternmyson Street, and Ormond/Glen Huntly Roads). These provide a range of goods and services to the local catchment.
- Four Local Activity Centres in St Kilda (Inkerman/Grey Streets, and Inkerman Street), East St Kilda (Inkerman Street), Elwood (Brighton Road), and Port Melbourne (Graham Street). These provide a limited range of convenience goods and services to the local community.
- Emerging activity centres in Fishermans Bend Urban Renewal Area (FBURA) within the Montague, Sandridge and Wirraway precincts. These are of different scales and functions.

These activity centres perform a range of retail, commercial, entertainment and housing functions and will continue to provide community hub roles for their catchments. Their individual distinctiveness and diversity are an important part of Port Phillip's identity, which needs to be protected and reinforced.

Port Phillip's activity centres also play an important role in supporting and hosting visitation, including local and regional entertainment. Cultural tourism should continue to be provided for within activity centres and managed to minimise adverse effects on the amenity of the area.

Council supports:

- Reinforcing a diverse network of economically viable activity centres across Port Phillip by promoting development that:
 - Is of a scale appropriate to the role and capacity of the centre.
 - Supports the distinct identity and social and cultural role of each centre.
 - Provides a diverse mix of uses, including employment generating ones, that service the catchment of each centre.
 - Provides for residential development at a scale appropriate to the role, capacity and economic function of each centre.

Neighbourhoods

Albert Park / Middle Park

Known for its Victorian and Edwardian era development, wide, tree-lined streets, beach and recreational facilities in Albert Park, this neighbourhood also includes part of St Kilda West. A -0.02 per cent population change is forecast by 2031.

Council supports:

- Maintaining the heritage character and low-rise scale of existing residential areas.
- Providing a range of passive and active recreational and sporting activities in attractive and accessible public spaces.

Balaclava / East St Kilda

This neighbourhood is primarily a residential area with diverse housing types and population. Key features include the

Carlisle Street Major Activity Centre, Balaclava Station and Alma Park. A 4 per cent population growth is forecast by 2031.

Council supports:

- Reinforcing the generally mixed architectural character within the existing residential areas, featuring setbacks and garden characteristics.
- Retaining the eclectic, bohemian and distinctly local character of the Carlisle Street Activity Centre, its range of independent businesses, convenience and weekly shopping role, and civic and community focus.
- Facilitating increased use of Balaclava Station.
- Maximising access to safe and direct pedestrian routes, due to the high rates of walking and the specific cultural needs of the Jewish community.
- Addressing the shortage of public open space, especially between Hotham Street and Orrong Road.

Elwood / Ripponlea

This neighbourhood is known for its leafy streets and suburban character, proximity to the Port Phillip Bay, the Elwood Canal and Ripponlea Station. Significant development occurred during the post-war years, with new apartment developments in the area from 2001. A 1.7 per cent population growth is forecast by 2031.

Council supports:

- Reinforcing the green, suburban character of the existing residential areas.
- Retaining the character and prominence of Marine Parade and Ormond Esplanade as seaside boulevards.
- Improving amenity and safety along the interface of Elwood Canal and abutting land uses.
- Facilitating safe and direct walking routes, recognising the specific cultural needs of the significant Jewish community in Ripponlea.

Port Melbourne

This neighbourhood includes Station Pier and the Waterfront Place Precinct, foreshore and beaches, and the Bay Street Major Activity Centre. It is marked by traditional residential heritage precincts, contrasting with the distinctive areas of Garden City, Beacon Cove and contemporary apartment development in the Bay Street Major Activity Centre. A 1.3 per cent population growth is forecast by 2031.

Council supports:

- Retaining Station Pier as a trade and freight gateway.
- Developing Station Pier and the Waterfront Place Precinct as a world class passenger shipping gateway to Melbourne.
- Strengthening the Bay Street Activity Centre as the hub of Port Melbourne, and a mixed use, sustainable and diverse bayside centre that has a strong sense of identity and community.
- Maintaining the consistent neighbourhood character of Beacon Cove.
- Managing the foreshore interface (including Station Pier) to minimise conflicts.

South Melbourne

This neighbourhood includes the South Melbourne Major Activity Centre (including Clarendon Street and the South Melbourne Market) and the South Melbourne Enterprise Precinct, which is an employment precinct of state significance. While the South Melbourne Major Activity Centre and South Melbourne have different roles, they share a complementary relationship. ~~and significant established business precincts which are~~ South Melbourne is experiencing residential and commercial development pressure. A ~~5.7-28.8~~ per cent population growth is forecast by ~~2041-2031~~, along with ~~an~~ a substantial increase in workers.

Council supports:

- Developing a sustainable and well-designed mixed use precinct anchored by ~~focussed on~~ the South Melbourne Major

Activity Centre.

- Strengthening and expanding the South Melbourne Enterprise Precinct's employment function.
- Developing the Emerald Hill precinct around the South Melbourne Town Hall and Emerald Hill Library and Heritage Centre as a major focus of cultural activity for the local and wider community.
- Maintaining high quality residential environments in established residential areas.
- Addressing the shortage of public open space, especially north of Park Dorcas Street, along with improving the quality of the public realm.
- Supporting ongoing investment in movement networks to improve accessibility within and outside the neighbourhood (including the South Melbourne Major Activity Centre, South Melbourne Enterprise Precinct, and surrounding established residential areas), along with encouraging investment and visitation throughout the South Melbourne neighbourhood.

St Kilda Road

With a grand landscape setting that is flanked by the Shrine of Remembrance (in the City of Melbourne), St Kilda Road is one of Melbourne's most famous boulevards. The new Anzac Station will enhance access to the area, and catalyse residential and commercial development and public realm improvements.

The area has a diverse population and is a significant employment hub with a workforce of over 20,000. A 37 per cent population growth is forecast by 2031, along with significant growth in worker numbers.

Council supports:

- Maintaining the role of St Kilda Road as a preferred location for premier office accommodation and well-designed, higher density residential development.
- Reinforcing the St Kilda Road North Precinct as a dynamic, highly connected, beautiful, integrated, safe and inclusive place to live, work and visit.
- Protecting the significance and landmark quality of the Shrine of Remembrance.

St Kilda

This neighbourhood contains the iconic Acland Street and Fitzroy Street retail strips, significant open spaces, the St Kilda foreshore and other significant open spaces such as the St Kilda Botanical Gardens and the Peanut Farm.

St Kilda attracts millions of visitors each year with its famous attractions including Luna Park, the Palais Theatre and St Kilda Beach. The neighbourhood contains an eclectic mix of architectural styles, eras and building typologies. A 10 per cent population growth is forecast by 2031, predominately in the St Kilda Road South Precinct.

Council supports:

- Maintaining the sense of community and cultural diversity that contribute to the unique character of St Kilda.
- Revitalising the St Kilda Foreshore (including the development of the 'Triangle' site).
- Retaining the unique heritage, character and generally low-rise built form of the established residential areas.
- Improving the liveability of the St Kilda Road South Precinct and strengthening its sense of place as it transitions to increased residential densities.

Fishermans Bend Urban Renewal Area

The Fishermans Bend Urban Renewal Area (FBURA) is a declared project of State significance. It includes the major urban renewal precincts of Montague, Sandridge and Wirraway in the City of Port Phillip, and Lorimer and the Fishermans Bend NEIC (Employment Precinct) in the City of Melbourne.

The vision for FBURA is “ *a thriving place that is a leading example for environmental sustainability, liveability, connectivity, diversity and innovation* ” that will by 2050, accommodate 80,000 residents and 80,000 jobs and be Australia's largest urban renewal *Green Star – Community* . FBURA is striving for a 6 Star *Green Star – Community* rating.

Council supports:

- Create thriving, lively, mixed-use neighbourhoods and a place of design excellence with highest densities of

employment opportunities close to existing and planned public transport.

- Provide housing opportunities for a diverse community including at least six per cent of dwellings as Affordable Housing, with additional Social Housing dwellings provided as part of a Social Housing uplift scheme.
- Create a benchmark for sustainable and resilient urban transformation that supports the creation of a climate adept, water sensitive, low carbon, low waste community, addresses potential flood impacts with measures which maintain activity at ground level, and is designed to provide best practice waste and resource recovery management.
- Create a connected, permeable and accessible community that prioritises walking, cycling, and public transport use and supports 80 per cent of movements via active and public transport.

02.03-2 Environmental and landscape values

14/04/2023
C203port

Port Phillip is a highly modified urban environment with remnant areas of indigenous vegetation confined to the Port Phillip Bay foreshore and Ripponlea area. Replanting efforts have led to a number of significant sites of indigenous vegetation.

The foreshore is Port Phillip's most outstanding natural and cultural asset and provides a number of functions including providing habitat for local flora and fauna and managing stormwater. Environmental management is essential in supporting the health of the Port Phillip Bay and ensuring that the foreshore remains an attractive destination that continues to support local biodiversity.

The City's public open spaces and landscaping within road reserves, transport corridors and on private land provide an essential balance to Port Phillip's urban environment and have a positive impact on the liveability and biodiversity of the City.

Council supports:

- Protecting Port Phillip's natural environment and landscape values.
- Protecting and enhancing Port Phillip's urban forest, including large canopy trees and vegetation.
- Protecting and enhancing Port Phillip's green spaces and corridors to provide habitat to native flora and fauna.
- Reducing the environmental impact of urban areas on waterways and receiving bodies by managing stormwater quality and quantity.

02.03-3 Environmental risks and amenity

14/04/2023
C203port

Climate change

Port Phillip is subject to the impacts of climate change, including increased heat, more extreme weather events, changed rainfall patterns, greater flooding due to overland flow and sea level rise, and increased erosion of the foreshore.

Designing and managing built form and physical infrastructure to better respond to a changing climate will contribute to improving the liveability and sustainability of the City for the benefit of the community. Reduced emissions and achieving sustainable land use and development within the built environment will help achieve a City that is adaptive and resilient to climate change.

Council supports:

- Ensuring development responds to a changing climate and helps mitigate against its impacts by:
 - Incorporating environmentally sustainable design.
 - Prioritising walking, cycling and use of public transport, shared transport modes and low-emissions vehicles.
 - Providing new housing and employment in locations close to activity centres and public transport.
- Creating a greener, cooler City that reduces urban heat island effect by:
 - Increasing canopy cover and diversity of tree species in public open spaces, road reserves and transport corridors.
 - Protecting and enhancing vegetation on private land and in development.
- Use and development of the foreshore that responds to the impacts of climate change.
- Use of sustainable and low energy infrastructure and renewable energy.

- Integrating water sensitive urban design in development to improve water quality to Port Phillip Bay and other

AUTHORISATION

receiving water bodies, reduce the impacts of localised flooding and sea level rise and to facilitate water conservation.

- Reducing potable water consumption through more efficient water use and establishing alternative water sources.
- Facilitating the maximisation of recycling and diversion from landfill, reduction in waste generation and the circular economy.

02.03-4 Built environment and heritage

14/04/2023
C203port

The Victorian Aboriginal Heritage Council have advised that they consider that the traditional owners of the land of Port Phillip are represented by the Bunurong Land Council Aboriginal Corporation (BLCAC) and Wurundjeri Woi-wurrung Cultural Heritage Aboriginal Corporation (WWCHAC). The traditional owners' continued connection to the land is reflected through intangible cultural heritage values and in sacred sites.

Port Phillip's built and natural heritage places are among the earliest and most significant in Melbourne, including buildings and structures, landscapes, streetscapes, precincts, subdivision patterns (comprising the layout of streets, lanes and boulevards) and cultural heritage.

Protecting, revealing and embracing the valued heritage and character of the City is a priority for Port Phillip.

The diversity of built form and valued elements of Port Phillip's urban structure and character make a valuable contribution to the attractiveness of the City as a place to live, work and visit.

A key challenge for Port Phillip is to enable development that responds to the context of the area, including its valued heritage and character, and that positively contributes to the public realm. A high quality, liveable and inclusive urban environment is critical to support the vitality and wellbeing of the City.

The character of areas planned for substantial growth will significantly change, being the FBURA urban renewal areas, parts of the Major Activity Centres and along St Kilda Road. Development in these areas needs to be managed to achieve a high-quality public realm to support the new higher-density mixed-use environment.

Development within Major Activity Centres needs to be managed to ensure that the unique and valued character of each centre is retained and enhanced.

The established residential areas and lower order activity centres are distinguished by their low-rise urban form and highly valued character. Development within and adjoining these areas needs to be managed to ensure that the existing neighbourhood character and amenity is not eroded.

Port Phillip also has a role in providing a setting for significant heritage buildings located in adjoining municipalities, including the Shrine of Remembrance.

Council supports:

- Protecting and enhancing the varied, distinctive and valued character of neighbourhoods across Port Phillip, and the physical elements therein.
- A new built form character within FBURA that transitions to surrounding established areas.
- Supporting development along the foreshore that enhances its significance as a natural, recreational and tourism asset by reinforcing the predominately low-rise scale of development (except where directed by a Design and Development Overlay) and avoids overshadowing the foreshore.
- Protecting Aboriginal cultural heritage and incorporating interpretive elements into built form and the public realm.
- Protecting and conserving valued heritage places and precincts by:
 - Retaining and conserving heritage places.
 - Development that respects and complements heritage places by using a contextual design approach that retains and enhances the significance of a heritage place.
 - Supporting adaptive reuse of heritage places that are no longer used for their original purpose, such as industrial buildings.
- Balancing sustainability outcomes and heritage conservation.

02.03-5 Housing

14/04/2023
C203port

Managing the impact of housing growth on the City's character and urban form is a key challenge for Port Phillip. Well-located strategic redevelopment sites and precincts provide the key opportunity to accommodate a large proportion of Port Phillip's new housing growth which will continue to bring change to the scale and density of those areas. Moderate residential growth may also be achieved in activity centres as part of shop-top housing or as larger mixed-use developments on strategic sites. Meeting the demand for new housing within Port Phillip's established residential areas, including around activity centres, must be carefully managed to protect the heritage value, neighbourhood character and amenity of those areas.

As a part of housing growth, provision must be made for a variety of dwellings to meet the diverse needs of Port Phillip's community and ensuring that in terms of household size, lifestyles, abilities, income levels and lifecycle stages. This includes households of varying sizes including shared, sole person, couple and family (including larger family) households. As well as ensuring accessible, visitable and adaptable housing for persons with disability or changing household life-cycle needs including the need to work from home, and older persons needing to age in place or have access to retirement housing and residential care accommodation.

With increasing land values and housing costs and the closure of private rooming houses and subdivision of rental flats, home ownership and private renting are increasingly unaffordable. Greater supply of affordable and social housing is needed to address priority local housing need, including for the most disadvantaged and marginalised residents.

Council supports:

- Providing significant opportunities for housing growth within designated strategic locations such as FBURA, that offer greatest accessibility to shops, services and public transport.
- Increasing residential densities within existing activity centres where the intensity and scale are appropriate to their scale, character and heritage values and does not detract from the centres' economic capacity.
- Providing affordable housing for very low, low and moderate-income households in locations across the municipality, including strategic redevelopment sites, activity centres and Fishermans Bend.
- Providing a diverse range of affordable housing types, such as supported social housing, social (public and community) housing, and private affordable housing that address local housing need.
- Providing a diverse range of accessible, visitable and adaptable housing that meet the needs of the community.

02.03-6 Economic development

14/04/2023
C203port

Port Phillip is strategically positioned between the Central City and Port Phillip Bay and has a strong and mixed economy with significant areas suitable for industrial and varied employment uses and a strong tourism industry.

Key employment areas are shown on the Strategic Framework Plan at Clause 02.04-1 and include:

- Activity Centres that are largely based on retail strips with new centres planned to be established in FBURA.
- Office and Mixed-Use Areas located within and outside of activity centres, including the St Kilda Road Corridor (identified within the expanded Central City Area), ~~South Melbourne Business Precincts~~ and FBURA.
- ~~The South Melbourne Enterprise Precinct, which is of state significance and is the largest remaining area of land in the City exclusively dedicated to employment uses.~~
- Industrial Areas located in Port Melbourne, South Melbourne and Balaclava provide for existing and emerging industrial activities. FBURA has historically accommodated significant industrial activity within the City. Strategically important existing industrial uses that provide services to the construction industry will continue to operate as the area transitions to a mixed-use precinct.

Port Phillip will need to retain an adequate supply of employment land in key precincts for growing job sectors, particularly office space for the 'knowledge economy' which is experiencing increased pressure for new residential development, and to strengthen existing creative industry clusters.

Port Phillip has a strong tourism industry, with Port Melbourne's waterfront and St Kilda's foreshore as key tourist attractors. The range of entertainment uses, facilities and festivals provided across key precincts are an important part of the local economy and contribute to the municipality's strong cultural identity. A key challenge is in balancing the social, economic and cultural benefits of tourism and entertainment uses (particularly live music venues, licensed premises and gaming venues) with minimising social harm and protecting residential amenity to ensure that Port Phillip continues to be a desirable place to visit and to live.

Port Phillip has a vibrant arts scene, and it is important that arts and cultural activities are supported. A key challenge in Port Phillip is retaining affordable and flexible creative workspaces.

Council supports:

- Planning for adequate employment land across the City to facilitate local employment and support the diversity of resident, business and visitor needs.
- Maintaining and strengthening the economic role of Port Phillip's commercial and, mixed use and industrial areas by:
 - Facilitating the transition of FBURA from an industrial to mixed-use area.
 - Supporting opportunities for high-intensity commercial uses within the Sandridge Precinct, and ancillary commercial and retail uses in the other precincts in FBURA.
 - Supporting urban manufacturing through industrial uses with limited amenity impacts.
 - Maintaining the role of St Kilda Road as the City's premier commercial strip.
 - Maintaining and strengthening creative industry clusters in South Melbourne and FBURA.
- Supporting an environment in which arts and creative industries can flourish.
- Supporting a local tourism industry and entertainment precincts that respect safety, amenity and the natural environment.
- Designing and locating sensitive land uses (such as residential uses) to minimise the potential conflict with existing and future employment uses.
- Managing the interfaces between industrial and residential areas (including the interface between Garden City, FBURA and the Port of Melbourne) to limit amenity impacts while ensuring ongoing viability and efficiency of industrial uses.

02.03-7 Transport

14/04/2023
C203port

Port Phillip is built on a walking scale with shopping, parks and local facilities within walking distances of most residential properties.

The City's well-established public transport network adequately serves the needs of most residents, however, there are some parts of the municipality where convenient access to public transport is limited. Providing more transport choices and managing parking is a key focus for Port Phillip as its population grows.

Road network congestion continues to be an issue with high rates of private vehicle ownership and car dependency placing pressure on the City's road infrastructure, creating high parking demand and congested road space.

To reduce car-based travel, walking, bicycle riding and public transport use should be promoted as the preferred modes of transport. Widespread use of more sustainable modes of transport will support the health and wellbeing of Port Phillip's community and provide environmental benefits.

Council supports:

- Creating 10-minute walkable neighbourhoods by integrating land use and transport planning to direct housing and employment growth close to high quality pedestrian routes and public transport services.
- Prioritising and facilitating sustainable transport modes, including active transport and public transport, over private vehicle use.
- Facilitating an integrated, safe and accessible walking and bicycle network that encourages more people to walk or cycle more often.
- Alternative transport options and technology that makes it more convenient for the community to move around.

02.03-8 Infrastructure

14/04/2023
C203port

Key challenges facing the City are meeting the physical and community infrastructure needs of a growing and changing

community across established and urban renewal areas, including the impacts of substantial growth in FBURA.

Council supports:

- Facilitating sustainable physical and community infrastructure that will support the needs of the existing and future community and respond to the demands of development.
- Strengthening the network of community facility ‘clusters’ by locating community facilities together or close to each other and within activity centres.

02.03-9 Open space

14/04/2023
C203port

There are a range of open space areas in Port Phillip, including parks, gardens, beach and Port Phillip Bay foreshore. Many of Port Phillip’s parks and gardens are of heritage significance, particularly those with formal landscapes such as St Kilda Botanical Gardens and St Vincent Gardens in South Melbourne.

The foreshore and Albert Park reserve are significant public open space assets and host a wide range of entertainment, sport and recreational activities. This influences the infrastructure needs of these areas and can impact public access and use.

The demand for existing open space in Port Phillip from residents, visitors and workers is increasing as the population grows and the provision of private open space declines.

Council supports:

- Establishing and improving open space linkages to connect public open space throughout Port Phillip and to the wider regional open space network.
- Pursuing opportunities to increase the amount of useable open space particularly in neighbourhoods identified as being deficient of open space.
- Ensuring the heritage significance of parks and gardens is protected and balanced with their role as places for leisure.
- Ensuring open spaces are safe, inclusive and accessible to all users.
- Maintaining the foreshore as an important social and recreational destination without diminishing its environmental conservation.
- Ensuring development on or adjacent to the foreshore is sympathetic to the surrounding coastal landscape and does not diminish its environmental, amenity, social or recreational values.
- Ensuring development does not detrimentally impact on the amenity, landscape and environmental values of public open space.

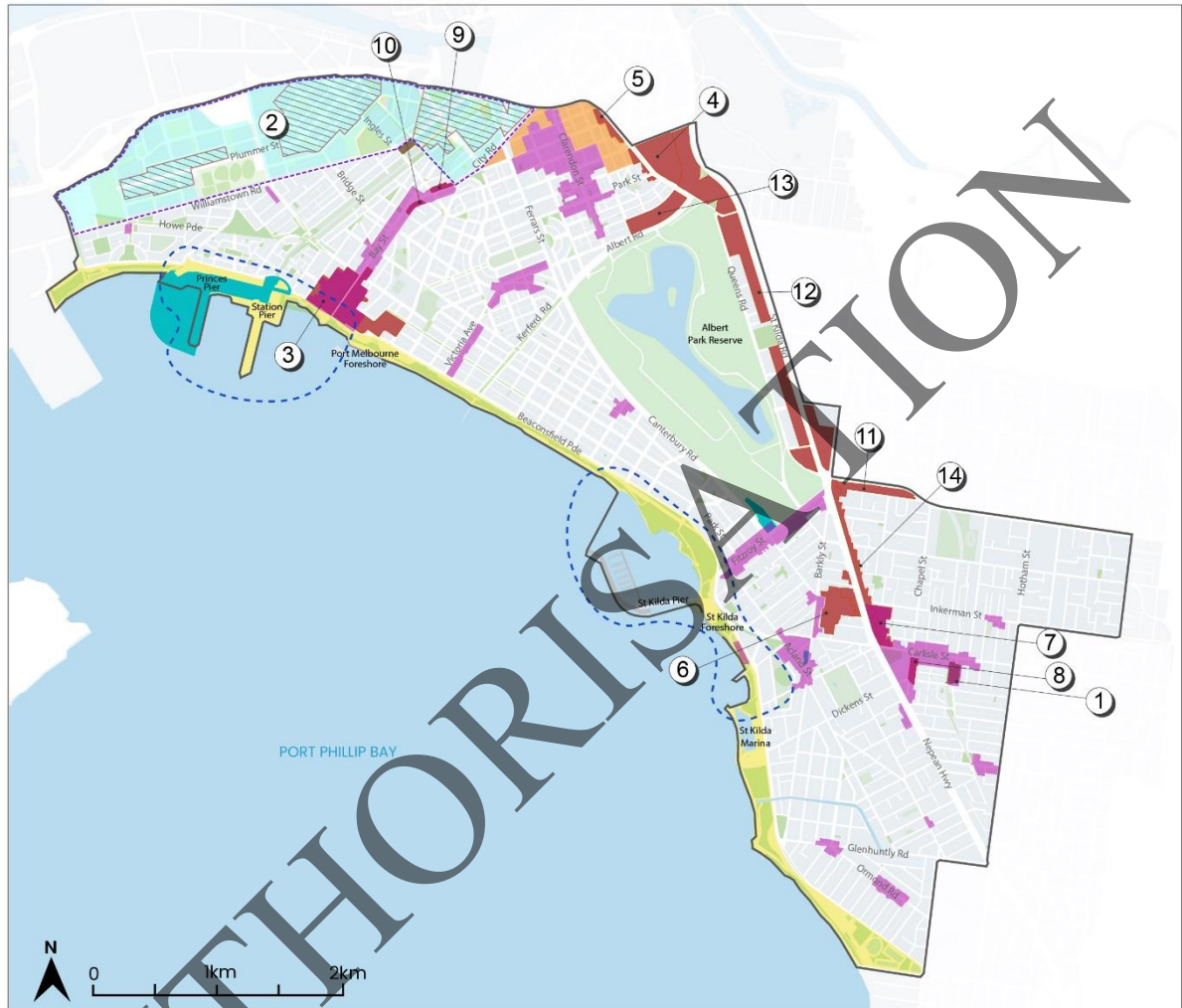
02.04
14/04/2023
C203port

STRATEGIC FRAMEWORK PLANS

The plans contained in Clause 02.04 are to be read in conjunction with the strategic directions in Clause 02.03.

02.04-1
14/04/2023
C203port

Economic Development Plan



LEGEND

- Municipal boundary
- Activity Centres (Major, Local & Neighbourhood)
- Mixed Use and Office area
- South Melbourne Enterprise Precinct
- Core Areas (Fishermans Bend)
- Residential focus
- Tourism / Entertainment
- Open Space
- Capital City Zone
- Comprehensive Development Zone

Industrial Areas

- 1 William Street, Balclava

Mixed Use Activity Areas

- 2 Fishermans Bend Urban Renewal Area
- 3 Bay Street, Port Melbourne
- 4 Kings Way / Dorcas Street, South Melbourne
- 5 Kings Way Mixed Use Corridor, South Melbourne
- 6 Greaves Street, St Kilda
- 7 Inkerman / Pakington Street, Balclava
- 8 Chapel Street, Balclava
- 9 Crockford Street, Port Melbourne
- 10 Bay Street (north of Spring Street), Port Melbourne
- 11 Wellington Street, St Kilda
- 12 St Kilda Road, South Melbourne
- 13 Albert Road, South Melbourne
- 14 St Kilda Road South

02.04-2
14/04/2023
C203port

Strategic Projects Plan

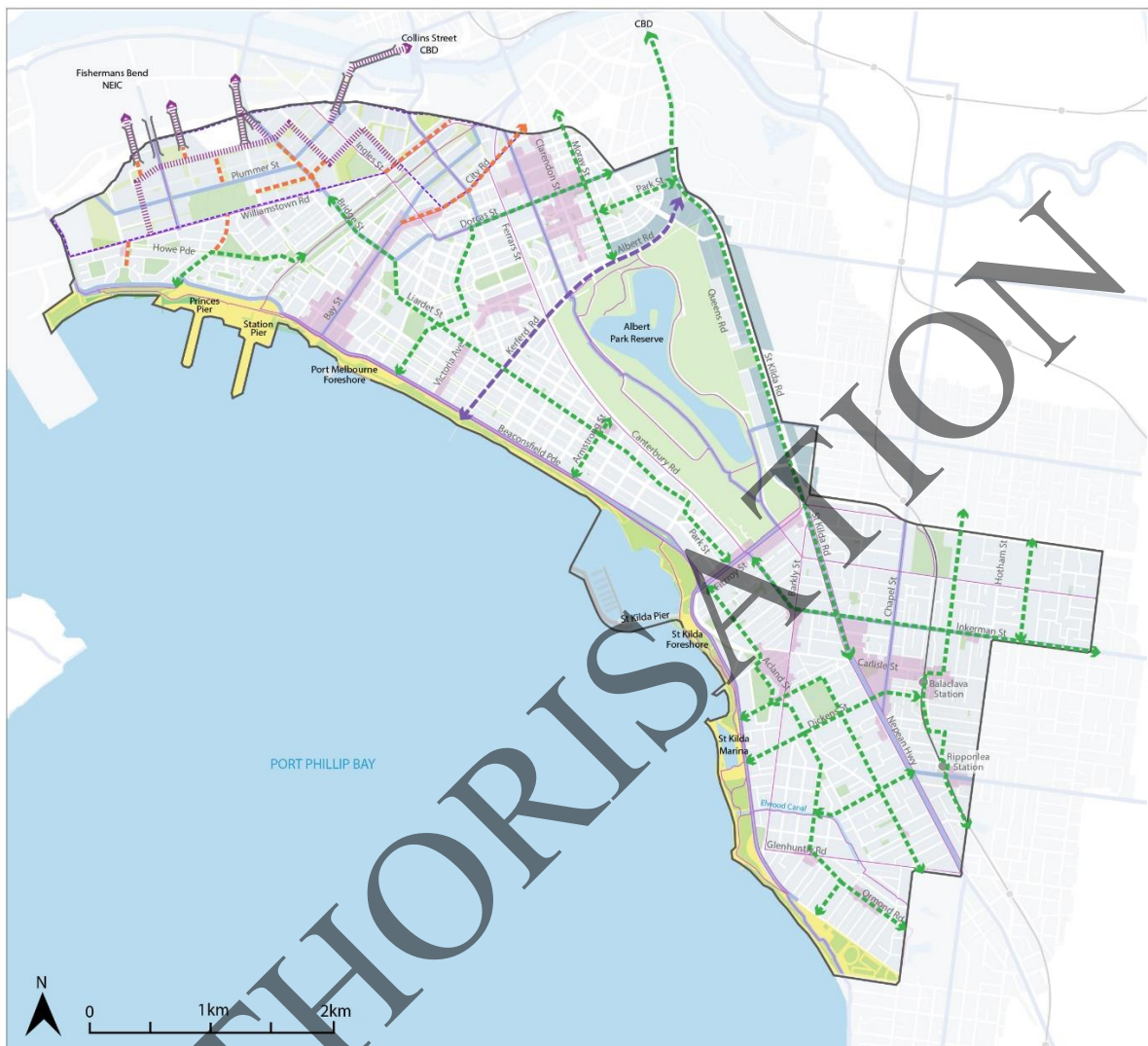


LEGEND

- Municipal boundary
- Activity Centres (Major, Local & Neighbourhood)
- Open Space
- Planned new open space
- Existing rail line & stations
- Proposed rail line & stations
- Existing tram line
- Proposed tram line
- Proposed & upgraded bridges
- Existing Boulevards
- Proposed/Future Boulevards
- Landmarks

02.04-3 Pedestrian and Bicycle Network Framework Plan

14/04/2023
C203port



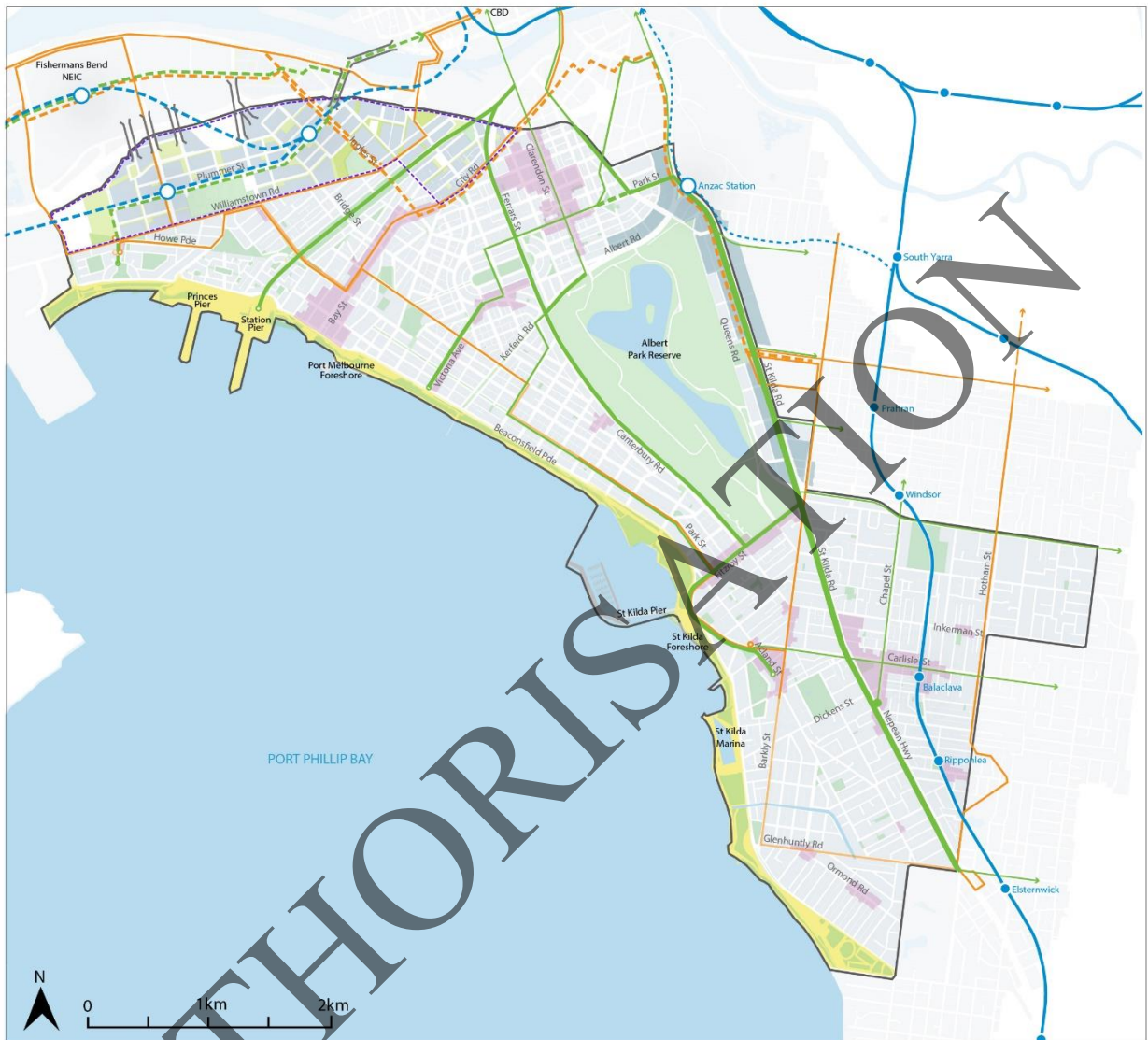
LEGEND

- Municipal boundary
- Fishermans Bend Urban Renewal Area
- St Kilda Road Commercial Precinct
- Activity Centre
- Open space
- Planned new open space
- State strategic cycling corridor
- Existing bike corridor
- Proposed priority walking and bike corridor
- Proposed bike corridor
- Fishermans Bend proposed bike corridor (on road)*
- Fishermans Bend proposed shared walking and bike corridor (off-road)*
- Proposed and upgraded bridges

* subject to change - please refer to
<https://www.fishermansbend.vic.gov.au/> for more information

02.04-4 Public Transport Network Framework Plan

14/04/2023
C203port

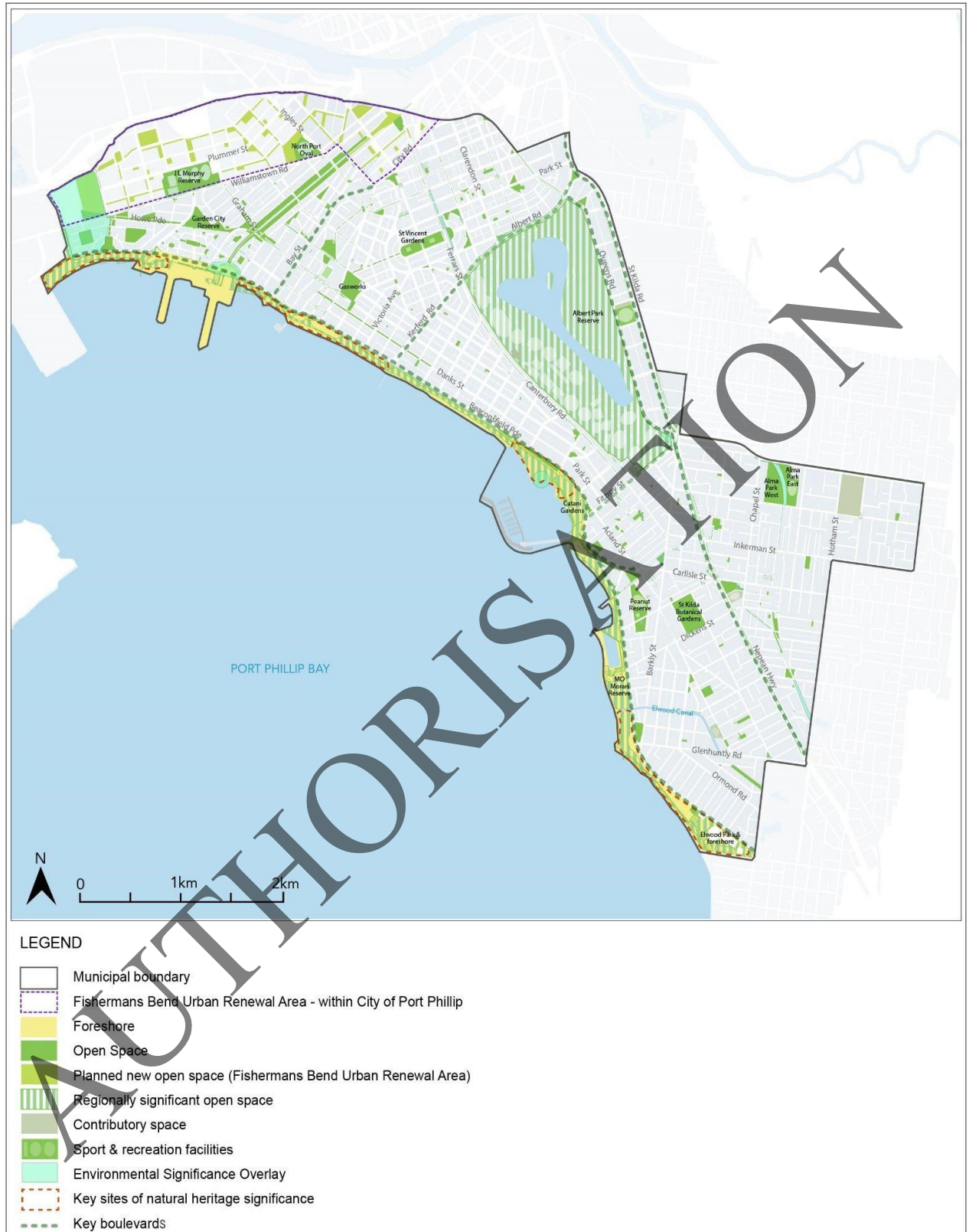


LEGEND

- Municipal boundary
- Fishermans Bend
- St Kilda Road Commercial Precinct
- Activity Centre
- Open space
- Planned new open space
- Proposed and upgraded bridges
- Existing rail line / station
- Metro Tunnel / ANZAC Station
- Options for proposed rail line / station
- Existing tram line
- - - Existing dedicated tram lane
- - - Proposed tram line
- Existing bus route
- - - Potential relocated or discontinued bus route
- - - Proposed bus route

02.04-5 Open Space and Environment Framework Plan

14/04/2023
C203port



AUTHORISATION

11.03
31/07/2018
VC148

PLANNING FOR PLACES

AUTHORISATION

11.03-1S Activity centres

03/02/2022
VC199

Objective

To encourage the concentration of major retail, residential, commercial, administrative, entertainment and cultural developments into activity centres that are highly accessible to the community.

Strategies

Build up activity centres as a focus for high-quality development, activity and living by developing a network of activity centres that:

- Comprises a range of centres that differ in size and function.
- Is a focus for business, shopping, working, leisure and community facilities.
- Provides different types of housing, including forms of higher density housing.
- Is connected by transport.
- Maximises choices in services, employment and social interaction.

Support the role and function of each centre in the context of its classification, the policies for housing intensification, and development of the public transport network.

Undertake strategic planning for the use and development of land in and around activity centres.

Give clear direction on preferred locations for investment.

Encourage a diversity of housing types at higher densities in and around activity centres.

Reduce the number of private motorised trips by concentrating activities that generate high numbers of (non-freight) trips in highly accessible activity centres.

Improve access by walking, cycling and public transport to services and facilities.

Support the continued growth and diversification of activity centres to give communities access to a wide range of goods and services, provide local employment and support local economies.

Encourage economic activity and business synergies.

Improve the social, economic and environmental performance and amenity of activity centres.

Policy documents

Consider as relevant:

- *Urban Design Guidelines for Victoria* (Department of Environment, Land, Water and Planning, 2017)
- *Apartment Design Guidelines for Victoria* (Department of Environment, Land, Water and Planning, 2021)
- *Precinct Structure Planning Guidelines* (Victorian Planning Authority, 2021)

11.03-1R Activity centres - Metropolitan Melbourne

31/07/2018
VC148

Strategies

Support the development and growth of Metropolitan Activity Centres by ensuring they:

- Are able to accommodate significant growth for a broad range of land uses.
- Are supported with appropriate infrastructure.

- Are hubs for public transport services.
- Offer good connectivity for a regional catchment.
- Provide high levels of amenity.

Locate significant new education, justice, community, administrative and health facilities that attract users from large geographic areas in or on the edge of Metropolitan Activity Centres or Major Activity Centres with good public transport.

Locate new small scale education, health and community facilities that meet local needs in or around Neighbourhood Activity Centres.

Ensure Neighbourhood Activity Centres are located within convenient walking distance in the design of new subdivisions.

AUTHORISATION

11.03-1L- Activity centres 01

07/08/2023
C213port

Objective

To maintain and strengthen a network of distinct, diverse, and viable activity centres that facilitate appropriate housing and economic growth.

Strategies

Ensure land use supports the strategic role and function of the activity centre.

Support land uses that contribute to the provision of goods and services for the local community in Major Activity Centres and Neighbourhood Activity Centres.

Intensify retail development within existing retail strips (Commercial 1 Zone), subject to heritage and character considerations.

Support commercial land uses beyond retail strips (Commercial 1 Zone) provided the use would:

- Improve the integration of the activity centre with the surrounding area.
- Respond to known retail gaps or shortfalls.
- Provide for additional or improved public space.

Support development within activity centres that positively contributes to the built form character of the centre whilst conserving heritage buildings, and streetscapes, and the distinctive and valued character of the traditional retail strips.

Encourage greater consistency in land use and built form intensity at the interface of activity centres and surrounding residential areas, including a transition in building scale to adjoining low-rise development.

Create continuous active frontages within core retail areas of Major Activity Centres and Neighbourhood Activity Centres with non-core retail uses located above or behind ground floor frontages.

Support residential development within Major Activity Centres and on key sites and precincts, as defined by Structure Plans.

Ensure residential development within activity centres does not diminish future opportunities for retail expansion, particularly within core retail areas.

Encourage retail development (including reformatting of retail space) that integrates with the established retail strip in response to a demonstrated increase in consumer demand.

Ensure that the heritage scale and form of buildings in the Bridport Street / Victoria Avenue, Albert Park; Armstrong Street, Middle Park; and Glen Eira Road, Ripponlea Neighbourhood Activity Centres, is respected.

Encourage residential use and development above or behind ground floor premises in Major Activity Centres and the Ormond Road / Glen Huntly Road Neighbourhood Activity Centre.

Limit residential development in the Neighbourhood Activity Centres of Tennyson Street, Elwood and Centre Avenue, Port Melbourne, being locations that do not offer direct access to the Principal Public Transport Network.

Policy guideline

Consider as relevant:

- Supporting new retail floor space exceeding 2000 square metres (Gross Leasable Floor Area) where there would be no adverse economic impacts.

Objective

To support in-centre cultural tourism that reflects each individual centre whilst minimising adverse amenity impacts.

Strategies

Direct larger scale regional entertainment uses to the Bay Street Major Activity Centre, Port Melbourne and the St Kilda Major Activity Centre.

Support smaller scale local entertainment uses in Major Activity Centres and Neighbourhood Activity Centres provided there are no adverse impacts on residential amenity.

Allow bar uses in association with existing ground floor restaurants and cafes. Support entertainment uses located outside designated activity centres, provided:

- There are no adverse amenity impacts on adjoining properties, including noise, hours of operation, traffic and car parking.
- There is convenient access to public transport or other transport means (for example taxi ranks).

Policy documents

Consider as relevant:

- *Bay Street Activity Centre Structure Plan Parts 1 and 2* (City of Port Phillip, May 2014)
- *Carlisle Street Activity Centre Structure Plan* (City of Port Phillip, 2009)
- *Carlisle Street Activity Centre Urban Design Framework* (City of Port Phillip, 2009)
- *City of Port Phillip Activity Centres Strategy* (City of Port Phillip, 2006)
- *City of Port Phillip Activity Centres Strategy Implementation Plan* (City of Port Phillip, 2007)
- *Ormond Road Urban Design Guidelines* (City of Port Phillip, 2007)
- ~~*South Melbourne Central Structure Plan* (City of Port Phillip, August 2007)~~ *South Melbourne Structure Plan* (City of Port Phillip, August 2024)

11.03-6S Regional and local places

31/07/2018
VC148

Objective

To facilitate integrated place-based planning.

Strategies

Integrate relevant planning considerations to provide specific direction for the planning of sites, places, neighbourhoods and towns.

Consider the distinctive characteristics and needs of regional and local places in planning for future land use and development.

AUTHORISATION

11.03-6L-08 South Melbourne

Policy application

This policy applies to all land in the South Melbourne Major Activity Centre, South Melbourne Enterprise Precinct and Kings Way Mixed Use Corridor as shown on Plan 1 to this clause.

Activities and uses

South Melbourne Major Activity Centre

Objective

To enhance the South Melbourne Major Activity Centre's provision of retail, employment, housing and entertainment opportunities within a well-connected, accessible, well-designed and sustainable community.

Strategies

Support the provision of retail, employment, housing and community uses, anchored by retail strips along Clarendon Street, Coventry Street and Park Street.

Support activities and uses that strengthen retail strips along Clarendon Street, Coventry Street and Park Street.

Encourage uses and development that enhances heritage places.

Provide opportunities for ongoing reinvestment, mixed use development and new employment floorspace in areas close to public transport.

Facilitate mixed-use development by:

- Retaining the Commercial 1 Zone.

Encourage housing that:

- Enhances the South Melbourne Major Activity Centre's viability and vibrancy.
- Provides diverse dwelling typologies and tenures.
- Provide adequate floor-to-floor heights that are adaptable to changes in future land use.
- Demonstrates high quality building design, promotes sustainable building design and internal amenity.
- Provides affordable housing for rent and purchase.

Support the retention of, upgrade of and provision of social and public housing.

Support and grow South Melbourne's economic specialisations and retail opportunities, and accommodate ongoing demand for office space in South Melbourne by:

- Retaining the Commercial 1 Zone.
- Protecting and advancing South Melbourne as a creative industries cluster.
- Encouraging the South Melbourne Major Activity Centre grow and improve its hospitality and specialty food offering.
- Accommodating projected employment growth and ongoing demand for office space, especially in small and medium premises.
- Preserving and enhancing the amenity of streets.
- Preserving valued aspects of South Melbourne's heritage and physical character.
- Maintaining a distinction between the low to medium rise character of South Melbourne and high rise buildings in the Central City, Southbank, Montague (Fishermans Bend) and along Kings Way.

Provide a range of affordable business spaces which suit micro, small and medium businesses by:

- Encouraging the retention and creation of affordable workspaces.
- Supporting the growing role and utilisation of coworking spaces.

Support the renewal of the South Melbourne Market and its interface with the public realm.

South Melbourne Enterprise Precinct

Objective

To enhance the South Melbourne Enterprise Precinct's role in providing diverse employment opportunities, facilitating innovation, enterprise and creativity, and providing a complementary function to the South Melbourne Major Activity Centre.

Strategies

Support the provision of a mix of employment uses.

Support activities and uses that strengthen the South Melbourne Enterprise Precinct's employment focus, including restricted retail premises, specialty shops and convenience shops.

Support supermarkets within the South Melbourne Enterprise Precinct where it is located adjacent to the South Melbourne Major Activity Centre and is well connected to the South Melbourne Market and retail shopping strips on Clarendon Street, Coventry Street or York Street.

Encourage uses and development that enhances heritage places.

Provide opportunities for ongoing reinvestment, mixed use development and new employment floorspace in areas close to public transport.

Support and grow South Melbourne's economic specialisations and retail opportunities, and accommodate ongoing demand for office space in South Melbourne by:

- Retaining the Commercial 2 Zone in the South Melbourne Enterprise Precinct.
- Protecting and advancing South Melbourne as a creative industries cluster.
- Encouraging the South Melbourne Enterprise Precinct grow and improve its hospitality and specialty food offering.
- Accommodating projected employment growth and ongoing demand for office space, especially in small and medium premises.
- Preserving and enhancing the amenity of streets.
- Preserving valued aspects of South Melbourne's heritage and physical character.
- Maintaining a distinction between the low to medium rise character of South Melbourne and high rise buildings in the Central City, Southbank and along Kings Way.

Provide a range of affordable business spaces which suit micro, small and medium businesses by:

- Encouraging the retention and creation of affordable workspaces.
- Supporting the growing role and utilisation of coworking spaces.

Kings Way Mixed Use Corridor

Objective

To support the Kings Way Mixed Use Corridor's integration and relationship with the South Melbourne Enterprise Precinct and interface with residential areas, along with reinforcing its primary function to accommodate greater residential densities.

Strategies

Facilitate mixed-use development by:

- Retaining the Mixed Use Zone.

Encourage housing that:

- Enhances the Kings Way Mixed Use Corridor's function to accommodate greater residential densities.
- Provides diverse dwelling typologies and tenures, accommodates changing work patterns, is sustainable and durable, and demonstrates high quality design and amenity.

- Provides affordable housing for rent and purchase.

Support the retention of, upgrade of and provision of social and public housing.

Built form in South Melbourne

Contextually responsive design:

Objective

To ensure development incorporates quality design that enhances South Melbourne's distinct characteristics.

Element 1 - Quality Design:

Strategies

Encourage human scaled and diverse building types that are mainly mid-rise with some higher built form in specified areas.

Design buildings to:

- Ensure development responds to the valued attributes of South Melbourne and contributes positively to the existing and future character within each precinct.
- Reinforce the fine grain, vertical rhythm and visual interest of streetscapes.
- Ensure development respects the height, scale and proportions of adjoining heritage places and residential areas.
- Maintain sunlight to key streets and open spaces.
- Maintain the existing street wall height of heritage places while enabling taller street wall heights in non-heritage areas.

Protect the following landmark views:

- The clock tower of South Melbourne Town Hall when viewed from the footpath on the south-eastern corner at the intersection of Clarendon and Bank streets.
- The Shrine of Remembrance when viewed along Bank Street from Clarendon Street.
- Looking north along Clarendon Street towards Melbourne's CBD and Southbank.
- Looking south along Clarendon Street towards Albert Park.

Policy guidelines

Consider as relevant:

- Development reinforces the fine grain, vertical rhythm and visual interest of streetscapes.
- Development incorporates a street wall that reinforces the built form character and reflects the street hierarchy.
- Development provides depth and detail to all visible facades using high quality materials and visually rich details in lower levels.
- Development incorporates awnings or verandahs that are responsive to the form and scale of adjoining verandahs.
- Taller built form is located to minimise visual bulk and overshadowing.
- Development on larger sites includes separate building elements to reduce visual bulk, maintains sky views from the street and improves internal amenity and environmentally sensitive design outcomes.
- Development includes splayed corners which are a prominent urban and architectural feature of the area, where appropriate.

Element 2 – Upper Level Design

Strategies

- Incorporate an architectural expression at upper levels that is distinct from but complementary to the street wall.
- Set upper levels back from the street wall to ensure that upper level additions as seen from the public realm do not detract from the character of the streetscape when viewed directly or obliquely along the street.
- Enable simple and legible building forms and efficient floor plates by avoiding more than one upper level setback above the street wall.

Policy guidelines

Consider as relevant:

- Containing upper level setbacks above the street wall within a maximum of one step.
- Where land not subject to the Heritage Overlay adjoins a Significant- or Contributory-graded building, setting back upper-levels from the street wall to avoid visually overwhelming the adjacent heritage building.
- Non-heritage buildings should match the setback requirement of any abutting heritage building for a minimum of 6 metres in length.

Element 3 – Upper level development behind heritage buildings

Strategies

Encourage upper level development that provides a sensitive distinction between the heritage fabric of lower building levels and the new upper building levels through changes in form, details and materials.

Encourage upper level development that is complementary of heritage fabric in terms of articulation, materiality and colour.

Policy guidelines

Consider as relevant:

- Utilising visually lightweight, but high quality, materials that create a juxtaposition with the heavier masonry typical of the heritage buildings.
- Incorporating simple architectural detailing so it does not detract from significant elements of the existing building or streetscape.
- Incorporating upper level setbacks above secondary street frontages to retain the visual prominence of return façades or end terrace side elevations that address a secondary street.
- Incorporating a further setback from the primary street frontage at the uppermost level, where appropriate.
- Incorporating materials and finishes that are recessive in texture and colour.

Engaging and walkable precincts:

Objective

To ensure development contributes to engaging and walkable precincts.

Element 1 – Frontages

Strategies

Provide high-quality, active frontages to key streets and laneways.

Achieve universal design principles to provide equitable and dignified access.

Encourage a mix of small and medium tenancies along key pedestrian streets and laneways.

Avoid signage treatments that reduce activation and passive surveillance of the public realm by obstructing

windows and doors.

Policy guidelines

Consider as relevant:

- Locating multiple entrances for larger tenancies and active frontages at the ground floor along the primary pedestrian streets, as shown in Plan 1.
- Incorporating vertical articulation in the street wall and upper levels that reflects and aligns with the prevailing pattern of subdivision and buildings.
- Providing continuous weather protection along main street frontages and other key pedestrian routes that allows for exposure to winter sun and shelter from summer sun.
- Carefully integrating signage into the design of the ground floor and surrounding public realm.

Element 2 - Pedestrian and bicycle access

Strategy

Provide pedestrian entrances that are clearly visible, safe, secure and have an identifiable sense of address.

Policy guidelines

Consider as relevant:

- Locating the primary pedestrian entrance to buildings, including upper level apartments from a street or lane.
- Designing resident and staff bicycle parking and end of trip facilities in an accessible, secured and convenient location from the street and associated uses.

Element 3 - Building services

Strategy

Avoid locating servicing and back of house areas on street frontages and other key pedestrian routes.

Policy guidelines

Consider as relevant:

- Minimising the extent of servicing located on main street frontages and other key pedestrian routes.
- Whether the design of building servicing is carefully integrated into the overall design, including of the ground floor and rooftop, to minimise visual bulk impact when viewed from the public realm, adjoining properties and long distance views.
- Co-locating service cabinets internal to loading, waste or parking areas where possible to avoid impact on the public realm.

Element 4 - Vehicular access

Strategies

Design vehicle ingress and egress, including loading facilities and building servicing, to ensure a high quality pedestrian amenity and limit potential conflict between vehicle movements and pedestrian activity.

Enhance the amenity and safety of laneways that provide pedestrian and vehicular access to buildings.

Policy guidelines

Consider as relevant:

- Providing vehicular access from rear lanes or from side streets.
- Locating car parking within a basement or concealed from the public realm.

High amenity and flexible housing and workplaces

Objective

To provide high-amenity and flexible housing and workplaces.

Strategies

- Provide internal amenity and equitable development between sites by ensuring adequate building separation.
- Support equitable development by ensuring that primary outlook is secured to the street or within the development site.
- Ensure that internal privacy is well-managed through building separation, landscape interventions and the careful location of windows.
- Avoid reliance on screening to manage privacy issues at the ground floor and at upper levels.
- Provide adequate floor-to-floor heights that support good internal amenity outcomes and are adaptable to changes in future land use over time, including the floor-to-floor heights of car parking.
- Ensure universal design principles are achieved to provide equitable and dignified access for all.

Policy guideline

Consider as relevant:

- Whether development provides high levels of internal amenity.
- .

Climate responsive design

Objective

To encourage development to integrate climate responsive design.

Strategies

Support best practice environmentally sensitive design outcomes.

Support increased urban greening through green walls and green roofs.

Where practicable, developments should create opportunities to reduce waste by:

- Optimising waste storage and efficient collection methods.
- Combining commercial and residential waste storage.
- Sharing storage or collections with adjacent developments.
- Separating collection for recycling, hard waste, and food and green waste.

Policy guidelines

Consider as relevant:

- Providing deep soil zones to support in ground planting and canopy trees in the private realm, where appropriate.
- Providing on site flood mitigation and Water Sensitive Urban Design (WSUD).

Access and movement

Objective

To support South Melbourne as a place that is connected and easy to move around for all.

Strategies

Enhance South Melbourne's movement network to prioritise sustainable and active travel, while improving capacity and circulation.

Provide an attractive and safe local pedestrian and cycling network that connects key destinations and strategic corridors from South Melbourne to Montague (Fishermans Bend), Southbank and Domain, as well as Anzac Station.

Encourage development to contribute to the upgrade of existing streets, laneways and public spaces adjoining the site to support the provision of an attractive and safe local pedestrian and cycling network.

Support improvements to public transport services and access to meet the needs of South Melbourne's residents, workers and visitors.

Encourage car parking and loading approaches that support more sustainable modes of transport and reduce the impacts of vehicle congestion, including car share.

Public realm

Objective

To create public spaces that are attractive, vibrant, climate-resilient and people-friendly.

Strategies

Create cool, green streets for people to improve amenity, comfort and public health by:

- Addressing gaps in canopy tree coverage.
- Softening the interface between buildings and the public realm.
- Incorporating innovative and best practice approaches to mitigate climate change impacts.
- Ensuring plantings create habitat and promote pollination and biodiversity connections for flora and fauna.
- Providing shaded places for people to enjoy, meet and relax under.

Encourage development to contribute to the upgrade of existing streets, laneways and public spaces adjoining the site to support the creation of cool, green streets and mitigate the urban heat island effect.

Development incorporates enhancements to Integrated Water Management approaches, including using Water Sensitive Urban Design (WSUD) principles.

Reinforce civic pride and sense of place by:

- Developing the South Melbourne Town Hall and Emerald Hill Precinct as the major focus of cultural activity and an integrated network of civic, cultural and community facilities for the local and wider community.
- Protecting and interpreting Aboriginal cultural values and heritage in the design of the public realm of South Melbourne.

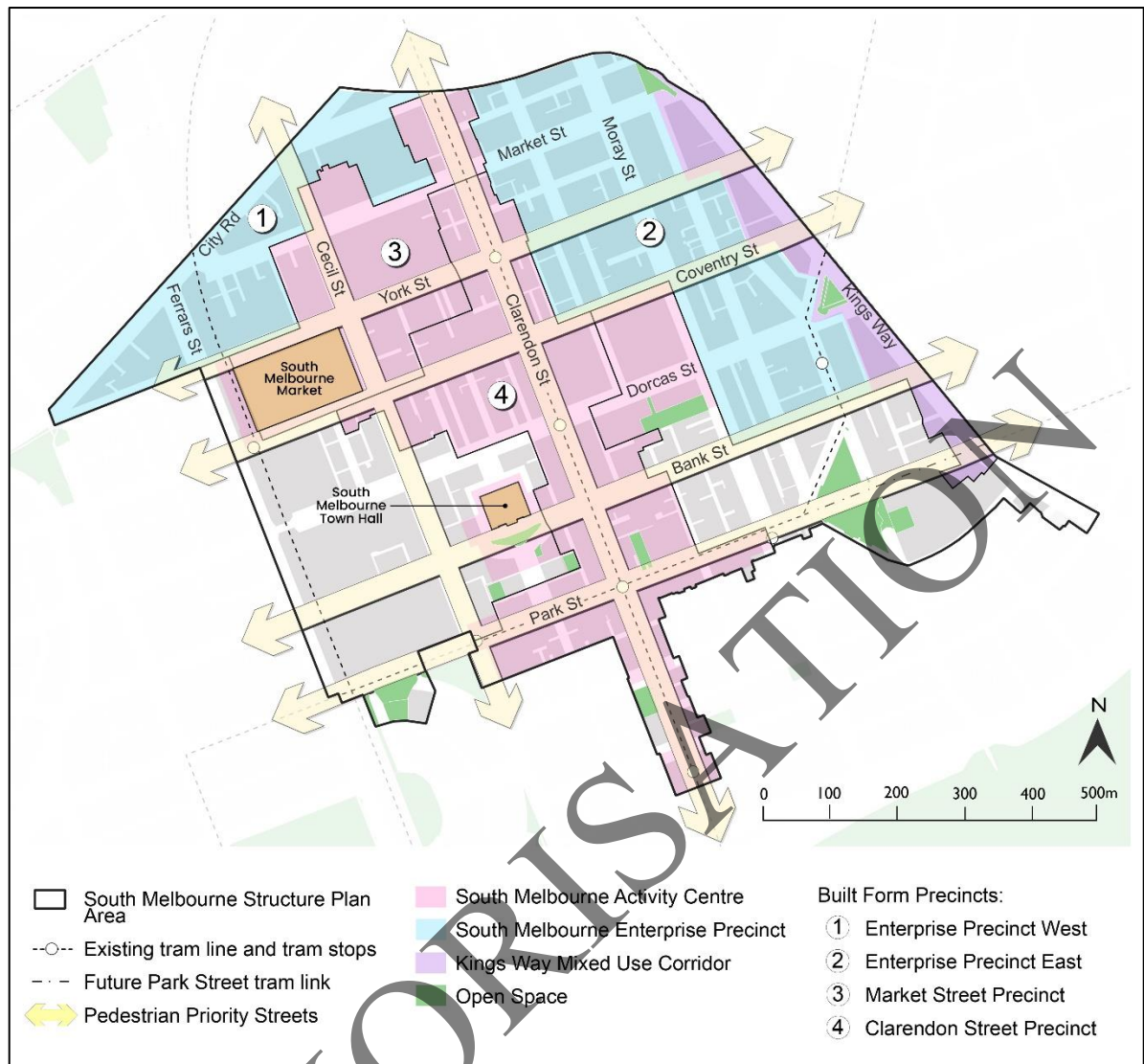
Development supports the provision of public spaces to accommodate a variety of uses and enterprises.

Policy documents

Consider as relevant:

- *South Melbourne Structure Plan* (City of Port Phillip, June 2024).

Plan 1: South Melbourne Structure Plan Area



15.01-1L-02 Urban design

xx/xx/xxxx
C219port

Landmarks, views and vistas

Objective

Maintain the visual prominence of and protect primary views to valued landmarks in Port Phillip.

Strategies

Support development that protects and enhances views of key landmarks from the public realm, including (but not limited to):

- The Shrine of Remembrance.
- Port Phillip Bay, the coastline and maritime structures such as St Kilda Pier, Kerferd Road Pier and Station Pier.
- High rise buildings in the Melbourne Central ~~Activities District~~ City, Southbank and parts of the Fishermans Bend Urban Renewal Area, including views from Port Phillip Bay foreshore and piers.

Support development that protects and enhances view corridors along key boulevards and promenades when viewed from the public realm, including (but not limited to):

- St Kilda Road, Bay Street Port Melbourne, Victoria Avenue Albert Park, Kerferd Road Albert Park, Beaconsfield Parade through various suburbs, Fitzroy Street St Kilda, The Esplanade St Kilda, Marine Parade St Kilda, Glen Huntly Road Elwood, Brighton Road Elwood, Ormond Esplanade Elwood.

Support development that retains and enhances the visual prominence of key landmarks that terminate important vistas, accentuate corner sites and provide points of interest and orientation, including (but not limited to):

- Landmarks of cultural or heritage significance such as the Shrine of Remembrance, town halls, clock towers, church spires, synagogues, grandstands and hotels.
- Public gardens and other key public open spaces, including Albert Park, Alma Park, St Kilda Botanical Gardens and the Port Phillip Bay foreshore.
- Along Bank Street between the South Melbourne Town Hall and the Shrine of Remembrance.

Ensure that development will not have a detrimental impact upon the setting or views of a memorial or monument.

Building form

Objective

To facilitate high quality urban design and architecture that integrates with the prevailing neighbourhood character and contributes to the amenity and vitality of the area.

Strategies

Support development that:

- Respects and enhances places with significant heritage, architectural, scientific and cultural significance.
- Maintain the existing or preferred grain and block pattern.
- Provide façade articulation to break up the mass of blank walls.
- Uses a 3D massing model to understand how the development integrates within the prevailing neighbourhood character and contributes to the amenity and vitality of the area.

Avoid concealed areas along exterior walls abutting the public realm and internal walkways.

Interfaces between commercial and residential uses should be designed and managed to protect residential amenity and improve the interface with established residential areas through a transition in the scale of development.

Public realm

Strategies

Encourage buildings that present a human scale and visual interest to the street frontage.

Support the design of buildings as well as public and communal spaces that are safe, inclusive, functional, flexible, legible and provide dignified access for all.

Encourage the integration, treatment and siting of ancillary structures such as substations, fire booster cupboards and gas metres as part of the building design strategy to minimise their visual impact.

Encourage developments to contribute towards streetscape improvements, such as weather protection in retail and commercial areas, urban art and improved pedestrian amenity.

Provide opportunities for social interaction at interfaces between the public and private realms, and within multi-storey residential developments.

Improve the quality, consistency, efficiency and application of lighting in the public realm.

Facilitate solar panels, satellite dishes, air conditioning units and other building equipment in areas that are as visually unobtrusive in the public realm as possible.

Protect and enhance pedestrian spaces and amenity in all streets, squares, parks, walkways and public spaces.

Minimise adverse micro-climatic impacts created by development such as overshadowing of the public realm (footpaths, open space) and wind tunnelling.

Avoid excessive visual bulk and massing that create unsafe and negative amenity impacts in pedestrian and communal areas.

Avoid development that dominates or implies private ownership of public spaces or impedes access for all.

Policy guidelines

Consider as relevant:

- Projections outside the site boundary should be limited to no more than 500 millimetres beyond the property line, and be no more than 2.5 metres in length.
- Buildings immediately adjacent to a public space, including a footpath, should have a maximum building height of 3 storeys (unless otherwise specified in a DDO).
- Building levels immediately adjacent to public space should be set back above the third storey (unless otherwise specified in a DDO).

Street level frontages

Strategies

Design building frontages at footpath level to support visual interest, transparency, interaction with the street, safety, shelter and convenience.

Promote pedestrian entrances to buildings that:

- Are safe, secure and legible from streets and other public areas.
- Provide shelter, a sense of address and a transitional space between the public and private realms.

Support windows, door openings, terraces and balconies at lower building levels to offer surveillance of and visual connections to surrounding public areas.

Avoid blank walls, services, vents and plant equipment in primary frontage and key pedestrian spaces.

Define corners at street intersections by addressing both street frontages and the surrounding context.

Create continuous active frontages and streetscapes within core retail areas of Major Activity Centres and Neighbourhood Activity Centres by encouraging:

- A diverse range of ground level retail and complementary commercial uses, with office and other non-core retail uses located above or behind ground floor frontages.
- The provision and extension of canopies to offer weather protection and preserve footpath space for pedestrians throughout retail cores.
- Outdoor living and dining.
- ‘Wrapping’ the edges of larger retail premises with smaller scale uses that have active frontages.
- Ground level floor-to-ceiling heights that allow for current or future commercial land uses.

Landscape

Strategies

Minimise hard paved areas to limit surface flows, where possible.

Locate vehicle access to avoid the removal of existing street trees and public landscape elements and to ensure their ongoing survival and health.

Support innovative approaches to landscape design and construction that:

- Supports food growing and urban agriculture.
- Includes species that benefit biodiversity.
- Uses recycled materials.

Foreshore environs

Strategies

Encourage designs that respect the established cultural, heritage, recreational and environmental values of the foreshore public realm.

Encourage innovative approaches to development, landscape design and construction, including greater use of indigenous plant species, plant species responsive to climate and conditions and structures and furniture using recycled materials.

Streets and laneways

Strategies

Encourage developments that protect and enhance the appearance and function of streets and laneways by:

- Maintaining the existing or preferred scale and rhythm of streets and laneways using articulation, fenestrations and entry points.
- Contributing visual interest, activation, amenity, public art and landscaping in streets and laneways, as appropriate.
- Avoiding conflict between vehicle access along laneways and any other identified laneway functions, where appropriate.

Large sites

Strategies

Create well-articulated development on larger or consolidated sites (with a frontage over 10 metres) through:

- Variations in form and materials.
- Openings.

- Vertical design elements.

Policy documents

Consider as relevant:

- *South Melbourne Structure Plan* (City of Port Phillip, August 2024)

AUTHORISATION

15.01-5L Neighbourhood character

Policy application

This policy applies to all land zoned Neighbourhood Residential Zone, General Residential Zone or Residential Growth Zone.

Strategies

All areas

Design development so that its height, scale, massing and bulk respects the scale and form of nearby buildings in areas where the existing built form character is to be retained, or a preferred character for an area has been identified.

Elwood and Ripponlea

Development should respond to the following existing character elements:

- Large detached dwellings on large allotments with generous front, rear and side setbacks for landscaped areas with established trees.
- The consistent architectural character of predominantly Federation and inter-war dwellings with hip or gable roof forms.
- The predominant low building heights of 1-2 storey (single residences) and 2-3 storey (flats).
- The highly consistent and intact inter-war streetscapes in the 'Poets' part of Elwood (generally bounded by Barkly, Dickens, Mitford and Shelley Streets).

Improve the amenity, safety and interface of the Elwood Canal, Linear Park and Reserve and abutting land uses by:

- Limiting direct vehicle access.
- Encouraging new development to address and provide visual surveillance over the canal.

Supporting development that is set back and landscaped to retain the landscape character of the canal corridor not overshadow the reserve.

Port Melbourne and Garden City

Design development to respond to the following existing character elements:

- The predominantly single storey scale of development in residential areas.
- The fine grain subdivision pattern and small lot sizes.

Fishermans Bend Residential Estate (Port Melbourne)

Support development in the Fishermans Bend Residential Estate that responds to the following existing character elements:

- The predominantly single storey scale of development.
- The design of the individual dwellings as pairs and their consistent repetition of elements and massing.
- Consistent use of similar materials.
- Consistent low front fences.

St Kilda

Development should respond to the following existing character elements:

- The diverse architectural styles and forms including single and multi-dwelling developments from the nineteenth and early to mid-twentieth centuries to the present.
- The predominantly 2-3 storey higher scale of development (and some 4 storey buildings) in Alma Road, Barkly Street,

Beaconsfield Parade, Chapel Street north of Argyle Street, and south of Carlisle Street, Grey Street, Inkerman Street, Marine Parade, Mitford Street, Princes Street and Robe Street.

- The stepping up of built form along The Esplanade from low-rise at Fitzroy Street and Acland Street to high-rise.
- Landmark buildings including the National Theatre, Sacred Heart Campanile in Grey Street, the Presbyterian Church and spire at the corner of Alma and Barkly Streets and the Synagogue in Charnwood Grove.

Encourage high quality, innovative buildings and spaces that are designed to:

- Respect the cultural values and architectural themes of St Kilda.
- Integrate with and enhance the public realm by incorporating useable community space, ground level activity and maximising solar access.

Respect the existing 4 and 5 storey scale of development in Inkerman Street and the lower 1 and 2 storey scale in other streets for new development in Greeves Street Mixed Activity Centre Precinct.

South Melbourne

Development should respond to the following existing character elements:

- The historic, low-rise Victorian and Edwardian architectural character of the area.
- The predominantly low scale (1 and 2 storeys) in most residential streets, mixed scale in the South Melbourne Major Activity Centre and Enterprise Precinct, and taller scale along Kings Way ~~except for taller buildings along the foreshore and in the vicinity of Albert Road.~~
- The wide streets and boulevards, and intricate network of small streets and back lanes.
- The small residential lot size.
- The diverse and distinctive neighbourhood character of smaller areas in Emerald Hill.

East St Kilda and Balaclava

Development should respond to the following existing character elements:

- The prevailing low rise (1 and 2 storey) scale throughout most residential streets, excepting pockets of flats (2 and 3 storey) along main roads.
- The consistent single storey scale, small setbacks, architectural style and 'fine grain' subdivision pattern in streets generally between the railway line and Chapel Street, and on the north side of Inkerman Street between the railway line and Hotham Street.
- The generous proportions and architectural style typical in the eastern part of the neighbourhood, including:
 - Larger front, rear and side setbacks and lot sizes.
 - Inter-war architectural style.
 - Detached or semi-detached houses.
 - Large landscaped areas with established trees.
 - Views towards the historic Sandringham Railway line bridges and embankments, particularly the bridge over Carlisle Street.
- The mature street trees in some areas including along Dandenong Road, Westbury Grove, Ravens Grove, and Westbury Street, south of Carlisle Street.

Encourage development immediately west of Alma Park to address the Park.

Middle Park and Albert Park

Protect the intact heritage character.

Reinstate the original verandah forms to the commercial buildings on Bridport Street and Victoria Avenue.

Development should respond to the following existing character elements:

- The small residential lot size.
- The historic, low-rise Victorian and Edwardian architecture.
- The predominant low scale of development (1 and 2 storeys, with the exception of some taller buildings along the foreshore and in the vicinity of Albert Road) with higher development setback from the principle street to minimise its visibility.
- The wide streets and boulevards and intricate network of small streets and back lanes.
- The consistent streetscape frontage widths to buildings.
- Prominent landmark buildings including the 'Biltmore' (152 Bridport Street), the Windsor Hotel (107 Victoria Avenue), the Albert Park Hotel (85 Dundas Place) and the former ES&A Bank (95 Dundas Place).
- Views to Albert Park and toward the Bay from Victoria Avenue.
- The island open space reserve (Broadway Tree Reserve) in Albert Park Village.

Development nearby land within a Heritage Overlay

Development outside of a Heritage Overlay should respect and enhance the scale, form and setbacks of nearby heritage buildings.

Policy guidelines

Consider as relevant:

- Supporting development that respects the character of the local area, as outlined in the neighbourhood character *ual* (City of Port Phillip, 2000), or preferred character statements in a Design and Development Overlay, or an approved Urban Design Framework.
- Encouraging the design of development to respond to contributory heritage buildings outside the Heritage Overlay as identified on the document incorporated in the Schedule to Clause 74.02 '*City of Port Phillip Neighbourhood Character Policy Map*'.

Policy documents

Consider as relevant:

- *City of Port Phillip Neighbourhood Character Policy Map* (City of Port Phillip, updated 2021)
- *Heritage Kerbs, Channels and Laneways Guidelines* (City of Port Phillip, 2006)
- *Port Phillip Design Manual, 2000* (City of Port Phillip, 2000) including:
 - *Dunstan Estate Guidelines* (City of Port Phillip, 2007)
 - *Fishermans Bend Estate Guidelines* (City of Port Phillip, updated 2021)
 - *Garden City Guidelines* (City of Port Phillip, updated 2021)
- [South Melbourne Structure Plan](#) (City of Port Phillip, August 2024)

17.01-1L South Melbourne Enterprise Precinct

Policy Application

This policy applies to all land in the South Melbourne Enterprise Precinct as shown on Map 1 to this clause.

Objective

To reinforce and expand the South Melbourne Enterprise Precinct's provision of diverse employment opportunities.

Strategies

Support the development of the South Melbourne Enterprise Precinct for employment uses, including in creative industries and professional services.

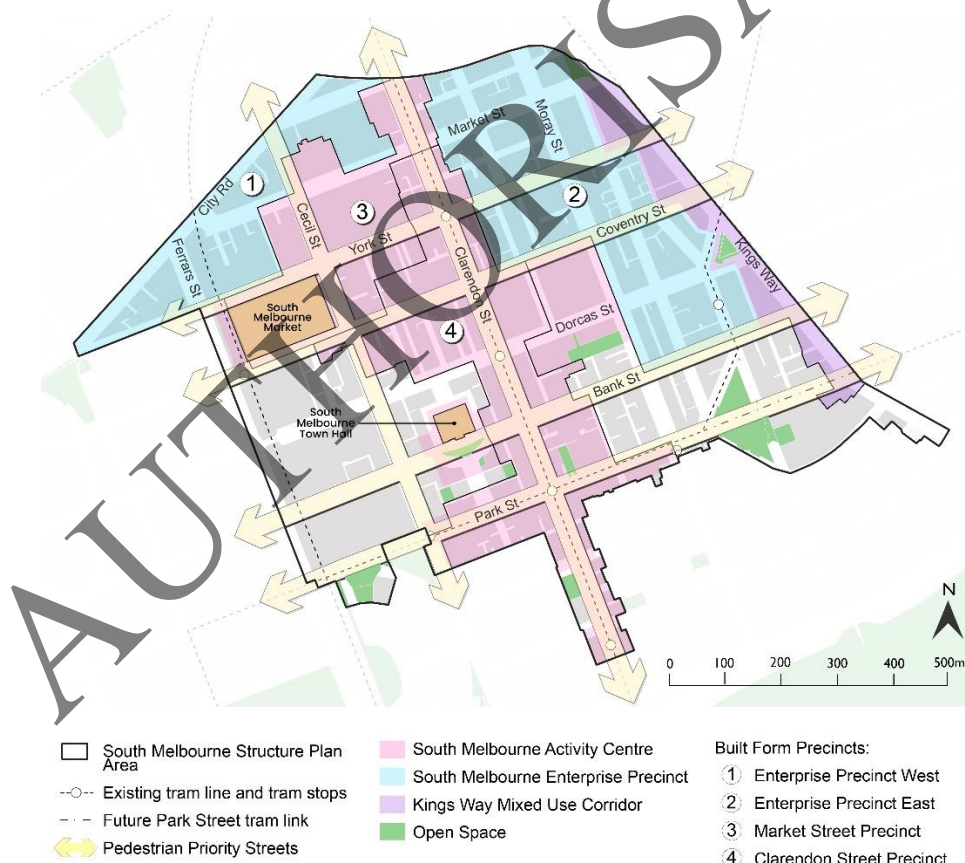
Maintain the existing Commercial 2 Zone to the South Melbourne Enterprise Precinct to strengthen its economic specialisations in creative industries, professional and financial services, retail, hospitality, and personal services.

Policy documents

Consider as relevant:

- *South Melbourne Structure Plan* (City of Port Phillip, August 2024).

Map 1: South Melbourne Structure Plan Area



17.01-2L Innovation and research – South Melbourne Enterprise Precinct

Policy Application

This policy applies to all land in the South Melbourne Enterprise Precinct as shown on Map 1 to this clause.

Activities and uses

Objective

To enhance the South Melbourne Enterprise Precinct's role in providing diverse employment opportunities, facilitating innovation, enterprise and creativity, and providing a complementary function to the South Melbourne Major Activity Centre.

Strategies

Support the development of the South Melbourne Enterprise Precinct for employment uses, including in creative industries and professional services.

Support the provision of diverse employment uses, including employment opportunities in emerging industries.

Maintain the existing Commercial 2 Zone to the South Melbourne Enterprise Precinct to strengthen its economic specialisations in creative industries, professional and financial services, retail, hospitality, and personal services.

Policy documents

Consider as relevant:

- *South Melbourne Structure Plan* (City of Port Phillip, August 2024).

Map 1: South Melbourne Structure Plan Area



17.02-1L Mixed use and office areasxx/xx/xxxx
C219port**Objective**

To support the strategic role, function, viability and vibrancy of Port Phillip's office and mixed-use areas.

Strategies

Ensure use supports the future strategic role and function of the office and mixed activity areas, as defined in Table 1.

Ensure that the nature and intensity of office and commercial activity is compatible with and responsive to its location, particularly in Mixed Use zones.

Support retail use and development in the Mixed Use Zone, where they do not compete with the primary retail function of an adjacent major activity centre and they do not adversely impact the amenity of adjoining areas, including from car parking.

Encourage uses within the St Kilda Road, South Melbourne and Albert Road, South Melbourne office and mixed use precincts and Schedule 1 to the Capital City Zone that support the daily convenience needs of local residents and workers, including take away food premises, convenience shops and personal services.

Encourage the redevelopment of the Greeves Street Mixed Activity Precinct to transition to predominantly residential uses.

Support office and compatible light industrial/warehouse uses in the Greeves Street Mixed Activity Precinct where they do not undermine or negatively impact on the primary residential function of this area.

Table 1: Office and Mixed Use Activity Areas

Activity area location and zone	Strategic role and function – future direction					
	Employment node	Commercial/ office role	Intensification of housing (subject to heritage and amenity considerations)	Active ground floor uses	Retail showrooms/ restricted retail	Light industrial/ warehouse role
Fishermans Bend Urban Renewal Area - Schedule 1 to the Capital City Zone	P	P	P	P	S	S
St Kilda Road, South Melbourne - Commercial 1 Zone	P	P	P	S		
Albert Road, South Melbourne - Commercial 1 Zone	P	P	P	S		

Activity area location and zone	Strategic role and function – future direction					
	Employment node	Commercial/ office role	Intensification of housing (subject to heritage and amenity considerations)	Active ground floor uses	Retail showrooms/ restricted retail	Light industrial/ warehouse role
St Kilda Road South - Commercial 1 Zone	S	P	P	P	S	
Bay Street (southern end), Port Melbourne - Mixed Use Zone		S	P	P	S	
Kings Way / Dorcas Street, South Melbourne - Mixed Use Zone	P	P	P			
Kings Way Mixed Use Corridor, South Melbourne - Mixed Use Zone	S	S	P	S		
Greeves Street, St Kilda - Mixed Use Zone		S	P			S
Inkerman/ Pakington Street, Balaclava - Mixed Use Zone		S	P	S		S
Chapel Street, Balaclava - Mixed Use Zone		S	P	S		
Crockford Street, Port Melbourne- Mixed Use Zone		P	P	P		
Bay Street (north		P	P	P		

Activity area location and zone	Strategic role and function – future direction					
	Employment node	Commercial/ office role	Intensification of housing (subject to heritage and amenity considerations)	Active ground floor uses	Retail showrooms/ restricted retail	Light industrial/ warehouse role
of Spring Street), Port Melbourne - Mixed Use Zone						
Wellington Street, St Kilda - Mixed Use Zone	S	S	P	P		

P - Primary function.

S - Secondary/supporting function.

Policy documents

Consider as relevant:

- [South Melbourne Structure Plan \(City of Port Phillip, August 2024\)](#)

17.03-2L Sustainable industry

xx/xx/xxxx
C219port

Objective

To protect and enhance the economic viability and sustainability of Port Phillip's industrial areas.

Strategies

Ensure industrial areas are protected from encroachment by sensitive land uses.

Ensure new uses support the future strategic role and function of Port Phillip's industrial areas as indicated in Table 1.

Support ongoing industrial uses in Fishermans Bend and Port Melbourne, as the area transitions towards a mixed use urban renewal area.

Avoid the expansion of commercial and office uses within industrial zones where they are not ancillary to a primary industrial use on the site.

Encourage uses that support the needs of the local workforce in industrial areas.

Support commercial operations that are environmentally sustainable, particularly in relation to resource use and waste management.

AUTHORISATION

Table 1: Industrial Areas in Port Phillip

Industry area location and zone	Strategic role and function – future direction				
	Primary employment role	Core industrial/manufacturing role	Light industrial/service business/warehouse role	Office/commercial role	High technology/knowledge based industry cluster
Fishermans Bend Urban Renewal Area - Schedule 1 to the Capital City Zone	•		•	•	•
South Melbourne - Industrial 1 Zone	•	•	•		•
William Street, Balaclava - Industrial 3 Zone			•	•	
Eastern Business District and Western Business District, South Melbourne - Central - Commercial 2 Zone	•		•	•	•
South Melbourne Enterprise Precinct Commercial 2 Zone	•		•	•	•

Policy documents

Consider as relevant:

- *South Melbourne Structure Plan* (City of Port Phillip, August 2024)

18.01-3L-01 Sustainable and safe transport in Port Phillip

xx/xx/xxxx
C219port

Objective

To provide a high-quality walking and bicycle network and facilities that are integrated, safe and accessible.

Strategies

Provide direct, connected and safe pedestrian and bicycle infrastructure to and between:

- Key facilities such as schools, community services / facilities and activity centres.
- Key destinations including St Kilda foreshore, Station Pier, South Melbourne Market and Port Melbourne foreshore.
- Employment nodes including Melbourne CBD, St Kilda Road, Fishermans Bend Urban Renewal Area, [South Melbourne Enterprise Precinct](#) and the Port Melbourne industrial area.
- Key open space areas including the foreshore and Albert Park Reserve.
- Public transport nodes, including Balaclava Train Station.

Provide high quality walking and bicycle riding routes that are located and designed to:

- Increase permeability throughout local neighbourhoods and large sites.
- Make road crossing safe, easy and reduce delays for pedestrians.
- Incorporate clear wayfinding, shade and signage

Design public streets and spaces to provide support infrastructure for pedestrians (such as footpaths, street furniture, lighting, drinking fountains and toilet facilities).

Support road space changes to prioritise pedestrian movement over private vehicles on key pedestrian routes.

Minimise the impact of development, including vehicle crossovers, on the pedestrian and bicycle network as identified in the Pedestrian and Bicycle Network Framework Plan at 02.04 Strategic Framework Plans.

Support the extension of the existing network of dedicated bicycle routes to improve the connections for on-road and off-road cyclists.

Encourage the provision of increased bicycle parking spaces and related facilities, including spaces suitable for electronic bikes (E-Bikes) and cargo bicycles for both tenants and visitors in mixed use, commercial and residential developments.

Policy guideline

Consider as relevant:

- Providing secure bicycle parking (including E-bikes and cargo bicycles) and end-of-trip facilities consistent with the Built Environment Sustainability Scorecard 'BESS' (Council Alliance for a Sustainable Built Environment, 2015)

Policy document

Consider as relevant:

- *Move, Connect, Live: Integrated Transport Strategy* (City of Port Phillip, August 2018)
- [South Melbourne Structure Plan](#) (City of Port Phillip, August 2024)

18.02-3L-01 Public transport

Strategies

Support improvements to the overall convenience, accessibility and safety of public transport, including public transport stops and interchanges.

Support new route connections and extensions to address network gaps, particularly in

- Fishermans Bend (to Docklands, Southbank, Central City, South Melbourne and Port Melbourne).
- Domain to Western CBD through South Melbourne.
- Port Melbourne to St Kilda.
- St Kilda Esplanade to Balaclava Road.
- Elwood.
- Albert Park, Middle Park and St Kilda to Caulfield.
- South Melbourne, to link the Park Street tram with the St Kilda Road tram.

Support walking, bike riding and public transport improvements as part of the development of the new Anzac Station and Domain Precinct.

Support road space changes to prioritise buses and trams over private vehicles on key routes.

Policy documents

Consider as relevant:

- [*South Melbourne Structure Plan* \(City of Port Phillip, August 2024\)](#)

19.02-4L Community facilities

Objective

To create a network of accessible, functional and adaptable community facilities.

Strategies

Support new community facilities in the following locations where there are existing clusters:

- Bay Street, Port Melbourne.
- South Melbourne ~~Central~~.
- Carlisle Street, Balaclava.
- Fitzroy / Acland Streets, St Kilda.
- Centre Avenue, Garden City.
- Ormond / Glen Huntly Roads, Elwood.

Support the development and delivery of flexible, fit-for-purpose and multi-functional community facilities that respond to changing community needs.

Support co-located and integrated facilities with shared use arrangements to maximise access to and delivery of services.

Support mixed use developments in accessible locations with integrated community facilities.

Policy documents

Consider as relevant:

- *South Melbourne Structure Plan (City of Port Phillip, August 2024)*

C219port
XX/XX/20XX**SCHEDULE 37 TO CLAUSE 43.02 DESIGN AND DEVELOPMENT
OVERLAY**

Shown on the planning scheme map as **DDO37**.

**CLARENDON STREET PRECINCT – SOUTH MELBOURNE MAJOR ACTIVITY
CENTRE****1.0 Design objectives**

To retain a low-rise historic built form character along Clarendon Street, while encouraging taller (6-8 storeys) buildings on larger sites and in the northern part of the precinct.

To ensure development is well spaced and sited to provide high amenity, avoid visual bulk and provide equitable access to an outlook and good daylight, and anticipates and is resilient to the potential impacts of climate change and inundation.

To support residential use above the ground floor of buildings that further enhance the vibrancy and vitality of the South Melbourne Major Activity Centre's retail, commercial and entertainment offering.

To ensure development respects and complements the existing heritage character.

To ensure that development is of high architectural quality and contributes to the creation of a public realm that preserves sunlight to key parks and streets, is attractive, vibrant, safe, engaging and supports active transport, including walking and cycling.

2.0 Buildings and works**2.1 Definitions**

Additional shadows means any shadow cast beyond that which is cast by existing buildings or works, but does not a shadow cast by incidental elements such as canopies, kiosks, artworks, screens or trees.

Floor Area Ratio means the gross floor area above ground of all buildings on a site divided by the area of the site. For the purposes of this calculation:

- Gross floor area includes all enclosed areas, services, lifts, car stackers and covered balconies. It also includes any floor area on a level that projects 1.2 metres above ground level.
- The area of the site includes all contiguous titles in the same ownership that form part of the proposed development.

Heritage building means any building subject to a Heritage Overlay, graded as either Contributory or Significant or any building on the Victorian Heritage Register.

Key streets include Clarendon Street, Cecil Street, York Street, Coventry Street, Dorcas Street, Bank Street, Park Street, Napier Street and Raglan Street.

Laneway means a road reserve with a width of 9 metres or less.

Narrow street means a road reserve of a public highway less than 12 metres wide.

Primary outlook refers to views from living spaces and balconies for residential development.

Public realm means all streets and spaces open to the public but does not include laneways.

Secondary outlook refers to views from bedrooms, including associated balconies, in residential development.

Southern footpath means the space extending 6 metres from the property line of buildings on the southern side of the street.

Street means a road reserve of a public highway more than 12 metres wide.

Street wall height means the height of the street wall measured by the vertical distance between the footpath at the centre of the frontage and the highest point of the building at the street edge or in the case of a heritage building if it is setback from the street from the centre of the building frontage to the highest point of the building, with the exception of non-habitable architectural features such as balustrades or eaves not more than 3.0 metres in height and building services setback at least 3.0 metres behind the street wall.

Street wall means the facade of a building at the street boundary, or, if the existing heritage building is set back from the street boundary, the front of the existing building within 0.3 metres of an existing or proposed street, laneway or public open space.

Upper level means development above the street wall.

2.2 Buildings and works for which no permit is required

A permit is not required to:

- Construct a building or construct or carry out works at ground level to provide access for persons with disabilities that comply with all legislative requirements;
- Construct a building or construct or carry out works associated with:
 - The installation of an automatic teller machine.
 - An alteration to an existing building façade provided:
 - The alteration does not include the installation of an external roller shutter.
 - At least 80 per cent of the building facade at ground floor level is maintained as an entry or window with clear glazing.
 - An awning that projects over a road if it is authorised by the relevant public land manager.

2.3 General requirements

A permit to construct a building or construct or carry out works cannot be granted to vary a requirement in clause 2.4 expressed with the term 'must'.

A permit to construct a building or construct or carry out works may be granted to vary a built form requirement in clause 2.4 expressed with the term 'should'.

Regardless of whether the built form requirements are met, an application for buildings and works must meet the relevant Design Objectives in clause 1.0 and Built Form Outcomes in clause 2.4.

Service equipment and/or structures including balustrades, unenclosed pergolas for communal areas, shading devices, plant rooms, plant screening, lift overruns, stair wells, structures associated with pedestrian access, green roof areas and other such equipment may exceed the mandatory building height provided that each of the following criteria are met for the equipment or structure:

- less than 50 per cent of the roof area is occupied by the equipment /structures (other than solar panels and green roofs);
- the equipment and/or structures do not cause additional shadows of any private open space areas associated with a residential use, opposite footpaths, kerb outstands or planting areas in the public realm; and
- the equipment/structures does not extend higher than 3.0 metres above the maximum building height.

2.4 Design Requirements

Floor Area Ratio

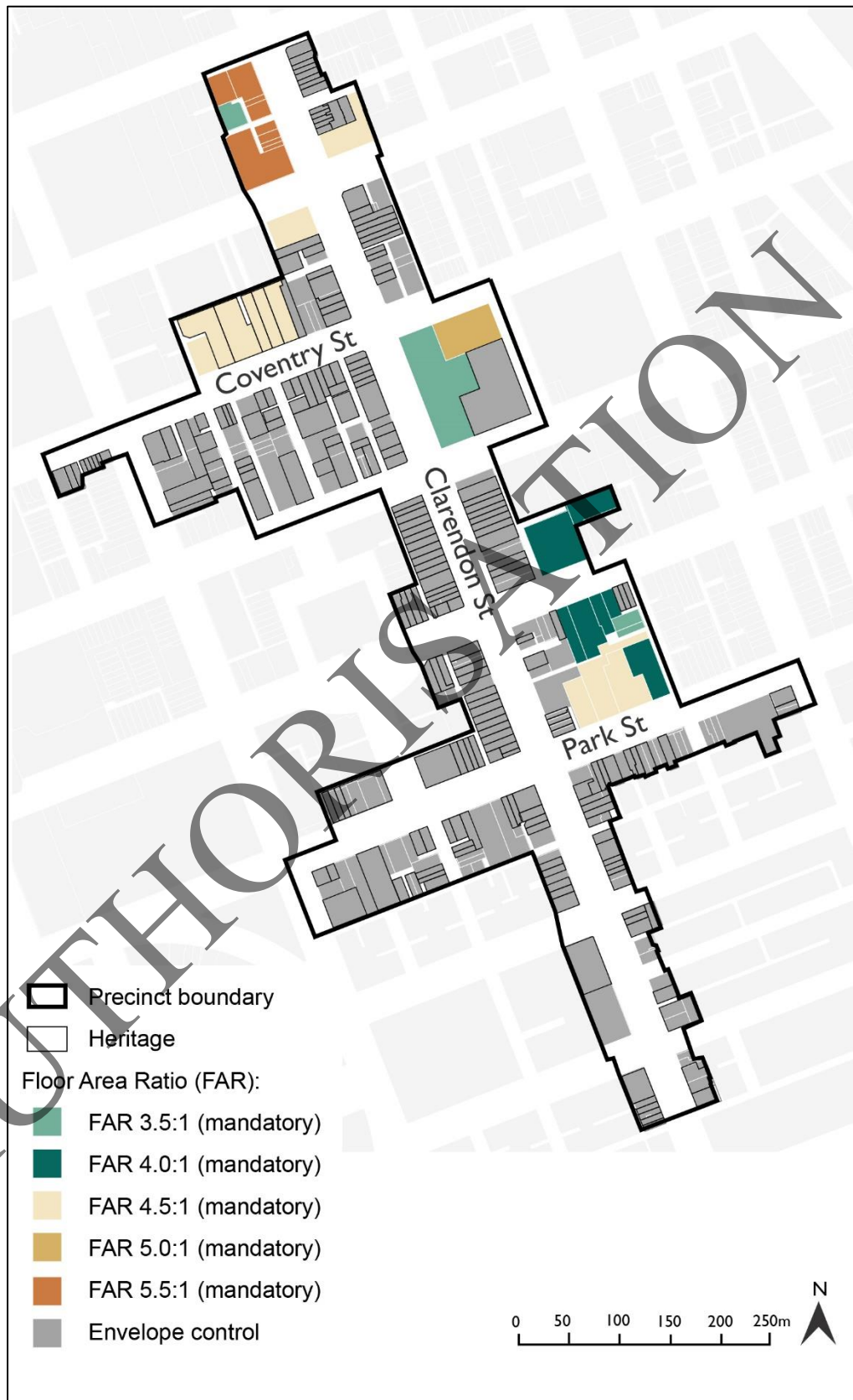
An application to construct a building or construct or carry out works must not exceed the Floor Area Ratio specified in Plan 1 of this schedule. Where a site is marked 'Envelope control', no Floor Area Ratio applies.

Where the site includes contiguous titles in the same ownership, a section 173 agreement must be entered into and registered on each title which records the amount of Floor Area Ratio developed across the entire site, and the amount (if any) of remaining Floor Area Ratio able to be developed on each title should it be individually redeveloped in future. The owner of the land must pay the reasonable costs of the preparation, execution and registration of the section 173 agreement.

A permit cannot be granted or amended to vary this requirement, unless in the case of an amendment to a permit, the amendment does not increase the extent of non-compliance.

AUTHORISATION

Plan 1: Floor Area Ratio to Schedule 37 of Clause 43.02



Interface with residential properties in NRZ or GRZ

Built Form Outcomes

Development that:

- Protects the amenity of existing residential uses in terms of visual bulk, overshadowing of private open space, overlooking and vehicle access.
- Responds to the low scale form of existing residential development through an appropriate transition in building height and setbacks.

Building height

Built Form Outcomes

Building heights that:

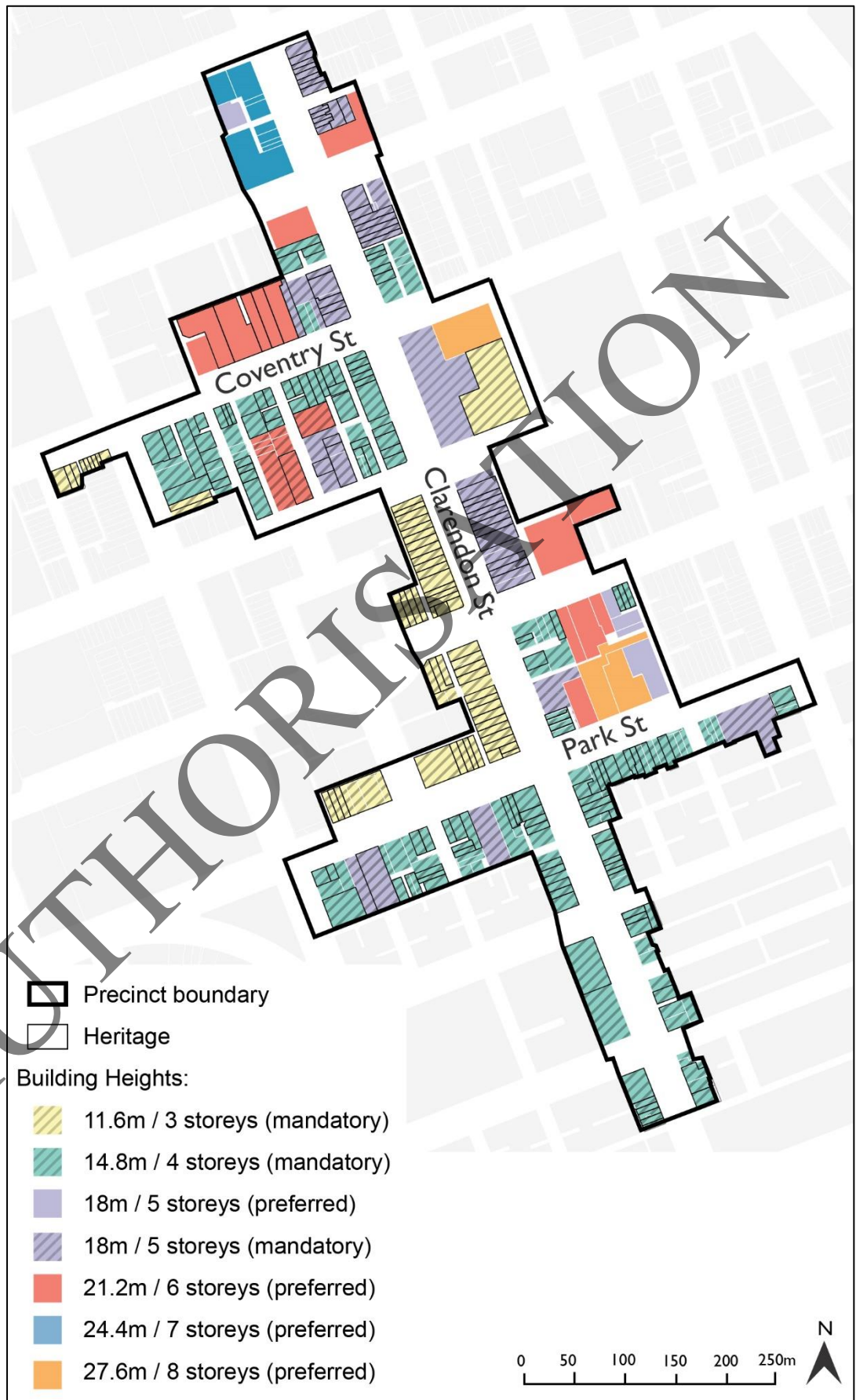
- Do not overwhelm the St Luke's Anglican Church complex on Dorcas Street.
- On larger sites, are broken up into a series of smaller building forms where appropriate and with variable heights.
- Provide a transition to low-rise residential properties.
- Limit impacts on the amenity of residential properties as a result of overshadowing.
- Limit the impact of visual bulk and mass on residential properties and the public realm.

Built Form Requirements

A permit must not be granted to construct a building or construct or carry out works that exceed the mandatory requirement specified in Plan 2 to this schedule.

A permit cannot be granted to vary this requirement, unless the slope of the natural ground level, measured at any cross section of the site of the building wider than 8 metres, is greater than 2.5 degrees, in which case a permit may be granted to exceed the mandatory building height by 1 metre.

Plan 2: Building height to Schedule 37 of Clause 43.02



Adaptable Buildings

Built Form Outcome

Buildings and works that:

- Provide for the future conversion of those parts of the building accommodating non-employment uses to employment uses including the ability to adapt car parking to other uses over time.
- Minimises the impact of car parking on the public realm.

Built Form Requirements

- The building elements in Table 1 should incorporate the adaptability opportunities identified in the table.

Table 1: Adaptable buildings

Building height	Adaptability opportunity
Lower levels up to the height of the street wall	At least 4.0m floor to floor height at ground level. At least 3.2m floor to floor height for other lower levels.
Car parking areas	In areas not in a basement: <ul style="list-style-type: none"> ▪ Level floors. ▪ A floor-to-floor height at least 3.8m. ▪ Mechanical parking systems to reduce the area required for car parking.
Dwelling layout	The ability for one and two-bedroom dwellings to be combined or adapted into three or more bedroom dwellings.
Internal layout	Minimal load bearing walls to maximise flexibility for retail or commercial refits.

Street wall and setbacks

Built Form Outcomes

Street wall heights and setbacks that:

- Respond to street widths, interfaces around the site and enable the protection of sunlight to footpaths.
- Avoid visual bulk.
- Support the provision of high amenity, equitable access to an outlook and good daylight.
- Respect adjacent heritage buildings.

Built Form Requirements

A permit must not be granted to construct a building or construct or carry out works that exceeds the mandatory street wall height requirement specified in Plan 3 to this schedule.

A permit cannot be granted to vary this requirement, unless the building is on a corner where more than one street wall height applies, in which case, the development should 'turn the corner' and continue the taller street wall height along the side street, with a transition to the lower street wall height along the side street towards the rear interface.

Heritage buildings should maintain the existing street wall height.

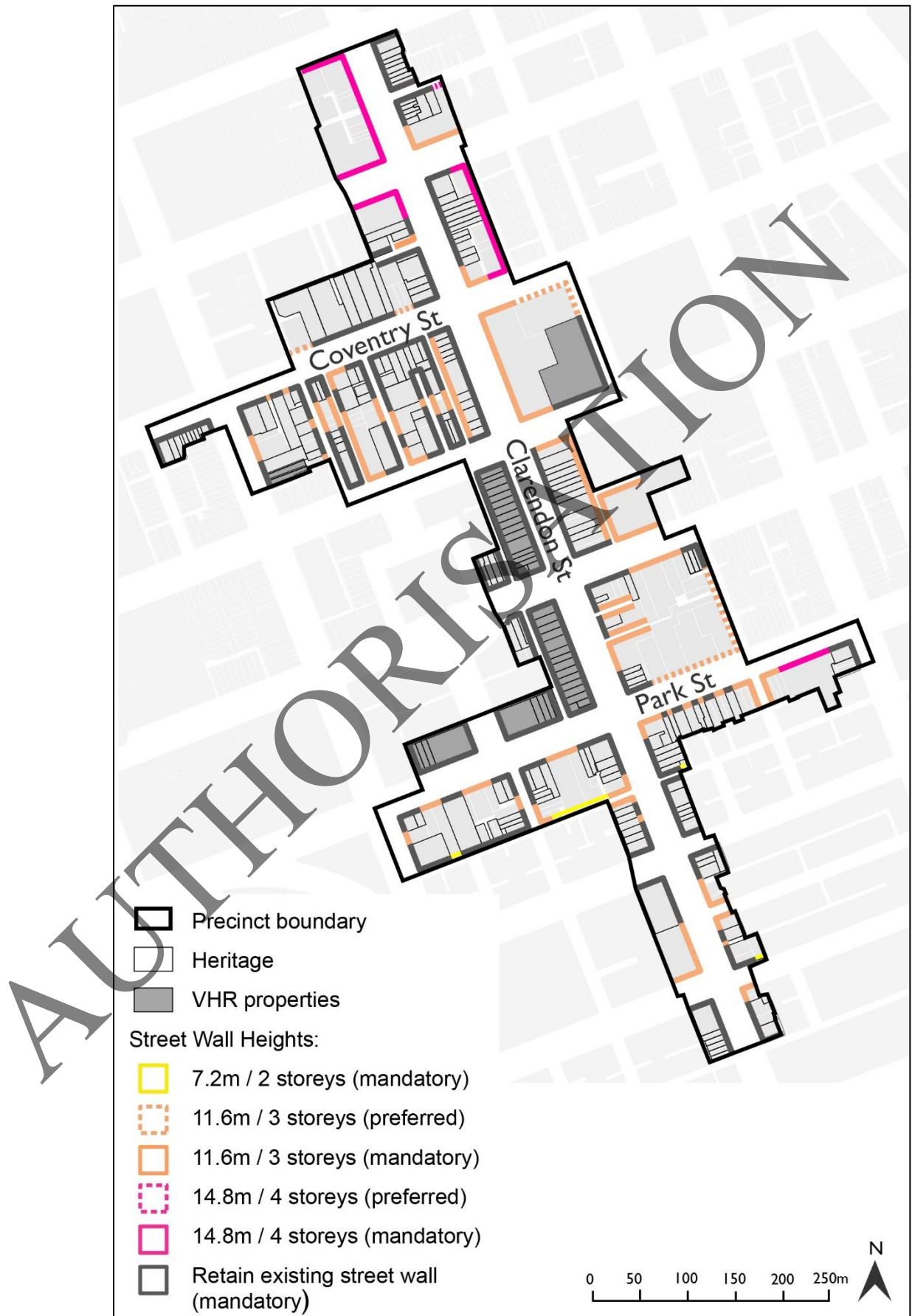
Projections such as building services and architectural features (other than shading devices, mouldings etc.), balconies, terraces and balustrades should not protrude into a setback.

Infill development adjoining a heritage building should match the street wall height for a distance equivalent to a typical structural or façade bay (approximately 6 metres in the case of a typical nineteenth century shop residence).

Where development is separated from a heritage building by a laneway, the street wall height of the proposed development should not be greater than two storeys taller than the neighbouring heritage building, for a minimum distance of that equivalent to a typical structural or façade bay.

AUTHORISATION

Plan 3: Street wall height to Schedule 37 of Clause 43.02



Upper levels

Built Form Outcomes

Upper levels that:

- Respond to street widths, interfaces around the site and enable the protection of sunlight to footpaths.
- Provide equitable development opportunities and adequate internal amenity outcomes in terms of daylight, privacy and outlook.
- Reinforce the fine grain, vertical rhythm and visual interest of streetscapes.
- For heritage buildings, are set back to ensure the visual primacy and legibility of the street wall and the retention of heritage building features such as chimneys and roof forms.

Built Form Requirements

For areas of the precinct covered by an envelope control as specified on Plan 1 to this schedule, to retain the visual prominence of the return facades of heritage buildings, upper level development must be set back at a minimum of 6 metres from Key Streets, including to both frontages of heritage buildings located at the following intersections:

- Clarendon Street with Market, York, Coventry, Dorcas, Bank and Park streets
- Cecil Street with Coventry Street
- Moray Street with Park Street
- Park Street with Perrins Street (on the key north-south axis to South Melbourne Town Hall).

A permit cannot be granted to vary this requirement.

For all other areas of the precinct covered by an envelope control as specified on Plan 1 to this schedule, upper level development should be set back a minimum of 3 metres from laneways and streets below 30 metres in width.

For areas of the precinct covered by a Floor Area Ratio control as specified on Plan 1 to this schedule, upper level development should be set back between 3 and 5 metres above the street wall.

Upper level development should provide appropriate building separation and be setback from side and rear boundaries to provide internal spaces with adequate levels of daylight, privacy and a primary outlook to the street, laneway or open spaces.

Development should match the upper level setback requirement of an adjoining heritage building for a minimum of 6 metres in length.

Overshadowing

Built Form Outcome

Development that preserves sunlight to the public realm and street network, and provides for high amenity, especially along Key Streets.

Built Form Requirements

A permit must not be granted to construct a building or construct or carry out works that would overshadow or cast additional shadows over the southern footpath of the following streets between 10am and 2pm on 22nd September:

- Market Street
- York Street
- Coventry Street
- Dorcas Street

- Bank Street
- Park Street.

A permit cannot be granted to vary this requirement.

Active street frontages

Built Form Outcome

Development that:

- Provides high-quality, active frontages to key streets and laneways.
- Provides car parking areas, structures and accessways that do not detract from the public realm.

Built Form Requirements

Provide for natural surveillance and a visual connection into the building through transparent windows and balconies.

Avoid blank walls, large areas of reflective surfaces, high fences, service areas, to key streets and laneways.

Car parking should be sleeved with active uses so that it is not visible from the public realm or adjoining sites.

Flood resilience

Built Form Outcome

Development that provides carefully considered design solutions for buildings in flood affected areas, particularly in the transition from the building to the public realm to ensure that building entries and frontages are accessible and active.

Built Form Requirements

Ensure that the internal area of buildings including any basements proposed on land susceptible to flooding are designed to be protected from inundation.

Ensure that safe access and egress is provided to that part of the land where there is no anticipated overland flow from floodwaters.

Building separation

Built Form Outcomes

Development that:

- Ensures good levels of daylight and sunlight enter buildings.
- Provides adequate cross ventilation.
- Provides outlook from within buildings.
- Manages privacy between neighbouring buildings, maintains equitable development between sites and avoids the need for visual screening.

Built Form Requirements

Where development shares a common boundary, a new building should be setback by the distance specified in Table 2.

Where two sites share a rear boundary, development abutting the boundary should be no higher than 3 storeys .

For sites with a side boundary to narrow laneways of equal to or less than 3 metres in

width, development abutting the laneway should be no higher than:

- 22 metres; or
- the maximum building height specified in this schedule,

whichever is lesser, provided that:

- the development does not result in unreasonable amenity impacts on the public realm or the rear of the subject property; and
- the façade to the laneway is of a slender form that creates fast moving shadows.

For non-residential development, these requirements in Table 2 may be varied for sites with a depth of 16 metres or less that share a rear boundary where appropriate amenity outcomes can be achieved through a single outlook to the street.

Table 2: Building separation distances

Building height	Living room, balcony and non-residential outlook -minimum site boundary separation	Bedroom outlook - minimum site boundary separation	Separation between multiple buildings on an individual site
Up to 22 metres	4.5 metres	3 metres	9 metres
Above 22 metres and up to 27 metres	6 metres	3 metres	12 metres
Above 27 metres	9 metres	4.5 metres	18 metres

Light wells

Built Form Outcomes

Development that:

- Ensures good levels of daylight and sunlight enter buildings.
- Provides adequate cross ventilation.

Built Form Requirements

- Light wells, if provided, should:
 - be consolidated into a larger courtyard space;
 - meet the dimensions for minimum width and overall area specified in Table 3 from the first level of residential use;
 - provide daylight access to bedrooms only;
 - be painted in a light reflective colour; and
 - be provided with ground level access via a door.
- The light well dimensions in Table 3 may be varied for buildings containing multiple levels of non-residential uses.

Table 3: Light well dimensions

Building height	Light well overall area	Light well minimum width
Up to 22 metres	18 square metres	3 metres

Above 22 metres and up to 27 metres	36 square metres	4.5 metres
Above 27 metres	54 metres	6 metres

3.0 Subdivision

None specified

4.0 Signs

None specified.

5.0 Application requirements

The following application requirements apply to an application for a permit under Clause 43.02, in addition to those specified elsewhere in the scheme and must accompany an application, as appropriate, to the satisfaction of the responsible authority:

- A site analysis and urban design context report which demonstrates how the proposal achieves the Design objectives, Built Form Outcomes and Built Form Requirements in Clause 2.4 to this schedule.
- A plan showing shadows and Additional shadows, as defined by Clause 2.1 of this schedule, cast by the proposal.
- Where a Floor Area Ratio applies to land, a development summary specifying floor-by-floor area, summary of proposed use and total Floor Area Ratio.

6.0 Decision guidelines

The following decision guidelines apply to an application for a permit under Clause 43.02, in addition to those specified in Clause 43.02 and elsewhere in the scheme which must be considered, as appropriate, by the responsible authority:

- The extent to which the development addresses the Built Form Outcomes and Built Form Requirements in this schedule.
- Whether the development achieves high architectural quality (including but not limited to contextual responsiveness, building siting, scale, massing, articulation and materials).
- Whether the development supports a high quality public realm, high levels of pedestrian amenity and supports active transport.
- Whether buildings on larger sites are designed and sited to relate and contribute positively to their context and their historic urban grain such as through variation of building height and building form.
- Whether new development is sited and designed with an active frontage to the street.
- Whether the proposed development is an acceptable outcome having regard to the risk of flooding.
- For an application to exceed the preferred building height shown in the Plan 2 to this schedule:
 - Whether the built form outcome that results from the proposed variation satisfies the relevant Built Form Outcomes and Built Form Requirements specified in Clause 2.4 of this schedule.
- For an application to exceed the street wall height shown in Plan 3 of this schedule:
 - Whether the built form outcome that results from the proposed variation satisfies the relevant Built Form Outcomes and Built Form Requirements specified in Clause 2.4 of this schedule; and

- Whether the street wall at ground floor level is designed to allow floor to floor ceiling heights suitable to accommodate commercial activity.
- For an application to reduce the preferred minimum upper level setback requirements specified in this schedule:
 - Whether the built form outcome that results from the proposed variation satisfies the relevant Built Form Outcomes and Built Form Requirements specified in Clause 2.4 of this schedule.

AUTHORISATION

C219port
XX/XX/20XX**SCHEDULE 38 TO CLAUSE 43.02 DESIGN AND DEVELOPMENT OVERLAY**

Shown on the planning scheme map as **DDO38**.

SOUTH MELBOURNE MARKET PRECINCT – SOUTH MELBOURNE MAJOR ACTIVITY CENTRE**1.0 Design objectives**

To ensure that development transitions from taller (8-12 storey) buildings to medium rise (4-5 storey) buildings adjacent to the historic Clarendon Street Precinct.

To ensure development in the South Melbourne Major Activity Centre contains floorplates and design features that support a diverse range of housing options and employment activities, including retail, professional services and creative industries.

To ensure development is well spaced and sited to provide high amenity, avoid visual bulk and provide equitable access to an outlook and good daylight and anticipates and is resilient to the potential impacts of climate change and inundation.

To enhance the commercial character of this precinct by encouraging the contemporary use of high quality materials.

To ensure that development is of high architectural quality and contributes to the creation of a public realm that preserves sunlight to key parks and streets, is attractive, vibrant, safe, engaging and supports active transport, including walking and cycling.

2.0 Buildings and works**2.1 Definitions**

Additional shadows means any shadow cast beyond that which is cast by existing buildings or works, but does not a shadow cast by incidental elements such as canopies, kiosks, artworks, screens or trees.

Floor Area Ratio means the gross floor area above ground of all buildings on a site divided by the area of the site. For the purposes of this calculation:

- Gross floor area includes all enclosed areas, services, lifts, car stackers and covered balconies. It also includes any flood area on a level that projects 1.2 metres above ground level.
- The area of the site includes all contiguous titles in the same ownership that form part of the proposed development.

Heritage building means any building subject to a Heritage Overlay, graded as either Contributory or Individually Significant or any building on the Victorian Heritage Register.

Key streets include Cecil Street, Clarendon Street, Coventry Street, York Street and Market Street.

Laneway means a road reserve with a width of 9 metres or less.

Narrow street means a road reserve of a public highway less than 12 metres wide.

Primary outlook refers to views from living spaces and balconies for residential development.

Public realm means all streets and spaces open to the public but does not include laneways.

Secondary outlook refers to views from bedrooms, including associated balconies, in residential development.

Southern footpath means the space extending 6 metres from the property line of buildings on the southern side of the street.

Street means a road reserve of a public highway more than 12 metres wide.

Street wall height means the height of the street wall measured by the vertical distance between the footpath at the centre of the frontage and the highest point of the building at the street edge or in the case of a heritage building if it is setback from the street from the centre of the building frontage to the highest point of the building, with the exception of non-habitable architectural features such as balustrades or eaves not more than 3.0 metres in height and building services setback at least 3.0 metres behind the street wall.

Street wall means the facade of a building at the street boundary, or, if the existing heritage building is set back from the street boundary, the front of the existing building within 0.3 metres of an existing or proposed street, laneway or public open space.

Upper level means development above the street wall.

2.2 Buildings and works for which no permit is required

A permit is not required to:

- Construct a building or construct or carry out works at ground level to provide access for persons with disabilities that comply with all legislative requirements.
- Construct a building or constructor carry out works associated with:
 - The installation of an automatic teller machine.
 - An alteration to an existing building façade provided:
 - The alteration does not include the installation of an external roller shutter.
 - At least 80 per cent of the building facade at ground floor level is maintained as an entry or window with clear glazing.
- An awning that projects over a road if it is authorised by the relevant public land manager.

2.3 General requirements

A permit to construct a building or construct or carry out works cannot be granted to vary a requirement in clause 2.4 expressed with the term 'must'.

A permit to construct a building or construct or carry out works may be granted to vary a built form requirement in clause 2.4 expressed with the term 'should'.

Regardless of whether the built form requirements are met, an application for buildings and works must meet the relevant Design Objectives in clause 1.0 and Built Form Outcomes in clause 2.4.

2.4 Design Requirements

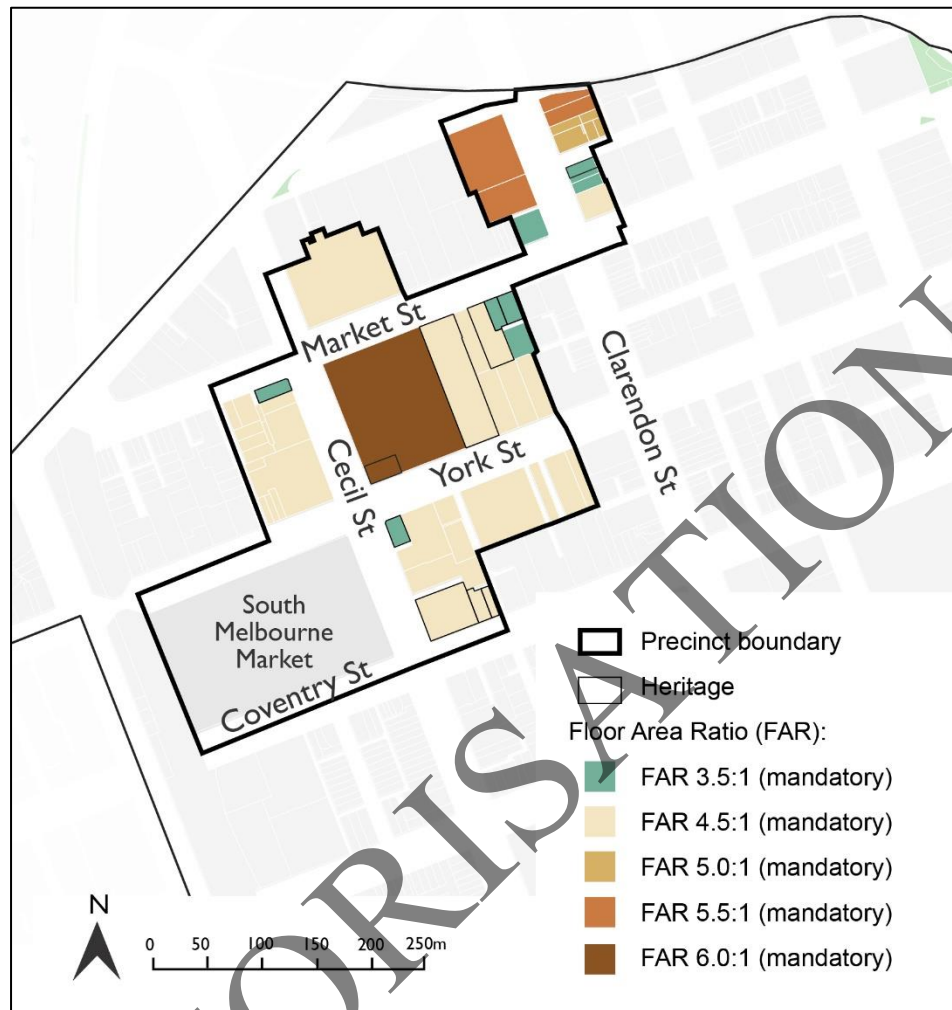
Floor Area Ratio

An application to construct a building or construct or carry out works must not exceed the Floor Area Ratio specified in Plan 1 of this schedule. Where a site is marked 'Envelope control', no Floor Area Ratio applies.

Where the site includes contiguous titles in the same ownership, a section 173 agreement must be entered into and registered on each title which records the amount of Floor Area Ratio developed across the entire site, and the amount (if any) of remaining Floor Area Ratio able to be developed on each title should it be individually redeveloped in future. The owner of the land must pay the reasonable costs of the preparation, execution and registration of the section 173 agreement.

A permit cannot be granted or amended to vary this requirement, unless in the case of an amendment to a permit, the amendment does not increase the extent of non-compliance.

Plan 1: Floor Area Ratio to Schedule 38 of Clause 43.02



Interface with residential properties in NRZ or GRZ

Built Form Outcome

Development that:

- Protects the amenity of existing residential properties in terms of visual bulk, overshadowing of private open space, overlooking and vehicle access.

Building height

Built Form Outcomes

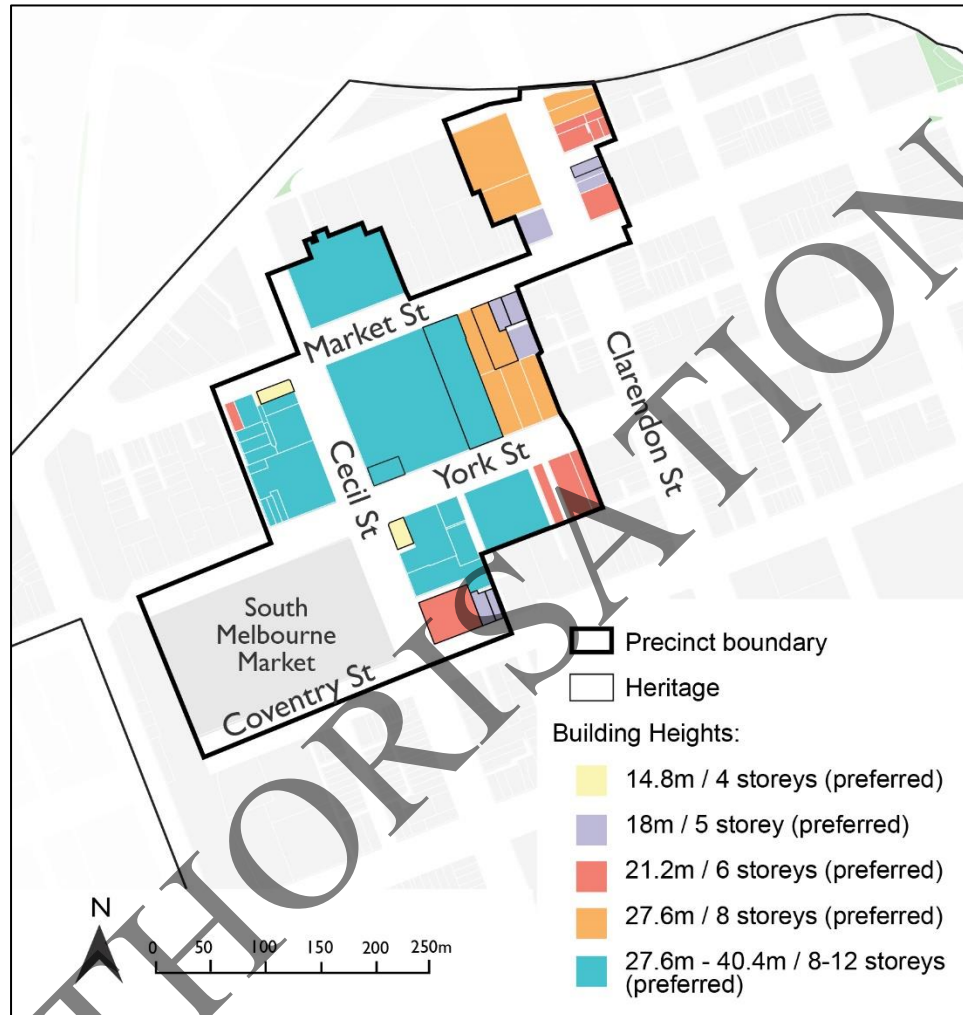
Building heights that:

- On larger sites, are broken up into a series of smaller building forms where appropriate and with variable heights to ensure they relate and contribute positively to their context and their historic urban grain.
- Provide an appropriate scale of development and transition from taller (8-12 storey) buildings to medium rise (4-5 storey buildings) adjacent to the historic Clarendon Street Precinct.
- Limits impacts on the amenity of York Street and potential future open space on the South Melbourne Market site as a result of overshadowing.

Built Form Requirements

Buildings and works should not exceed the preferred building height requirement specified in Plan 2 to this schedule.

Plan 2: Building height to Schedule 38 of Clause 43.02



Adaptable Buildings

Built Form Outcome

Buildings and works that:

- Provide for the future conversion of those parts of the building accommodating non-employment uses to employment uses including the ability to adapt car parking to other uses over time.
- Minimises the impact of car parking on the public realm.

Built Form Requirements

- The building elements in Table 1 should incorporate the adaptability opportunities identified in the table.

Table 1: Adaptable buildings

Building height	Adaptability opportunity
Lower levels up to the height of the street wall	At least 4.0m floor to floor height at ground level. At least 3.2m floor to floor height for other lower levels.
Car parking areas	In areas not in a basement: <ul style="list-style-type: none"> ▪ Level floors. ▪ A floor-to-floor height at least 3.8m. ▪ Mechanical parking systems to reduce the area required for car parking.
Dwelling layout	The ability for one and two-bedroom dwellings to be combined or adapted into three or more bedroom dwellings.
Internal layout	Minimal load bearing walls to maximise flexibility for retail or commercial refits.

Street wall and setbacks**Built Form Outcomes**

Street wall heights and setbacks that:

- Respond to street widths, interfaces around the site and enable the protection of sunlight to footpaths.
- Avoid visual bulk.
- Support the provision of high amenity, equitable access to an outlook and good daylight.
- On the north side of York Street between Cecil Street and the western boundary of South Melbourne Market, provides greater setbacks to protect sunlight to the southern footpath adjacent to the South Melbourne Market site.

Built Form Requirements

Buildings and works should not exceed preferred street wall height requirement specified in Plan 3 to this schedule.

Heritage buildings should maintain the existing street wall height.

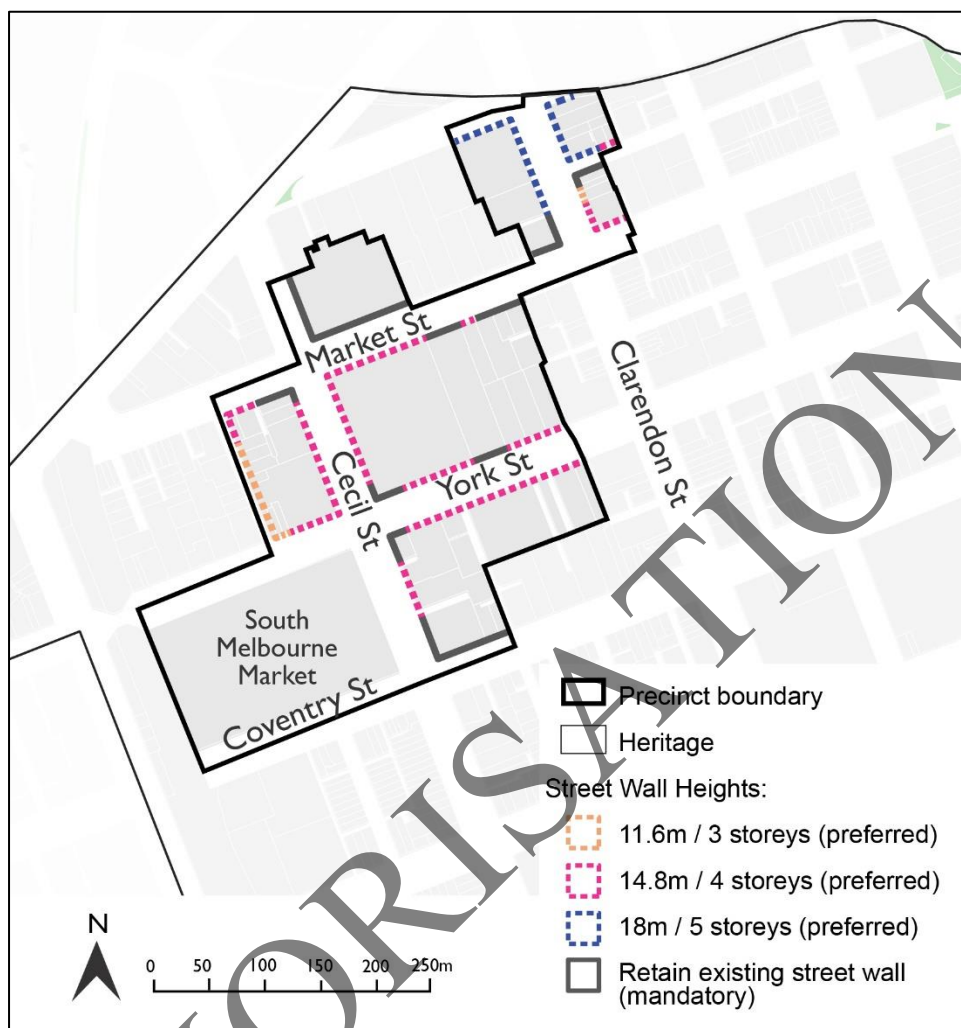
For corner sites where two different street wall heights are nominated, development should 'turn the corner' and continue the taller street wall height along the side street, with a transition to the lower street wall height along the side street towards the rear interface.

Projections such as building services and architectural features (other than shading devices, mouldings etc.), balconies, terraces and balustrades should not protrude into a setback.

Infill development adjoining a heritage building should match the street wall height for a distance equivalent to a typical structural or façade bay (approximately 6 metres in the case of a typical nineteenth century shop residence).

Where development is separated from a heritage building by a laneway, the street wall height of the proposed development should not be greater than two storeys taller than the neighbouring heritage building, for a minimum distance of that equivalent to a typical structural or façade bay.

Plan 3: Street wall height to Schedule 38 of Clause 43.02



Upper levels

Built Form Outcomes

Upper levels that:

- Respond to street widths, interfaces around the site and enable the protection of sunlight to footpaths.
- Provide equitable development opportunities and adequate internal amenity outcomes in terms of daylight, privacy and outlook..
- Reinforce the fine grain, vertical rhythm and visual interest of streetscapes.
- For heritage buildings, are set back to ensure the visual primary and legibility of the street wall and the retention of heritage building features such as chimneys and roof forms.

Built Form Requirements

To retain the visual prominence of the return facades of heritage buildings, upper level development must be set back at a minimum of 6 metres from Key Streets, including to both frontages of heritage buildings located at the following intersections:

- Clarendon Street with Market Street.
- Cecil Street with Market, York and Coventry streets.

A permit cannot be granted to vary this requirement.

Upper level development for all other areas should be set back between 3 metres and 5 metres from the street wall.

Upper level development should provide appropriate building separation and setback from side and rear boundaries to provide internal spaces with adequate levels of daylight, privacy and a primary outlook to the street, laneway or open spaces.

Development should match the upper level setback requirement of an adjoining heritage building for a minimum of 6 meters in length.

Overshadowing

Built Form Outcomes

Development that:

- Preserves sunlight to the public realm and street network and provides for high amenity, especially along key streets.
- On the north side of York Street between Cecil Street and the western boundary of South Melbourne Market, provides greater setbacks to protect sunlight to the southern footpath and enable potential future open space on the South Melbourne Market site.

Built Form Requirements

A permit must not be granted to construct a building or construct or carry out works that would overshadow or cast additional shadows over the southern footpaths of the following streets between 10am and 2pm on 22nd September:

- Market Street
- York Street to the east of Cecil Street
- Coventry Street.

A permit cannot be granted to vary this requirement.

A permit must not be granted to construct a building or construct or carry out works that would overshadow the southern footpaths of the following streets between 10am and 2pm on 22nd June:

- York Street from Cecil Street to the western boundary of South Melbourne Market.

A permit cannot be granted to vary this requirement.

Active street frontages

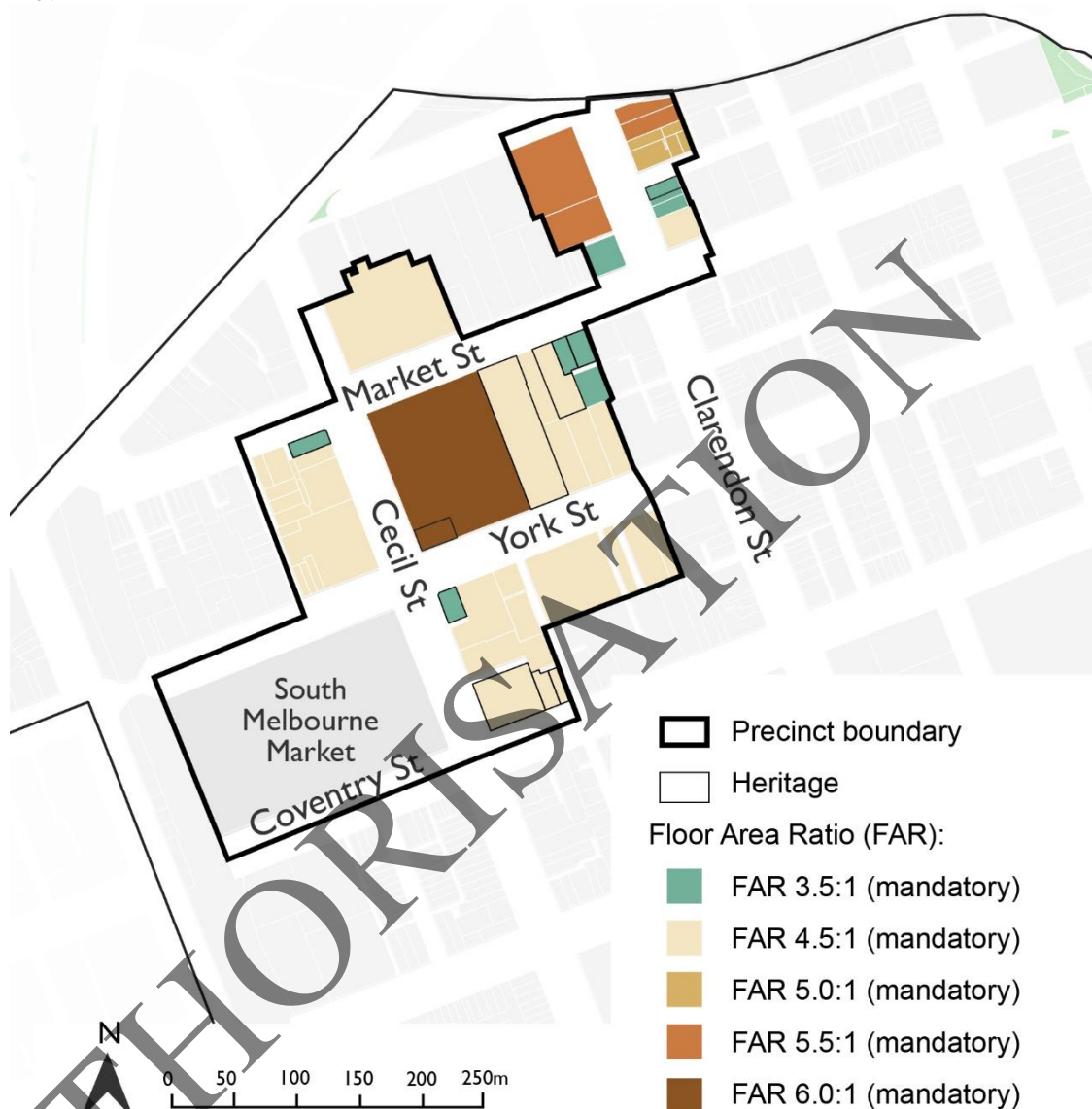
Built Form Outcome

Development that:

- Provides high-quality, active frontages to key streets and laneways.
- Provides car parking areas, structures and accessways that do not detract from the public realm.

Built Form Requirements

Provi



de for natural surveillance and a visual connection into the building through transparent windows and balconies.

Avoid blank walls, large areas of reflective surfaces, high fences, service areas, to key streets and laneways.

Car parking should be sleeved with active uses so that it is not visible from the public realm or adjoining sites.

Flood resilience

Built Form Outcome

Development that provides carefully considered design solutions for buildings in flood affected areas, particularly in the transition from the building to the public realm to ensure that building entries and frontages are accessible and active.

Built Form Requirements

Ensure that the internal area of buildings including any basements proposed on land susceptible to flooding are designed to be protected from inundation.

Ensure that safe access and egress is provided to that part of the land where there is no anticipated overland flow from floodwaters.

Building separation

Built Form Outcomes

Development that:

- Ensures good levels of daylight and sunlight enter buildings.
- Provides adequate cross ventilation.
- Provides outlook from within buildings.
- Manages privacy between neighbouring buildings, maintains equitable development between sites and avoids the need for visual screening.

Built Form Requirements

Where development shares a common boundary, a new building should be setback by the distance specified in Table 2.

Where two sites share a rear boundary, development abutting the boundary should be no higher than 3 storeys.

For sites with a side boundary to narrow laneways of equal to or less than 3 metres in width, development abutting the laneway should be no higher than:

- 22 metres; or
- the maximum building height specified in this schedule,

whichever is lesser, provided that:

- the development does not result in unreasonable amenity impacts on the public realm or the rear of the subject property; and
- the façade to the laneway is of a slender form that creates fast moving shadows.

For non-residential development, the requirements in Table 2 may be varied for sites with a depth of 16 metres or less that share a rear boundary where appropriate amenity outcomes can be achieved through a single outlook to the street.

Table 2: Building separation distances

Building height	Living room, balcony and non-residential outlook -minimum site boundary separation	Bedroom outlook - minimum site boundary separation	Separation between multiple buildings on an individual site
Up to 22 metres	4.5 metres	3 metres	9 metres
Above 22 metres and up to 27 metres	6 metres	3 metres	12 metres
Above 27 metres	9 metres	4.5 metres	18 metres

Light wells

Built Form Outcomes

Development that:

- Ensures good levels of daylight and sunlight enter buildings.
- Provides adequate cross ventilation.

Built Form Requirements

- Light wells, if provided, should:
 - be consolidated into a larger courtyard space;
 - meet the dimensions for minimum width and overall area specified in Table 3 from the first level of residential use;
 - provide daylight access to bedrooms only;
 - be painted in a light reflective colour; and
 - be provided with ground level access via a door.
- The light well dimensions in Table 3 may be varied for buildings containing multiple levels of non-residential uses.

Table 3: Light well dimensions

Building height	Light well overall area	Light well minimum width
Up to 22 metres	18 square metres	3 metres
Above 22 metres and up to 27 metres	36 square metres	4.5 metres
Above 27 metres	54 metres	6 metres

Wind effects on the public realm

Built Form Outcome

Local wind conditions that:

- Maintain a safe and pleasant pedestrian environment on footpaths and other public spaces for walking, sitting or standing.

Built Form Requirement

Buildings and works higher than 40 metres:

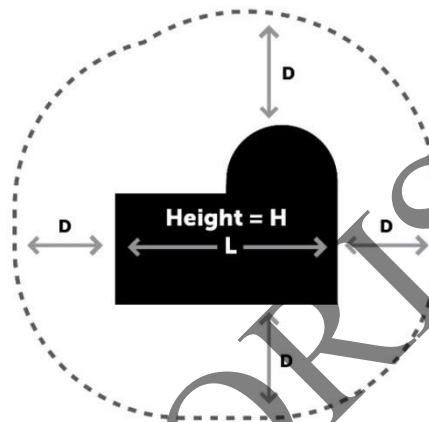
- Must not cause unsafe wind conditions as specified in Table 4 in publicly accessible areas within the assessment distance from all façades. A permit cannot be granted to vary this requirement.
- Should achieve comfortable wind conditions as specified in Table 2 in publicly accessible areas within the assessment distance from all façades.

The assessment distance is shown in the figure below and is the greater of:

- Half the longest width of the building.
- Half the total height of the building.

Table 4: Wind effects on the public realm

Wind condition	Specification
Comfortable wind conditions	<p>The Hourly mean wind speed from all wind directions combined with a probability of exceedance of 20 per cent, is less than or equal to:</p> <ul style="list-style-type: none"> 3m/second for sitting areas. 4m/second for standing areas. 5m/second for walking areas. <p>Hourly mean wind speed is the maximum of:</p> <ul style="list-style-type: none"> The hourly mean wind speed. The gust equivalent mean speed (3 second gust wind speed divided by 1.85).
Unsafe wind conditions	<p>The hourly maximum 3 second gust from any wind direction (considering at least 16 wind directions) with a corresponding probability of exceedance percentage greater than 20m per second.</p>



Assessment distance D = greater of:
 L/2 (Half longest width of building)
 OR
 H/2 (Half overall height of building)

3.0**Subdivision**

None specified.

4.0**Signs**

None specified.

5.0**Application requirements**

The following application requirements apply to an application for a permit under Clause 43.02, in addition to those specified elsewhere in the scheme and must accompany an application, as appropriate, to the satisfaction of the responsible authority:

- A site analysis and urban design context report which demonstrates how the proposal achieves the Design Objectives, Built Form Outcomes, and Built Form Requirements in Clause 2.4 to this schedule.
- A plan showing shadows and Additional shadows, as defined by Clause 2.1 of this schedule, cast by the proposal.

- Where a Floor Area Ratio applies to land, a development summary specifying floor-by-floor area, summary of proposed use and total Floor Area Ratio.
- For buildings and works higher than 40 metres, a wind assessment prepared by a suitably qualified person that:
 - Models the wind effects of the proposed development and its surrounding buildings.
 - Demonstrates the proposed development will not cause unsafe wind conditions.
 - Explains the effect of the proposed development on the wind conditions in publicly accessible areas.
 - Shows the development will allow for comfortable sitting in any public open space, standing in any pedestrian entrance and walking in any pedestrian walkway.

6.0 Decision guidelines

The following decision guidelines apply to an application for a permit under Clause 43.02, in addition to those specified in Clause 43.02 and elsewhere in the scheme which must be considered, as appropriate, by the responsible authority:

- The extent to which the development addresses the Built Form Outcomes and Built Form Requirements in this schedule.
- Whether the development achieves high architectural quality (including but not limited to contextual responsiveness, building siting, scale, massing, articulation and materials).
- Whether the development supports a high quality public realm, high levels of pedestrian amenity and supports active transport.
- Whether buildings on larger sites are designed and sited to relate and contribute positively to their context and their historic urban grain such as through variation of building height and building form.
- Whether new development is sited and designed with an active frontage to the street.
- Whether the proposed development is an acceptable outcome having regard to the risk of flooding.
- Whether new development supports local wind conditions that maintain a safe and pleasant pedestrian environment on footpaths and other public spaces for walking, sitting or standing.
- For an application to exceed the preferred building height shown in the Plan 2 to this schedule:
 - Whether the built form outcome that results from the proposed variation satisfies the relevant Built Form Outcomes and Built Form Requirements specified in Clause 2.4 of this schedule.
- For an application to exceed the street wall height shown in Plan 3 of this schedule:
 - Whether the built form outcome that results from the proposed variation satisfies the relevant Built Form Outcomes and Built Form Requirements specified in Clause 2.4 of this schedule; and
 - Whether the street wall at ground floor level is designed to allow floor to floor ceiling heights suitable to accommodate commercial activity.
- For an application to reduce the preferred minimum setback requirements specified in this schedule:
 - Whether the built form outcome that results from the proposed variation satisfies the relevant Built Form Outcomes and Built Form Requirements specified in Clause 2.4 of this schedule.

C219port
XX/XX/20XX**SCHEDULE 39 TO CLAUSE 43.02 DESIGN AND DEVELOPMENT OVERLAY**

Shown on the planning scheme map as **DDO39**.

SOUTH MELBOURNE ENTERPRISE PRECINCT EAST AND KINGS WAY MIXED USE CORRIDOR**1.0 Design objectives**

To ensure that development transitions from taller (8-12 storey) buildings along Kings Way to medium rise (5-6 storey) buildings in locations with a historic industrial and mixed use character.

To ensure development in the South Melbourne Enterprise Precinct contains floorplates and design features that support a diverse range of employment activities, including creative industries and professional services.

To ensure development is well spaced and sited to provide high amenity, avoid visual bulk and provide equitable access to an outlook and good daylight and anticipates and is resilient to the potential impacts of climate change and inundation.

To reference the industrial history of the precinct by supporting the adaptive reuse of existing buildings and encouraging the contemporary use of common industrial materials.

To ensure that development is of high architectural quality and contributes to the creation of a public realm that preserves sunlight to key parks and streets, is attractive, vibrant, safe, engaging and supports active transport, including walking and cycling.

2.0 Buildings and works**2.1 Definitions**

Additional shadows means any shadow cast beyond that which is cast by existing buildings or works, but does not a shadow cast by incidental elements such as canopies, kiosks, artworks, screens or trees.

Floor Area Ratio means the gross floor area above ground of all buildings on a site divided by the area of the site. For the purposes of this calculation:

- Gross floor area includes all enclosed areas, services, lifts, car stackers and covered balconies. It also includes any floor area on a level that projects 1.2 metres above ground level.
- The area of the site includes all contiguous titles in the same ownership that form part of the proposed development.

Heritage building means any building subject to a Heritage Overlay, graded as either Contributory or Individually Significant or any building on the Victorian Heritage Register.

Key streets include Moray Street, Market Street, York Street, Coventry Street, Bank Street and Eastern Road between Kings Way and Dorcas Street.

Laneway means a road reserve with a width of 9 metres or less.

Narrow street means a road reserve of a public highway less than 12 metres wide.

Primary outlook refers to views from living spaces and balconies for residential development.

Public realm means all streets and spaces open to the public but does not include laneways.

Southern footpath as the space extending 6 metres from the property line of buildings on the southern side of the street.

Secondary outlook refers to views from bedrooms in residential development.

Street means a road reserve of a public highway more than 12 metres wide.

Street wall height means the height of the street wall measured by the vertical distance between the footpath at the centre of the frontage and the highest point of the building at the street edge or in the case of a heritage building if it is setback from the street from the centre of the building frontage to the highest point of the building, with the exception of non-habitable architectural features such as balustrades or eaves not more than 3.0 metres in height and building services setback at least 3.0 metres behind the street wall.

Street wall means the facade of a building at the street boundary, or, if the existing heritage building is set back from the street boundary, the front of the existing building within 0.3 metres of an existing or proposed street, laneway or public open space.

Upper level means development above the street wall.

2.2 Buildings and works for which no permit is required

A permit is not required to:

- Construct a building or construct or carry out works at ground level to provide access for persons with disabilities that comply with all legislative requirements.
- Construct a building or constructor carry out works associated with:
 - The installation of an automatic teller machine.
 - An alteration to an existing building façade provided:
 - The alteration does not include the installation of an external roller shutter.
 - At least 80 per cent of the building facade at ground floor level is maintained as an entry or window with clear glazing.
- An awning that projects over a road if it is authorised by the relevant public land manager.

2.3 Exemption from notice and review

An application to subdivide land or construct a building or construct or carry out works is exempt from the notice requirements of section 52(1)(a), (b) and (d), the decision requirements of section 64(1), (2) and (3) and the review rights of section 82(1) of the Act. This exemption does not apply to land within 30 metres of land (not a road) which is in a residential zone, land used for a hospital or an education centre or land in a Public Acquisition Overlay to be acquired for a hospital or an education centre.

2.4 General requirements

A permit to construct a building or construct or carry out works cannot be granted to vary a requirement in clause 2.5 expressed with the term 'must'.

A permit to construct a building or construct or carry out works may be granted to vary a built form requirement in clause 2.5 expressed with the term 'should'.

Regardless of whether the built form requirements are met, an application for buildings and works must meet the relevant Design Objectives in clause 1.0 and Built Form Outcomes in clause 2.5. Architectural features, trees and other vegetation may exceed the preferred height.

2.5 Design Requirements

Floor Area Ratio

An application to construct a building or construct or carry out works must not exceed the Floor Area Ratio specified in Plan 1 of this schedule.

Where the site includes contiguous titles in the same ownership, a section 173 agreement must be entered into and registered on each title which records the amount of Floor Area Ratio developed across the entire site, and the amount (if any) of remaining Floor Area Ratio able to be developed on each title should it be individually redeveloped in future. The

owner of the land must pay the reasonable costs of the preparation, execution and registration of the section 173 agreement.

A permit cannot be granted or amended to vary this requirement, unless in the case of an amendment to a permit, the amendment does not increase the extent of non-compliance.

Plan 1: Floor Area Ratio to Schedule 39 of Clause 43.02



Interface with residential properties in NRZ or GRZ

Built Form Outcome

Development that:

- Protects the amenity of existing residential properties in terms of visual bulk, overshadowing of private open space, overlooking and vehicle access.

Building height

Built Form Outcomes

Building heights that:

- Along Kings Way are integrated into the precinct with frontages and building entries oriented towards the precinct or addressing key east-west streets that provide connections from South Melbourne to the eastern side of Kings Way.
- On larger sites are broken up into a series of smaller building forms where appropriate and with variable heights.

- Provide an appropriate scale of development and transition from taller (8-12 storey) buildings along Kings Way to medium rise (5-6 storey) buildings adjacent to established residential areas.
- Taller buildings are massed to enable greater retention of the front elements of industrial heritage buildings.

Built Form Requirements

Buildings and works should not exceed the preferred building height requirement specified in Plan 2 to this schedule.

Plan 2: Building height to Schedule 39 of Clause 43.02



Adaptable Buildings

Built Form Outcome

Buildings and works that:

- Provide for the future conversion of those parts of the building accommodating non-employment uses to employment uses including the ability to adapt car parking to other uses over time.
- Minimises the impact of car parking on the public realm.

Built Form Requirements

- The building elements in Table 1 should incorporate the adaptability opportunities identified in the table.

Table 1: Adaptable buildings

Building height	Adaptability opportunity
Lower levels up to the height of the street wall	At least 4.0m floor to floor height at ground level. At least 3.2m floor to floor height for other lower levels.
Car parking areas	In areas not in a basement: <ul style="list-style-type: none"> Level floors. A floor-to-floor height at least 3.8m. Mechanical parking systems to reduce the area required for car parking.
Dwelling layout	The ability for one and two-bedroom dwellings to be combined or adapted into three or more bedroom dwellings.
Internal layout	Minimal load bearing walls to maximise flexibility for retail or commercial refits.

Street wall and setbacks

Built Form Outcomes

Street wall heights and setbacks that:

Respond to street widths, interfaces around the site and enable the protection of sunlight to footpaths.

- Avoid visual bulk.
- Support the provision of high amenity, equitable access to an outlook and good daylight.
- Provide lower street wall heights as a transition to the low-rise residential properties on the south side of Bank Street.
- Respect adjacent heritage buildings.

Built Form Requirements

Buildings and works should not exceed the preferred street wall height requirement specified in Plan 3 to this schedule.

Heritage buildings should maintain the existing street wall height.

For corner sites where two different street wall heights are nominated, development should 'turn the corner' and continue the taller street wall height along the side street, with a transition to the lower street wall height along the side street towards the rear interface.

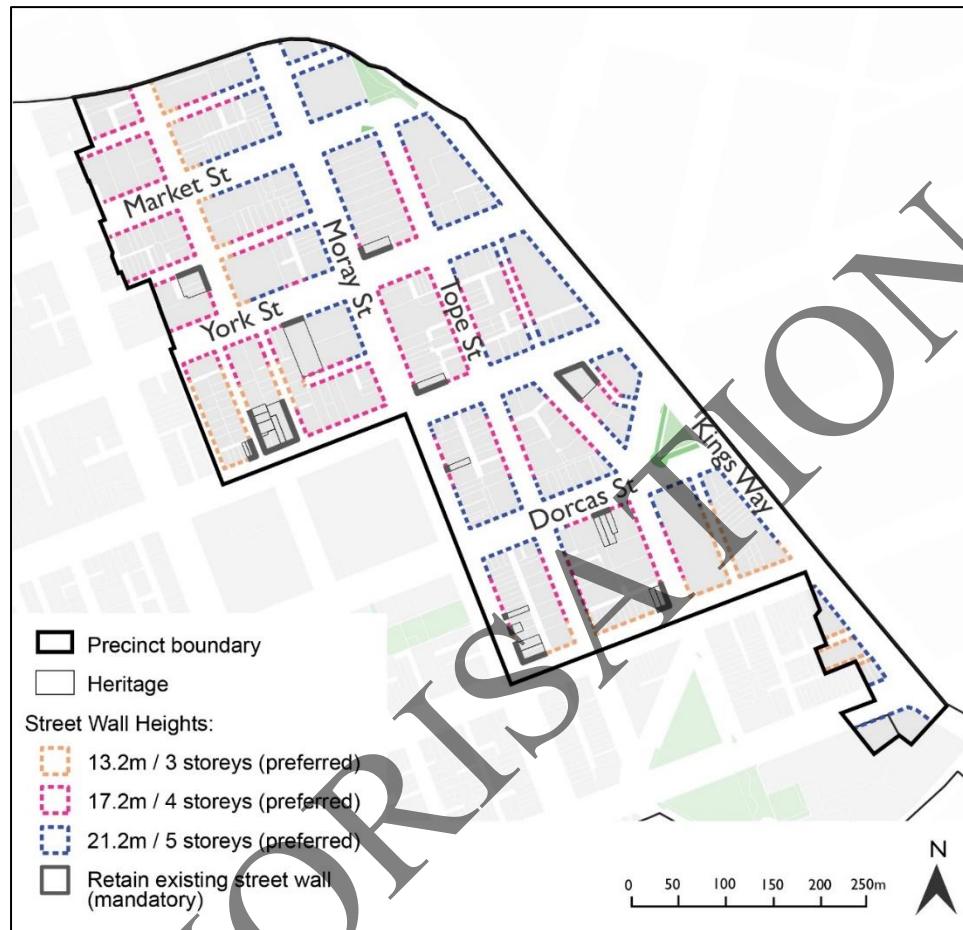
Projections such as building services and architectural features (other than shading devices, mouldings etc.), balconies, terraces and balustrades should not protrude into a setback.

Infill development adjoining a heritage building should match the street wall height for a distance equivalent to a typical structural or façade bay (approximately 6 metres in the case of a typical nineteenth century shop residence).

Where development is separated from a heritage building by a laneway, the street wall height of the proposed development on adjacent land should not be greater than two storeys

taller than the neighbouring heritage building, for a minimum distance of that equivalent to a typical structural or façade bay.

Plan 3: Street wall height to Schedule 39 of Clause 43.02



Upper levels

Built Form Outcomes

Upper levels that:

- Respond to street widths, interfaces around the site and enable the protection of sunlight to footpaths.
- Provide equitable development opportunities and adequate internal amenity outcomes in terms of daylight, privacy and outlook.
- Reinforce the fine grain, vertical rhythm and visual interest of streetscapes.
- For heritage buildings, are set back to ensure the visual primary and legibility of the street wall and the retention of heritage building features such as chimneys and roof forms.

Built Form Requirements

To retain the visual prominence of the return facades of heritage buildings, upper level development must be set back a minimum of 6 metres from key streets, including to both frontages of heritage buildings located at the following intersections:

- Moray Street with York, Coventry, Bank and Park streets.
- Eastern Road and Bank Street.

A permit cannot be granted to vary this requirement.

Upper level development for all other areas should be set back between 3 metres and 5 metres from the street wall.

Upper level development should provide appropriate building separation and be setback from side and rear boundaries to provide internal spaces with adequate levels of daylight, privacy and a primary outlook to the street, laneway or open spaces.

Development should match the upper level setback requirement of an adjoining heritage building for a minimum of 6 meters in length.

Overshadowing

Built Form Outcome

Development that:

- Preserves sunlight to the public realm and street network and provides for high amenity, especially along Key Streets.

Built Form Requirements

A permit must not be granted to construct a building or construct or carry out works that would overshadow or cast additional shadows over the southern footpaths of the following streets between 10am and 2pm on 22nd September:

- Market Street
- York Street
- Coventry Street
- Dorcas Street
- Bank Street.

A permit cannot be granted to vary this requirement.

Active street frontages

Built Form Outcome

Development that:

- Provides high-quality, active frontages to key streets and laneways.
- Provides car parking areas, structures and accessways that do not detract from the public realm.

Built Form Requirements

Provide for natural surveillance and a visual connection into the building through transparent windows and balconies.

Avoid blank walls, large areas of reflective surfaces, high fences, service areas, to key streets and laneways.

Car parking should be sleeved with active uses so that it is not visible from the public realm or adjoining sites.

Flood resilience

Built Form Outcome

Development that provides carefully considered design solutions for buildings in flood affected areas, particularly in the transition from the building to the public realm to ensure that building entries and frontages are accessible and active.

Built Form Requirements

Ensure that the internal area of buildings including any basements proposed on land susceptible to flooding are designed to be protected from inundation.

Ensure that safe access and egress is provided to that part of the land where there is no anticipated overland flow from floodwaters.

Building separation

Built Form Outcomes

Development that:

- Ensures good levels of daylight and sunlight enter buildings.
- Provides adequate cross ventilation.
- Provides outlook from within buildings.
- Manages privacy between neighbouring buildings, maintains equitable development between sites and avoids the need for visual screening.

Built Form Requirements

Where development shares a common boundary, a new building should be setback by the distance specified in Table 1.

Where two sites share a rear boundary, development abutting the boundary should be no higher than 3 storeys .

For sites with a side boundary to narrow laneways of equal to or less than 3 metres in width, development abutting the laneway should be no higher than:

- 22 metres; or
- the maximum building height specified in this schedule,

whichever is lesser, provided that:

- the development does not result in unreasonable amenity impacts on the public realm or the rear of the subject property; and
- the façade to the laneway is of a slender form that creates fast moving shadows.

For non-residential development, these requirements in Table 2 may be varied for sites with a depth of 16 metres or less that share a rear boundary where appropriate amenity outcomes can be achieved through a single outlook to the street.

Table 2: Building separation distances

Building height	Living room, balcony and non-residential outlook -minimum site boundary separation	Bedroom outlook - minimum site boundary separation	Separation between multiple buildings on an individual site
Up to 22 metres	4.5 metres	3 metres	9 metres
Above 22 metres and up to 27 metres	6 metres	3 metres	12 metres
Above 27 metres	9 metres	4.5 metres	18 metres

Light wells

Built Form Outcomes

Development that:

- Ensures good levels of daylight and sunlight enter buildings.
- Provides adequate cross ventilation.

Built Form Requirements

- Light wells, if provided, should:
 - be consolidated into a larger courtyard space;
 - meet the dimensions for minimum width and overall area specified in Table 3 from the first level of residential use;
 - provide daylight access to bedrooms only;
 - be painted in a light reflective colour; and
 - be provided with ground level access via a door.
- The light well dimensions in Table 3 may be varied for buildings containing multiple levels of non-residential uses.

Table 3: Light well dimensions

Building height	Light well overall area	Light well minimum width
Up to 22 metres	18 square metres	3 metres
Above 22 metres and up to 27 metres	36 square metres	4.5 metres
Above 27 metres	54 square metres	6 metres

Wind effects on the public realm

Built Form Outcome

Local wind conditions that:

- Maintain a safe and pleasant pedestrian environment on footpaths and other public spaces for walking, sitting or standing.

Built Form Requirement

Buildings and works higher than 40 metres:

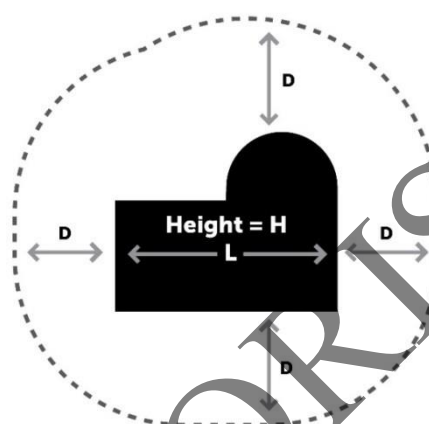
- Must not cause unsafe wind conditions as specified in Table 4 in publicly accessible areas within the assessment distance from all façades. A permit cannot be granted to vary this requirement.
- Should achieve comfortable wind conditions as specified in Table 4 in publicly accessible areas within the assessment distance from all façades.

The assessment distance is shown in the figure below and is the greater of:

- Half the longest width of the building.
- Half the total height of the building.

Table 4: Wind effects on the public realm

Wind condition	Specification
Comfortable wind conditions	<p>The Hourly mean wind speed from all wind directions combined with a probability of exceedance of 20 per cent, is less than or equal to:</p> <ul style="list-style-type: none"> 3m/second for sitting areas. 4m/second for standing areas. 5m/second for walking areas. <p>Hourly mean wind speed is the maximum of:</p> <ul style="list-style-type: none"> The hourly mean wind speed. The gust equivalent mean speed (3 second gust wind speed divided by 1.85).
Unsafe wind conditions	<p>The hourly maximum 3 second gust from any wind direction (considering at least 16 wind directions) with a corresponding probability of exceedance percentage greater than 20m per second.</p>



Assessment distance D = greater of:
 $L/2$ (Half longest width of building)
 OR
 $H/2$ (Half overall height of building)

3.0**Subdivision**

None specified

4.0**Signs**

None specified.

5.0**Application requirements**

The following application requirements apply to an application for a permit under Clause 43.02, in addition to those specified elsewhere in the scheme and must accompany an application, as appropriate, to the satisfaction of the responsible authority:

- A site analysis and urban design context report which demonstrates how the proposal achieves the Design objectives, Built form outcomes and Built form requirements in Clause 2.5 of this schedule.
- A plan showing shadows and Additional shadows, as defined by Clause 2.1 of this schedule, cast by the proposal.

- Where a Floor Area Ratio applies to land, a development summary specifying floor-by-floor area, summary of proposed use and total Floor Area Ratio.
- For buildings and works higher than 40 metres, a wind assessment prepared by a suitably qualified person that:
 - Models the wind effects of the proposed development and its surrounding buildings.
 - Demonstrates the proposed development will not cause unsafe wind conditions.
 - Explains the effect of the proposed development on the wind conditions in publicly accessible areas.
 - Shows the development will allow for comfortable sitting in any public open space, standing in any pedestrian entrance and walking in any pedestrian walkway.

6.0 Decision guidelines

The following decision guidelines apply to an application for a permit under Clause 43.02, in addition to those specified in Clause 43.02 and elsewhere in the scheme which must be considered, as appropriate, by the responsible authority:

- The extent to which the development addresses Built Form Outcomes and Built Form Requirements in this schedule.
- Whether the development achieves high architectural quality (including but not limited to contextual responsiveness, building siting, scale, massing, articulation and materials).
- Whether the development supports a high quality public realm, high levels of pedestrian amenity and supports active transport.
- Whether buildings on larger sites are designed and sited to relate and contribute positively to their context and their historic urban grain such as through variation of building height and building form.
- Whether new development is sited and designed with an active frontage to the street.
- Whether new development supports local wind conditions that maintain a safe and pleasant pedestrian environment on footpaths and other public spaces for walking, sitting or standing.
- Whether the proposed development is an acceptable outcome having regard to the risk of flooding.
- For an application to exceed the preferred building height shown in the Plan 2 to this schedule:
 - Whether the built form outcome that results from the proposed variation satisfies the relevant Built Form Outcomes and Built Form Requirements specified in Clause 2.5 of this schedule.
- For an application to exceed the street wall height shown in Plan 3 of this schedule:
 - Whether the built form outcome that results from the proposed variation satisfies the relevant Built Form Outcomes and Built Form Requirements specified in Clause 2.5 of this schedule; and
 - Whether the street wall at ground floor level is designed to allow floor to floor ceiling heights suitable to accommodate commercial activity.
- For an application to reduce the preferred minimum setback requirements specified in this schedule:
 - Whether the built form outcome that results from the proposed variation satisfies the relevant Built Form Outcomes and Built Form Requirements specified in Clause 2.5 of this schedule.

C219port
XX/XX/20XX**SCHEDULE 40 TO CLAUSE 43.02 DESIGN AND DEVELOPMENT
OVERLAY**

Shown on the planning scheme map as **DDO40**.

SOUTH MELBOURNE ENTERPRISE PRECINCT WEST**1.0 Design objectives**

To ensure that development transitions from taller (8 storey) buildings behind City Road to medium rise (5-7 storey) buildings adjacent to established residential areas.

To ensure development in the South Melbourne Enterprise Precinct contains floorplates and design features that support a diverse range of employment activities, including creative industries and professional services.

To ensure development is well spaced and sited to provide high amenity, avoid visual bulk and provide equitable access to an outlook and good daylight, and anticipates and is resilient to the potential impacts of climate change and inundation.

To reference the industrial history of the precinct by supporting the adaptive reuse of existing buildings and encouraging the contemporary use of common industrial materials.

To ensure that development is of high architectural quality and contributes to the creation of a public realm that preserves sunlight to key parks and streets, is attractive, vibrant, safe, engaging and supports active transport, including walking and cycling.

2.0 Buildings and works**2.1 Definitions**

Additional shadows means any shadow cast beyond that which is cast by existing buildings or works, but does not a shadow cast by incidental elements such as canopies, kiosks, artworks, screens or trees.

Floor Area Ratio means the gross floor area above ground of all buildings on a site divided by the area of the site. For the purposes of this calculation:

- Gross floor area includes all enclosed areas, services, lifts, car stackers and covered balconies. It also includes any floor area on a level that projects 1.2 metres above ground level.
- The area of the site includes all contiguous titles in the same ownership that form part of the proposed development.

Heritage building means any building subject to a Heritage Overlay, graded as either Contributory or Significant or any building on the Victorian Heritage Register.

Key streets include City Road, Ballantyne Street, Market Street, York Street, Ferrars Street and Cecil Street.

Laneway means a road reserve with a width of 9 metres or less.

Narrow street means a road reserve of a public highway less than 12 metres wide.

Public realm means all streets and spaces open to the public but does not include laneways.

Southern footpath as the space extending 6 metres from the property line of buildings on the southern side of the street.

Street means a road reserve of a public highway more than 12 metres wide.

Street wall height means the height of the street wall measured by the vertical distance between the footpath at the centre of the frontage and the highest point of the building at the street edge or in the case of a heritage building if it is setback from the street from the centre of the building frontage to the highest point of the building, with the exception of non-habitable architectural features such as balustrades or eaves not more than 3.0 metres in height and building services setback at least 3.0 metres behind the street wall.

Street wall means the facade of a building at the street boundary, or, if the existing heritage building is set back from the street boundary, the front of the existing building within 0.3 metres of an existing or proposed street, laneway or public open space.

Upper level means development above the street wall.

2.2 Buildings and works for which no permit is required

A permit is not required to:

- construct a building or construct or carry out works at ground level to provide access for persons with disabilities that comply with all legislative requirements;
- construct a building or construct or carry out works associated with:
 - The installation of an automatic teller machine.
 - An alteration to an existing building façade provided:
 - The alteration does not include the installation of an external roller shutter.
 - At least 80 per cent of the building facade at ground floor level is maintained as an entry or window with clear glazing.
 - An awning that projects over a road if it is authorised by the relevant public land manager.

2.3 Exemption from notice and review

An application to subdivide land or construct a building or construct or carry out works is exempt from the notice requirements of section 52(1)(a), (b) and (d), the decision requirements of section 64(1), (2) and (3) and the review rights of section 82(1) of the Act. This exemption does not apply to land within 30 metres of land (not a road) which is in a residential zone, land used for a hospital or an education centre or land in a Public Acquisition Overlay to be acquired for a hospital or an education centre.

2.4 General Requirements

A permit to construct a building or construct or carry out works cannot be granted to vary a requirement in clause 2.5 expressed with the term 'must'.

A permit to construct a building or construct or carry out works may be granted to vary a built form requirement in clause 2.5 expressed with the term 'should'.

Regardless of whether the built form requirements are met, an application for buildings and works must meet the relevant Design Objectives in clause 1.0 and Built Form Outcomes in clause 2.5.

2.5 Design Requirements

Floor Area Ratio

An application to construct a building or construct or carry out works must not exceed the Floor Area Ratio specified in Plan 1 of this schedule.

Where the site includes contiguous titles in the same ownership, a section 173 agreement must be entered into and registered on each title which records the amount of Floor Area Ratio developed across the entire site, and the amount (if any) of remaining Floor Area Ratio able to be developed on each title should it be individually redeveloped in future. The owner of the land must pay the reasonable costs of the preparation, execution and registration of the section 173 agreement.

A permit cannot be granted or amended to vary this requirement, unless in the case of an amendment to a permit, the amendment does not increase the extent of non-compliance.

Plan 1: Floor Area Ratio to Schedule 40 of Clause 43.02



Interface with residential properties in NRZ or GRZ

Built Form Outcome

Development that:

- Protects the amenity of existing residential uses in terms of visual bulk, overshadowing of private open space, overlooking and vehicle access.

Building height

Built Form Outcomes

Building heights that:

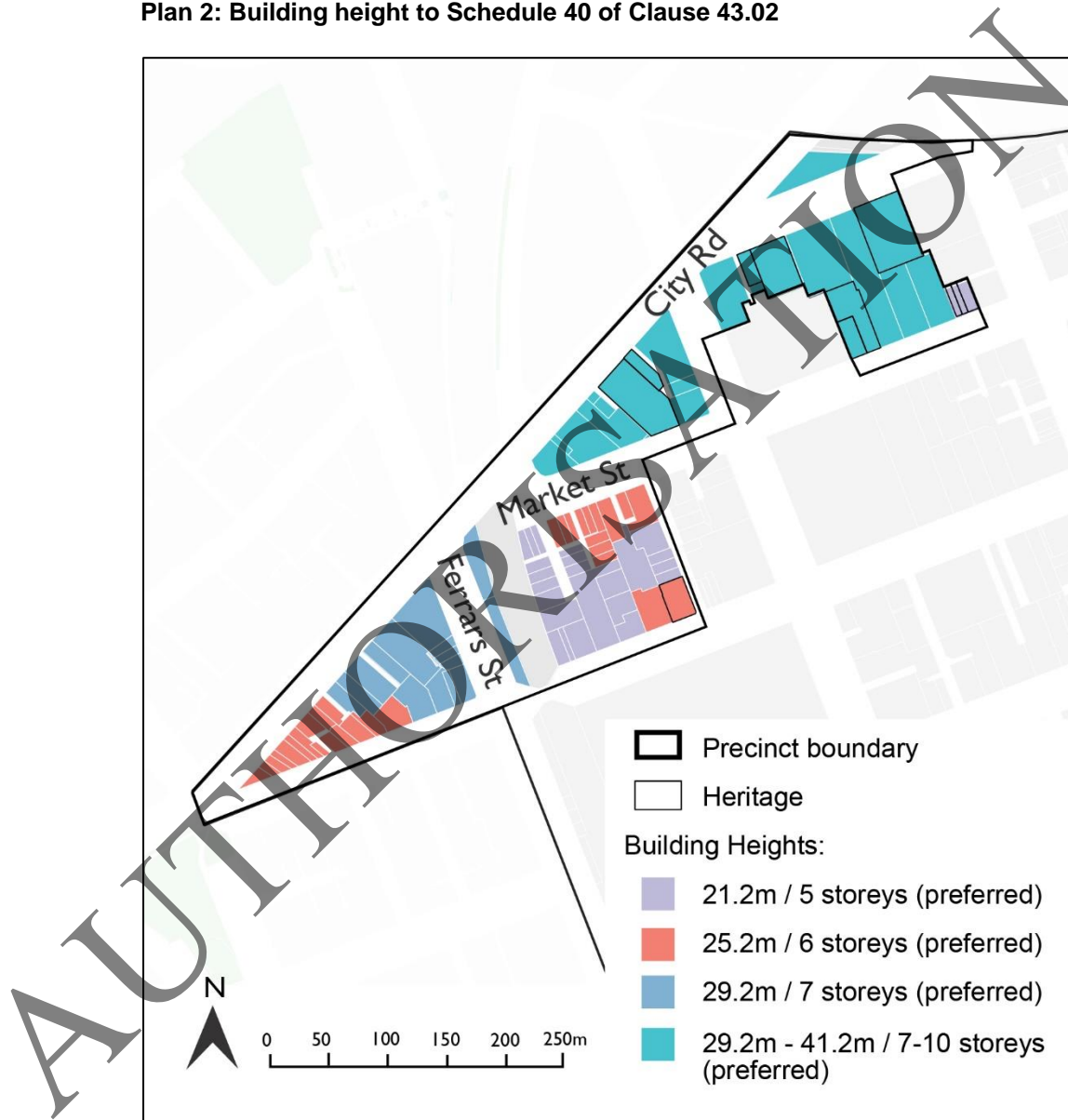
- On larger sites, are broken up into a series of smaller building forms where appropriate and with variable heights.
- Provide an appropriate scale of development and transition from taller (8 storey) buildings behind City Road to medium rise (5-7 storey) buildings adjacent to established residential areas south of York Street (west of Ferrars Street)

- Limits impacts on the amenity of York Street and potential future open space on the South Melbourne Market site as a result of overshadowing.
- Taller buildings are massed to enable greater retention of the front elements of industrial heritage buildings.

Built Form Requirements

Buildings and works should not exceed the preferred maximum building height requirement specified in Plan 2 to this schedule.

Plan 2: Building height to Schedule 40 of Clause 43.02



Adaptable Buildings

Built Form Outcome

Buildings and works that:

- Provide for the future conversion of those parts of the building accommodating non-employment uses to employment uses including the ability to adapt car parking to other uses over time.

- Minimises the impact of car parking on the public realm.

Built Form Requirements

- The building elements in Table 1 should incorporate the adaptability opportunities identified in the table.

Table 1: Adaptable buildings

Building height	Adaptability opportunity
Lower levels up to the height of the street wall	At least 4.0m floor to floor height at ground level. At least 3.2m floor to floor height for other lower levels.
Car parking areas	In areas not in a basement: <ul style="list-style-type: none"> ▪ Level floors. ▪ A floor-to-floor height at least 3.8m. ▪ Mechanical parking systems to reduce the area required for car parking.
Dwelling layout	The ability for one and two-bedroom dwellings to be combined or adapted into three or more bedroom dwellings.
Internal layout	Minimal load bearing walls to maximise flexibility for retail or commercial refits.

Street wall and setbacks

Built Form Outcomes

Street wall heights and setbacks that:

- Respond to street widths, interfaces around the site and enable the protection of sunlight to footpaths.
- Avoid visual bulk.
- Support the provision of high amenity, equitable access to an outlook and good daylight.
- Street wall heights and setbacks respect adjacent heritage buildings.
- On the north side of York Street, provides greater setbacks to protect sunlight to the southern footpath and enable potential future open space on the South Melbourne Market site.

Built Form Requirements

Buildings and works should not exceed the preferred maximum street wall height requirement specified in Plan 3 to this schedule.

Heritage buildings should maintain the existing street wall height.

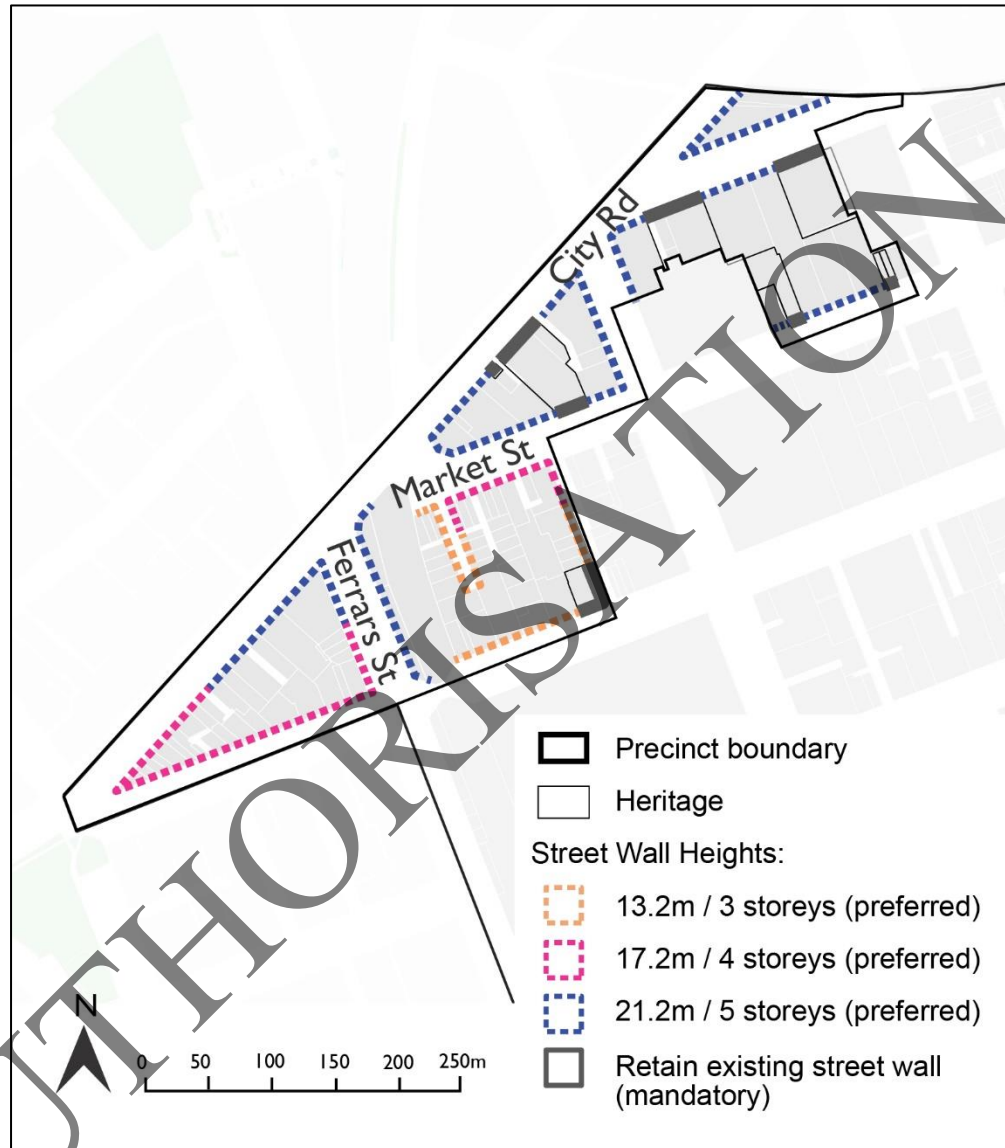
For corner sites where two different street wall heights are nominated, development should 'turn the corner' and continue the taller street wall height along the side street, with a transition to the lower street wall height along the side street towards the rear interface.

Projections such as building services and architectural features (other than shading devices, mouldings etc.), balconies, terraces and balustrades should not protrude into a setback.

Infill development adjoining a heritage building should match the street wall height for a distance equivalent to a typical structural or façade bay (approximately 6 metres in the case of a typical nineteenth century shop residence).

Where development is separated from a heritage building by a laneway, the street wall height of the proposed development on adjacent land should not be greater than two storeys taller than the neighbouring heritage building, for a minimum distance of that equivalent to a typical structural or façade bay.

Plan 3: Street wall height to Schedule 40 of Clause 43.02



Upper levels

Built Form Outcomes

Upper levels that:

- Respond to street widths, interfaces around the site and enable the protection of sunlight to footpaths.
- Provide equitable development opportunities and adequate internal amenity outcomes in terms of daylight, privacy and outlook.
- Reinforce the fine grain, vertical rhythm and visual interest of streetscapes.
- For heritage buildings, are set back to ensure the visual primary and legibility of the street wall and the retention of heritage building features such as chimneys and roof forms.

Built Form Requirements

To retain the visual prominence of the return facades of heritage buildings, upper level development must be set back a minimum of 6 metres from 30 metre wide streets.

A permit cannot be granted to vary this requirement.

Upper level development for all other areas should be set back between 3 metres and 5 metres from the street wall.

Upper level development should provide appropriate building separation and be setback from side and rear boundaries to provide internal spaces with adequate levels of daylight, privacy and a primary outlook to the street, laneway or open spaces.

Development should match the upper level setback of an adjoining heritage building for a minimum of 6 metres in length.

Overshadowing

Built Form Outcomes

Development that:

- Preserves sunlight to the public realm and street network and provides for high amenity, especially along 30 metre wide streets.
- On the north side of York Street, provides greater setbacks to protect sunlight to the southern footpath and enable potential future open space on the South Melbourne Market site.

Built Form Requirements

A permit must not be granted to construct a building or construct or carry out works that would overshadow or cast additional shadows over the southern footpath of the following streets between 10am and 2pm on 22nd September:

- Market Street
- York Street from City Road to the western boundary of South Melbourne Market.

A permit cannot be granted to vary this requirement.

A permit must not be granted to construct a building or construct or carry out works that would overshadow the following streets between 10am and 2pm on 22nd June:

- York Street from Cecil Street to the western boundary of South Melbourne Market.

A permit cannot be granted to vary this requirement.

Active street frontages

Built Form Outcome

Development that:

- Provides high-quality, active frontages to key streets and laneways.
- Provides car parking areas, structures and accessways that do not detract from the public realm.

Built Form Requirements

Provide for natural surveillance and a visual connection into the building through transparent windows and balconies.

Avoid blank walls, large areas of reflective surfaces, high fences, service areas, to key streets and laneways.

Car parking should be sleeved with active uses so that it is not visible from the public realm or adjoining sites.

Flood resilience

Built Form Outcome

Development that provides carefully considered design solutions for buildings in flood affected areas, particularly in the transition from the building to the public realm to ensure that building entries and frontages are accessible and active.

Built Form Requirements

Ensure that the internal area of buildings including any basements proposed on land susceptible to flooding are designed to be protected from inundation.

Ensure that safe access and egress is provided to that part of the land where there is no anticipated overland flow from floodwaters.

Building separation

Built Form Outcomes

Development that:

- Ensures good levels of daylight and sunlight enter buildings.
- Provides adequate cross ventilation.
- Provides outlook from within buildings.
- Manages privacy between neighbouring buildings, maintains equitable development between sites and avoids the need for visual screening.

Built Form Requirements

Where development shares a common boundary, a new building should be setback by the distance specified in Table 2.

Where two sites share a rear boundary, development abutting the boundary should be no higher than 3 storeys.

For sites with a side boundary to narrow laneways of equal to or less than 3 metres in width, development abutting the laneway should be no higher than:

- 22 metres; or
- the maximum building height specified in this schedule, whichever is lesser, provided that:
 - the development does not result in unreasonable amenity impacts on the public realm or the rear of the subject property; and
 - the façade to the laneway is of a slender form that creates fast moving shadows.

For non-residential development, these requirements in Table 2 may be varied for sites with a depth of 16 metres or less that share a rear boundary where appropriate amenity outcomes can be achieved through a single outlook to the street.

Table 2: Building separation distances

Building height	Living room, balcony and non-residential outlook - minimum site boundary separation	Bedroom outlook - minimum site boundary separation	Separation between multiple buildings on an individual site
Up to 22 metres	4.5 metres	3 metres	9 metres

Above 22 metres and up to 27 metres	6 metres	3 metres	12 metres
Above 27 metres	9 metres	4.5 metres	18 metres

Light wells

Built Form Outcomes

Development that:

- Ensures good levels of daylight and sunlight enter buildings.
- Provides adequate cross ventilation.

Built Form Requirements

- Light wells, if provided, should:
 - be consolidated into a larger courtyard space;
 - meet the dimensions for minimum width and overall area specified in Table 3 from the first level of residential use;
 - provide daylight access to bedrooms only;
 - be painted in a light reflective colour; and
 - be provided with ground level access via a door.
- The light well dimensions in Table 3 may be varied for buildings containing multiple levels of non-residential uses.

Table 3: Light well dimensions

Building height	Light well overall area	Light well minimum width
Up to 22 metres	18 square metres	3 metres
Above 22 metres and up to 27 metres	36 square metres	4.5 metres
Above 27 metres	54 metres	6 metres

Wind effects on the public realm

Built Form Outcome

Local wind conditions that:

- Maintain a safe and pleasant pedestrian environment on footpaths and other public spaces for walking, sitting or standing.

Built Form Requirement

Buildings and works higher than 40 metres:

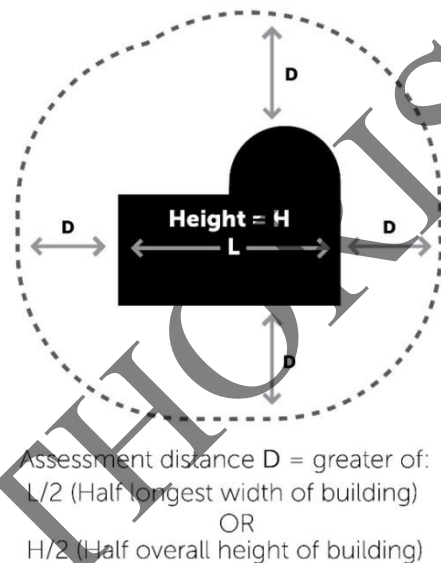
- Must not cause unsafe wind conditions as specified in Table 4 in publicly accessible areas within the assessment distance from all façades. A permit cannot be granted to vary this requirement.
- Should achieve comfortable wind conditions as specified in Table 4 in publicly accessible areas within the assessment distance from all façades.

The assessment distance is shown in the figure below and is the greater of:

- Half the longest width of the building.
- Half the total height of the building.

Table 4: Wind effects on the public realm

Wind condition	Specification
Comfortable wind conditions	<p>The Hourly mean wind speed from all wind directions combined with a probability of exceedance of 20 per cent, is less than or equal to:</p> <ul style="list-style-type: none"> ▪ 3m/second for sitting areas. ▪ 4m/second for standing areas. ▪ 5m/second for walking areas. <p>Hourly mean wind speed is the maximum of:</p> <ul style="list-style-type: none"> ▪ The hourly mean wind speed. ▪ The gust equivalent mean speed (3 second gust wind speed divided by 1.85).
Unsafe wind conditions	<p>The hourly maximum 3 second gust from any wind direction (considering at least 16 wind directions) with a corresponding probability of exceedance percentage greater than 20m per second.</p>



3.0 Subdivision

None specified

4.0 Signs

None specified.

5.0 Application requirements

The following application requirements apply to an application for a permit under Clause 43.02, in addition to those specified elsewhere in the scheme and must accompany an application, as appropriate, to the satisfaction of the responsible authority:

- A site analysis and urban design context report which demonstrates how the proposal achieves the Design objectives, Built Form Outcomes, and Built Form Requirements in clause 2.5 in this schedule.

- A plan showing shadows and Additional shadows, as defined by Clause 2.1 of this schedule, cast by the proposal.
- Where a Floor Area Ratio applies to land, a development summary specifying floor-by-floor area, summary of proposed use and total Floor Area Ratio.
- For buildings and works higher than 40 metres, a wind assessment prepared by a suitably qualified person that:
 - Models the wind effects of the proposed development and its surrounding buildings.
 - Demonstrates the proposed development will not cause unsafe wind conditions.
 - Explains the effect of the proposed development on the wind conditions in publicly accessible areas.
 - Shows the development will allow for comfortable sitting in any public open space, standing in any pedestrian entrance and walking in any pedestrian walkway.

6.0 Decision guidelines

The following decision guidelines apply to an application for a permit under Clause 43.02, in addition to those specified in Clause 43.02 and elsewhere in the scheme which must be considered, as appropriate, by the responsible authority:

- The extent to which the development addresses the Built Form Outcomes and Built Form Requirements in this schedule.
- Whether the development achieves high architectural quality (including but not limited to contextual responsiveness, building siting, scale, massing, articulation and materials).
- Whether the development supports a high quality public realm, high levels of pedestrian amenity and supports active transport.
- Whether buildings on larger sites are designed and sited to relate and contribute positively to their context and their historic urban grain such as through variation of building height and building form.
- Whether new development is sited and designed with an active frontage to the street.
- Whether new development supports local wind conditions that maintain a safe and pleasant pedestrian environment on footpaths and other public spaces for walking, sitting or standing.
- Whether the proposed development is an acceptable outcome having regard to the risk of flooding.
- For an application to exceed the preferred building height shown in the Plan 2 to this schedule:
 - Whether the built form outcome that results from the proposed variation satisfies the relevant Built Form Outcomes and Built Form Requirements specified in Clause 2.5 of this schedule.
- For an application to exceed the street wall height shown in Plan 3 of this schedule:
 - Whether the built form outcome that results from the proposed variation satisfies the relevant Built Form Outcomes and Built Form Requirements specified in Clause 2.5 of this schedule; and
 - Whether the street wall at ground floor level is designed to allow floor to floor ceiling heights suitable to accommodate commercial activity.
- For an application to reduce the preferred minimum setback requirements specified in this schedule:

- Whether the built form outcome that results from the proposed variation satisfies the relevant Built Form Outcomes and Built Form Requirements specified in Clause 2.5 of this schedule.

AUTHORISATION

11.03

31/07/2018
VC148

PLANNING FOR PLACES

AUTHORISATION

11.03-1S03/02/2022
VC199**Activity centres****Objective**

To encourage the concentration of major retail, residential, commercial, administrative, entertainment and cultural developments into activity centres that are highly accessible to the community.

Strategies

Build up activity centres as a focus for high-quality development, activity and living by developing a network of activity centres that:

- Comprises a range of centres that differ in size and function.
- Is a focus for business, shopping, working, leisure and community facilities.
- Provides different types of housing, including forms of higher density housing.
- Is connected by transport.
- Maximises choices in services, employment and social interaction.

Support the role and function of each centre in the context of its classification, the policies for housing intensification, and development of the public transport network.

Undertake strategic planning for the use and development of land in and around activity centres.

Give clear direction on preferred locations for investment.

Encourage a diversity of housing types at higher densities in and around activity centres.

Reduce the number of private motorised trips by concentrating activities that generate high numbers of (non-freight) trips in highly accessible activity centres.

Improve access by walking, cycling and public transport to services and facilities.

Support the continued growth and diversification of activity centres to give communities access to a wide range of goods and services, provide local employment and support local economies.

Encourage economic activity and business synergies.

Improve the social, economic and environmental performance and amenity of activity centres.

Policy documents

Consider as relevant:

- *Urban Design Guidelines for Victoria* (Department of Environment, Land, Water and Planning, 2017)
- *Apartment Design Guidelines for Victoria* (Department of Environment, Land, Water and Planning, 2021)
- *Precinct Structure Planning Guidelines* (Victorian Planning Authority, 2021)

11.03-1R31/07/2018
VC148**Activity centres - Metropolitan Melbourne****Strategies**

Support the development and growth of Metropolitan Activity Centres by ensuring they:

- Are able to accommodate significant growth for a broad range of land uses.
- Are supported with appropriate infrastructure.
- Are hubs for public transport services.
- Offer good connectivity for a regional catchment.
- Provide high levels of amenity.

Locate significant new education, justice, community, administrative and health facilities that attract users from large geographic areas in or on the edge of Metropolitan Activity Centres or Major Activity Centres with good public transport.

Locate new small scale education, health and community facilities that meet local needs in or around Neighbourhood Activity Centres.

Ensure Neighbourhood Activity Centres are located within convenient walking distance in the design of new subdivisions.

AUTHORISATION

11.03-1L-01 Activity centres

07/08/2023 --/--/----
 G2+3port Proposed C219port

**Objective**

To maintain and strengthen a network of distinct, diverse, and viable activity centres that facilitate appropriate housing and economic growth.

Strategies

Ensure land use supports the strategic role and function of the activity centre.

Support land uses that contribute to the provision of goods and services for the local community in Major Activity Centres and Neighbourhood Activity Centres.

Intensify retail development within existing retail strips (Commercial 1 Zone), subject to heritage and character considerations.

Support commercial land uses beyond retail strips (Commercial 1 Zone) provided the use would:

- Improve the integration of the activity centre with the surrounding area.
- Respond to known retail gaps or shortfalls.
- Provide for additional or improved public space.

Support development within activity centres that positively contributes to the built form character of the centre whilst conserving heritage buildings, and streetscapes, and the distinctive and valued character of the traditional retail strips.

Encourage greater consistency in land use and built form intensity at the interface of activity centres and surrounding residential areas, including a transition in building scale to adjoining low-rise development.

Create continuous active frontages within core retail areas of Major Activity Centres and Neighbourhood Activity Centres with non-core retail uses located above or behind ground floor frontages.

Support residential development within Major Activity Centres and on key sites and precincts, as defined by Structure Plans.

Ensure residential development within activity centres does not diminish future opportunities for retail expansion, particularly within core retail areas.

Encourage retail development (including reformatting of retail space) that integrates with the established retail strip in response to a demonstrated increase in consumer demand.

Ensure that the heritage scale and form of buildings in the Bridport Street / Victoria Avenue, Albert Park; Armstrong Street, Middle Park; and Glen Eira Road, Ripponlea Neighbourhood Activity Centres, is respected.

Encourage residential use and development above or behind ground floor premises in Major Activity Centres and the Ormond Road / Glen Huntly Road Neighbourhood Activity Centre.

Limit residential development in the Neighbourhood Activity Centres of Tennyson Street, Elwood and Centre Avenue, Port Melbourne, being locations that do not offer direct access to the Principal Public Transport Network.

Policy guideline

Consider as relevant:

- Supporting new retail floor space exceeding 2000 square metres (Gross Leasable Floor Area) where there would be no adverse economic impacts.

Objective

To support in-centre cultural tourism that reflects each individual centre whilst minimising adverse amenity impacts.

Strategies

Direct larger scale regional entertainment uses to the Bay Street Major Activity Centre, Port Melbourne and the St Kilda Major Activity Centre.

Support smaller scale local entertainment uses in Major Activity Centres and Neighbourhood Activity Centres provided there are no adverse impacts on residential amenity.

Allow bar uses in association with existing ground floor restaurants and cafes.

Support entertainment uses located outside designated activity centres, provided:

- There are no adverse amenity impacts on adjoining properties, including noise, hours or operation, traffic and car parking.
- There is convenient access to public transport or other transport means (for example taxi ranks).

Policy documents

Consider as relevant:

- *Bay Street Activity Centre Structure Plan Parts 1 and 2* (City of Port Phillip, May 2014)
- *Carlisle Street Activity Centre Structure Plan* (City of Port Phillip, 2009)
- *Carlisle Street Activity Centre Urban Design Framework* (City of Port Phillip, 2009)
- *City of Port Phillip Activity Centres Strategy* (City of Port Phillip, 2006)
- *City of Port Phillip Activity Centres Strategy Implementation Plan* (City of Port Phillip, 2007)
- *Ormond Road Urban Design Guidelines* (City of Port Phillip, 2007)
- *South Melbourne ~~Central~~ Structure Plan* (City of Port Phillip, ~~August 2007~~ July 2024)

11.03-1L-02 Bay Street Major Activity Centre

14/04/2023
C203port

Policy application

This applies to the Bay Street Major Activity Centre, and environs, as shown on the map to this clause.

General

Objective

To ensure the continued development of the Bay Street Major Activity Centre as a multi-functional and sustainable bay-side activity centre, a local civic and community hub with a strong sense of identity and community, and a wide range of goods and services, for locals and visitors.

Strategies

Reinforce the distinct and contrasting urban character of the different precincts of the Bay Street Major Activity Centre as shown on the map to this clause.

Encourage complementary land use clusters around key anchors including:

- The foreshore (for visitation).
- The supermarket (for grocery needs).
- The Library and Town Hall (for complementary retail, business and personal services).

Encourage use and development that leverage off the Port Melbourne waterfront as a visitor destination, particularly hospitality uses that support visitation to the activity centre, including after hours and at weekends.

Create a cultural, tourism, leisure and retail gateway and pedestrian environment in the Bay Street Southern Gateway (Precinct 4 on the map to this clause).

Support the provision of school facilities for primary and secondary school aged children and early childhood education and child-care services.

Support the redevelopment or reuse of larger sites south of Bridge Street that include retail anchors at ground level.

Facilitate the renewal of under-utilised sites and precincts across the centre including the desired future outcomes for the key strategic sites listed in Table 1 and shown on the map to this clause.

Table 1 – Strategic renewal sites

Key strategic site	Desired future outcome
<p>A</p> <p>Corner Bay Street and Liardet Street (Coles supermarket)</p>	<ul style="list-style-type: none"> Residential, community and / or commercial uses above the existing retail anchor at ground level. Existing car parking is to be retained with additional car parking for any new land use.
<p>B</p> <p>7-33 Bay Street (Mitchell Crescent Public Housing Estate)</p>	<ul style="list-style-type: none"> Redevelopment should include social housing on upper levels and to the rear of the site, with no net loss of social housing. Potential for a future retail anchor with active land uses along the Bay Street frontage.
<p>C</p> <p>160 and 162 Bay Street (church and manse)</p>	<ul style="list-style-type: none"> Office or residential uses to the rear of the existing place of worship and manse.
<p>D</p> <p>420 Bay Street (service station site)</p>	<ul style="list-style-type: none"> Residential development (dwellings).
<p>E</p> <p>86 Crockford Street (service station site)</p>	<ul style="list-style-type: none"> Residential development (dwellings) and office based commercial activity.

Housing

Objective

To concentrate new housing growth opportunities in identified strategic precincts within the activity centre.

Strategies

Concentrate housing growth (with some at higher densities) in the following listed areas:

- The mixed use area (south of Graham Street).
- Land on the north-western side of Crockford Street.
- The under-utilised residential sites on the south-eastern side of Crockford Street and Bay Street, north of Spring Street.

Facilitate remaining opportunities for housing growth within the activity centre that responds to the heritage, low-rise character and amenity of the surrounding established residential areas.

Support development designed to either:

- Protect and reinforce the existing character in areas with an identified highly consistent neighbourhood character.
- Respond to the preferred neighbourhood character in areas identified as having a mixed character.

Facilitate a new contemporary higher-rise character in the mixed use area south of Graham Street while respecting remaining significant heritage places.

Protect residential amenity and character at the interface between the mixed use 'growth' area and established 'sensitive' residential areas south of Graham Street (generally between Dow Street and Stokes Street) through a transition down in building scale as indicated on the *Built Form Framework Plan* in the *Bay Street Activity Centre Structure Plan* (City of Port Phillip, 2014).

Encourage medium scale infill residential development on the southern side of Crockford Street.

Policy guideline

Consider as relevant:

- Designing development in residential areas to respond to neighbourhood character as identified in the *Bay Street Activity Centre Structure Plan* (City of Port Phillip, 2014) and the neighbourhood character statements for the Bay Street Major Activity Environs in the *Port Phillip Design Manual* (City of Port Phillip, 2000).

Urban structure and built form

Objective

To reinforce the identity of Bay Street including its 'village' feel, urban structure and unique built form elements.

Strategies

Reinforce the low scale, heritage character of the traditional retail strip north of Graham Street.

Encourage street wall heights consistent with the prevailing streetscape context.

Maintain public view lines as identified on the *Built Form Framework Plan* in the *Bay Street Activity Centre Structure Plan* (City of Port Phillip, 2014), by limiting the height of development fronting Bay, Heath and Lalor Streets.

Create well articulated development (through variations in form and materials, openings and the inclusion of vertical design elements) on larger or consolidated sites (with a frontage over 10 metres).

Encourage development along Bay Street, north of Graham Street, to respect the scale of heritage buildings and continue the consistent street wall parapet height.

Design buildings to limit overshadowing and preserve core hours of sunlight access to public paths and foreshore areas.

Policy guideline

Consider as relevant:

- Designing development so that it avoids diminishing sunlight access on 21 June (winter solstice) to:
 - The eastern footpath of Bay Street – between 10.00am and 3.00pm.
 - The south-western side of Rouse Street – between 11.00am and 2.00pm.
 - The foreshore area including bicycle and pedestrian paths.

Sustainable transport

Objective

To facilitate an integrated and sustainable transport network that supports Bay Street's primary role as a shopping street.

Strategies

Enhance Bay Street and Beach Street as the primary spines of pedestrian activity.

Support improved walking and cycling links within Port Melbourne, particularly between Station Pier, Bay Street and the foreshore.

Reduce the impact of traffic along Bay Street to support a primary shopping street and pedestrian priority area.

Public realm

Objective

To strengthen the identity, connectivity and cohesion of the activity centre through enhancements to the public realm.

Strategies

Reinforce the spatial definition, safety, convenience, and pedestrian accessibility and activation of streets, lanes and public spaces within the activity centre through development that:

- Provides cantilevered verandas over footpaths along the length of Bay Street and along 30 metre wide streets in the mixed use area.
- Creates a sense of street enclosure.
- Creates well-articulated, attractive and detailed facades on all visible elevations, including exposed boundary walls.
- Limits posted verandas or other fixed structures at ground level unless strongly respectful of heritage conservation objectives.
- Minimises vehicle crossovers along Bay Street or Beach Street and along 'priority pedestrian' routes as identified on the *Pedestrian Network Framework Plan* in the *Bay Street Activity Centre Structure Plan* (City of Port Phillip, 2014).
- Encourages the provision and extension of canopies to offer weather protection and preserve footpath space for pedestrians throughout the retail core.
- Ensures that the floor levels of commercial premises development along Bay Street are level with the street.

Activate existing laneways through active land use edges and use the footpaths for street trading activities in main streets south of Graham Street.

Provide 'Active Retail Edges':

- Through the design of ground level premises:
 - Along both sides of Bay Street south of Bridge Street.
 - On the north-western side of Bay Street between Bridge and Raglan Streets.
- In locations identified on the *Pedestrian Network Framework Plan* in the *Bay Street Activity Centre Structure Plan* (City of Port Phillip, 2014) including:
 - At the north-west corner of Bay Street and Pool Street.

- At the corners of Bay Street and Beach Street.
- Along both sides of Bay Street between Beach Street and Rouse Street.

Enable visual connections between building occupants and persons in the public realm through the creation of other 'Active Edges' along all other identified priority pedestrian spines or streets.

Crockford Street Precinct (DDO25)

Objective

To facilitate the renewal of the Crockford Street precinct with commercial and residential land use.

Strategies

Ensure new use provides a sensitive interface, which is of a scale and nature that will not significantly impact the amenity of adjacent residential areas including development stepped down to the adjoining low scale residential context.

Support a vertical mix of uses to achieve active commercial frontages at ground level through office-based commercial activity with and residential uses at upper levels.

Lalor and Heath Street Precinct

Objective

To create 'infill' residential development along the eastern side of Heath and Lalor Streets (Precinct 2 on the map to this clause).

Strategies

Maintain the residential character and amenity of Heath Street and limit commercial encroachment.

Support development that reinstates a residential edge in Heath, Lalor and Garton Streets with development at the rear of properties on Bay Street creating improved interfaces with existing residential properties.

Avoid commercial or industrial use development forms that may detrimentally impact the amenity of established residential areas.

Ensure that development on the Bay Street retail strip:

- Maintains the visual integrity of the street wall parapet along Bay Street.
- Is recessed from the Bay Street frontage so that it is not visible from the opposite side (property line) of Bay Street.
- Achieves a transition down in height to the established residential area opposite.

Reflect the fine grain building character of the precinct through vertical design elements in development.

Limit vehicle access and parking entrances in ground floor level façades.

Policy guidelines

Consider as relevant:

- Maintaining a 7 metre (equivalent to a two storey street wall) street-wall parapet to Heath and Lalor Streets.
- Maintaining a maximum overall building height of 14 metres (four storey four storey equivalent).
- Setting back upper levels from the Heath / Lalor Street front property boundary as follows:

- Any third level by a minimum of 3 metres.
- Any fourth level so that it is not visible from the opposite side of Heath / Lalor Streets, or from Bay Street when viewed from the opposite side of the street at standing eye level (1.6 metres).
- A building form for development on corner sites with a principal frontage to Bridge, Spring, or Raglan streets that is:
 - Setback behind the principal frontage so it is not visible when viewed from the opposite street frontage at standing eye level (1.6 metres) above the footpath level.
 - A maximum overall height of two storeys / 7 metres.
 - Designed to address all street frontages.

Town Hall Business Precinct

Objective

To support use and development of a broad range of commercial uses that respect existing environs in the Town Hall Business Precinct (Precinct 3 on the map to this clause)

Strategies

Support office-based business and residential uses north of Bridge Street.

Create a business services node along Bay Street, between Bridge Street and Raglan Street.

Encourage street level and upper level residential and office uses in existing residential properties between Bridge Street and Spring Street East.

Encourage development of Bay Street, north of Spring Street East, to act as a transitional precinct providing for a mix of office and residential uses.

Ensure development on sites outside the Heritage Overlay:

- Respects the scale of heritage properties opposite and further south along Bay Street.
- Achieves a transition in scale to a site located directly adjacent to a heritage place and to the established residential area to the rear.
- Creates a strong built form connection across underutilised sites.

Ensure that development to the rear of the Bay Street retail strip:

- Maintains the visual integrity of the street wall parapet along Bay Street.
- Achieves a transition down in height to the established residential area opposite.

Policy guidelines

Consider as relevant:

- A street wall height of 2 storeys in Bay Street and 3 storeys in Lyons Street, with a maximum overall height of 4 storeys.
- An 8 metre minimum parapet height with a 9.5 metre maximum street wall height along Bay Street.
- Limiting development at the rear of 'significant' and 'contributory' heritage places that is visible within a view line taken from the opposite side of Bay Street (at 1.6 metres from street level).
- Designing development on sites outside the Heritage Overlay to have a height no more than one storey above the prevailing heritage streetscape of Bay Street.

Policy documents

Consider as relevant:

- *Bay Street Activity Centre Structure Plan - Parts 1 and 2* (City of Port Phillip, May 2014)
- *Port Phillip Design Manual – Chapter 8: Neighbourhood Character Statements – Bay Street Activity Centre Environs* (City of Port Phillip, 2000)
- *Sustainable Transport Policy and Parking Rates Report* (Ratio, March 2007)

AUTHORISATION

Bay Street Major Activity Centre study area plan



11.03-1L-03 Carlisle Street Major Activity Centre14/04/2023
C203port**Policy application**

This policy applies to all land in the Carlisle Street Major Activity Centre area, and environs, as shown on the map to this clause.

General**Objective**

To enhance the Carlisle Street Major Activity Centre as a focus for the local community, with a diverse mix of retail, commercial, civic and community services (west of Chapel Street) and leisure and living opportunities.

Strategies

Encourage significant new residential and mixed use development at increased densities within the activity centre to meet growth objectives, whilst ensuring that the heritage and neighbourhood character of established residential areas is retained.

Facilitate significant land use change and development on identified strategic sites and precincts in the Carlisle Street Activity Centre Structure Plan.

Discourage the under-development of strategic sites and precincts within the activity centre that are identified for substantial change.

Reinforce Carlisle Street Activity Centre's primary convenience retailing (daily and weekly goods and services), and personal and business services roles, and eclectic, bohemian and distinctly local character.

Facilitate the centre to retain its metropolitan role in the provision of specialist continental and kosher goods.

Increase night-time presence within the centre through a mix of activities, including residential, that contributes to the ongoing passive surveillance of streets and public spaces.

Ensure entertainment uses and restaurants and bars are of a smaller scale and limited concentration to provide for local needs whilst discouraging the centre's development as a regional entertainment destination.

Encourage uses that reinforce the civic and community services role of the centre, particularly west of Chapel Street.

Support and retain a range of community services and spaces (including smaller scale cafes and restaurants outside the retail core) that meet local community needs, including higher needs groups.

Ensure community spaces are provided within larger-scale mixed use developments.

Ensure development on sites included in a Heritage Overlay maintains the two storey scale along Carlisle Street, with recessed upper level development.

Incorporate culturally relevant public art in new development that contributes to place making in the activity centre.

Design development to respect the following elements:

- The predominant two storey heritage streetscape, human scale, and fine grain streetscape pattern of Carlisle Street.
- The zero frontage setbacks of buildings.
- The civic precinct west of Chapel Street, characterised by public buildings in a landscape setting including the Town Hall, library and state school.
- The established network of streets and laneways, to improve legibility, permeability and connections between activities.

Provide for incremental change, including well-designed medium density development, in remaining areas outside of the heritage overlay, surrounding the activity centre.

Facilitate land use change and new development within Alfred and Marlborough Streets that improves the interface of the activity centre with adjacent residential areas.

Ensure a pedestrian focussed public realm through the design of streets and adjoining development, and management of car parking.

Reinforce and support the role of the activity centre as a sustainable transport hub focused on Balaclava Station, with increased usage, improved integration with other sustainable transport modes (including trams) and as part of a safe, convenient and connected pedestrian environment.

Policy guidelines

Consider as relevant:

- Consolidating sites and supporting three storey residential development in Alfred Street, Nelson Street (eastern side) and Camden Street (south of Edward Street) to improve the interface with the activity centre.
- Encouraging the progressive development of large 'at grade' car parks (through their underground relocation) as key opportunities to consolidate growth within the activity centre.
- Designing development to be self-sufficient in on-site car parking and providing on-site car parking based on the 'empirical rate' specified below.

Use	Empirical rate
Supermarket	4 spaces per 100m ² net floor area
Restaurant	0.3 per seat
Office	3.5 spaces per 100m ² net floor area (unshared)
	3.0 spaces per 100m ² net floor area (shared)

- A reduction (or waiver) of car parking from the empirical rate, where conditions that would result in a lower demand for car parking can be demonstrated.
- Encouraging at least 10 per cent of new dwellings within the activity centre to be affordable (private and community) housing.

Precinct strategies

Precinct 1 - Carlisle Street Retail Precinct (DDO21)

Consolidate a legible, compact and walkable retail core between Chapel Street and the Rail Bridge.

Concentrate retail activity within the retail core through the retention and integrated renewal of the existing supermarket sites and adjacent at grade car parks.

Ensure larger-scale retail premises maintain and reinstate the fine grain retail frontages along Carlisle, Nelson and Camden Streets.

Ensure new use and development contributes to continuous retail activity at street level, with new retail floor space to enhance and integrate with the existing retail strip, and residential and/or commercial uses located above or behind retail premises.

Precinct 2 - St Kilda Road Commercial Precinct (DDO21)

Support commercial (office) as the primary use.

Encourage land uses that benefit from main road exposure.

Discourage shop uses, except restricted retail premises, unless the activity is secondary to another commercial use on the site.

Encourage a continuous active commercial edge along the St Kilda Road frontage.

Discourage bar, restaurant and nightclub uses that do not provide daytime activity from occupying street level premises where these may adversely impact on residential uses.

Precinct 3 - Civic and Community Precinct

Encourage ongoing community support services at the St Kilda Parish Mission site (corner Carlisle Street and Chapel Street).

Develop the St Kilda Library as a key cultural hub and learning centre.

Support the development of a contemporary 'Family and Children's Services Hub' at 171 Chapel Street.

Precinct 4 - William Street Industrial Precinct

Retain the William Street industrial precinct as a vibrant industrial / service industrial and specialist business cluster in the short term (1 – 5 years).

Support office uses only in association with an industrial, warehouse or specialist business activity.

Ensure new use and development contributes to the creation of new pedestrian links along Stuart and Charles Streets, to connect William Street to Balaclava Station.

Precinct 5 - Inkerman / Pakington Streets Mixed Use Precinct (DDO21)

Encourage the establishment of home-based businesses.

Discourage shop uses, except where ancillary to another business use on the site.

Facilitate the renewal of the Inkerman / Pakington Street precinct as a preferred location for housing growth within the activity centre.

Support the transition of this precinct to a mixed residential and commercial (office) area, to provide new housing and employment opportunities.

Discourage licensed premises (bars, restaurants and nightclubs).

Precinct 6 - Chapel Street Mixed Use and Residential Precinct (DDO21)

Encourage high-quality redevelopment of the Australia Post site (corner of Chapel Street and Brighton Road) that contributes to the site's role as a key 'entry' to the activity centre, with significant new housing with potential for commercial use or display-based retailing at ground level.

Support the transition of the Chapel Street precinct to predominantly residential use, with active commercial frontages to Chapel Street.

Limit shop uses south of Marlborough Street, except where ancillary to another business use on the site.

Discourage licensed premises (bars/taverns, restaurant and nightclubs).

Precinct 7 - Marlborough Street Interface Precinct (DDO21)

Ensure that redevelopment of the station car park (44-60 Marlborough Street) respects the heritage values and low-rise, fine-grain built form on the south-side of Marlborough Street.

Facilitate redevelopment of the station car park (44-60 Marlborough Street) for affordable (social) housing, with a range of dwelling types suitable for older persons, singles and family households, and a component of private housing addressing Marlborough Street.

Ensure any future use and development of the site at 4-20 Marlborough Street reinstates residential use (or entries) on the north side of the street.

Precinct 8 – Established Residential Areas

Ensure any new residential development in established residential areas proximate to the Carlisle Street Major Activity Centre is consistent with the level of change and the preferred neighbourhood character, as identified in the *Carlisle Street Activity Centre Structure Plan* (City of Port Phillip, 2009).

Ensure established residential areas retain their generally mixed architectural character and diverse housing stock, while heritage buildings and streetscapes are conserved and enhanced.

Ensure development within Balston Street, Carlisle Avenue, Marlborough Street, Rosamond Street, Nightingale Street and Bothwell Street maintains the highly consistent neighbourhood character.

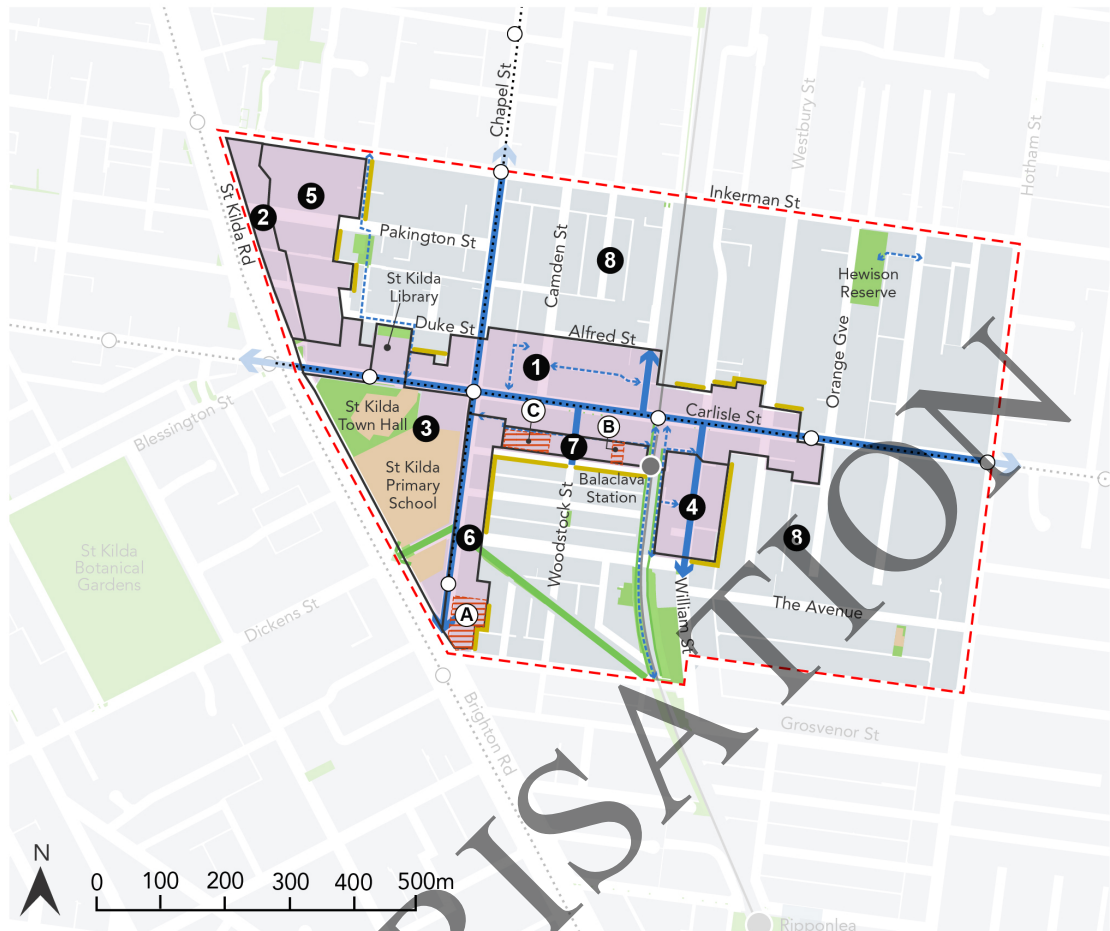
Policy documents

Consider as relevant:

- *Carlisle Street Activity Centre Structure Plan* (City of Port Phillip, 2009)
- *Carlisle Street Urban Design Framework* (City of Port Phillip and David Lock Associates, 2009)
- *Port Phillip Design Manual* (City of Port Phillip, 2000)
- *Sustainable Transport Policy and Parking Rates Report* (Ratio, 2007)

AUTHORISATION

Carlisle Street Major Activity Centre and study area



- Study area boundary
- Activity Centre
- Open space
- Principle Activity Centre streets
- Laneway connections
- Existing green link
- Residential interface
- Existing tram line / tram stop
- Existing train line / station

Precinct/Character Areas

- 1** Carlisle Street Retail Precinct
- 2** St Kilda Road Commercial Precinct
- 3** Civic & Community Precinct
- 4** William Industrial Street Precinct
- 5** Inkerman/Pakington Mixed Use Precinct
- 6** Chapel Street Mixed Use & Residential Precinct
- 7** Marlborough Street Interface Precinct
- 8** Established Residential Areas

Key Strategic Sites

- (A)** Australia Post site
- (B)** 44-60 Marlborough Street, Balclava
- (C)** 4-20 Marlborough Street, Balclava

11.03-1L-04 Local and neighbourhood activity centres

14/04/2023
C203port

Policy application

This policy applies to the local and neighbourhood activity centres, as shown on the map to this clause.

Objective

To reinforce the role and character of local and neighbourhood centres to provide goods, services and employment opportunities that serve the needs of the surrounding community.

Strategies

Centre Avenue Neighbourhood Activity Centre, Port Melbourne

Support the development of the centre by encouraging:

- A convenience goods and services role.
- Community facilities and services.

Build upon the centre's role as public transport terminus.

Maintain an attractive, streetscape with a high level of amenity for users of the centre.

Bridport Street / Victoria Avenue Neighbourhood Activity Centre, Albert Park

Support new licenced and entertainment premises provided there is on site car parking adequate to meet the needs of patrons and staff, and the use will complement the primary retail role of the centre.

Encourage community uses to establish in the centre.

Design development to respect the following elements:

- The predominant one and two storey scale of Victorian buildings, with higher development setback from the principle street to minimise its visibility.
- The prominence of landmark buildings including the 'Biltmore' (152 Bridport Street), the Windsor Hotel (107 Victoria Avenue), the Albert Park Hotel (85 Dundas Place) and the former ES&A Bank (95 Dundas Place).
- The consistent streetscape frontage widths to buildings.
- Views to Albert Park toward the Bay from Victoria Avenue.
- The island open space reserve (Broadway Tree Reserve) in Albert Park Village.

Support the reinstatement of original verandah forms to the commercial buildings on Bridport Street and Victoria Avenue.

Armstrong Street Neighbourhood Activity Centre, Middle Park

Support the daily and weekly retail goods and services role, and local entertainment role of the centre.

Design development to respect the following elements:

- The predominant 1 and 2 storey scale of Victorian buildings, with higher development setback from the principal street to minimise its visibility.
- The regular streetscape pattern created by consistent frontage widths to buildings.
- Views to Albert Park.

Ormond / Glen Huntly Road Neighbourhood Activity Centre, Elwood

Reinforce the primary daily / weekly retail goods and services role of Elwood Junction and Elwood Village.

Support new local entertainment premises in Elwood Junction and Elwood Village, provided there is on site car parking adequate to meet the needs of patrons and staff, and the use will complement the primary retail role of the centre.

Encourage moderate intensification of housing.

Tennyson Street Neighbourhood Activity Centre, Elwood

Support the daily and weekly retail goods and services role of the centre.

Design development to respect the following elements:

- The two storey scale of buildings.
- The zero frontage setbacks.
- The mature street trees.

Glen Eira Road Neighbourhood Activity Centre, Ripponlea

Support the daily and weekly retail goods and services role of the centre.

Support new local entertainment uses provided there is adequate on site car parking, and the use will complement the primary retail role of this centre.

Design development to respect the following elements:

- The consistent two storey scale of the centre, architectural style and streetscape pattern of Federation and Inter-war buildings.
- The zero frontage setbacks of buildings.
- The Ripponlea Railway Station and surrounding reserve that defines the western end of the centre.

Brighton Road Local Activity Centre, St Kilda (Elwood)

Support a convenience retail goods and services role for the centre, that maximises opportunities to improve the economic viability of the centre and strengthen its appeal to passing trade.

Design development to respect the predominant two storey scale of development in Brighton Road, and the consistent streetscape pattern and grain created by the inter-war shop fronts.

Inkerman Street / Grey Street Local Activity Centre, St Kilda

Encourage a convenience retail goods and services role for the centre.

Encourage a zero street setback from front boundaries in Barkly, Vale and Inkerman Streets.

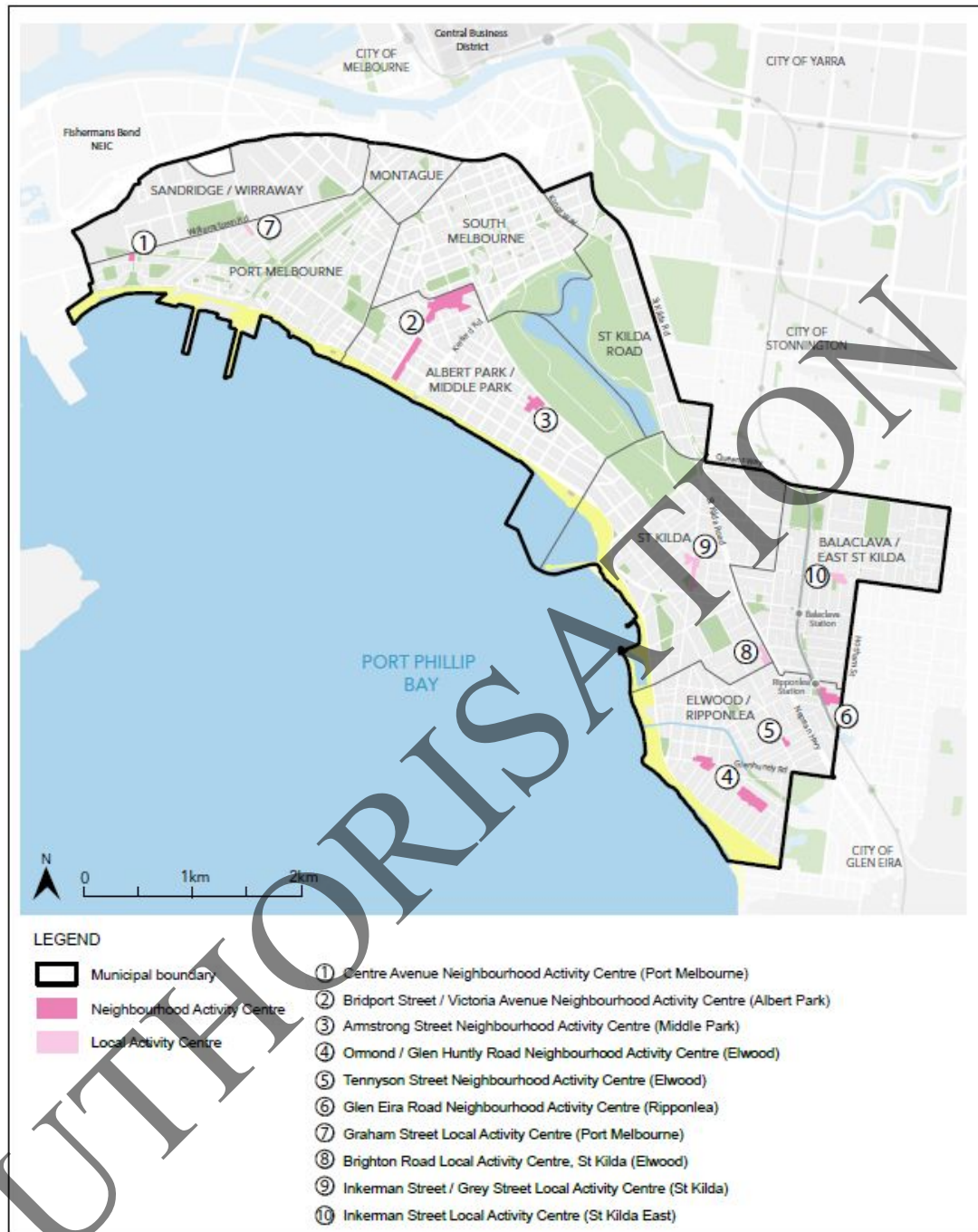
Inkerman Street Local Activity Centre, St Kilda East

Encourage a convenience retailing, and personal / business services role for the centre.

Design development to respect the following elements:

- The predominant two storey building scale of the centre.
- The zero frontage setbacks of buildings.
- The park at the corner of Orange Grove and Inkerman Street.

Local and neighbourhood activity centres



41-03-4L-05 South Melbourne Central Major Activity Centre

14/04/2023
6203port

This policy applies to all land in the South Melbourne Central Major Activity Centre and environs, as shown on the map to this clause.

Activity and business mix

Objective

To develop a sustainable mixed use precinct focused on the South Melbourne Central Major Activity Centre, that includes local and specialised retailing, while retaining the unique urban village character and street life.

Strategies-

Promote activity, diversity and vitality by:-

- Ensuring that the centre retains its vital local convenience retail and service role which serves the surrounding residential community and workforce.

Support the local economy by:-

- Protecting and enhancing the core sub-regional retail role of the centre.
- Promoting the centre as a sub-regional retail destination with a business services and light industry focus.
- Exploiting the centre's strategic location, adjacent to the Central City, as an expanding location for knowledge-based and creative industries.
- Protecting the core industrial role of the centre's industrial precincts from pressure for alternative land uses.

Create a great place to live by:-

- Encouraging a moderate intensification of housing, with a consistently high design quality to enhance the visual and streetscape amenity of the area.
- Balancing increased mixed use and residential development pressure in the northern precincts of the centre, given their proximity to Southbank and the Central City.
- Encouraging the provision of additional open space opportunities, particularly north of Park Street within the centre's wide footpaths.

Ensuring that new development respects:-

- The views of the South Melbourne Town Hall clock tower in Clarendon and Park Street.
- The view of the Shrine of Remembrance along Bank Street.
- Views to the City and Albert Park.

South Melbourne Central Precincts

Objective

To provide for high quality, well designed use and development in the precincts of South Melbourne Central, as shown in the map to this policy:

Strategies

Clarendon Street Core Retail Strip-

Retain the daily / weekly retail goods and services role as the retail focus of the wider South Melbourne Central precinct with upper level residential or small office uses.

Strengthen Clarendon Street as a key pedestrian connection between the centre and the Central City.

Encourage activities that complement the core retail function of Clarendon Street.

Goventry Street Specialty Shopping Centre (within South Melbourne Central)

Reinforce the precinct as a regionally significant specialty retailing area and as a vital pedestrian link between Clarendon Street and the South Melbourne Market.

South Melbourne Market Precinct-

Reinforce South Melbourne Market as a principal retail and community focus for the local and wider area.

Encourage residential, community or commercial uses on the western edge of the market, provided the operation of the market is ongoing.

Emerging Activity Precinct

Encourage activities that complement the core retail function of Clarendon Street.

Diversify land uses through mixed use development with ground level retail or commercial frontages and upper level office and residential.

Northern Mixed Activity Edge

Support conference or function centres on larger sized lots, subject to parking considerations.

Support upper level office and residential development.

Southern Mixed Activity Edge

Support retail, service business, small-medium or home offices, and residential uses in the precinct.

Eastern Business District

Support the substantial light industrial / service business and expanding high technology / knowledge based industries in the precinct.

Support small scale office use and development for start-up and emerging business, and the creative arts, including small studio spaces.

Western Business District

Support new office development and the precinct's substantial light industrial / service business and showroom role and expanding high technology / knowledge-based industry business roles.

Maintain an attractive and vibrant interface between the precinct and South Melbourne Central along Market Street.

Kings Way Mixed Use Corridor

Create a regionally significant mixed use area.

Provide quality, higher density residential use and development (including above larger scale commercial development).

Create an attractive boulevard between the City of Port Phillip and the Central City by ensuring excellence in the design of new buildings and the public realm.

Ferrars Street Light Rail Corridor

Support 'out of centre' and restricted retail premises where they will not detrimentally impact on the core retailing role of the activity centre.

Encourage transit-based development through mixed use retail (including small scale convenience retail clustered around transport stops) and increased residential density.

Emerald Hill Civic, Cultural and Community Hub

Develop the South Melbourne Town Hall and Emerald Hill as the major focus of cultural activity and an integrated network of civic, cultural and community facilities for the local and wider community.

Encourage cultural, community and educational facilities to establish in and adjacent to this precinct.

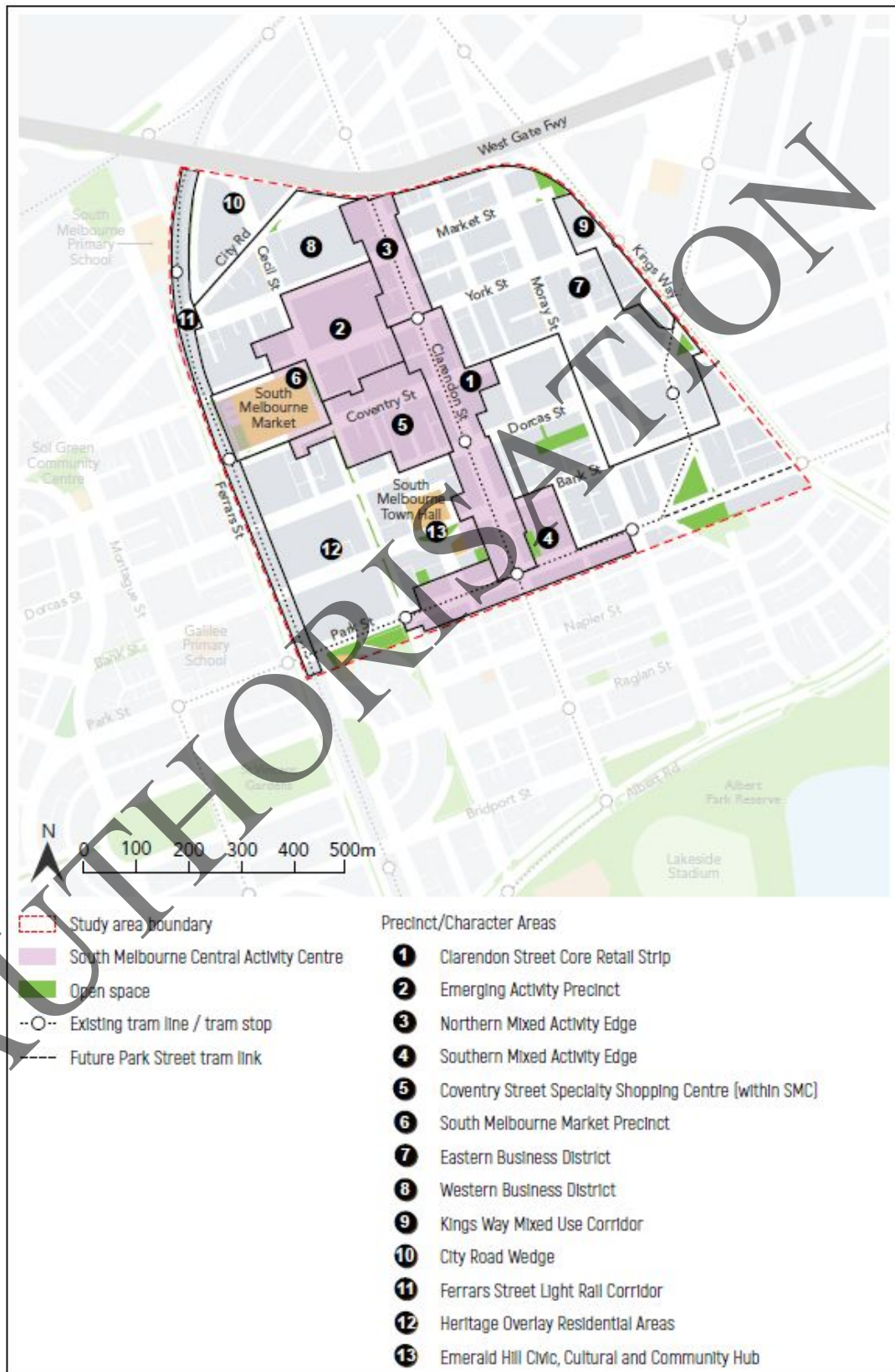
Support the establishment of an Urban History Centre in the Emerald Hill precinct.

Policy documents

Consider as relevant:

- *South Melbourne Central Structure Plan* (City of Port Phillip, August 2007)
- *South Melbourne Central Urban Design Framework* (David Lock Associates and City of Port Phillip, August 2007)

South Melbourne Central Major Activity Centre and Study Area



11.03-1L-06 St Kilda Major Activity Centre07/08/2023
C213port**Policy application**

This policy applies to all land in the St Kilda Major Activity Centre as defined by the boundaries of the Commercial 1 Zone along Fitzroy and Acland Streets, St Kilda.

Objective

To reinforce the St Kilda Major Activity Centre as a significant retail, recreational, tourism, entertainment and leisure destination, whilst managing the cumulative impacts on local amenity and community safety.

Fitzroy Street**Strategies**

Retain the spacious boulevard atmosphere of Fitzroy Street.

Promote the tourism and entertainment role of Fitzroy Street, while maintaining the local retail servicing role, including core retail along Fitzroy Street between Princes and Acland Streets.

Encourage office and non-retail commercial uses north of Princes Street and facilitate a transition to St Kilda Junction.

Encourage non-retail commercial uses and residential development on the north-west side of Fitzroy Street, between Beaconsfield Parade and Canterbury Road.

Ensure the design of new development respects:

- The slope of the street toward the sea.
- The wide pavements and spacious character of Fitzroy Street.

Acland Street**Strategies**

Promote the tourism and entertainment role of Acland Street, while retaining the distinctive village atmosphere and local retailing services role.

Ensure new development respects the strong seaside location and the low scale, fine grain built form at the street edge.

St Kilda Major Activity Centre



11.03-2S

04/05/2022
VC210

Growth areas

Objective

To locate urban growth close to transport corridors and services and provide efficient and effective infrastructure to create sustainability benefits while protecting primary production, major sources of raw materials and valued environmental areas.

Strategies

Concentrate urban expansion into growth areas that are served by high-capacity public transport.

Implement the strategic directions in the Growth Area Framework Plans.

Encourage average overall residential densities in the growth areas of a minimum of 15 dwellings per net developable hectare, and over time, seek an overall increase in residential densities to more than 20 dwellings per net developable hectare.

Deliver timely and adequate provision of public transport and local and regional infrastructure and services, in line with a preferred sequence of land release.

Provide for significant amounts of local employment opportunities and in some areas, provide large scale industrial or other more regional employment generators.

Create a network of mixed-use activity centres that are high quality, well designed and create a sense of place.

Provide a diversity of housing type and distribution.

Retain unique characteristics of established areas impacted by growth.

Protect and manage natural resources and areas of heritage, cultural and environmental significance.

Create well planned, easy to maintain and safe streets and neighbourhoods that reduce opportunities for crime, improve perceptions of safety and increase levels of community participation.

Develop Growth Area Framework Plans that will:

- Include objectives for each growth area.
- Identify the long term pattern of urban growth.
- Identify the location of broad urban development types, for example activity centre, residential, employment, freight centres and mixed use employment.
- Identify the boundaries of individual communities, landscape values and, as appropriate, the need for discrete urban breaks and how land uses in these breaks will be managed.
- Identify transport networks and options for investigation, such as future railway lines and stations, freight activity centres, freeways and arterial roads.
- Identify the location of open space to be retained for recreation, and/or biodiversity protection and/or flood risk reduction purposes guided and directed by regional biodiversity conservation strategies.
- Show significant waterways as opportunities for creating linear trails, along with areas required to be retained for biodiversity protection and/or flood risk reduction purposes.
- Identify appropriate uses for constrained areas, including quarry buffers.

Develop precinct structure plans consistent with the *Precinct Structure Planning Guidelines* (Victorian Planning Authority, 2021) approved by the Minister for Planning to:

- Establish a sense of place and community.
- Create greater housing choice, diversity and affordable places to live.
- Create highly accessible and vibrant activity centres.
- Provide for local employment and business activity.
- Provide better transport choices.
- Respond to climate change and increase environmental sustainability.
- Deliver accessible, integrated and adaptable community infrastructure.

Policy documents

Consider as relevant:

- Any applicable Growth Area Framework Plans (Department of Sustainability and Environment, 2006)
- *Precinct Structure Planning Guidelines* (Victorian Planning Authority, 2021)
- *Ministerial Direction No. 12 – Urban Growth Areas*

AUTHORISATION

11.03-3S31/07/2018
VC148**Peri-urban areas****Objective**

To manage growth in peri-urban areas to protect and enhance their identified valued attributes.

Strategies

Identify and protect areas that are strategically important for the environment, biodiversity, landscape, open space, water, agriculture, energy, recreation, tourism, environment, cultural heritage, infrastructure, extractive and other natural resources.

Provide for development in established settlements that have capacity for growth having regard to complex ecosystems, landscapes, agricultural and recreational activities including in Warragul-Drouin, Bacchus Marsh, Torquay-Jan Juc, Gisborne, Kyneton, Wonthaggi, Kilmore, Broadford, Seymour and Ballan and other towns identified by Regional Growth Plans as having potential for growth.

Establish growth boundaries for peri-urban towns to avoid urban sprawl and protect agricultural land and environmental assets.

Enhance the character, identity, attractiveness and amenity of peri-urban towns.

Prevent dispersed settlement and provide for non-urban breaks between urban areas.

Ensure development is linked to the timely and viable provision of physical and social infrastructure.

Improve connections to regional and metropolitan transport services.

AUTHORISATION

11.03-4S20/03/2023
VC229**Coastal settlement****Objective**

To plan for sustainable coastal development.

Strategies

Plan and manage coastal population growth and increased visitation so that impacts do not cause unsustainable use of coastal resources.

Support a network of diverse coastal settlements that provide for a broad range of housing types, economic opportunities and services.

Identify a clear settlement boundary around coastal settlements to ensure that growth in coastal areas is planned and coastal values are protected. Where no settlement boundary is identified, the extent of a settlement is defined by the extent of existing urban zoned land and any land identified on a plan in the planning scheme for future urban settlement.

Minimise linear urban sprawl along the coastal edge and ribbon development in rural landscapes.

Protect areas between settlements for non-urban use.

Limit development in identified coastal hazard areas, on ridgelines, primary coastal dune systems, shorelines of estuaries, wetlands and low-lying coastal areas, or where coastal processes may be detrimentally impacted.

Encourage the restructure of old and inappropriate subdivisions to reduce development impacts on the environment.

Ensure a sustainable water supply, stormwater management and sewerage treatment for all development.

Minimise the quantity and enhance the quality of stormwater discharge from new development into the ocean, bays and estuaries.

Prevent the development of new residential canal estates.

Policy documents

Consider as relevant:

- *G21 Regional Growth Plan* (Geelong Region Alliance, 2013)
- *Gippsland Regional Growth Plan* (Victorian Government, 2014)
- *Great South Coast Regional Growth Plan* (Victorian Government, 2014)
- *Marine and Coastal Policy* (Department of Environment, Land, Water and Planning, 2020)
- *Marine and Coastal Strategy* (Department of Environment, Land, Water and Planning, 2022)
- *Siting and Design Guidelines for Structures on the Victorian Coast* (Department of Environment, Land, Water and Planning, 2020)

11.03-5S30/04/2021
VC185**Distinctive areas and landscapes****Objective**

To recognise the importance of distinctive areas and landscapes to the people of Victoria and protect and enhance the valued attributes of identified or declared distinctive areas and landscapes.

Strategies

Recognise the unique features and special characteristics of these areas and landscapes.

Implement the strategic directions of approved Localised Planning Statements and Statements of Planning Policy.

Integrate policy development, implementation and decision-making for declared areas under Statements of Planning policy.

Recognise the important role these areas play in the state as tourist destinations.

Protect the identified key values and activities of these areas.

Enhance conservation of the environment, including the unique habitats, ecosystems and biodiversity of these areas.

Support use and development where it enhances the valued characteristics of these areas.

Avoid use and development that could undermine the long-term natural or non-urban use of land in these areas.

Protect areas that are important for food production.

Policy documents

Consider as relevant:

- *Bellarine Peninsula Localised Planning Statement* (Victorian Government, 2015)
- *Macedon Ranges Statement of Planning Policy* (Victorian Government, 2019)
- *Mornington Peninsula Localised Planning Statement* (Victorian Government, 2014)
- *Yarra Ranges Localised Planning Statement* (Victorian Government, 2017)

11.03-6S

31/07/2018
VC148

Regional and local places

Objective

To facilitate integrated place-based planning.

Strategies

Integrate relevant planning considerations to provide specific direction for the planning of sites, places, neighbourhoods and towns.

Consider the distinctive characteristics and needs of regional and local places in planning for future land use and development.

AUTHORISATION

11.03-6L-01 St Kilda Foreshore07/08/2023
C213port**Policy application**

This policy applies to the St Kilda Foreshore area, as shown on the map to this clause.

General**Objectives**

To create an equitable balance between the needs of the local community, the Foreshore's important regional tourism role and the ecological future of the Bay.

To ensure an integrated approach to the revitalisation of the St Kilda Foreshore area, including any development of the Triangle site through the activity mix, built form and improvements to the public realm.

Cultural heritage and activity mix**Objective**

To maintain and reinforce the unique cultural heritage and recreational importance of the St Kilda Foreshore area.

Strategies

Support land use and development that contributes to the diverse character and reinforces the St Kilda Foreshore area as a key leisure and entertainment precinct.

Encourage uses and facilities that reinforce the role of key activity destinations including St Kilda Pier, St Kilda Harbour, West Beach area and the Triangle site.

Encourage the co-location of uses within defined activity nodes, including:

- New public space at the Palais Theatre.
- St Kilda Pier entry area.
- West Beach Pavilion.

Encourage development to incorporate urban art to enhance the layering of cultural elements along the St Kilda Foreshore area, and support legibility.

Retain the residential role of existing residential properties fronting the St Kilda foreshore.

Enhance the function of specific sites by:

- Consolidating yacht support services at the Royal Melbourne Yacht Squadron.
- Encouraging the development of new public facilities at the St Kilda Pier entry area.
- Encouraging the refurbishment of the West Beach Pavilion for community use with some complementary commercial use(s).
- Encouraging the use of the Veg Out site at the Peanut Farm Reserve as a community open space site.
- Encouraging the retention of community gardens as a vital community activity.

Built form**Objective**

To ensure development does not dominate the Foreshore.

Strategies

Maintain the low-rise, (one and two storey) character of the Foreshore Reserve.

Support future building heights and forms that maintain and enhance the views from:

- The Upper Esplanade to the horizon, Williamstown, Marine lighthouse, the Stokehouse and Catani Arch.
- South Beach to the entrance of Luna Park.

Public space network and connections

Strategies

Improve cross connectivity between the built urban area and the Foreshore Reserve by:

- Establishing Shakespeare Grove and the Cowderoy Street – Pier Road link as safe and attractive pedestrian routes.
- Facilitating additional pedestrian crossings of Beach Road at locations identified in the map to this clause.

Encourage visual integration of the Foreshore Reserve by:

- Restoring and reinforcing established landscape themes.
- Co-ordinating street furniture and street design details.

Transport networks

Strategy

Manage adverse impacts of through traffic, particularly on Jacka Boulevard, Marine Parade, The Upper Esplanade and residential streets.

St Kilda Triangle site

Objective

To encourage the integrated renewal of the site for a variety of public spaces, and entertainment and cultural venues.

Strategies

Ensure that future use and development on the St Kilda Triangle site:

- Enhances local liveability.
- Creates a hub focused on the arts, entertainment and leisure.
- Provides a multipurpose community space.
- Includes limited commercial and retail land uses.
- Creates large areas of open space.
- Links Acland Street, Fitzroy Street and the foreshore through a highly pedestrianised environment.
- Establishes a key activity node within the St Kilda Foreshore area.
- Links new and existing buildings.

Maintain the traditional cultural use of the site and allow for new contemporary leisure activities.

Minimise noise transference through high quality facility design, landscaping and buffer zones.

Jacka Boulevard

Objective

To encourage the development of Jacka Boulevard as an attractive seaside boulevard with a multi-functional role.

Strategies

Support provision of safe and accessible pedestrian crossings.

Minimise impacts from vehicular traffic by planning for shared use of street spaces, where appropriate.

Improve direct pedestrian connection to the beach at the intersection with Fitzroy Street.

Encourage new pedestrian links to the Foreshore Reserve:

- From the Triangle Site.
- Between the Esplanade Hotel and St Kilda Pier.

St Kilda Harbour

Objective

To enable the sustainable use and development of the St Kilda Harbour.

Strategy

Facilitate sustainable boat usage of the Harbour by:

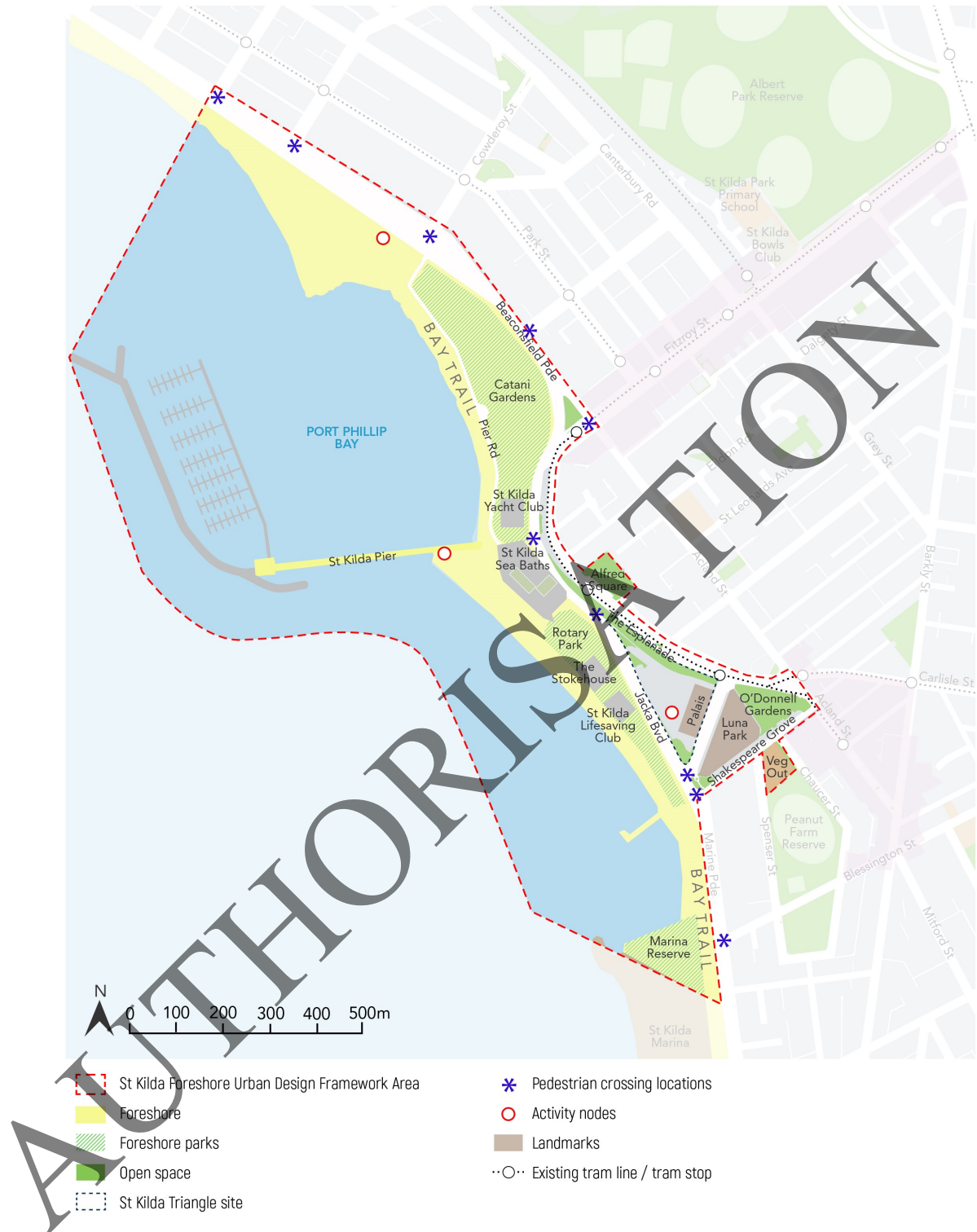
- Protecting significant natural coastal values.
- Protecting the water quality of the Harbour from adverse impacts of boating.
- Ensuring access to habitat areas are protected and managed.

Policy documents

Consider as relevant:

- *St Kilda Foreshore Urban Design Framework* (City of Port Phillip, 2002)
- *Port Phillip Urban Art Strategy* (City of Port Phillip, 2002)
- *Port Phillip Urban Iconography Study* (City of Port Phillip, 2004)
- *Recreational Boating Facilities Framework* (Central Coastal Board, 2014)

St Kilda Foreshore



11.03-6L-02 St Kilda Road North Precinct

14/04/2023
C203port

Policy application

This policy applies to the St Kilda Road North Precinct, as shown on the map to this clause.

Objectives

To reinforce the St Kilda Road North Precinct as a dynamic, connected, integrated, safe and inclusive place to live, work and visit.

To maintain the role of St Kilda Road as a preferred location for premier office accommodation and well-designed, higher density residential development.

Strategies

General

Encourage development of a vibrant area, providing for residential, commercial and community uses.

Encourage community spaces (such as meeting rooms) to be incorporated into new development.

Sub-Precinct 2: Northwest Corner (DDO26-2)

Ensure that use and development create a residential and mixed use environment, through an increased scale and density of development.

Sub-Precinct 3: Albert Road South (DDO26-3) and Sub-Precinct 4: Albert Road North and Bowen Crescent (DDO26-4)

- Encourage uses that support and enhance the strategic role of the Albert Road Mixed Activity Precinct as a key location for:
 - Office and related commercial development.
 - Residential development at higher density and larger scale than in the established residential area.

Sub-Precinct 5: St Kilda Road South of Kings Way (DDO26-5)

Ensure that the Sub-Precinct continues to develop as a premier office location outside of the Melbourne Central Activity District and a highly desirable residential location.

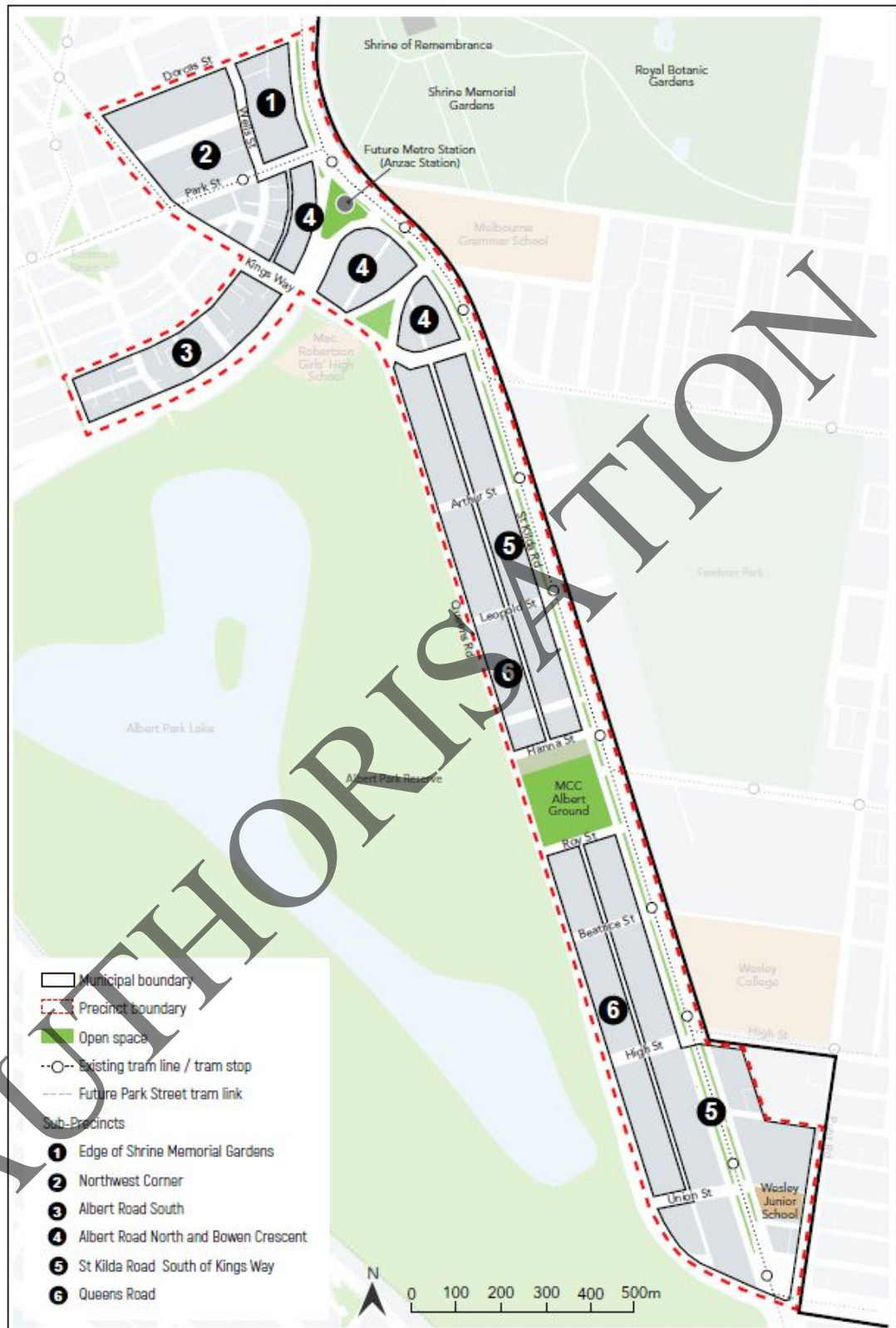
Encourage the development of a mixed use area on Raleigh and Union Streets.

Policy document

Consider as relevant:

- *St Kilda Road North Precinct Plan 2013* (City of Port Phillip, updated 2015)

St Kilda Road North Precinct and Sub-Precincts



11.03-6L-03 St Kilda Road South Precinct

07/08/2023
C213port

Policy application

This policy applies to the St Kilda Road South Precinct, as shown on the map to this clause.

General

Objective

To strengthen the St Kilda Road South Precinct's image, liveability and sense of place as it transitions to increased residential uses.

Strategies

Create a diverse series of neighbourhoods with a strong sense of place, community and local identity.

Encourage land uses that create activity nodes and community focal points for local residents and workers.

Provide additional opportunities for housing growth throughout the Precinct due to its proximity to public transport and activity centres.

Improve the amenity and function of St Kilda Road as a key pedestrian spine by providing active land use 'edges' at street level throughout commercial and mixed use areas.

Maintain solar access to key pedestrian streets and green links, including St Kilda Road, Wellington Street, Alma Road and Carlisle Street.

Ensure the interface between commercial and residential uses is well designed and non-residential uses are managed to protect residential amenity.

Discourage larger-scale licensed premises, bars and nightclubs that may impact on the amenity of the surrounding area.

Policy guidelines

Consider as relevant:

- Designing development to be self-sufficient in onsite car parking and providing onsite parking as follows:
 - Residential development:
 - One car space to each one and two bedroom dwelling.
 - Two car spaces to each three or more bedroom dwelling.
 - Secure bicycle parking at a rate of one space per dwelling.
 - Publicly accessible bicycle parking for visitors at a rate of one space per five dwellings.
 - Office: Three car spaces per 100 square metres.
 - Motorcycle parking at a minimum rate of one motorcycle parking space for every 100 car parking spaces to be provided onsite within new development.
 - Residential development of six or more storeys: Off-street loading facilities and allocated spaces for service vehicles.
- Limiting the hours of operation of licensed premises to 10pm.

Wellington Street Neighbourhood

Objective

To enhance the Wellington Street Neighbourhood as a local and mixed use 'village', and a green link between Chapel Street and Albert Park.

Strategies

Ensure development makes a positive contribution to the creation of a landscaped (green) pedestrian link along Wellington Street.

Along the north side of Wellington Street (DDO35):

- Facilitate a transition from a commercial to a mixed use with increasing residential uses.
- Facilitate clustering of cafes, convenience retailing and spaces for community use at the western end of Wellington Street and immediately east of Upton Road.
- Encourage commercial activities and residential frontages that activate the balance of Wellington Street, with upper level residential use.
- Limit land uses with the potential for adverse residential amenity impacts.

Along the south side of Wellington Street in a residential zone:

- Maintain the established residential role, with some additional medium density residential development consistent with the scale, neighbourhood character and heritage of the street.
- Reinforce the established low scale (1-2 storey) and fine grain of the heritage area, generally east of Upton Road.
- Reinforce the established 2-3 storey scale generally west of Upton Road, with well-designed 'infill' development on larger sites consistent with the established neighbourhood character.
- Maintain the established street rhythm of space between buildings and landscaping in the frontage setback.

Along the south side of Wellington Street at the western end in the Commercial 1 and Mixed Use zones (DDO35) facilitate an emerging activity hub by ensuring active, small scale ground level retail uses that contribute to street-life, such as cafes and shops.

St Kilda Road Neighbourhood

Objective

To reinforce the St Kilda Road Neighbourhood as a niche retail and business area, a growing residential community and a safe and friendly pedestrian environment.

Strategies

Support residential redevelopment in Alma Road (east of St Kilda Road).

Along the eastern side of St Kilda Road (DDO34):

- Create a continuous activated edge to the street through a diverse range of ground level retail and complementary commercial uses.
- Support upper level office and residential, and residential to the rear of commercial premises.

Along the western side of St Kilda Road (DDO27 and DDO36):

- Support residential development at the intersection of and north of Alma Road, and in Barkly Street, and Alma Road.
- Establish a ground level residential edge within residential side streets, south of Alma Road.

Carlisle Street Neighbourhood

Objective

To retain the established residential role and neighbourhood character of the Carlisle Street Neighbourhood with a mix of dwelling types, set along a 'green link' between East St Kilda and St Kilda.

Strategies

Facilitate medium density 'infill' residential development, consistent with the scale and neighbourhood character of the area.

Reinforce the prevailing low to mid-rise scale (2 to 4 storeys) of residential development.

Ensure new development respects and responds to the height and scale of existing residential buildings, through a comparable street-wall height and recessed upper level.

Ensure new development utilises articulation and building form to create the sense of a fine-grain subdivision pattern.

Maintain the street rhythm of space between buildings and landscaping in the frontage setback.

Create a sense of a fine-grain subdivision pattern through the articulation and building form of new development.

Policy guideline

Consider as relevant:

- Designing development to respect Carlisle Street as a landscaped (green) pedestrian link by maintaining solar access to the southern footpath of Carlisle Street between 10am and 3pm at the equinox (21 September).

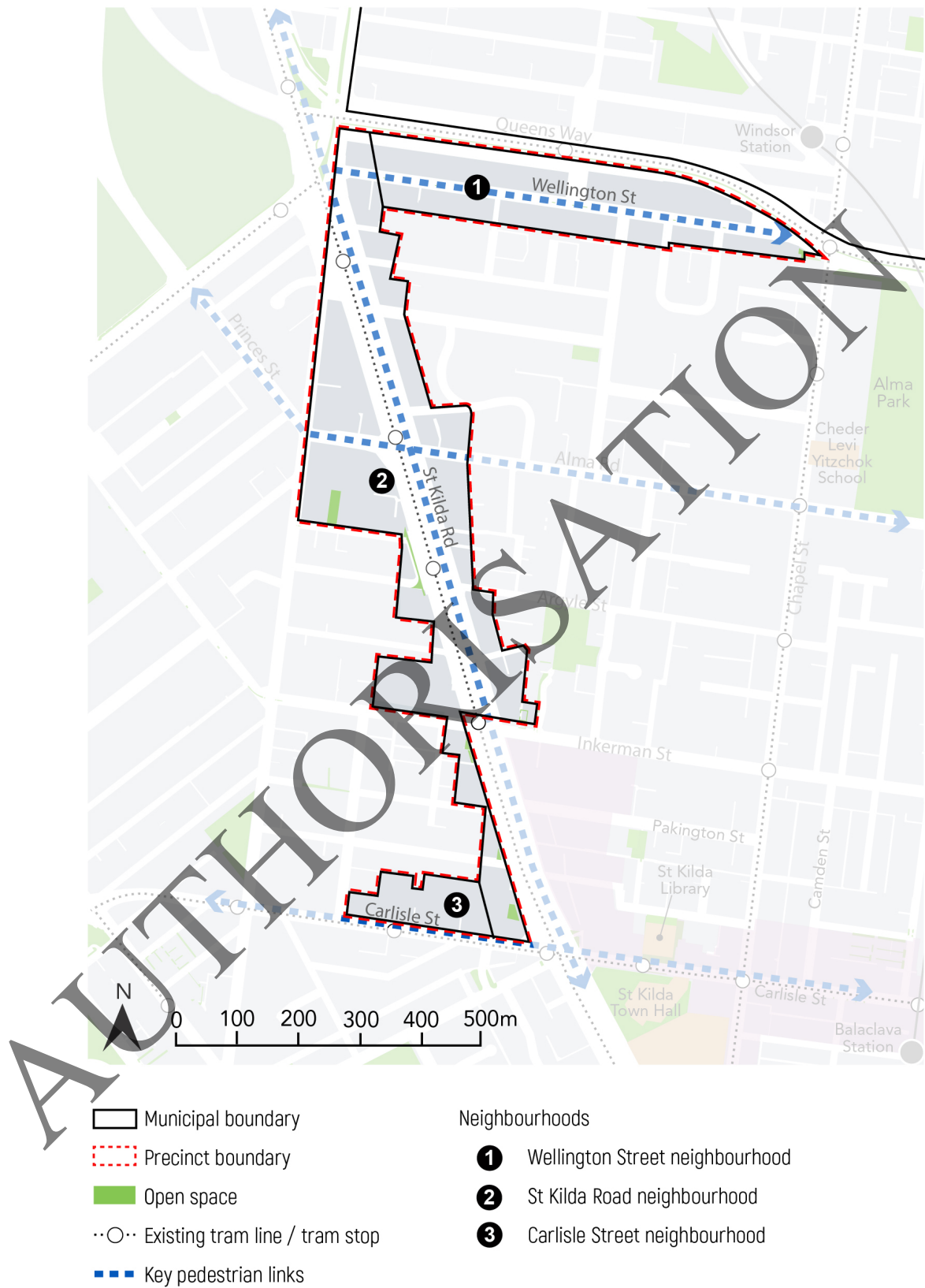
Policy document

Consider as relevant:

- *St Kilda Road South Urban Design and Land Use Framework* (Planisphere and City of Port Phillip, 2015)

AUTHORISATION

St Kilda Road South Precinct and Neighbourhoods



11.03-6L-04 Fishermans Bend Urban Renewal Area14/04/2023
C203port**Policy application**

This policy applies to use and development of all land within Fishermans Bend affected by Schedule 1 to the Capital City Zone in the Port Phillip Planning Scheme.

Employment floor area objective

To support diverse employment opportunities across all precincts that build on proximity to the Central City and Port of Melbourne.

Employment floor area strategies

Promote employment generating floor space that supports jobs growth including, but not limited to, the knowledge, creative, design, innovation, engineering, and service sectors.

Employment floor area policy guidelines

Consider as relevant:

- Providing floor area for employment generating uses as part of development in a Core area, as identified on the relevant Map in Schedule 1 to the Capital City Zone, consistent with the preferred minimum plot ratio set out in Table 1, unless any of the following apply:
 - The built form envelope available on the site makes it impractical to provide the minimum plot ratios.
 - The application is associated with the continued operation or expansion of an existing employment or residential use on site.
 - The buildings floor to floor heights, layout and design of the development will facilitate future conversion from residential to employment generating uses or from car parking areas to other employment generating uses.
 - The development contributes to the employment objectives and strategies of this policy while providing less than the minimum plot ratio.

Table 1: Minimum plot ratio not used for Dwelling

Precinct	Minimum plot ratio not used for Dwelling (Core Areas)
Montague	1.6:1
Sandridge	3.7:1
Wirraway	1.9:1

Community and diversity objective

To ensure housing diversity, and the provision of community infrastructure and an open space network that supports a diverse and inclusive community.

Community and diversity strategies

Encourage a diversity of dwelling typologies and sizes within each precinct and within development sites.

Facilitate Affordable Housing within a range of built form typologies.

Support development that delivers a range of housing types suitable for households with children through:

- The development of mid-rise housing with access to private open space.

- Living room sizes that exceed minimum requirements.
- Access to outdoor communal open green space including children's play spaces on ground level, podium levels or roof tops and locating some dwellings to achieve direct visual connection to those play spaces.
- Providing children's communal active indoor play or recreation space as part of indoor communal spaces.
- Locating sufficient storage areas in areas with easy access to dwellings.

Support the delivery of adaptable floor plates including the opportunity to combine one and two-bedroom units to form larger apartments.

Encourage the early delivery of community infrastructure hubs.

Community and diversity policy guidelines

Consider as relevant:

- Providing the following percentage of three-bedroom dwellings for developments of more than 100 dwellings:
 - Montague: 25 per cent
 - Sandridge: 20 per cent
 - Wirraway: 30 per cent

Affordable Housing including Social Housing objective

To encourage the provision of Affordable Housing including Social Housing.

Affordable housing strategies

Design Affordable Housing so that it:

- Is a mix of one, two and three bedrooms that reflects the overall dwelling composition of the building.
- Provides internal layouts identical to other comparable dwellings in the building.
- Is externally indistinguishable from other dwellings.

Affordable housing policy guideline

Consider as relevant:

- Providing at least six per cent of dwellings permitted under the dwelling density requirements in the Capital City Zone (excluding any Social housing uplift dwellings) as Affordable housing, unless, any of the following apply:
 - The built form envelope available on the site makes it impractical to do so.
 - The development will contribute to the Affordable Housing objective of this policy while providing less than the minimum amount.
 - The Affordable housing objective of this policy would render the proposed development economically unviable.

Social housing strategies

Encourage development to provide a Social Housing uplift equivalent to eight additional private dwellings of equivalent size for each Social Housing dwelling, in addition to the provision of six per cent Affordable housing.

Social housing policy guidelines

Consider as relevant:

- Providing a Social Housing uplift, where:
 - The proposed Social Housing can be realistically delivered and secured by suitable legal agreement.
 - The proposed Social Housing is agreed to be received and managed by a registered Social Housing provider in perpetuity.
 - The Social Housing uplift will have acceptable consequences, having regard to the preferred character of the area, and the level of public transport and other infrastructure available.

Design excellence objective

To create a place of design excellence, with a distinct identity and character.

Design excellence strategies

Encourage varied built form typologies that align with the preferred precinct character specified in the relevant Schedule to the Design and Development Overlay.

Encourage fine grain, pedestrian scale environments.

Ensure buildings contribute to a high quality public realm.

Encourage developments to deliver spaces, including open spaces, for people to meet, gather, socialise, exercise and relax.

Ensure developments to deliver variation in massing, building height, and roof forms and the staggering or offsetting of tower footprints.

Achieving a climate adept, water sensitive, low carbon, low waste community objectives

To achieve a climate adept, water sensitive, low carbon, low waste community.

To ensure ecologically sustainable development.

To build resilience against the impacts of sea level rise and flooding from storm events without compromising the urban form at the ground level.

To create a low waste community that is designed to provide best practice waste and resource recovery management.

Achieving a climate adept, water sensitive, low carbon, low waste community strategies

Design development to incorporate renewable energy generation, on-site energy storage, and opportunities to connect to a future precinct-wide or locally distributed low-carbon energy supply.

Raise internal ground floor level above street level as a last resort, except where the implementation of other measures coupled with an evidence based approach to risk management reasonably necessitates raising internal floor levels above street level.

Where internal floor levels are raised, maintain a strong physical and visual connection between the street and internal floor levels through building design.

Where practicable, developments should create opportunities to:

- Optimise waste storage and efficient collection methods.
- Combine commercial and residential waste storage.
- Share storage or collections with adjacent developments.
- Separate collection for recycling, hard waste, and food and green waste.

Achieving a climate adept, water sensitive, low carbon, low waste community policy guidelines

Consider as relevant:

- Applying a 20 per cent improvement on current National Construction Code energy efficiency standards, including energy efficiency standards for building envelopes and for lighting and building services.
- Applying an average 7 star Nationwide House Energy Rating Scheme (NatHERS) rating for residential development.
- At least 70 per cent of the total site area to comprise building or landscape elements that reduce the impact of the urban heat island effect, including:
 - Vegetation, green roofs and water bodies.
 - Roof materials, shade structures, solar panels or hard scaping materials with high solar reflectivity index.
 - Non-glazed facade materials exposed to summer sun that have a low solar absorptance.
- Applying design elements and materials that are resilient to flooding, including water proof doors and windows, elevated power outlets and the like.
- Encouraging land uses at ground floor level that can easily recover from the impacts of temporary flooding.
- Integrating changes in floor levels between the street and internal ground floor into the design of the development.
- Locating essential services, such as power connections, switchboards and other critical services to avoid disruption in potential flooding events.
- Development and public realm layout and design should integrate best practice Water Sensitive Urban Design.

Communal spaces objective

To encourage residential development that includes private and communal spaces that cater for a range of users.

Communal spaces strategies

Create private and communal spaces within developments with a range of facilities, garden and recreation areas to supplement the public open space network.

Ensure development with an interface to existing or proposed open space avoids unreasonable impacts, including through vehicle movement to or from the development on the:

- Amenity or microclimate impacts to the open space.
- Function or useability of the open space.

Support internal and external communal spaces within the same development to connect to one another and be designed as multifunctional, adaptable spaces.

Support the provision of additional publicly accessible areas at ground level that contribute to the creation of a network of passive, formal and informal recreational spaces.

Ensure communal open space is designed to meet the needs of a range of potential users.

Ensure the location, design and layout of publicly accessible open space areas at ground level is integrated with adjoining areas of open space.

Landscaping objective

To ensure developments provide landscaping in all areas of open space including public open space, communal open space and private open space.

Landscaping strategies

Ensure landscape areas:

- Contribute to the creation of a sense of place and identity and the preferred character sought for the precinct.
- Incorporate innovative approaches to flood mitigation and stormwater run-off, and best practice Water Sensitive Urban Design.
- Incorporate opportunities for community gardens.
- Interpret and celebrate heritage and culture, including Aboriginal cultural heritage in public open space design.

Encourage plant selection to:

- Support the creation of complex and biodiverse habitat that includes indigenous flora and fauna.
- Balance the provision of native plants with exotic climate resilient plants that provide for biodiversity.
- Support the creation of vegetation links within Fishermans Bend to surrounding areas of biodiversity through plant selection and landscape design.
- Incorporate food plants.

Incorporate green facades, rooftop, podium or terrace planting into development that is water efficient, responds to micro-climate conditions and is located and designed to be sustainable and resilient.

Landscaping policy guidelines

Consider as relevant:

- Minimum deep soil areas with a minimum depth of 1.5 metres for canopy trees.

New streets, laneways and pedestrian connections objective

To create a network of new streets and laneways that provide permeability and accessibility through all precincts.

New streets, laneways and pedestrian connections strategies

Facilitate streets, laneways and pedestrian connections that:

- Provide direct access to existing or proposed public transport stations and routes, and existing or proposed public open space.
- Prioritise pedestrian movement and safety in shared streets or shared laneways.

Design streets and laneways to:

- Enable views through the street block.
- Have active frontages in a Core area.
- Be open to the sky.
- Allow for canopy tree planting.

Provide new streets, laneways or paths to create mid-block through links and define and separate buildings on sites of more than 3000 square metres.

New streets, laneways and pedestrian connections policy guidelines

Consider as relevant:

- Spacing streets, laneways and pedestrian connections:
 - In Core areas, not more than 50 to 70 metres apart in a north-south direction and 100 metres apart in the other direction in a block.
 - In Non-core areas, not more than 100 metres apart and be oriented in a north-south direction.

Sustainable transport objective

To create a connected, permeable and accessible community that prioritises walking, cycling, and public transport use.

Sustainable transport strategies

Ensure development does not compromise the delivery of future public transport including new tram, train and bus routes.

Reduce impacts of new vehicle access points on pedestrian, public transport and bicycle priority routes.

Design internal connections to give priority to pedestrian and bicycle movements. Provide easy access to bicycle parking facilities, including end of trip change rooms, showers and lockers.

Encourage developments to provide less than the preferred maximum number of car spaces.

Encourage developments to provide for future conversion of car parking to alternative uses.

Land use transition objectives

To facilitate the transition from a primarily industrial area to a high-density mixed use area.

To support the continued operation of existing uses which are of strategic importance to the urban renewal of Fishermans Bend.

Land use transition strategies

Ensure new uses and the expansion of existing uses with potential adverse amenity impacts do not prejudice the urban renewal of Fishermans Bend.

Ensure new development addresses the amenity impacts of nearby existing uses.

Policy documents

Consider as relevant:

- *Fishermans Bend Vision* (Department of Environment, Land, Water and Planning, September 2016)
- *Fishermans Bend Framework* (Department of Environment, Land, Water and Planning, September 2018)

11.03-6L-05 Fishermans Bend Urban Renewal Area - Montague Precinct

14/04/2023
C203port

Policy application

This policy applies to the use and development of all land within the Montague Precinct of the Fishermans Bend Urban Renewal Area, as defined by the DDO30.

Objective

To establish Montague as a diverse and well-connected mixed use neighbourhood with its own distinct character and identity celebrating its significant cultural and built heritage, and network of gritty streets and laneways.

Strategies

Support mixed use development that provides active street edges and establishes a high quality public realm.

Encourage the highest concentration and mix of uses in the defined core areas, located on public transport nodes and routes.

Support big box retail development that compliments existing and planned retail uses in its catchment.

Ensure the social impacts of developments accommodating more than 100 dwellings are considered and addressed through the preparation of a Social Impact Assessment.

Enhance the existing network of laneways with new laneways and through block links that provide permeability and connectivity through street blocks.

Montague North (M1)

Strategies

Support the establishment of Montague North as a gateway to Fishermans Bend from the Central City, Southbank and Docklands.

Support development that allows for sunlight access to the southern side of Normanby Road at September equinox.

Support buildings where parts of the street wall are setback from the street boundary at ground level to create forecourts, courtyards and landscaping at building entrances.

Support the transformation of Normanby Road into a landscaped, pedestrian friendly boulevard and civic spine defined by active edges that provides a key cycling connection through the precinct.

Support the delivery of a Sports and Recreation Hub (or part of cluster) as part of mixed use development within the 'investigation area' shown on Map 2.

Support the provision of walking and cycling links that enhance connections to the eastern part of Sandridge and Montague South (M2-6).

Support the provision of commercial and some retail and community uses to be located within podium and upper levels of mixed use buildings.

Encourage businesses to locate in proximity to nearby cultural uses, and high quality, high amenity public realm.

Encourage development to provide active frontages to the new 'Montague North Park' open space located at the intersection of Montague Street and Munro Street.

Montague South (M2-6)

Strategies

Support the establishment of Buckhurst Street as the heart of the neighbourhood and the primary focus of commercial and civic amenity within Montague South anchored by community hubs.

Encourage new laneways to complete 'missing links' between primary and secondary active frontages/retail streets.

Encourage smaller building footprints to add to existing the fine grain character.

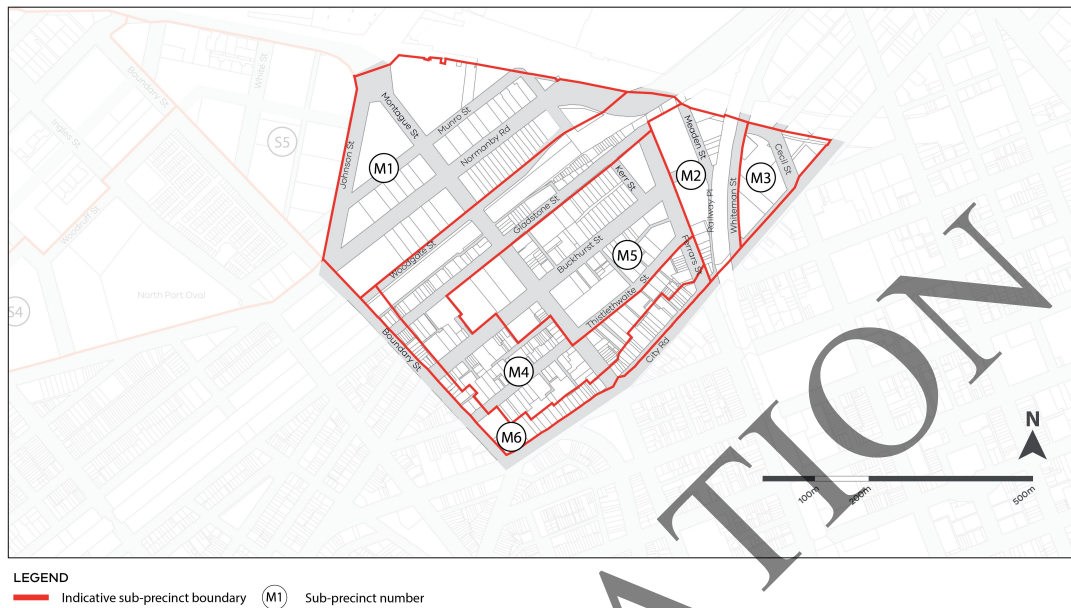
Support the establishment of a high amenity, linear green spine along Buckhurst Street that accommodates the 'Bay to City' cycling connection.

Support the establishment of Buckhurst Street as a primary anchor for the precinct.

Support the creation of parks and community hubs, and high amenity streets that provide high quality social spaces to gather, relax and connect.

Encourage the establishment of a diverse range of small-medium sized businesses, co-working spaces, small creative businesses and studios that contribute to the identity of the area.

Map 1: Sub-precincts within the Montague precinct



Map 2: Community infrastructure investigation areas within the Montague precinct



11.03-6L-06 Fishermans Bend Urban Renewal Area - Sandridge Precinct

14/04/2023
C203port

Policy application

This policy applies to the use and development of all land within Sandridge Precinct of the Fishermans Bend Urban Renewal Area, as defined by DDO32.

Objective

To establish Sandridge as a premium office and commercial neighbourhood with diverse housing and retail opportunities, and its own distinct character and identity.

Strategies

Encourage development that integrates community facilities, larger format commercial offices and retail uses such as supermarkets and department stores within core areas.

Encourage the highest concentration and mix of uses in the defined core areas, located on public transport nodes and routes.

Support big box retail development that compliments existing and planned retail uses in its catchment.

Ensure the social impacts of developments accommodating more than 100 dwellings are considered and addressed through the preparation of a Social Impact Assessment.

Support tower development within core areas that produce a strong vertical form or landmarks.

Support low rise buildings with defined, active frontages around the perimeter of the North Port Oval.

Support the delivery of a centrally located underground metro rail station with transport interchange and public square, connecting directly to the Central City and to Melbourne's western region.

Support the delivery of a tram route along the future Fennell Street and Plummer Street civic spine that provides a direct, high frequency public transport connection to Docklands and the Central City and services the core area.

Support the delivery of a new and upgraded bridges over the Westgate Freeway at Ingles Street and Graham Street, and Hartley Street and Fennell Street, to provide public transport, cycling infrastructure and pedestrian access.

Enhance the planned network of public open spaces and plazas through strategic road closures to create new plazas and linear parks.

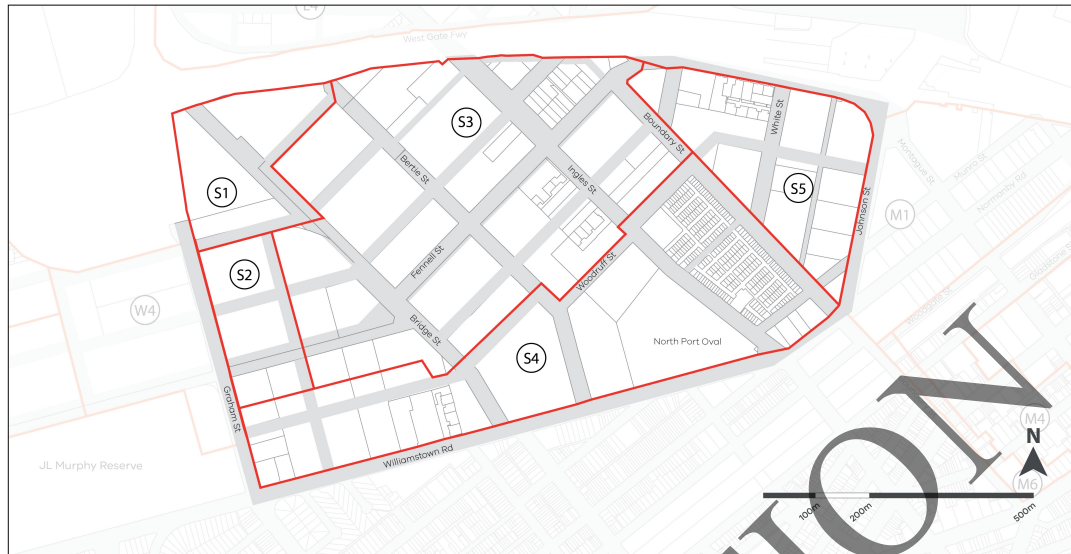
Support the delivery of an Arts and Cultural Hub as an integrated part of mixed use development, located within the 'investigation area' shown on Map 2 generally surrounding the tram route.

Support the delivery of a Sports and Recreation Hub as part of mixed use development within the 'investigation area' shown on Map 2 at the eastern part of the precinct.

Integrate the North Port Oval and grandstand with expanded open space, creating a key anchor for community, civic and recreational uses.

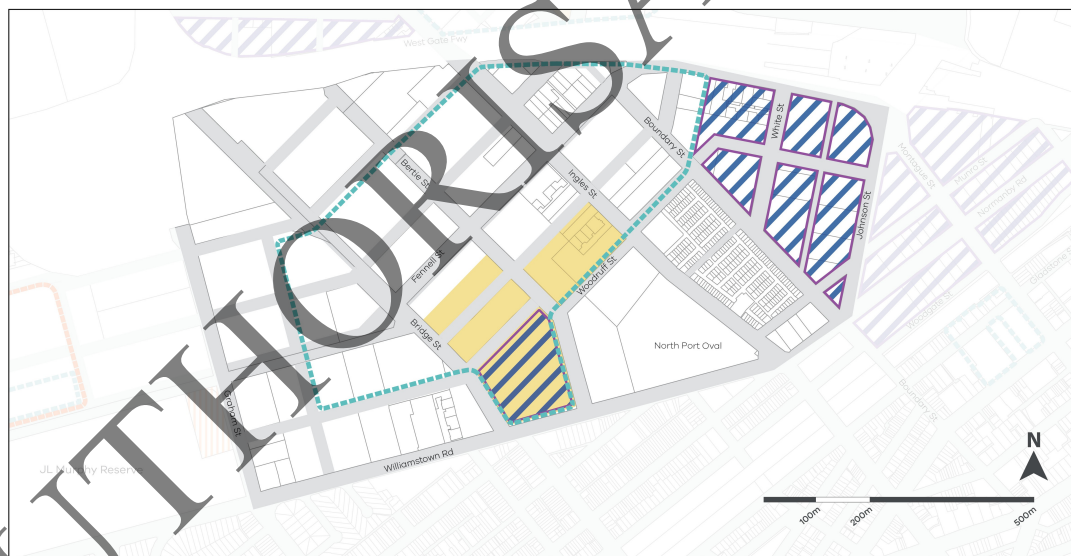
Support the delivery of an Education and Community Hub (primary) as part of mixed use development near the expanded North Port Oval parkland.

Map 1: Sub-precincts within the Sandridge precinct



LEGEND
— Indicative sub-precinct boundary (S1) Sub-precinct number

Map 2: Community infrastructure investigation areas within the Sandridge precinct



LEGEND
 Community Infrastructure Investigation Areas
▨ Sports and recreation hub ▭ Arts and cultural hub ▭ Education and community hub

11.03-6L-07 Fishermans Bend Urban Renewal Area - Wirraway Precinct

14/04/2023
C203port

Policy application

This policy applies to the use and development of all land within the within Wirraway Precinct of the Fishermans Bend Urban Renewal Area, as defined by the DDO33.

Objective

To establish Wirraway as a family-friendly inner city neighbourhood and place for innovation and creativity with its own distinct character and identity.

Strategies

Encourage the establishment of a neighbourhood centre that supports local cafes, restaurants, shops and businesses and a high degree of housing choice, including apartment buildings with a focus on family friendly housing.

Support the development of the precinct as a thriving arts scene and a place for innovation and creativity with small galleries, art and design centres and cultural facilities attract visitors from across Melbourne and beyond.

Support the development of industrial uses, including high technology uses including research and development, that are employment intensive, clean and sustainable and that limit amenity impacts.

Encourage the highest concentration and mix of uses in the defined core areas, located on public transport nodes and routes.

Support big box retail development that compliments existing and planned retail uses in its catchment.

Ensure the social impacts of developments accommodating more than 100 dwellings are considered and addressed through the preparation of a Social Impact Assessment.

Support mid-rise buildings with landscaped frontage around the perimeter of Wirraway North open space.

Support the provision of tree lined streets, small parks, plazas and playgrounds, with easy walking and cycling access to Westgate Park and Sandridge Beach.

Support the development of the intersection of Plummer Street and Salmon Street as the heart of Wirraway and focus of activity with an engaging pedestrian experience along Plummer Street Boulevard.

Facilitate a key public transport spine and interchange node along Plummer Street to provide direct connections to Sandridge, the CBD, Docklands and the Fishermans Bend National Employment and Innovation Cluster with:

- The extension of the Southern Tram Route.
- Bus routes.
- The potential for an underground metro rail station at the junction with Salmon Street.

Support JL Murphy Reserve as a focus for active recreation with organised sports during the day and night.

Support the creation of new open space at Prohasky Reserve, and in Wirraway North (W1) and Wirraway East (W4), linked by green linear parkway and a network of smaller open spaces.

Support the delivery of new and upgraded bridges over the Westgate Freeway at Rocklea Drive, Salmon Street, Thackray Road and Graham Street that provide public transport, bike and pedestrian access to the Fishermans Bend Employment Precinct.

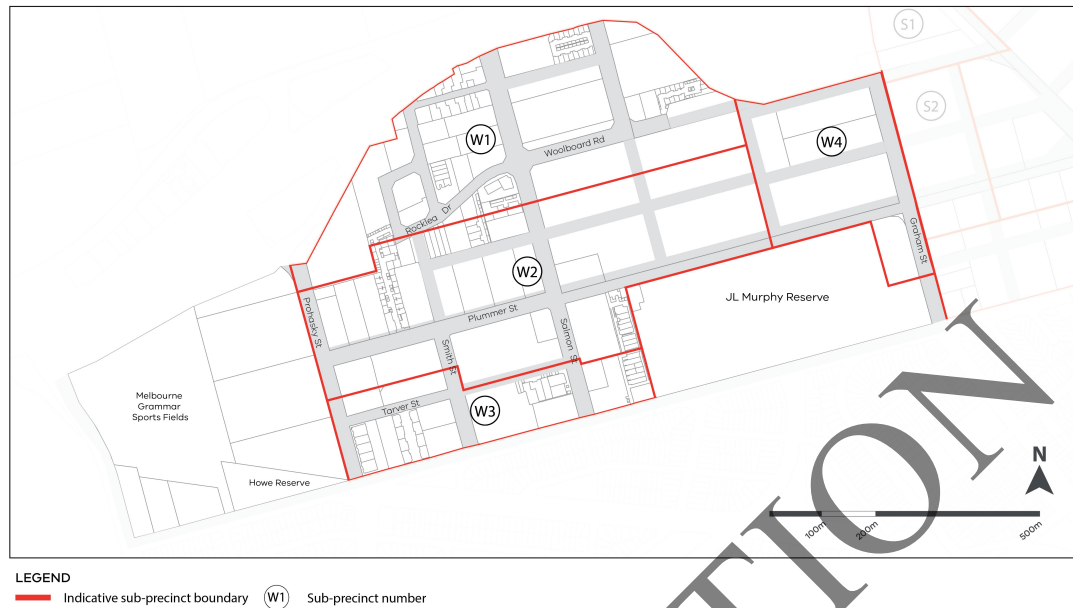
Support the delivery of the largest Arts and Cultural Hub in Fishermans Bend near the Southern Tram Route along Plummer Street in the 'investigation areas' shown on Map 2.

Support the delivery of an Education and Community Hub (secondary) and an Education and Community Hub (primary) in the 'investigation areas' shown on Map 2, close to open space and the tram route.

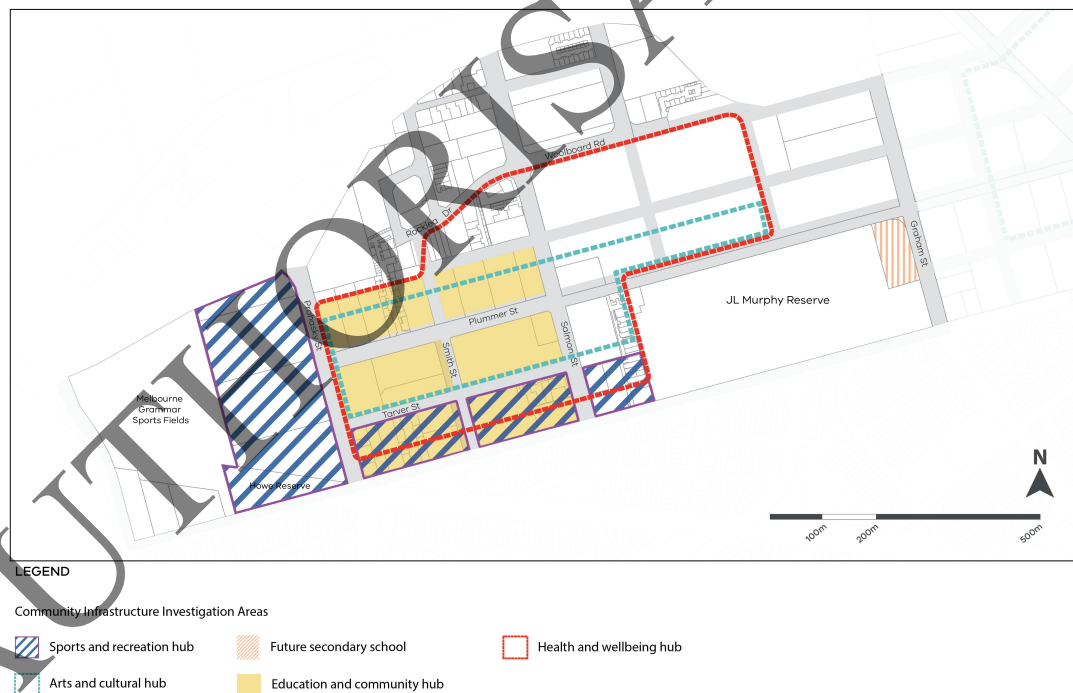
Support the delivery of a centrally located Health and Well-Being Hub within the 'investigation area' shown on Map 2.

Support the delivery of a Sports and Recreation Hub within the 'investigation area' adjoining Williamstown Road shown on Map 2.

Map 1: Sub-precincts within the Wirraway precinct



Map 2: Community infrastructure investigation areas within the Wirraway precinct



11.03-6L-08 South Melbourne

Proposed C219port

Policy application

This policy applies to all land in the South Melbourne Major Activity Centre, South Melbourne Enterprise Precinct and Kings Way Mixed Use Corridor as shown on Plan 1 to this clause.

Activities and uses

South Melbourne Major Activity Centre

Objective

To enhance the South Melbourne Major Activity Centre's provision of retail, employment, housing and entertainment opportunities within a well-connected, accessible, well-designed and sustainable community.

Strategies

Support the provision of retail, employment, housing and community uses, anchored by retail strips along Clarendon Street, Coventry Street and Park Street.

Support activities and uses that strengthen retail strips along Clarendon Street, Coventry Street and Park Street.

Encourage uses and development that enhances heritage places.

Provide opportunities for ongoing reinvestment, mixed use development and new employment floorspace in areas close to public transport.

Facilitate mixed-use development by:

- Retaining the Commercial 1 Zone.

Encourage housing that:

- Enhances the South Melbourne Major Activity Centre's viability and vibrancy.
- Provides diverse dwelling typologies and tenures.
- Provide adequate floor-to-floor heights that are adaptable to changes in future land use.
- Demonstrates high quality building design, promotes sustainable building design and internal amenity.
- Provides affordable housing for rent and purchase.

Support the retention of, upgrade of and provision of social and public housing.

Support and grow South Melbourne's economic specialisations and retail opportunities, and accommodate ongoing demand for office space in South Melbourne by:

- Retaining the Commercial 1 Zone.
- Protecting and advancing South Melbourne as a creative industries cluster.
- Encouraging the South Melbourne Major Activity Centre grow and improve its hospitality and specialty food offering.
- Accommodating projected employment growth and ongoing demand for office space, especially in small and medium premises.
- Preserving and enhancing the amenity of streets.
- Preserving valued aspects of South Melbourne's heritage and physical character.
- Maintaining a distinction between the low to medium rise character of South Melbourne and high rise buildings in the Central City, Southbank, Montague (Fishermans Bend) and along Kings Way.

Provide a range of affordable business spaces which suit micro, small and medium businesses by:

- Encouraging the retention and creation of affordable workspaces.
- Supporting the growing role and utilisation of coworking spaces.
- Support the renewal of the South Melbourne Market and its interface with the public realm.

South Melbourne Enterprise Precinct

Objective

To enhance the South Melbourne Enterprise Precinct's role in providing diverse employment opportunities, facilitating innovation, enterprise and creativity, and providing a complementary function to the South Melbourne Major Activity Centre.

Strategies

Support the provision of a mix of employment uses.

Support activities and uses that strengthen the South Melbourne Enterprise Precinct's employment focus, including restricted retail premises, specialty shops and convenience shops.

Support supermarkets within the South Melbourne Enterprise Precinct where it is located adjacent to the South Melbourne Major Activity Centre and is well connected to the South Melbourne Market and retail shopping strips on Clarendon Street, Coventry Street or York Street.

Encourage uses and development that enhances heritage places.

Provide opportunities for ongoing reinvestment, mixed use development and new employment floorspace in areas close to public transport.

Support and grow South Melbourne's economic specialisations and retail opportunities, and accommodate ongoing demand for office space in South Melbourne by:

- Retaining the Commercial 2 Zone in the South Melbourne Enterprise Precinct.
- Protecting and advancing South Melbourne as a creative industries cluster.
- Encouraging the South Melbourne Enterprise Precinct grow and improve its hospitality and specialty food offering.
- Accommodating projected employment growth and ongoing demand for office space, especially in small and medium premises.
- Preserving and enhancing the amenity of streets.
- Preserving valued aspects of South Melbourne's heritage and physical character.
- Maintaining a distinction between the low to medium rise character of South Melbourne and high rise buildings in the Central City, Southbank and along Kings Way.

Provide a range of affordable business spaces which suit micro, small and medium businesses by:

- Encouraging the retention and creation of affordable workspaces.
- Supporting the growing role and utilisation of coworking spaces.

Kings Way Mixed Use Corridor

Objective

To support the Kings Way Mixed Use Corridor's integration and relationship with the South Melbourne Enterprise Precinct and interface with residential areas, along with reinforcing its primary function to accommodate greater residential densities.

Strategies

Facilitate mixed-use development by:

- Retaining the Mixed Use Zone.

Encourage housing that:

- Enhances the Kings Way Mixed Use Corridor's function to accommodate greater residential densities.

- Provides diverse dwelling typologies and tenures, accommodates changing work patterns, is sustainable and durable, and demonstrates high quality design and amenity.
 - Provides affordable housing for rent and purchase.
- Support the retention of, upgrade of and provision of social and public housing.

Built form in South Melbourne

Contextually responsive design:

Objective

To ensure development incorporates quality design that enhances South Melbourne's distinct characteristics.

Element 1 - Quality Design:

Strategies

Encourage human scaled and diverse building types that are mainly mid-rise with some higher built form in specified areas.

Design buildings to:

- Ensure development responds to the valued attributes of South Melbourne and contributes positively to the existing and future character within each precinct.
- Reinforce the fine grain, vertical rhythm and visual interest of streetscapes.
- Ensure development respects the height, scale and proportions of adjoining heritage places and residential areas.
- Maintain sunlight to key streets and open spaces.
- Maintain the existing street wall height of heritage places while enabling taller street wall heights in non-heritage areas.

Protect the following landmark views:

- The clock tower of South Melbourne Town Hall when viewed from the footpath on the south-eastern corner at the intersection of Clarendon and Bank streets.
- The Shrine of Remembrance when viewed along Bank Street from Clarendon Street.
- Looking north along Clarendon Street towards Melbourne's CBD and Southbank.
- Looking south along Clarendon Street towards Albert Park.

Policy guidelines

Consider as relevant:

- Development reinforces the fine grain, vertical rhythm and visual interest of streetscapes.
- Development incorporates a street wall that reinforces the built form character and reflects the street hierarchy.
- Development provides depth and detail to all visible facades using high quality materials and visually rich details in lower levels.
- Development incorporates awnings or verandahs that are responsive to the form and scale of adjoining verandahs.
- Taller built form is located to minimise visual bulk and overshadowing.

- Development on larger sites includes separate building elements to reduce visual bulk, maintains sky views from the street and improves internal amenity and environmentally sensitive design outcomes.
- Development includes splayed corners which are a prominent urban and architectural feature of the area, where appropriate.

Element 2 – Upper Level Design

Strategies

- Incorporate an architectural expression at upper levels that is distinct from but complementary to the street wall.
- Set upper levels back from the street wall to ensure that upper level additions as seen from the public realm do not detract from the character of the streetscape when viewed directly or obliquely along the street.
- Enable simple and legible building forms and efficient floor plates by avoiding more than one upper level setback above the street wall.

Policy guidelines

Consider as relevant:

- Containing upper level setbacks above the street wall within a maximum of one step.
- Where land not subject to the Heritage Overlay adjoins a Significant- or Contributory-graded building, setting back upper-levels from the street wall to avoid visually overwhelming the adjacent heritage building.
- Matching the upper level setback of an adjoining heritage building for a minimum of 6 meters in length.

Element 3 – Upper level development behind heritage buildings

Strategies

Encourage upper level development that provide a sensitive distinction between the heritage fabric of lower building levels and the new upper building levels through changes in form, details and materials.

Encourage upper level development that is complementary of heritage fabric in terms of articulation, materiality and colour.

Policy guidelines

Consider as relevant:

- Utilising visually lightweight, but high quality, materials that create a juxtaposition with the heavier masonry typical of the heritage buildings.
- Incorporating simple architectural detailing so it does not detract from significant elements of the existing building or streetscape.
- Incorporating upper level setbacks above secondary street frontages to retain the visual prominence of return façades or end terrace side elevations that address a secondary street.

- Incorporating a further setback from the primary street frontage at the uppermost level, where appropriate.
- Incorporating materials and finishes that are recessive in texture and colour.

Engaging and walkable precincts:

Objective

To ensure development contributes to engaging and walkable precincts.

Element 1 – Frontages

Strategies

Provide high-quality frontages to streets and laneways.

Achieve universal design principles to provide equitable and dignified access.

Encourage a mix of small and medium tenancies along key pedestrian streets and laneways.

Avoid signage treatments that reduce activation and passive surveillance of the public realm by obstructing windows and doors.

Policy guidelines

Consider as relevant:

- Locating multiple entrances for larger tenancies and active frontages at the ground floor along the primary pedestrian streets, as shown in Plan 1.
- Incorporating vertical articulation in the street wall and upper levels that reflects and aligns with the prevailing pattern of subdivision and buildings.
- Providing continuous weather protection along main street frontages and other key pedestrian routes that allows for exposure to winter sun and shelter from summer sun.
- Carefully integrating signage into the design of the ground floor and surrounding public realm.

Element 2 - Pedestrian and bicycle access

Strategy

Provide pedestrian entrances that are clearly visible, safe, secure and have an identifiable sense of address.

Policy guidelines

Consider as relevant:

- Locating the primary pedestrian entrance to buildings, including upper level apartments from a street or lane.
- Designing resident and staff bicycle parking and end of trip facilities in an accessible, secured and convenient location from the street and associated uses.

Element 3 - Building services

Strategy

Avoid locating servicing and back of house areas on street frontages and other key pedestrian routes.

Policy guidelines

Consider as relevant:

- Minimising the extent of servicing located on main street frontages and other key pedestrian routes.
- Whether the design of building servicing is carefully integrated into the overall design, including of the ground floor and rooftop, to mitigate visual bulk impact when viewed from the public realm, adjoining properties and long distance views.
- Co-locating service cabinets internal to loading, waste or parking areas where possible to avoid impact on the public realm.

Element 4 - Vehicular access

Strategies

Design vehicle ingress and egress, including loading facilities and building servicing, to ensure a high quality pedestrian amenity and limit potential conflict between vehicle movements and pedestrian activity.

Enhance the amenity and safety of laneways that provide pedestrian and vehicular access to buildings.

Policy guidelines

Consider as relevant:

- Providing vehicular access from rear lanes or from side streets.
- Locating car parking within a basement or concealed from the public realm.

High amenity and flexible housing and workplaces

Objective

To provide high-amenity and flexible housing and workplaces.

Element 1 - Floor to floor heights

Strategy

Provide adequate floor-to-floor heights that support good internal amenity outcomes and are adaptable to changes in future land use over time, including addressing:

- Dwelling layout: The ability for one and two-bedroom dwellings to be combined or adapted into three or more bedroom dwellings.
- Internal layout: Minimal load bearing walls to maximise flexibility for retail or commercial refits.
- The floor-to-floor heights of car parking, including appropriately sleeving above ground car parking with active uses.

Policy guidelines

Consider as relevant:

- For non-residential development, incorporating a floor to floor height of 4 metres.
- For residential development, incorporating a floor to floor height of 3.2 metres.

Element 2 - Building separation

Strategies

Design buildings to:

- Allow adequate daylight to living rooms and bedrooms.
- Provide a reasonable outlook from living areas.
- Support equitable development by ensuring that the outlook from bedrooms and living areas is secured to the street or within the development site.
- Avoid visual screening to manage internal amenity and equitable development between sites by ensuring adequate building separation.
- Ensure that internal privacy is well managed through building separation and the careful location of windows.

Policy guidelines

Consider as relevant:

- Where development shares a common boundary whether the building separation distances specified in Table 1 has been achieved.
- Where two sites share a rear boundary, whether development abutting the boundary is maximum of 3 storeys subject to any daylighting and ventilation requirements.
- For sites with a side boundary to narrow laneways of equal to or less than 3 meters in width, development abutting the laneway is up to 22 metres or the height limit (whichever is lesser) if amenity is secured to streets or the rear of the property, and if the facade to the laneway is slender.
- For non-residential development, these requirements may be varied for sites with a depth of 16 metres or less that share a rear boundary where appropriate amenity can be achieved through a single outlook to the street.

Table 1 – Building separation distances

Building height	Living room, balcony and non-residential outlook - minimum site boundary separation	Bedroom outlook - minimum site boundary separation	Separation between multiple buildings on an individual site
Up to 22 metres	6 metres	3 metres	12 metres
Above 22 metres and up to 27 metres	9 metres	4.5 metres	18 metres
Above 27 metres			

Element 3 - Light Wells

Strategies

Avoid using light wells to provide daylight to buildings.

Design and locate windows in light spills to protect the privacy of occupants without relying solely on screening including through:

- Staggering light wells to avoid direct overlooking into existing bedroom windows in separate dwellings.

Design light wells to provide usable space at ground level and opportunities for landscaping.

Policy guidelines

Consider as relevant:

- Avoid designs which rely on multiple small light wells by consolidating light wells into a larger courtyard space.
- Meeting the minimum width and area dimensions for light wells specified in Table 2 and:
 - Providing daylight access to bedrooms only.
 - Painting lightwells in a light reflective colour.
 - Providing ground level access to the light well via a door.
- Applying the light well requirements specified in Table 2 from the first level of residential use. The light well minimum width and area dimensions may be varied for buildings containing multiple levels of non-residential uses.

Table 2 - Light well dimensions

Building height	Light well overall area	Light well minimum width
Up to 22 metres	18 square metres	3 metres
Above 22 metres and up to 27 metres	36 square metres	4.5 metres
Above 27 metres	54 square metres	6 metres

Climate responsive design

Objective

To encourage development to integrate climate responsive design.

Strategies

Support best practice environmentally sensitive design outcomes.

Support increased urban greening through green walls and green roofs.

Where practicable, developments should create opportunities to reduce waste:

- Optimise waste storage and efficient collection methods.
- Combine commercial and residential waste storage.
- Share storage or collections with adjacent developments.
- Separate collection for recycling, hard waste, and food and green waste.

Policy guidelines

Consider as relevant:

- Providing deep soil zones to support in ground planting and canopy trees in the private realm, where appropriate.
- Providing on site flood mitigation and Water Sensitive Urban Design (WSUD).

Access and movement

Objective

To support South Melbourne as a place that is connected and easy to move around for all.

Strategies

Enhance South Melbourne's movement network to prioritise sustainable and active travel, while improving capacity and circulation.

Provide an attractive and safe local pedestrian and cycling network that connects key destinations and strategic corridors from South Melbourne to the Montague (Fishermans Bend), Southbank and Domain, as well as Anzac Station.

Encourage development to contribute to the upgrade of existing streets, laneways and public spaces adjoining the site to support the provision of an attractive and safe local pedestrian and cycling network.

Support improvements to public transport services and access to meet the needs of South Melbourne's residents, workers and visitors.

Encourage car parking and loading approaches that support more sustainable modes of transport and reduce the impacts of vehicle congestion, including car share.

Public realm

Objective

To create public spaces that are attractive, vibrant, climate-resilient and people-friendly.

Strategies

Create cool, green streets for people to improve amenity, comfort and public health by:

- Addressing gaps in canopy tree coverage.
- Softening the interface between buildings and the public realm.
- Incorporating innovative and best practice approaches to mitigate climate change impacts.
- Ensuring plantings create habitat and promote pollination and biodiversity connections for flora and fauna.
- Providing shaded places for people to enjoy, meet and relax under.

Encourage development to contribute to the upgrade of existing streets, laneways and public spaces adjoining the site to support the creation of cool, green streets and mitigate the urban heat island effect.

Development incorporates enhancements to Integrated Water Management approaches, including using Water Sensitive Urban Design (WSUD) principles.

Reinforce civic pride and sense of place by:

- Developing the South Melbourne Town Hall and Emerald Hill Precinct as the major focus of cultural activity and an integrated network of civic, cultural and community facilities for the local and wider community.
- Protecting and interpreting Aboriginal cultural values and heritage in the design of the public realm of South Melbourne.

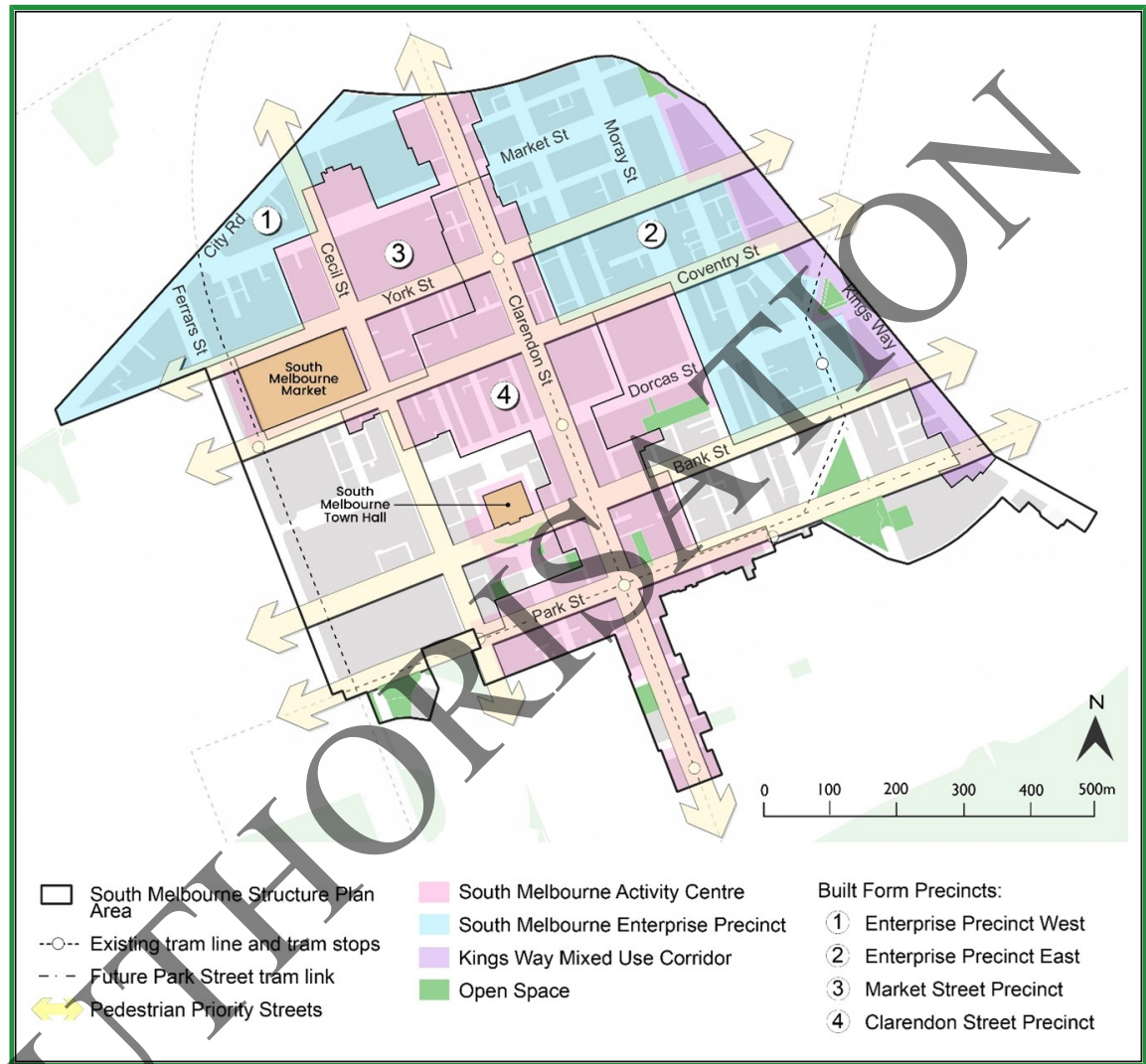
Development supports the provision of public spaces to accommodate a variety of uses and enterprises.

Policy documents

Consider as relevant:

- *South Melbourne Structure Plan* (City of Port Phillip, July 2024).

PLAN 1: SOUTH MELBOURNE STRUCTURE PLAN AREA



08/07/2024
G462port**SCHEDULE 16 TO CLAUSE 43.02 DESIGN AND DEVELOPMENT OVERLAY**Shown on the planning scheme map as **DDO16**.**GNR YORK, CECIL & MARKET STREETS, SOUTH MELBOURNE****1.0**08/07/2024
G462port**Design objectives**

To ensure new development respects and enhances the valued elements of the built heritage, including principal landmarks and view lines which add to the area's identity and assist orientation; these include the South Melbourne Town Hall clock tower, the Westgate Freeway, Park Towers and the high rise buildings in Southbank and the CAD.

To recognise that larger lot sizes and amalgamation of lots provide particular opportunities for more intensive development.

To foster a new and distinct urban character that complements the remnant industrial nature of the site and surrounds.

To ensure new development complements the diversity of the urban fabric and character, reflects the incremental layers of development and activity in South Melbourne and responds to the fine grain of the surrounding area and subdivision.

To ensure new development is of a high standard of architectural and urban design.

To retain sunlight access to the principle activity streets and the South Melbourne Market all year round.

To ensure that new development on sites containing or adjacent to a heritage place is of a form and scale that is respectful of the heritage place.

To encourage pedestrian movement and links to, from and through the site and to other activity nodes, such as the South Melbourne Market, Clarendon Street and Coventry Street.

To ensure that active frontages are achieved and are designed to provide shop entrances or display windows facing the street, and avoid blank walls, non-transparent detail and non-retail uses.

To ensure new development responds and relates to the South Melbourne Market, on the diagonally opposite corner of York and Cecil Streets, and adds to this existing activity node, by including:

- A focus of pedestrian activity and entrances on the corner of York and Cecil Streets.
- An orientation to and high quality active design interfaces on York and Cecil streets especially.

To ensure new development maintains the network of laneways by maintaining the public role of laneways for circulation, service access areas and through block pedestrian connections.

To ensure new development is designed to provide for commercial activity on the lower floors by providing higher floor to floor at ground and first floor levels.

2.008/07/2024
G462port**Buildings and works****Height**

Buildings and works are considered to meet the design objectives for height if:

- They do not exceed the preferred maximum height specified in the table to this schedule.
- Architectural features such as air scoops, domes, towers, masts and building services do not exceed the preferred maximum height by more than 4 metres and do not exceed 10 per cent of the gross floor area of the top of the building level.

The responsible authority will only consider a permit to exceed the preferred maximum height where it can be clearly demonstrated that the proposed development will continue to meet all the relevant design objectives, achieve all the outcomes specified in the table to this schedule and not result in any additional detriment to the amenity of any surrounding public realm or private residential areas as a result of increased height.

Setbacks

Buildings and works are considered to meet the design objectives for setbacks if:

- They are no less than the setback distance from boundary specified in the table to this schedule.
- Setbacks of any heritage place adjoining the land are matched. If the land is adjoining more than one heritage place then the setback should be no greater than the largest setback and no less than the smallest setback.

Buildings should not comprise more than one step in their setbacks.

Minor buildings and works, such as verandahs, architectural features, balconies, shelters, sunshades, art works, street furniture, fences, and also basements which do not project above ground level, may be constructed within the setback distance from boundaries specified in the table to this schedule.

The responsible authority will only consider a permit to vary the setback distance from the distance from the boundary specified in the table, where it can be demonstrated to the satisfaction of the responsible authority that the variation will:

- Achieve the design objectives.
- Allow the retention of a heritage place on the site, or have less impact upon the significance of a heritage place on the site or on any adjoining or nearby property.
- Be consistent with the surrounding built form.

Building design

Buildings and works are considered to meet the design objectives for building design if:

- At ground floor level, floor-to-floor heights are at least 4.5 metres;
- At first floor level, floor-to-floor heights are at least 3.5 metres.

Overshadowing

Buildings must not overshadow the footpath on the southern side of York Street between 11:00am and 2:00pm on 21 June.

Buildings must not overshadow the footpath on the western side of Cecil Street opposite the Market after 10:00am on 21 June.

The footpath is defined here as the space extending 6 metres from the property line of the buildings.

Table 1 to Schedule 16 to Clause 43.02

Area	Distance from boundary	Preferred maximum building height	Outcome
DDO16	0m from York and Market Streets	11m	Sunlight access to the southern side of York Street is maintained. A pedestrian scale that complements the existing hard edged character.
	0m from Cecil Street	10m	Sunlight access to the western side of Cecil Street is protected

Area	Distance from boundary	Preferred maximum building height	Outcome
			A pedestrian-scale that complements the existing hard edged character and the South Melbourne Market.
	7m or more from York and market Streets	19.5m	Sunlight access to the southern side of York Street is maintained: Upper levels are setback to read as a separate building form and reduce building bulk.
	5--10m or more from Cecil Street	19.5m	Sunlight access to the western side of Cecil Street is maintained: Upper levels are setback to read as a separate building form and reduce building bulk.

3.008/07/2024
G162port**Subdivision**

None specified.

4.008/07/2024
G162port**Signs**

None specified.

5.008/07/2024
G162port**Application requirements**

None specified.

6.008/07/2024
G162port**Decision guidelines**

None specified.

SCHEDULE TO CLAUSE 43.01 HERITAGE OVERLAY**1.0****Application requirements**

The following application requirements apply to an application under Clause 43.01, in addition to those specified elsewhere in the planning scheme and must accompany an application, as appropriate, to the satisfaction of the responsible authority:

- For applications to alter, extend or demolish a heritage place, building or structure a report prepared by a suitably qualified heritage consultant, that assesses the impact of the application upon the significance of the heritage place, building or structure. This report should demonstrate how the application has considered and responded to the objectives of this clause and the heritage strategies and policy guidelines in Clause 15.03.
- For applications that propose full or substantial demolition of a heritage place, building or structure:
 - Plans and elevations showing the extent of all buildings and features to be demolished or removed.
 - A structural engineering report prepared by a suitably qualified person that assesses the condition of the heritage place, building or structure and whether any defects can be rectified.
 - A demolition method statement that describes how any retained contributory features will be protected and conserved during demolition and construction.
- For applications to paint a heritage place, building or structure, a schedule, plans and elevations showing the colours and finishes to be used on all surfaces, when external paint controls are triggered.
- For applications that include changes to contributory features, at least one image of the existing feature.
- For applications that include restoration or reconstruction of original fabric, plans prepared at a scale that clearly shows the proposed details.
- For an addition to a heritage place, building or structure or new development:
 - Fully scaled and dimensioned site plan showing existing and proposed circumstances including outbuildings, fences, significant vegetation, car parking, new crossovers, onsite parking space locations and any other noteworthy features.
 - A site context report that identifies contributory features of surrounding buildings including building form, scale, siting, massing, materiality and detailing, and fence types and heights.
 - Photographs of the existing streetscape.
 - For additions, sightline diagrams in accordance with the relevant policy guidelines for additions in Clause 15.03-1L.

- When located within a precinct, elevations that show the addition or new development in the context of the streetscape if they are visible from the public realm.
- Three dimensional illustrative renders or photographic montages showing views of the addition or development in the context of the streetscape including from oblique (side on) views. For corner sites, this should include views of the addition from side streets or laneways.
- For applications to remove a tree with identified heritage value a report prepared by a suitably qualified person in relation to the health, condition and expected lifespan of the tree and why removal is necessary.
- For applications to prune or lop a tree or for development in proximity to a tree with identified heritage value a report prepared by a suitably qualified person that assesses potential impacts upon the health and viability of the tree, when external tree controls are triggered.

2.0

Heritage places

01/08/2024 - / - / -
G499port Proposed C219port The requirements of this overlay apply to both the heritage place and its associated land.



PS map ref	Heritage place	External paint controls apply?	Internal alteration controls apply?	Tree controls apply?	Solar energy system controls apply?	Outbuildings or fences not exempt under Clause 43.01-4	Included on the Victorian Heritage Register under the Heritage Act 2017?	Prohibited uses permitted?	Aboriginal heritage place?
HO1	<i>Port Melbourne</i> Area generally bound by Clark Street to the north, Ingles and Boundary Streets to the east, Pickles Street to the south and Graham Street to the west	Yes	No	Yes – but limited to the Port Melbourne Light Rail Reserve, area zoned PPRZ.	Yes	No	No	No	No
HO2	<i>The Garden City Housing Estates</i> Port Melbourne	Yes	No	No	Yes	No	No	No	No
HO4	<i>City Rd Industrial Area</i> South Melbourne	Yes	No	No	Yes	No	No	No	No
HO5	<i>St Kilda Hill</i>	Yes	No	No	Yes	No	No	No	No

PORT PHILLIP PLANNING SCHEME

PS map ref	Heritage place	External paint controls apply?	Internal alteration controls apply?	Tree controls apply?	Solar energy system controls apply?	Outbuildings or fences not exempt under Clause 43.01-4	Included on the Victorian Heritage Register under the Heritage Act 2017?	Prohibited uses permitted?	Aboriginal heritage place?
	Area generally bound by Fitzroy St to the North, Barkly St to the east, Carlisle St to the south and Port Phillip Bay to the west								
HO6	<i>St Kilda East</i> Area generally bound by Wellington Rd and Dandenong Rd to the north, Inkerman Rd to the south, St Kilda Rd to the west and Orrong Rd to the east	Yes	No	No	Yes	No	No	No	No
HO7	<i>St Kilda, Elwood, Balaclava, Ripponlea</i> Area generally bound by Carlisle St to the north, Glenhuntly Rd to the south, Hotham St to the east and Mitford St and Broadway to the west	Yes	No	No	Yes	No	No	No	No
HO8	<i>Elwood-Glen Huntly Rd, Ormond Rd</i> Area generally comprising Glen Huntly Rd as the east-west spine, extending north just beyond Shelley St between Marine Parade and the Elwood Canal, and south generally between Ormond Rd and Ormond Esplanade	Yes	No	No	Yes	No	No	No	No
HO318	<i>Brighton Rd (Elwood)</i> Brighton Rd, Burns St, Glenhuntly Rd, Heaton St	Yes	No	No	Yes	No	No	No	No
HO316	Carlisle St (East Carlisle St, Hawsleigh Ave	Yes	No	No	Yes	No	No	No	No
HO30	<i>Emerald Hill Estate</i>	-	-	-	-	-	Yes Ref No H1136	Yes	No

PORT PHILLIP PLANNING SCHEME

PS map ref	Heritage place	External paint controls apply?	Internal alteration controls apply?	Tree controls apply?	Solar energy system controls apply?	Outbuildings or fences not exempt under Clause 43.01-4	Included on the Victorian Heritage Register under the Heritage Act 2017?	Prohibited uses permitted?	Aboriginal heritage place?
	Bank, Clarendon, Park, Cecil, Dorcas, Marshall, Fishley, Layfield and Perrins Streets, South Melbourne								
HO317	<i>Hotham St (Balaclava)</i> Nos. 113-125 Hotham St	Yes	No	No	Yes	No	No	No	No
HO315	<i>Inkerman St (East)</i> Chapel St, Inkerman St	Yes	No	No	Yes	No	No	No	No
HO258	<i>St Vincent Place Precinct</i> 1-99 St Vincent Place South and 2-94 St Vincent Place North and St Vincent Gardens East and St Vincent Gardens West and 10 Merton St and Montague St (Part), Albert Park	-	-	-	-	-	Yes Ref No H1291 (see also HO340/H813)	Yes	No
HO382	<i>Swallow Street Precinct</i> 49-77 Swallow Street, Port Melbourne	Yes	No	No	Yes	No	No	No	No
HO385	<i>Chusan Street Precinct</i> 1-37 Chusan Street, East St Kilda	Yes	No	No	Yes	No	No	No	No
HO386	<i>Godfrey Av/Raglan Street Precinct</i> 1-41, 2-36 Godfrey Avenue and 9-23 Raglan Street, East St Kilda	Yes	No	No	Yes	No	No	No	No
HO387	<i>Hammerdale Av Precinct</i> 2-24, 28-32, 3-17 Hammerdale Avenue, 2-8 Jervois Street, East St Kilda	Yes	No	No	Yes	No	No	No	No
HO388	<i>Holroyd Court Precinct</i> 1-7 Holroyd Court, East St Kilda	Yes	No	Yes – 4 mature camphor laurel	Yes	No	No	No	No

PORT PHILLIP PLANNING SCHEME

PS map ref	Heritage place	External paint controls apply?	Internal alteration controls apply?	Tree controls apply?	Solar energy system controls apply?	Outbuildings or fences not exempt under Clause 43.01-4	Included on the Victorian Heritage Register under the Heritage Act 2017?	Prohibited uses permitted?	Aboriginal heritage place?
				street trees (cinnamomum camphora)					
HO389	<i>Kalymna Grove/Inkerman St Precinct</i> 1-35, 2-30 Kalymna Gr, 342-374 Inkerman Street, East St Kilda	Yes	No	No	Yes	No	No	No	No
HO390	<i>Mooltan Av Precinct</i> 10-16, 9-15 Mooltan Avenue, East St Kilda	Yes	No	No	Yes	No	No	No	No
HO391	<i>St Kilda East: Murchison Street & Wavenhoe Avenue Precinct</i> 1-29, 2-24 Murchison St, 20, 22, 26 Lansdowne Street, 160-188, 183-211 Alma Road, 57, 69, 71, 73, 75 and 77 Alexandra Street, 157, 159 and 161 Alma Road, 28, 30, 32, 34,36, 38, 40, 42 and 44 Hotham Street, 18 and 26A Lansdowne Road, 1-13, 15, 17, 19 and 21 Wavenhoe Avenue, 2 and 4 Mooltan Avenue (duplex at corner of Hotham Street only), St Kilda East	Yes	No	No	Yes	No	No	No	No
HO392	<i>Orange Grove Precinct</i> 4-30, 11-33 Orange Gr and 331 Inkerman Street, East St Kilda	Yes	No	No	Yes	No	No	No	No
HO402	<i>Elwood Canal Precinct</i> Area covered by Elwood Canal and riparian margins, from Port Phillip Bay foreshore to St Kilda Street municipal boundary.	No	No	Yes	Yes	Yes	No	No	No

PORT PHILLIP PLANNING SCHEME

PS map ref	Heritage place	External paint controls apply?	Internal alteration controls apply?	Tree controls apply?	Solar energy system controls apply?	Outbuildings or fences not exempt under Clause 43.01-4	Included on the Victorian Heritage Register under the Heritage Act 2017?	Prohibited uses permitted?	Aboriginal heritage place?
HO403	<i>Addison Street /Milton Street Precinct (Elwood)</i> 2 – 74 & 11-75 Addison St; 296 – 350 & 323 - 377 Barkly St; 77 – 85 Dickens St; 7 Hood St, 1 – 7 & 2 - 10 Lawson St; 1 – 12 Lytton St; 52 – 54 Marine Pde; 6 – 56 & 9 – 37 Meredith St; 53 – 83 & 66 – 100 Milton St; 6 – 24, 29 & 46 - 84 Ruskin St; 1 – 23 & 2 - 8 Thackeray St; Elwood	Yes	No	No	Yes	No	No	No	No
HO404	<i>Byron Street / Mason Avenue Precinct (Elwood)</i> 2 – 24 & 9 -23 Byron St; 1 – 17 & 2 –18 Cyril St; 1 – 37 Mason St; 1 – 33 & 2 - 28 Moore St; Elwood	Yes	No	No	Yes	No	No	No	No
HO405	<i>McCrae Street Precinct (Elwood)</i> 31 – 33 John St; 1 – 13 & 2 – 14 McCrae St; Elwood	Yes	No	No	Yes	No	No	No	No
HO439	<i>Nightingale Street Precinct (Balaclava)</i> 31 – 55 Marlborough St, 24 – 58 Rosamond St, 27 – 57 Rosamond St, 32 - 58 Nightingale St, 19 - 47 Nightingale St, 19 & 21 Woodstock St, 1 – 17 Gibbs St, 2 – 20 Gibbs St and 1 to 5 Railway Place, Balaclava.	Yes	No	No	Yes	No	No	No	No
HO440	<i>Emerald Hill Residential Precinct</i>	Yes	No	No	Yes	No	No	No	No

PORT PHILLIP PLANNING SCHEME

PS map ref	Heritage place	External paint controls apply?	Internal alteration controls apply?	Tree controls apply?	Solar energy system controls apply?	Outbuildings or fences not exempt under Clause 43.01-4	Included on the Victorian Heritage Register under the Heritage Act 2017?	Prohibited uses permitted?	Aboriginal heritage place?
	Area generally bound by Pickles St, York St and Kings Way to the north, Queens Rd to the east, Albert Rd to the south and Nelson and Ferrars St to the west								
HO441	<i>St Vincent Place East</i> South Melbourne Bound By Park St, Cecil St, Albert Rd and Ferrars St	Yes	No	No	Yes	No	No	No	No
HO442	<i>Albert Park Residential Precinct/ Part Montague Precinct</i> Area generally bound by Pickles St to the North, Ferrars St to the east, Kerferd Rd to the south and Beaconsfield Parade to the west (including shops and dwellings along City Road/ Montague Street)	Yes	No	No	Yes	No	No	No	No
HO443	<i>Bridport Street / Victoria Avenue Commercial Precinct</i> Albert Park	Yes	No	No	Yes	No	No	No	No
HO444	<i>Middle Park and St Kilda West Precinct</i> Bound by Kerferd Rd, Canterbury Rd, Fitzroy St and Beaconsfield Parade	Yes	No	No	Yes	No	No	No	No
HO445	<i>Armstrong Street Commercial Precinct</i> Middle Park	Yes	No	No	Yes	No	No	No	No
HO446	<i>Albert Park Lake Precinct</i>	Yes	No	No	Yes	No	No	No	No

PORT PHILLIP PLANNING SCHEME

PS map ref	Heritage place	External paint controls apply?	Internal alteration controls apply?	Tree controls apply?	Solar energy system controls apply?	Outbuildings or fences not exempt under Clause 43.01-4	Included on the Victorian Heritage Register under the Heritage Act 2017?	Prohibited uses permitted?	Aboriginal heritage place?
	Area generally bound by Fitzroy St to the south, Queens Road to the east, Albert Road to the North and Canterbury Road to the west Incorporated plan Albert Park Master plan								
HO513	<i>Montague Commercial Precinct</i> Area including 496-546 City Road and 151-163 Montague Street, South Melbourne Statement of Significance: Montague Commercial Precinct, February 2021	No	No	No	Yes	No	No	No	No
HO9	<i>Christ Church complex</i> 14 Acland St and 1 St Leonards Ave, St Kilda	-	-	-	-	-	Yes Ref No H996	Yes	No
HO10	<i>Linden</i> 26 Acland St, St Kilda	-	-	-	-	-	Yes Ref No H213	Yes	No
HO11	<i>Halcyon</i> 53 Acland St, St Kilda	-	-	-	-	-	Yes Ref No H775	Yes	No
HO12	<i>South African Soldiers Memorial</i> 29A Albert Rd, Melbourne	-	-	-	-	-	Yes Ref No H1374	Yes	No
HO319	<i>Former BP House</i> 1-29 Albert Rd, Melbourne	Yes	No	No	Yes	No	No	No	No
HO320	<i>Offices</i>	Yes	No	No	Yes	No	No	No	No

PORT PHILLIP PLANNING SCHEME

PS map ref	Heritage place	External paint controls apply?	Internal alteration controls apply?	Tree controls apply?	Solar energy system controls apply?	Outbuildings or fences not exempt under Clause 43.01-4	Included on the Victorian Heritage Register under the Heritage Act 2017?	Prohibited uses permitted?	Aboriginal heritage place?
	31-33 Albert Rd, Melbourne								
HO334	<i>Former Residence</i> 42 Albert Rd, Melbourne	Yes	No	No	Yes	No	No	No	No
HO361	<i>Houses</i> 140-144 Albert Road, South Melbourne	Yes	No	No	Yes	No	No	No	No
HO312	<i>Flats</i> 194 Albert Rd, South Melbourne	Yes	No	No	Yes	No	No	No	No
HO13	<i>Dalkeith</i> 314 Albert Rd, South Melbourne	-	-	-	-	-	Yes Ref No H840	Yes	No
HO525	<i>Former Royal Australian Corps of Signals Drill Hall</i> 29A Albert Road Drive South, Albert Park	-	-	-	-	-	Yes Ref No H2362	Yes	No
HO14	<i>Ngargee Tree</i> Albert Park	No	No	Yes	Yes	No	No	No	Yes
HO15	<i>South Melbourne Cricket and Football Club grandstand</i> Albert Road Drive, Albert Park	Yes	Yes	No	Yes	No	No	No	No
HO393	<i>Houses</i> 31-35 Alexandra Street, East St Kilda	Yes	No	No	Yes	No	No	No	No
HO19	<i>Holmwood, including Canary Island date palm (6) jacaranda (2) Bhutan cypress (1)</i> 61 Alexandra St, Balaclava	Yes	Yes	Yes	Yes	No	No	No	No
HO18	<i>South African War Memorial</i>	-	-	-	-	-	Yes	Yes	No

PORT PHILLIP PLANNING SCHEME

PS map ref	Heritage place	External paint controls apply?	Internal alteration controls apply?	Tree controls apply?	Solar energy system controls apply?	Outbuildings or fences not exempt under Clause 43.01-4	Included on the Victorian Heritage Register under the Heritage Act 2017?	Prohibited uses permitted?	Aboriginal heritage place?
	9 Alfred Square, St Kilda						Ref No H1375		
HO16	<i>House</i> 1 Alfred Square, St Kilda	Yes	Yes	No	Yes	No	No	No	No
HO17	<i>House</i> 2 Alfred Square, St Kilda	Yes	Yes	No	Yes	No	No	No	No
HO31	<i>Presbyterian Church</i> 2 Alma Rd, St Kilda	Yes	Yes	No	Yes	No	No	No	No
HO20	<i>Former Priory Ladies School</i> 61 Alma Rd, St Kilda	-	-	-	-	-	Yes Ref No H726	Yes	No
HO21	<i>Belmont Flats</i> 86 Alma Rd, St Kilda	-	-	-	-	-	Yes Ref No H805	Yes	No
HO350	<i>House</i> 127 Alma Rd, St Kilda East	Yes	No	No	Yes	No	No	No	No
HO23	<i>House</i> 200 Alma Rd, St Kilda East	Yes	No	No	Yes	No	No	No	No
HO25	<i>House</i> 217 Alma Rd, St Kilda East	Yes	No	No	Yes	No	No	No	No
HO407	<i>House</i> 6 Austin St, Elwood	Yes	No	No	Yes	No	No	No	No
HO408	<i>Zone Substation</i> 36 Austin St, Elwood	Yes	No	No	Yes	No	No	No	No

PORT PHILLIP PLANNING SCHEME

PS map ref	Heritage place	External paint controls apply?	Internal alteration controls apply?	Tree controls apply?	Solar energy system controls apply?	Outbuildings or fences not exempt under Clause 43.01-4	Included on the Victorian Heritage Register under the Heritage Act 2017?	Prohibited uses permitted?	Aboriginal heritage place?
HO313	<i>House</i> 13 Baker St, St Kilda	Yes	No	No	Yes	No	No	No	No
HO394	<i>Former Factory</i> 5-7A Balston Street, East St Kilda	Yes	No	No	Yes	No	No	No	No
HO395	<i>Cottage</i> 16 Balston Street, East St Kilda	Yes	No	No	Yes	No	No	No	No
HO396	<i>Substation</i> 52 Balston Street, East St Kilda	Yes	No	No	Yes	No	No	No	No
HO369	Terrace Row, houses and Algerian Oak (<i>Quernac canariensis</i>) at the rear of 145 Bank Street and land beneath the canopy for a distance of five metres from the canopy edge. 141-147 Bank Street, 1-5 Palmer Street and 2-4 Wilson Street, South Melbourne	Yes	No	Yes	Yes	No	No	No	No
HO27	<i>Former South Melbourne Post Office</i> 195-207 Bank St, South Melbourne	-	-	-	-	-	Yes Ref No H1771	No	No
HO28	<i>South Melbourne Town Hall</i> 208-220 Bank St, South Melbourne	-	-	-	-	-	Yes Ref No H217	Yes	No
HO29	<i>South Melbourne Court House and Police Station</i> 209-213 Bank St, South Melbourne	-	-	-H	-	-	Yes Ref No H1486	Yes	No
HO341	<i>Former Lauders Riding School (Emerald Hall)</i>	-	-	-	-	-	Yes	Yes	No

PORT PHILLIP PLANNING SCHEME

PS map ref	Heritage place	External paint controls apply?	Internal alteration controls apply?	Tree controls apply?	Solar energy system controls apply?	Outbuildings or fences not exempt under Clause 43.01-4	Included on the Victorian Heritage Register under the Heritage Act 2017?	Prohibited uses permitted?	Aboriginal heritage place?
	295 Bank St, South Melbourne						Ref No H1749		
HO32	<i>The Manse</i> 42 Barkly St, St Kilda	-	-	-	-	-	Yes Ref No H212	Yes	No
HO285	<i>House</i> 52 Barkly St, St Kilda	Yes	No	No	Yes	No	No	No	No
HO370	<i>Hampden, including two mature Canary Island Date Palms, scoria rock edging and tapered rough cast gate posts.</i> 74 Barkly Street, St Kilda	Yes	No	Yes	Yes	No	No	No	No
HO33	<i>Village Belle Hotel</i> 202 Barkly St, St Kilda	Yes	Yes	No	Yes	No	No	No	No
HO34	<i>House</i> 269 Barkly St, St Kilda	Yes	No	No	Yes	No	No	No	No
HO35	<i>Flats</i> 289 Barkly St, St Kilda	Yes	No	No	Yes	No	No	No	No
HO409	<i>Jerry's Milk Bar</i> 345 Barkly St, Elwood	Yes	No	No	Yes	No	No	No	No
HO38	<i>Former Morley's Coal Depot</i> 2-6 Bay St, Port Melbourne	Yes	Yes	No	Yes	No	No	No	No
HO39	<i>Former Post Office and Naval drill hall</i> 40 Bay St, Port Melbourne	-	-	-	-	-	Yes Ref No H1378	Yes	No

PORT PHILLIP PLANNING SCHEME

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HO452	<i>Former National Bank</i> 96 Bay Street, Port Melbourne	Yes	No	No	Yes	No	No	No	No
HO40	<i>Port Melbourne Court house, Police station and lock up</i> 111 and 113-119 Bay St and Graham St, Port Melbourne	-	-	-	-	-	Yes Ref No H1318	Yes	No
HO41	<i>Port Melbourne Town Hall</i> 333 Bay St, Port Melbourne	Yes	Yes	Yes	Yes	No	No	No	No
HO42	<i>Shop</i> 397 Bay St, Port Melbourne	Yes	Yes	No	Yes	No	No	No	No
HO43	<i>Princes Pier</i> Beach St, Port Melbourne	-	-	-	-	-	Yes Ref No H981	Yes	No
HO465	<i>Station Pier</i> 110 Beach St, Port Melbourne	-	-	-	-	-	Yes Ref No H985	Yes	No
HO46	<i>Port Melbourne railway station</i> 101 Beach St, Port Melbourne	-	-	-	-	-	Yes Ref No H983	Yes	No
HO47	<i>Centenary bridge pillar</i> Beach St, Port Melbourne	Yes	No	No	Yes	No	No	No	No
HO48	<i>Maskell and McNab Memorial</i> Beach St, Port Melbourne	Yes	No	No	Yes	No	No	No	No
HO49	<i>Band Rotunda (opposite Stokes Street)</i> Beach St, Port Melbourne	-	-	-	-	-	Yes Ref No. H1735	No	No

PORT PHILLIP PLANNING SCHEME

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HO50	<i>Public toilets</i> Beach St, Port Melbourne	Yes	No	No	Yes	No	No	No	No
HO51	<i>Bi-Centennial memorial</i> Beach St, Port Melbourne	Yes	No	No	Yes	No	No	No	No
HO362	<i>WW1 Memorial</i> Beach St, Port Melbourne	Yes	No	No	Yes	No	No	No	No
HO52	<i>Port Melbourne Yacht Club</i> Beach St, Port Melbourne	Yes	No	No	Yes	No	No	No	No
HO286	<i>Sandridge Hotel</i> 69 Beach St, Port Melbourne	Yes	No	No	Yes	No	No	No	No
HO44	<i>Leading lights</i> Beacon Vista and Hobsons Bay, Port Melbourne	-	-	-	-	-	Yes Ref No H982	Yes	No
HO53	<i>Cast iron lamp standards</i> Beaconsfield Pde, St. Kilda	Yes	No	No	Yes	No	No	No	No
HO348	<i>Catani Gardens</i> Beaconsfield Pde and Jacka Blvd, St Kilda	-	-	-	-	-	Yes Ref No H1805	No	No
HO54	<i>Open sea bathing house</i> Beaconsfield Pde, St. Kilda	Yes	Yes	Yes	Yes	No	No	No	No
HO450	<i>Sea Wall and Promenade</i>	Yes	No	No	Yes	No	No	No	No

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	Beaconsfield Parade (south side), between Cowderoy Street and McGregor Street, St Kilda West and Middle Park Incorporated plan Sea Wall and Promenade - September 2008								
HO55	<i>Duart</i> 121 Beaconsfield Pde, Albert Park	Yes	Yes	No	Yes	No	No	No	No
HO56	<i>Hotel Victoria</i> 123 Beaconsfield Pde, Albert Park	-	-	-	-	-	Yes Ref No H820	Yes	No
HO57	<i>Hughenden</i> 177 Beaconsfield Pde, Middle Park	Yes	Yes	No	Yes	No	No	No	No
HO287	<i>Flats</i> 333 Beaconsfield Parade, St Kilda	Yes	No	No	Yes	No	No	No	No
HO58	<i>Shandon</i> 361 Beaconsfield Pde, St Kilda	-	-	-	-	-	Yes Ref No H669	Yes	No
HO59	<i>The Belford</i> 24 Belford St, St Kilda	Yes	No	No	Yes	No	No	No	No
HO410	<i>Houses</i> 1 – 5 Bell St, Ripponlea	Yes	No	No	Yes	No	No	No	No
HO357	<i>Former Australian Natives Association Hall</i> 49A Blanche Street, St Kilda	Yes	No	No	Yes	No	No	No	No

PORT PHILLIP PLANNING SCHEME

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HO344	<i>St Kilda Botanical Gardens</i> 55 Blessington St, St Kilda	-	-	-	-	-	Yes Ref No H1804	Yes	No
HO60	<i>House</i> 4 Blessington St, St Kilda	Yes	No	No	Yes	No	No	No	No
HO349	<i>Public Reserve</i> Bowen Cres, Melbourne	No	No	Yes	Yes	No	No	No	No
HO62	<i>Biltmore private hotel</i> 152 Bridport St, Albert Park	-	-	-	-	-	Yes Ref No H475	Yes	No
HO63	<i>St Kilda Town Hall</i> Brighton Rd, St Kilda	Yes	Yes – but limited to the interior of the original 1890 and 1939 Town Hall building only	Yes	Yes	No	No	No	No
HO64	<i>St Kilda Primary School No. 1479</i> 2 Brighton Rd, St Kilda	-	-	-	-	-	Yes Ref No H1712	Yes	No
HO371	<i>Moreton Bay fig in the northwest corner of 43 Brighton Road and land beneath the canopy for a distance of five metres from the canopy edge.</i>	No	No	Yes	Yes	No	No	No	No

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	39-43 Brighton Road (part), 4 Dickens Street (part) and 1-5 Mozart Street (part), St Kilda								
HO65	<i>Holy Trinity complex</i> 2 Brighton Rd, St Kilda	Yes	Yes	No	Yes	No	No	No	No
HO355	<i>House</i> 148 Brighton Road, Ripponlea	Yes	No	No	Yes	No	No	No	No
HO66	<i>Fire Station complex</i> 201 Brighton Rd, Elwood	Yes	No	No	Yes	No	No	No	No
HO67	<i>Windermere Flats</i> 49 Broadway, Elwood	-	-	-	-	-	Yes Ref No H911	Yes	No
HO411	<i>Houses</i> 2, 4 & 6 Browning St, Elwood	Yes	No	No	Yes	No	No	No	No
HO68	<i>Wilgah</i> 6 Burnett St, St Kilda	-	-	-	-	-	Yes Ref No H760	Yes	No
HO453	<i>Wilgah</i> 8 Burnett St, St Kilda	-	-	-	-	-	Yes Ref No H761	Yes	No
HO69	<i>Fenagh Cottage</i> 7 Burnett St, St Kilda	-	-	-	-	-	Yes Ref No H629	Yes	No
HO343	<i>Marion Terrace</i> 14-24 Burnett St, St Kilda	-	-	-	-	-	Yes Ref No H1802	Yes	No
HO70	<i>Oberwyl</i>	-	-	-	-	-	Yes	Yes	No

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	33-35 Burnett St, St Kilda						Ref No H214		
HO71	<i>Fleurs</i> 48 Burnett St, St Kilda	-	-	-	-	-	Yes Ref No H753	Yes	No
HO412	<i>House</i> 32 Byron St, Elwood	Yes	No	No	Yes	No	No	No	No
HO413	<i>Ravensmead House</i> 38 Byron St, Elwood	Yes	No	No	Yes	No	No	No	No
HO72	<i>Former Salvation Army Citadel</i> 17 Camden St, Balaclava	Yes	No	No	Yes	No	No	No	No
HO378	<i>Pepper tree at the rear of 208 Canterbury Road and land beneath the canopy for a distance of five metres from the canopy edge.</i> 208-209 Canterbury Road, St Kilda West	No	No	Yes	Yes	No	No	No	No
HO73	<i>The Canterbury</i> 236 Canterbury Rd, St Kilda West	-	-	-	-	-	Yes Ref No H1836	No	No
HO74	<i>National Theatre</i> 20 Carlisle St, St Kilda	-	-	-	-	-	Yes Ref No H2092	Yes	No
HO75	<i>St. Colman's Church</i> 291 Carlisle St, Balaclava	Yes	Yes	No	Yes	No	No	No	No
HO76	<i>Luna Park</i> 18 Cavell St, St Kilda	-	-	-	-	-	Yes Ref No H938	Yes	No

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HO454	<i>Luna Park- local significance</i> 18 Cavell St, St Kilda Incorporated plan Luna Park	Yes	No	No	Yes	No	No	No	No
HO289	<i>Southern Cross Hotel</i> 78 Cecil St, South Melbourne	Yes	No	No	Yes	No	No	No	No
HO77	<i>Port View House</i> 153 Cecil St, South Melbourne	-	-	-	-	-	Yes Ref No H702	Yes	No
HO78	<i>Finn Barr</i> 155 Cecil St, South Melbourne	-	-	-	-	-	Yes Ref No H715	Yes	No
HO79	<i>Former Emerald Hill Mechanics Institute and Hall of Commerce (Italian Club Cavour)</i> 170-172 Cecil St, South Melbourne	-	-	-	-	-	Yes Ref No H537	Yes	No
HO466	<i>Former St Vincent de Paul Boys Orphanage</i> 231-241 Cecil St and 199-201 Napier St and 34 Church St, South Melbourne	-	-	-	-	-	Yes Ref No H2170	Yes	No
HO80	<i>Former Free Presbyterian Church</i> Chapel St, St Kilda East	Yes	Yes	No	Yes	No	No	No	No
HO81	<i>All Saints Church, Hall and former Vicarage</i> 2 Chapel St, St Kilda East	-	-	-	-	-	Yes Ref No H1364	Yes	No
HO82	<i>St Georges Uniting Church</i>	-	-	-	-	-	Yes	Yes	No

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	4 Chapel St, St Kilda						Ref No H864		
HO83	<i>The Astor Theatre</i> 1-9 Chapel St, St Kilda	-	-	-	-	-	Yes Ref No H1751	No	No
HO84	<i>Drill Hall</i> 10 Chapel St, St Kilda East	Yes	Yes	No	Yes	No	No	No	No
HO85	<i>Cloyne</i> 12 Chapel St, St Kilda	-	-	-	-	-	Yes Ref No H733	Yes	No
HO86	<i>Rondebosch and Elmwood</i> 25-27 Chapel St, St Kilda	-	-	-	-	-	Yes Ref No H754	Yes	No
HO351	<i>Flats</i> 45-47 Chapel Street, St Kilda	Yes	No	No	Yes	No	No	No	No
HO87	<i>Mahnud</i> 65 Chapel St, St Kilda	Yes	No	No	Yes	No	No	No	No
HO290	<i>Police Station</i> 92 Chapel St, St Kilda	Yes	No	No	Yes	No	No	No	No
HO88	<i>Charnwood Oaks</i> 4-8 Charnwood Cr, St Kilda	Yes	Yes	No	Yes	No	No	No	No
HO89	<i>St Kilda Hebrew Congregation Synagogue</i> 10-12 Charnwood Grove, St Kilda	-	-	-	-	-	Yes Ref No 1968	Yes	No
HO90	<i>Houses</i> 80-82 Chaucer St, St. Kilda	Yes	No	No	Yes	No	No	No	No

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HO91	<i>House</i> 9 Church St, South Melbourne	Yes	Yes	No	Yes	No	No	No	No
HO92	<i>Ballarat</i> 51 Church St, South Melbourne	Yes	Yes	No	Yes	No	No	No	No
HO93	<i>Wayside Inn Hotel</i> 448 City Rd, South Melbourne	Yes	No	No	Yes	No	No	No	No
HO291	<i>Shop</i> 155 Clarendon St, South Melbourne	Yes	No	No	Yes	No	No	No	No
HO367	<i>Prefabricated Cottage</i> 17 Coventry Place, South Melbourne	-	-	-	-	-	Yes Ref No H1958	Yes	No
HO96	<i>House</i> 378 Coventry St, South Melbourne	Yes	Yes	No	Yes	No	No	No	No
HO97	<i>Bellhouse Iron House and Iron House</i> 399 Coventry St and 6 Patterson Place, South Melbourne	-	-	-	-	-	Yes Ref No H220 & Ref No H1888	Yes	No
HO98	<i>Beaconsfield Hotel</i> 20 Cowderoy St, St Kilda	Yes	Yes	No	Yes	No	No	No	No
HO99	<i>Fountain Inn</i> 1 Crockford St, Port Melbourne	Yes	Yes	No	Yes	No	No	No	No
HO100	<i>Eden Terrace</i> 4-18 Dalgety St, St Kilda	-	-	-	-	-	Yes Ref No H847	Yes	No

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HO414	<i>Electrical Substation</i> 13 Daley St, Elwood	Yes	No	No	Yes	No	No	No	No
HO101	<i>Ornamental Tramway Overhead Poles</i> Dandenong Rd, St Kilda	-	-	-	-	-	Yes Ref No H1023 (see also HO125)	Yes	No
HO102	<i>Park Keeper's Lodge</i> Alma Park, Dandenong Rd, St Kilda	Yes	Yes	No	Yes	No	No	No	No
HO103	<i>St Mary's Church</i> 208 Dandenong Rd, St Kilda	-	-	-	-	-	Yes Ref No H750	Yes	No
HO104	<i>Former Ardoch Educational Centre</i> 216-238 Dandenong Rd and 1-9 Ardoch Ave and 2-8 Pilley St and 2-10 Ardoch Ave, St Kilda East	-	-	-	-	-	Yes Ref No H969	Yes	No
HO455	<i>St Kilda Cemetery</i> 260-288 Dandenong Rd, St Kilda East	-	-	-	-	-	Yes Ref No H1081	Yes	No
HO105	<i>House</i> 70 Dickens St Elwood	Yes	No	No	Yes	No	No	No	No
HO106	<i>Glenronald</i> 75 Dickens St, St. Kilda	Yes	No	No	Yes	No	No	No	No
HO415	<i>Antigone Flats</i> 34 Docker St, Elwood	Yes	No	No	Yes	No	No	No	No
HO107	<i>Monterey Flats</i>	Yes	No	No	Yes	No	No	No	No

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	35 Docker St, Elwood								
HO108	<i>St Luke's Church</i> 210 Dorcas St, South Melbourne	-	-	-	-	-	Yes Ref No H218	Yes	No
HO109	<i>Former Salvation Army Citadel</i> 232 Dorcas St, South Melbourne	Yes	Yes	No	Yes	No	No	No	No
HO110	<i>Former Baptist Church</i> 250 Dorcas St, South Melbourne	Yes	Yes	No	Yes	No	No	No	No
HO111	<i>Dorcas Street Primary School No 1253</i> 284 Dorcas St, South Melbourne	-	-	-	-	-	Yes Ref No H1346	Yes	No
HO112	<i>Former Presbyterian Church</i> 317-329 Dorcas St, South Melbourne	Yes	Yes	No	Yes	No	No	No	No
HO113	<i>Former Queens Arms Hotel</i> 330-334 Dorcas St, South Melbourne	-	-	-	-	-	Yes Ref No H1827	No	No
HO114	<i>House</i> 337 Dorcas St, South Melbourne	Yes	Yes	No	Yes	No	No	No	No
HO115	<i>Former Morley and Carrick Warehouse</i> 93 Dow St, Port Melbourne	Yes	Yes	No	Yes	No	No	No	No
HO116	<i>State School</i> 113 Eastern Rd, South Melbourne	Yes	Yes	No	Yes	No	No	No	No
HO347	<i>Biltmore</i>	-	-	-	-	-	Yes	No	No

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	36 Eildon St, St Kilda						Ref No H1809		
HO119	<i>Albert Park railway station complex</i> 365 Ferrars St, South Melbourne	-	-	-	-	-	Yes Ref No H1558	Yes	No
HO120	<i>South Melbourne railway station complex</i> 221 Ferrars St, South Melbourne	-	-	-	-	-	Yes Ref No H1593	Yes	No
HO121	<i>Balladonia</i> 40 Ferrars Pl, South Melbourne	-	-	-	-	-	Yes Ref No H772	Yes	No
HO122	<i>Former Freemasons Hall</i> 254-256 Ferrars St, South Melbourne	-	-	-	-	-	Yes Ref No H538	Yes	No
HO336	<i>Emerald Hill Estate Residences</i> 2 Fishley St, South Melbourne	-	-	-	-	-	Yes Ref No H1727	Yes	No
HO438	<i>Emerald Hill Estate Residences</i> 6 Fishley St, South Melbourne	-	-	-	-	-	Yes Ref No H1728	Yes	No
HO125	<i>Ornamental Tramway Overhead Poles</i> Fitzroy St, St Kilda	-	-	-	-	-	Yes Ref No H1023 (see also HO101)	Yes	No
HO345	<i>Summerland Mansions</i>	-	-	-	-	-	Yes	Yes	No

PORT PHILLIP PLANNING SCHEME

PS map ref	Heritage place	External paint controls apply?	Internal alteration controls apply?	Tree controls apply?	Solar energy system controls apply?	Outbuildings or fences not exempt under Clause 43.01-4	Included on the Victorian Heritage Register under the Heritage Act 2017?	Prohibited uses permitted?	Aboriginal heritage place?
	17-27 Fitzroy St, St Kilda						Ref No H1808		
HO126	<i>Tolarno Hotel</i> 42 Fitzroy St, St Kilda	-	-	-	-	-	Yes Ref No H2207	Yes	No
HO123	<i>Former St Kilda railway station complex</i> 60 Fitzroy St and 352 Canterbury Rd, St Kilda	-	-	-	-	-	Yes Ref No H1719	Yes	No
HO366	<i>St Kilda Bowling Club</i> 66 Fitzroy Street, St Kilda	-	-	-	-	-	Yes Ref No H1913	Yes	No
HO124	<i>St Kilda Park Primary School No 2460</i> 70 Fitzroy St, St Kilda	-	-	-	-	-	Yes Ref No H1637	Yes	No
HO127	<i>George Hotel</i> 123-127 Fitzroy St and 2-10 Grey St, St Kilda	-	-	-	-	-	Yes Ref No H706	Yes	No
HO128	<i>Former Wesleyan Methodist Church</i> 167 Fitzroy St, St Kilda	-	-	-	-	-	Yes Ref No H818	Yes	No
HO129	<i>Ritz Mansions</i> 171 Fitzroy St, St Kilda	Yes	Yes, public foyers and lift only	No	Yes	No	No	No	No
HO416	<i>Duplex</i> 22 – 22A Foam St, Elwood	Yes	No	No	Yes	No	No	No	No
HO131	<i>House</i>	-	-	-	-	-	Yes	Yes	No

PORT PHILLIP PLANNING SCHEME

PS map ref	Heritage place	External paint controls apply?	Internal alteration controls apply?	Tree controls apply?	Solar energy system controls apply?	Outbuildings or fences not exempt under Clause 43.01-4	Included on the Victorian Heritage Register under the Heritage Act 2017?	Prohibited uses permitted?	Aboriginal heritage place?
	108 Gladstone St, Southbank						Ref No H834		
HO132	<i>House</i> 110 Gladstone St, Southbank	-	-	-	-	-	Yes Ref No H835	Yes	No
HO133	<i>House</i> 112 Gladstone St, Southbank	-	-	-	-	-	Yes Ref No H836	Yes	No
HO134	<i>House</i> 114 Gladstone St, Southbank	-	-	-	-	-	Yes Ref No H837	Yes	No
HO135	<i>House</i> 116 Gladstone St, Southbank	-	-	-	-	-	Yes Ref No H838	Yes	No
HO372	<i>Maisonettes, including the courtyard garden and the mature bull bay magnolia, privet and concrete pond.</i> 3 Glen Eira Road, Ripponlea	Yes	No	Yes	Yes	No	No	No	No
HO373	<i>House and Gleneira, including the black locust trees at the front and rear-western boundary of 12 Glen Eira Road and land beneath the canopy of these trees for a distance of five metres from the canopy edge.</i> 10-12 Glen Eira Road and 3 Victoria Avenue, Ripponlea	Yes	No	Yes	Yes	No	No	No	No
HO137	<i>Ripponlea railway station complex</i> 11-13 Glen Eira Rd, Ripponlea	-	-	-	-	-	Yes Ref No H1588	Yes	No
HO136	<i>Brinsmeads Pharmacy</i>	-	-	-	-	-	Yes	Yes	No

PORT PHILLIP PLANNING SCHEME

PS map ref	Heritage place	External paint controls apply?	Internal alteration controls apply?	Tree controls apply?	Solar energy system controls apply?	Outbuildings or fences not exempt under Clause 43.01-4	Included on the Victorian Heritage Register under the Heritage Act 2017?	Prohibited uses permitted?	Aboriginal heritage place?
	71-73 Glen Eira Rd, Ripponlea						Ref No H725		
HO219	<i>St Columbas Church complex</i> 24 and 26 Glenhuntly Rd and 2 Normandy Rd, Elwood	Yes	Yes	No	Yes	No	No	No	No
HO354	<i>Flats</i> 60-66 Glenhuntly Road, Elwood	Yes	No	No	Yes	No	No	No	No
HO138	<i>House</i> 136 Glenhuntly Rd, Elwood	Yes	No	No	Yes	No	No	No	No
HO139	<i>Former Gasworks complex with wall</i> Graham St, South Melbourne	Yes	Yes	No	Yes	No	No	No	No
HO140	<i>Palmville</i> 240 Graham St, Port Melbourne	Yes	No	No	Yes	No	No	No	No
HO141	<i>Former shop and house</i> 249 Graham St, Port Melbourne	Yes	No	No	Yes	No	No	No	No
HO142	<i>State School</i> 415 Graham St, Port Melbourne	Yes	Yes	Yes	Yes	No	No	No	No
HO143	<i>Ripplemere</i> 34 Grey St, St Kilda	-	-	-	-	-	Yes Ref No H747	Yes	No
HO144	<i>Eildon</i> 51 Grey St, St Kilda	-	-	-	-	-	Yes Ref No H746	Yes	No
HO145	<i>Residences</i> 77-79 Grey St, St Kilda	-	-	-	-	-	Yes Ref No H796	Yes	No

PORT PHILLIP PLANNING SCHEME

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HO146	<i>Sacred Heart Church, Hall and Presbytery</i> 83-87 Grey St, St Kilda	-	-	-	-	-	Yes Ref No H1765	No	No
HO147	<i>Rail over Road Bridges Carlisle Street, Nightingale Street and Grosvenor Street, Balaclava</i>	Yes	No	No	Yes	No	No	No	No
HO374	<i>Stone pine (Pinu pinea) in rear garden and land beneath the canopy for a distance of five metres from the canopy edge.</i> 3, 5, 5A and 7 Hammerdale Ave, St Kilda East	No	No	Yes	Yes	No	No	No	No
HO375	<i>Hawsleigh Court and central courtyard garden, including two mature Liquidambers (Liquidamber styraciflua), and silver birch.</i> 2B Hawsleigh Court, Balaclava	Yes	No	Yes	Yes	No	No	No	No
HO417	<i>Hood's Court Flats</i> 2 Hood St, Elwood	Yes	No	No	Yes	No	No	No	No
HO418	<i>House</i> 2 Hotham Gve, Ripponlea	Yes	No	No	Yes	No	No	No	No
HO419	<i>Houses</i> 7 – 9 Hotham Gve, Ripponlea	Yes	No	No	Yes	No	No	No	No
HO149	<i>House</i> 23 Hotham Gve, Ripponlea	Yes	No	No	Yes	No	No	No	No
HO150	<i>Road over Rail Bridge</i>	Yes	Yes	No	Yes	No	No	No	No

PORT PHILLIP PLANNING SCHEME

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	Hotham Street, Ripponlea								
HO292	<i>House</i> 45 Hotham St, St Kilda East	Yes	No	No	Yes	No	No	No	No
HO293	<i>Flats</i> 50 Hotham St, St Kilda East	Yes	No	No	Yes	No	No	No	No
HO152	<i>Congregational Church</i> 72 Hotham St, St Kilda East	Yes	Yes	No	Yes	No	No	No	No
HO337	<i>B'nai B'rith House</i> 99 Hotham St, Balaclava	Yes	No	No	Yes	No	No	No	No
HO153	<i>Robert Russell House and former Congregational Church Hall</i> 30-33 Howe Cr, South Melbourne	-	-	-	-	-	Yes Ref No H574	Yes	No
HO154	<i>Former Congregational Church</i> 34-36 Howe Cr, South Melbourne	-	-	-	-	-	Yes Ref No H573	Yes	No
HO155	<i>Hazelwood Terrace</i> 46 Howe Cr, South Melbourne	-	-	-	-	-	Yes Ref No H221	Yes	No
HO156	<i>Hazelwood Terrace</i> 47 Howe Cr, South Melbourne	-	-	-	-	-	Yes Ref No H222	Yes	No
HO157	<i>Hazelwood Terrace</i> 48 Howe Cr, South Melbourne	-	-	-	-	-	Yes Ref No H223	Yes	No
HO158	<i>Experimental Concrete Houses</i>	-	-	-	-	-	Yes	Yes	No

PORT PHILLIP PLANNING SCHEME

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	324-326 Howe Pde, Port Melbourne						Ref No H1863		
HO164	<i>Former J. Kitchen and Sons Pty Ltd offices</i> 164 Ingles Street and 14 Woodruff Street, Port Melbourne	Yes	Yes	No	Yes	No	No	No	No
HO165	<i>Former Australian Motor Industries Factory</i> 289 Ingles St, Port Melbourne	Yes	No	No	Yes	No	No	No	No
HO294	<i>Former St Kilda Council Depot Offices-Destructor Building</i> 33 Inkerman St, St Kilda	Yes	No	No	Yes	No	No	No	No
HO159	<i>House</i> 59 Inkerman St, St Kilda	Yes	No	No	Yes	No	No	No	No
HO160	<i>Telephone exchange</i> 62 -78 Inkerman St, St Kilda	Yes	No	No	Yes	No	No	No	No
HO161	<i>Shops</i> 77-79 Inkerman St, St Kilda	Yes	No	No	Yes	No	No	No	No
HO162	<i>Shops</i> 268-276 Inkerman St, St Kilda	Yes	No	No	Yes	No	No	No	No
HO163	<i>House</i> 290 Inkerman St, St Kilda	Yes	No	No	Yes	No	No	No	No
HO356	<i>Flats</i> 290A Inkerman St, St Kilda East	Yes	No	No	Yes	No	No	No	No

PORT PHILLIP PLANNING SCHEME

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HO398	<i>Houses</i> 316-320 Inkerman Street, East St Kilda	Yes	No	No	Yes	No	No	No	No
HO168	<i>St Kilda Seabaths</i> Jacka Boulevard, St Kilda	Yes	Yes	No	Yes	No	No	No	No
HO169	<i>Catani arch</i> Jacka Boulevard, St. Kilda	Yes	No	No	Yes	No	No	No	No
HO170	<i>Obelisks</i> Jacka Boulevard, St. Kilda	Yes	No	No	Yes	No	No	No	No
HO172	<i>Dressing Pavilion</i> 40 Jacka Boulevard, St. Kilda	Yes	Yes	No	Yes	No	No	No	No
HO173	<i>Wattle House</i> 53 Jackson St, St Kilda	-	-	-	-	-	Yes Ref No H632	Yes	No
HO420	<i>Houses</i> 24 – 30 John St, Elwood	Yes	No	No	Yes	No	No	No	No
HO342	<i>Terrace</i> 27-35 Kerferd Rd, Albert Park	-	-	-	-	-	Yes Ref no H1831	Yes	No
HO176	<i>MacRobertson Girls High School</i> 350-370 Kings Way, Melbourne	-	-	-	-	-	Yes Ref No H1641	Yes	No
HO177	<i>Houses</i> 328 Kings Way, South Melbourne	Yes	No	No	Yes	No	No	No	No

PORT PHILLIP PLANNING SCHEME

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HO175	<i>Kingsley Court</i> 4-6 Kingsley St, Elwood	Yes	No	No	Yes	No	No	No	No
HO421	<i>Cromer Court</i> 22 – 24 Kingsley St, Elwood	Yes	No	No	Yes	No	No	No	No
HO295	<i>House</i> 40 Kingsley St, Elwood	Yes	No	No	Yes	No	No	No	No
HO178	<i>McAdam House</i> Lakeside Drive, Albert Park	Yes	Yes	No	Yes	No	No	No	No
HO296	<i>House</i> 40 Lansdowne Rd, St Kilda East	Yes	No	No	Yes	No	No	No	No
HO182	<i>Former Fire Station</i> 147 Liardet St, Port Melbourne	Yes	No	No	Yes	No	No	No	No
HO422	<i>House</i> 19 Lindsay Ave, Elwood	Yes	No	No	Yes	No	No	No	No
HO226	<i>St Kilda Pavilion</i> Off Lower Esplanade, St Kilda	-	-	-	-	-	Yes Ref No 1533	Yes	No
HO184	<i>Palais Theatre</i> 14 Lower Esplanade, St Kilda	-	-	-	-	-	Yes Ref No H947	Yes	No
HO186	<i>Tintara</i> 20 Lyndon St, Ripponlea	-	-	-	-	-	Yes Ref No H842	Yes	No
HO187	<i>Beacon</i> Marine Pde, Elwood	Yes	No	No	Yes	No	No	No	No

PORT PHILLIP PLANNING SCHEME

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HO298	<i>Houses</i> 1-2 Marine Pde, Elwood	Yes	No	No	Yes	No	No	No	No
HO299	<i>House</i> 19 Marine Parade	Yes	No	No	Yes	No	No	No	No
HO300	<i>House</i> 29 Marine Parade, Elwood	Yes	No	No	Yes	No	No	No	No
HO188	<i>House</i> 32 Marine Pde, St Kilda	Yes	No	No	Yes	No	No	No	No
HO423	<i>Garden Court Flats</i> 73 Marine Pde, Elwood	Yes	No	No	Yes	No	No	No	No
HO301	<i>Flats</i> 77 Marine Parade, Elwood	Yes	No	No	Yes	No	No	No	No
HO365	<i>Flats</i> 4 & 4A Meredith St, Elwood	Yes	No	No	Yes	No	No	No	No
HO191	<i>Hartpury</i> 9 Milton St, Elwood	Yes	Yes	No	Yes	No	No	No	No
HO190	<i>Hartpury Court complex</i> 9-11 Milton St, Elwood	-	-	-	-	-	Yes Ref No H767	Yes	No
HO192	<i>Birnam apartments</i> 15 Mitford St, Elwood	Yes	No	No	Yes	No	No	No	No
HO368	<i>Residence and Air Raid Shelter</i> 23 Mitford St, St Kilda	-	-	-	-	-	Yes Ref No H616	Yes	No

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HO376	<i>Poets Corner, including Bhutan cypress and two Italian cypress.</i> 38 Mitford St, Elwood	Yes	No	Yes	Yes	No	No	No	No
HO194	<i>Houses</i> 86-88 Mitford St, Elwood	Yes	No	No	Yes	No	No	No	No
HO195	<i>House</i> 96 Mitford St, Elwood	Yes	No	No	Yes	No	No	No	No
HO196	<i>Rail overbridge</i> Montague St, South Melbourne	Yes	No	No	Yes	No	No	No	No
HO218	<i>Former Dunlop Pneumatic Tyre Co. mill</i> 66 Montague Street, South Melbourne (see also 223-229 Normanby Rd, South Melbourne)	Yes	No	No	Yes	No	No	No	No
HO197	<i>Residence</i> 83 Montague St, Southbank	-	-	-	-	-	Yes Ref No H830	Yes	No
HO198	<i>Residence</i> 85 Montague St, Southbank	-	-	-	-	-	Yes Ref No H831	Yes	No
HO199	<i>Residence</i> 87 Montague St, Southbank	-	-	-	-	-	Yes Ref No H832	Yes	No
HO200	<i>Residence</i> 89 Montague St, Southbank	-	-	-	-	-	Yes Ref No H833	Yes	No
HO201	<i>State School</i>	Yes	Yes	No	Yes	No	No	No	No

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	90 Montague St, South Melbourne								
HO202	<i>Golden Fleece Hotel</i> 120 Montague St, South Melbourne	Yes	No	No	Yes	No	No	No	No
HO203	<i>Former Talbot Inn</i> 144 Montague St, South Melbourne	Yes	No	No	Yes	No	No	No	No
HO272	<i>Former residence and shop</i> 146 Montague St, South Melbourne	Yes	No	No	Yes	No	No	No	No
HO204	<i>St Peter and Paul Church and school complex</i> 217 Montague St, South Melbourne	Yes	Yes	No	Yes	No	No	No	No
HO302	<i>Flats</i> 8 Mooltan Ave, East St Kilda	Yes	No	No	Yes	No	No	No	No
HO205	<i>Maori Chief Hotel</i> 117-119 Moray St, South Melbourne	Yes	No	No	Yes	No	No	No	No
HO206	<i>Former Freer's Family Hotel</i> 147 Moray St, South Melbourne	Yes	No	No	Yes	No	No	No	No
HO207	<i>Temperance House</i> 175 Moray St, South Melbourne	Yes	No	No	Yes	No	No	No	No
HO208	<i>House</i> 347 Moray St, South Melbourne	Yes	Yes	No	Yes	No	No	No	No
HO209	<i>Park House</i> 352 Moray St, South Melbourne	-	-	-	-	-	Yes Ref No H224	Yes	No

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HO210	<i>House</i> 1 Morris St, South Melbourne	Yes	Yes	No	Yes	No	No	No	No
HO377	<i>WA Flowering Gum at 27 Murchison Street and land beneath the canopy for a distance of five metres from the canopy edge.</i> 27 Murchison Street, East St Kilda	No	No	Yes	Yes	No	No	No	No
HO211	<i>House</i> 88 Napier St, South Melbourne	Yes	Yes	No	Yes	No	No	No	No
HO212	<i>Former St Vincent de Pauls Girls Orphanage</i> 179-197 Napier St, South Melbourne	-	-	-	-	-	Yes Ref No H1531	Yes	No
HO213	<i>Former Temperance Hall</i> 199-207 Napier St, South Melbourne	Yes	Yes	No	Yes	No	No	No	No
HO214	<i>Kindergarten, including Pepper tree at rear of property.</i> 23 Nelson St, St. Kilda East	Yes	No	Yes	Yes	No	No	No	No
HO216	<i>House</i> 8 Nightingale St, Balaclava	Yes	No	No	Yes	No	No	No	No
HO217	<i>Former Laycock Son and Co. woollen mills</i> 179-185 Normanby Rd, South Melbourne	Yes	No	No	Yes	No	No	No	No
HO218	<i>Former Dunlop Pneumatic Tyre Co. mill</i> 223-229 Normanby Rd, South Melbourne	Yes	No	No	Yes	No	No	No	No

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	(see also 66 Montague Street, South Melbourne)								
HO338	<i>Jubilee Terrace</i> 118-130 Nott St, Port Melbourne	-	-	-	-	-	Yes Ref No H1829	Yes	No
HO339	<i>Queens Terrace</i> 132-144 Nott St, Port Melbourne	-	-	-	-	-	Yes Ref No H1830	Yes	No
HO358	<i>Houses</i> 14-18 Octavia Street, St Kilda	Yes	No	No	Yes	No	No	No	No
HO353	<i>Flats</i> 46 Ormond Esplanade, Elwood	Yes	No	No	Yes	No	No	No	No
HO424	<i>Flats</i> 51 Ormond Esplanade, Elwood	Yes	No	No	Yes	No	No	No	No
HO425	<i>House</i> 123 Ormond Esplanade, Elwood	Yes	No	No	Yes	No	No	No	No
HO426	<i>Beach Kiosk</i> Ormond Esplanade, Elwood Foreshore	Yes	No	No	Yes	No	No	No	No
HO427	<i>Community Centre & Cafe</i> Ormond Esplanade, Elwood Foreshore	Yes	No	No	Yes	No	No	No	No
HO428	<i>Surf Lifesaving Club</i> Ormond Esplanade, Elwood Foreshore	Yes	No	No	Yes	No	No	No	No
HO220	<i>Surrey Court</i>	Yes	No	No	Yes	No	No	No	No

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	71 Ormond Rd, Elwood								
HO303	<i>House</i> 77 Ormond Rd, Elwood	Yes	No	No	Yes	No	No	No	No
HO221	<i>Former Chemist shop</i> 90 Ormond Rd, Elwood	-	-	-	-	-	Yes Ref No H782	Yes	No
HO222	<i>Avalon</i> 285 Orrong Rd, St. Kilda East	Yes	No	No	Yes	No	No	No	No
HO223	<i>House</i> 311 Orrong Rd, St Kilda East	Yes	No	No	Yes	No	No	No	No
HO224	<i>Former Church of Christ</i> 96 Pakington St, St. Kilda	Yes	No	No	Yes	No	No	No	No
HO304	<i>Shops & Residence</i> 116 Park St, South Melbourne	Yes	No	No	Yes	No	No	No	No
HO225	<i>Royal Melbourne Yacht Squadron</i> Pier Rd, St. Kilda	Yes	No	No	Yes	No	No	No	No
HO458	<i>Abercrombie House</i> 8 Patterson Place, South Melbourne	-	-	-	-	-	Yes Ref No H404	Yes	No
HO429	<i>Flats</i> 33 Pine Ave, Elwood	Yes	No	No	Yes	No	No	No	No
HO227	<i>Point Ormond and Surrounding Landscape</i> Point Ormond Rd, Elwood	No	No	Yes	Yes	No	No	No	Yes

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HO228	<i>House</i> 4 Pozieres Ave, St. Kilda	Yes	No	No	Yes	No	No	No	No
HO229	<i>Residence, also known as Berkeley Hall</i> 11 Princes St, St Kilda	-	-	-	-	-	Yes Ref No H491	Yes	No
HO230	<i>House</i> 24 Princes St, St Kilda	Yes	Yes	No	Yes	No	No	No	No
HO231	<i>Vacunce</i> 112 Princes St, Port Melbourne	Yes	No	No	Yes	No	No	No	No
HO232	<i>Former shop and residence</i> 14-16 Punt Rd, St Kilda	Yes	No	No	Yes	No	No	No	No
HO463	<i>St Kilda Cricket Ground</i> Queens Road and Fitzroy Street and Lakeside Drive, St Kilda	-	-	-	-	-	Yes Ref No H2234	Yes	No
HO233	<i>Netherby</i> 8 Queens Rd, Melbourne	Yes	No	No	Yes	No	No	No	No
HO330	<i>Clive Fairbairn Pavillion</i> 494-498 & 500-520 St Kilda Road, Melbourne	Yes	No	No	Yes	No	No	No	No
HO321	<i>Lancaster House</i> 18 Queens Rd, Melbourne	Yes	No	No	Yes	No	No	No	No
HO234	<i>Former Bendigonia</i> 25 Queens Rd, Melbourne	-	-	-	-	-	Yes Ref No H909	Yes	No

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HO235	<i>Newburn flats</i> 30 Queens Rd, Melbourne	-	-	-	-	-	Yes Ref No H578	Yes	No
HO335	<i>Former Brookwood Flats</i> 32 Queens Rd, Melbourne	Yes	No	No	Yes	No	No	No	No
HO346	<i>Stanhill</i> <i>The heritage place includes</i> <i>Residential apartment building</i> 33-34 Queens Rd, Melbourne	-	-	-	-	-	Yes Ref No H1875	No	No
HO322	<i>Formerly Lanark</i> 55 Queens Rd, Melbourne	Yes	No	No	Yes	No	No	No	No
HO323	<i>Former John Bateman Motor Inn</i> 69 Queens Rd, Melbourne	Yes	No	No	Yes	No	No	No	No
HO324	<i>Avalon</i> 70 Queens Rd, Melbourne	Yes	No	No	Yes	No	No	No	No
HO325	<i>Myer House (former)</i> 72 Queens Rd, Melbourne	Yes	No	No	Yes	No	No	No	No
HO326	<i>Former Queenslodge Motor Inn</i> 81 Queens Rd, Melbourne	Yes	No	No	Yes	No	No	No	No
HO328	<i>The Mansion</i> 83 Queens Rd, Melbourne	Yes	No	No	Yes	No	No	No	No
HO236	<i>Chinese Temple</i> 76 Raglan St, South Melbourne	-	-	-	-	-	Yes Ref No H219	Yes	No

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HO237	<i>House</i> 115 Raglan St, South Melbourne	Yes	Yes	No	Yes	No	No	No	No
HO238	<i>Our Lady of Mt Carmel complex</i> Richardson St, Middle Park	Yes	Yes	No	Yes	No	No	No	No
HO239	<i>Middle Park Primary School No 2815</i> 194 Richardson Street (part), Middle Park	-	-	-	-	-	Yes Ref No H1711	Yes	No
HO240	<i>House</i> 21 Ross St, Port Melbourne	Yes	No	No	Yes	No	No	No	No
HO241	<i>Denmark Villa</i> 123 Ross St, Port Melbourne	Yes	No	No	Yes	No	No	No	No
HO242	<i>The Presbytery</i> 127 Ross St, Port Melbourne	Yes	No	No	Yes	No	No	No	No
HO305	<i>Shop</i> 207 Ross St, Port Melbourne	Yes	No	No	Yes	No	No	No	No
HO243	<i>St Joseph's Mission complex</i> 268-274 Rouse St, and 93-95 Stokes St, Port Melbourne	Yes	Yes	No	Yes	No	No	No	No
HO306	<i>House</i> 41 Ruskin St, Elwood	Yes	No	No	Yes	No	No	No	No
HO246	<i>Gunnersens Pty. Ltd.</i> 112 Salmon St, Port Melbourne	Yes	No	No	Yes	No	No	No	No

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HO488	<i>St Kilda Road</i> Melbourne and <i>Tram shelter</i> St Kilda Rd and Lorne St Melbourne and <i>Tram shelter</i> St Kilda Rd and Dorcas St, Melbourne	-	-	-	-	-	Yes Ref No H2359 and Yes Ref No H1867 and Yes Ref No H1869	No	No
HO248	<i>Shops</i> 170-172 St Kilda Rd, St Kilda	Yes	No	No	Yes	No	No	No	No
HO249	<i>Shop</i> 228 St Kilda Rd, St Kilda	Yes	No	No	Yes	No	No	No	No
HO250	<i>Linton</i> 238 St. Kilda Rd, St. Kilda	Yes	No	No	Yes	No	No	No	No
HO251	<i>Post Office</i> 306A St Kilda Rd, St Kilda	Yes	Yes	No	Yes	No	No	No	No
HO252	<i>First Church of Christian Scientist, Melbourne</i> 336-340 St Kilda Rd, Melbourne	-	-	-	-	-	Yes Ref No H1766	Yes	No
HO253	<i>Airlie, St Kilda Road</i> 452 St Kilda Rd, Melbourne	-	-	-	-	-	Yes Ref No H722	Yes	No

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PS map ref	Heritage place	External paint controls apply?	Internal alteration controls apply?	Tree controls apply?	Solar energy system controls apply?	Outbuildings or fences not exempt under Clause 43.01-4	Included on the Victorian Heritage Register under the Heritage Act 2017?	Prohibited uses permitted?	Aboriginal heritage place?
HO254	<i>Rathgael – The Willows</i> 462 St Kilda Rd, Melbourne	-	-	-	-	-	Yes Ref No H96	Yes	No
HO307	<i>Offices</i> 464 St Kilda Rd, Melbourne	Yes	No	No	Yes	No	No	No	No
HO329	<i>Charsfield</i> 478 St Kilda Rd, Melbourne	Yes	No	No	Yes	No	No	No	No
HO331	<i>Landene</i> 490 St Kilda Rd, Melbourne	Yes	No	No	Yes	No	No	No	No
HO332	<i>Warwilla</i> 572 St Kilda Rd, Melbourne	Yes	No	No	Yes	No	No	No	No
HO255	<i>Gas Valve House (Former)</i> 615 St Kilda Rd, Melbourne	-	-	-	-	-	Yes Ref No H675	Yes	No
HO333	<i>Flats</i> 628 St Kilda Rd, Melbourne	Yes	No	No	Yes	No	No	No	No
HO256	<i>Ulimaroa</i> 630 St Kilda Rd, Melbourne	-	-	-	-	-	Yes Ref No H658	Yes	No
HO257	<i>Luzmore Terrace</i> 645-649 St Kilda Rd, Melbourne	Yes	No	No	Yes	No	No	No	No
HO461	<i>St Kilda Street Bridge</i> St Kilda St, Elwood	-	-	-	-	-	Yes Ref No H2080	No	No
HO435	<i>Park Court Flat</i>	Yes	No	No	Yes	No	No	No	No

PORT PHILLIP PLANNING SCHEME

PS map ref	Heritage place	External paint controls apply?	Internal alteration controls apply?	Tree controls apply?	Solar energy system controls apply?	Outbuildings or fences not exempt under Clause 43.01-4	Included on the Victorian Heritage Register under the Heritage Act 2017?	Prohibited uses permitted?	Aboriginal heritage place?
	473 St Kilda St, Elwood								
HO340	<i>Rochester Terrace</i> 33-51 St Vincent Place South, Albert Park	-	-	-			Yes Ref No H813 and Yes Ref No H1291	Yes	No
HO259	<i>House</i> 20 Scott St, Elwood	Yes	No	No	Yes	No	No	No	No
HO431	<i>Scots Church</i> 31a Scott St, Elwood	Yes	No	No	Yes	No	No	No	No
HO432	<i>Elwood Children's Centre</i> 31a Scott St, Elwood	Yes	No	No	Yes	No	No	No	No
HO260	<i>Elwood Central School No. 3942</i> 49 Scott Street and 161 Mitford Street (part), Elwood	Yes	No	No	Yes	No	No	No	No
HO399	<i>Shirley Court</i> 20 Shirley Grove, East St Kilda	Yes	No	No	Yes	No	No	No	No
HO400	<i>House, including iron gateway</i> 22 Shirley Grove, East St Kilda	Yes	No	Yes - Canary Island (2) date palm (<i>Phoenix canariensis</i>)	Yes	No	No	No	No
HO433	<i>House</i> 54 Southey St, Elwood	Yes	No	No	Yes	No	No	No	No
HO261	<i>Elenora</i> 51 Spenser St, St. Kilda	Yes	No	No	Yes	No	No	No	No

PORT PHILLIP PLANNING SCHEME

PS map ref	Heritage place	External paint controls apply?	Internal alteration controls apply?	Tree controls apply?	Solar energy system controls apply?	Outbuildings or fences not exempt under Clause 43.01-4	Included on the Victorian Heritage Register under the Heritage Act 2017?	Prohibited uses permitted?	Aboriginal heritage place?
HO262	<i>Los Angeles Court flats</i> 81A Spenser St, St Kilda	Yes	No	No	Yes	No	No	No	No
HO352	<i>House</i> 2A Spray Street, Elwood	Yes	No	No	Yes	No	No	No	No
HO434	<i>House</i> 54 & 56 Spray St, Elwood	Yes	No	No	Yes	No	No	No	No
HO244	<i>Former Swallow & Ariell Biscuit Factory</i> 1 Stokes St and 241-265 Rouse St and 40-44 Princes St and 29-53 Stokes St and 2-14 Capistrano Pl and 2-10 Murtons Way, Port Melbourne	-	-	-	-	-	Yes Ref No H567	Yes	No
HO263	<i>Former Masonic Hall</i> 18 Stokes St, Port Melbourne	Yes	No	No	Yes	No	No	No	No
HO264	<i>Laywell Terrace</i> 20-24 Stokes St, Port Melbourne	Yes	No	No	Yes	No	No	No	No
HO497	<i>Edith</i> 26 Stokes Street, Port Melbourne	Yes	No	No	Yes	No	No	No	No
HO265	<i>Former Swallow and Ariell offices</i> 60 Stokes St, Port Melbourne	Yes	No	No	Yes	No	No	No	No
HO308	<i>Thelma</i> 106 Stokes St, Port Melbourne	Yes	No	No	Yes	No	No	No	No
HO309	<i>Carmel</i> 108 Stokes St, Port Melbourne	Yes	No	No	Yes	No	No	No	No

PORT PHILLIP PLANNING SCHEME

PS map ref	Heritage place	External paint controls apply?	Internal alteration controls apply?	Tree controls apply?	Solar energy system controls apply?	Outbuildings or fences not exempt under Clause 43.01-4	Included on the Victorian Heritage Register under the Heritage Act 2017?	Prohibited uses permitted?	Aboriginal heritage place?
HO266	<i>Former St. Osyth Hotel</i> 135 Stokes St, Port Melbourne	Yes	Yes	No	Yes	No	No	No	No
HO267	<i>Woodlands</i> 168 Stokes St, Port Melbourne	Yes	No	No	Yes	No	No	No	No
HO310	<i>House</i> 169 Stokes St, Port Melbourne	Yes	No	No	Yes	No	No	No	No
HO268	<i>Former shop and residence</i> 175 Stokes St, Port Melbourne	-	-	-	-	-	Yes Ref No H1789	No	No
HO269	<i>Bundalohn</i> 6 Tennyson St, St Kilda	Yes	Yes	No	Yes	No	No	No	No
HO436	<i>Flats</i> 63 - 69 Tennyson St, St Kilda	Yes	Yes	No	Yes	No	No	No	No
HO270	<i>Glanfell</i> 87 Tennyson St, Elwood	Yes	No	No	Yes	No	No	No	No
HO437	<i>Flats</i> 104 - 108 Tennyson St, St Kilda	Yes	Yes	No	Yes	No	No	No	No
HO271	<i>House</i> 109 Tennyson St, Elwood	Yes	No	No	Yes	No	No	No	No
HO117	<i>The Esplanade Hotel</i> 11 The Esplanade, St Kilda	Yes	Yes	No	Yes	No	No	No	No
HO118	<i>Belvedere</i>	-	-	-	-	-	Yes	Yes	No

PORT PHILLIP PLANNING SCHEME

PS map ref	Heritage place	External paint controls apply?	Internal alteration controls apply?	Tree controls apply?	Solar energy system controls apply?	Outbuildings or fences not exempt under Clause 43.01-4	Included on the Victorian Heritage Register under the Heritage Act 2017?	Prohibited uses permitted?	Aboriginal heritage place?
	22 The Esplanade, St Kilda						Ref No H812		
HO272	<i>Factory</i> 79 Thistlethwaite St, South Melbourne	Yes	No	No	Yes	No	No	No	No
HO273	<i>House</i> 28 Vale St, St Kilda	Yes	No	No	Yes	No	No	No	No
HO274	<i>House</i> 30/30A Vautier St, Elwood	Yes	Yes	No	Yes	No	No	No	No
HO61	<i>Albert Park Primary School No 1181 (and former Wesleyan Methodist Church)</i> 2-38 Victoria Avenue, Albert Park	-	-	-	-	-	Yes Ref No H1629	Yes	No
HO275	<i>Mona</i> 12 Waterloo Cr, St Kilda	Yes	No	No	Yes	No	No	No	No
HO276	<i>Residence</i> 34 Waterloo Cr, St Kilda	-	-	-	-	-	Yes Ref No H780	Yes	No
HO363	<i>Flats</i> 22 Wellington Street, St Kilda	Yes	No	No	Yes	No	No	No	No
HO364	<i>House</i> 67 Wellington Street, St Kilda	Yes	No	No	Yes	No	No	No	No
HO359	<i>House</i> 99 Wellington Street, St Kilda	Yes	No	No	Yes	No	No	No	No
HO360	<i>Former Tramway Engine House</i> 105 Wellington Street, St Kilda	Yes	No	No	Yes	No	No	No	No

PORT PHILLIP PLANNING SCHEME

PS map ref	Heritage place	External paint controls apply?	Internal alteration controls apply?	Tree controls apply?	Solar energy system controls apply?	Outbuildings or fences not exempt under Clause 43.01-4	Included on the Victorian Heritage Register under the Heritage Act 2017?	Prohibited uses permitted?	Aboriginal heritage place?
HO277	<i>House</i> 48 Westbury St, St. Kilda East	Yes	No	No	Yes	No	No	No	No
HO278	<i>Frenchay</i> 89 Westbury St, St Kilda East	Yes	No	No	Yes	No	No	No	No
HO279	<i>Flats</i> 54-56 Wilgah St, St. Kilda East	Yes	No	No	Yes	No	No	No	No
HO282	<i>Former BALM Paints offices</i> 2 Salmon Street, Port Melbourne	Yes	No	No	Yes	No	No	No	No
HO280	<i>Fire Station complex</i> 425 Williamstown Rd, Port Melbourne	Yes	No	No	Yes	No	No	No	No
HO281	<i>Former Disco Manufacturing corporation P.L. factory</i> 477 Williamstown Rd, Port Melbourne	Yes	No	No	Yes	No	No	No	No
HO283	<i>Petrol filling station complex</i> north-west cnr, Williamstown Rd and Salmon St, Port Melbourne	Yes	No	No	Yes	No	No	No	No
HO284	<i>House</i> 33 Wordsworth St, St. Kilda	Yes	No	No	Yes	No	No	No	No
HO401	<i>House</i> 34 Young Street, East St Kilda	Yes	No	No	Yes	No	No	No	No
HO311	<i>Former Albion Hotel</i> 172 York St, South Melbourne	Yes	No	No	Yes	No	No	No	No

PORT PHILLIP PLANNING SCHEME

PS map ref	Heritage place	External paint controls apply?	Internal alteration controls apply?	Tree controls apply?	Solar energy system controls apply?	Outbuildings or fences not exempt under Clause 43.01-4	Included on the Victorian Heritage Register under the Heritage Act 2017?	Prohibited uses permitted?	Aboriginal heritage place?
HO447	<i>Port Melbourne Cricket Ground</i> 525 Williamstown Road, Port Melbourne	Yes	No	No	Yes	No	No	No	No
HO448	<i>Edwards Park</i> 219 Esplanade East, Port Melbourne	No	No	Yes	Yes	No	No	No	No
HO470	<i>Substation</i> 98 Johnson Street (corner of Johnson and Munro Streets)	Yes	No	No	Yes	No	No	No	No
HO471	<i>Horse Trough</i> Ingles Street, near intersection of Ingles Street and Williamstown Road	Yes	No	No	Yes	No	No	No	No
HO472	<i>Former Commonwealth WWII Facility and Rootes Factory</i> 19 Salmon Street, Port Melbourne	Yes	Yes – tower component only	No	Yes	No	No	No	No
HO449	<i>Former WJ Carr Warehouse</i> 95 Bay Street, Port Melbourne	Yes	No	No	Yes	No	No	No	No
HO451	<i>Exchange Hotel</i> 39 Bay Street, Port Melbourne	Yes	No	No	Yes	No	No	No	No
HO456	<i>Former Australasian Sugar Refining Company complex</i> Land bounded by Beach, Rouse, Dow Streets and Esplanade West except the north western corner	Yes (10 Beach Street only)	No	No	Yes	No	No	No	No
HO457	<i>Former Army and Navy Hotel, 95 Dow Street</i>	Yes	No	No	Yes	No	No	No	No

PORT PHILLIP PLANNING SCHEME

PS map ref	Heritage place	External paint controls apply?	Internal alteration controls apply?	Tree controls apply?	Solar energy system controls apply?	Outbuildings or fences not exempt under Clause 43.01-4	Included on the Victorian Heritage Register under the Heritage Act 2017?	Prohibited uses permitted?	Aboriginal heritage place?
	Part 100-128 Bay Street, Port Melbourne - fronting Dow Street								
HO459	<i>Four shops</i> 79-85 Bay Street, Port Melbourne	Yes	No	No	Yes	No	No	No	No
HO462	<i>Pier Hotel</i> 1 Bay Street, Port Melbourne	Yes	No	No	Yes	No	No	No	No
HO464	<i>Former Mission to Seamen</i> 49 Beach Street, Port Melbourne	Yes	No	No	Yes	No	No	No	No
HO485	<i>Local Hotel</i> 22 Bay Street, Port Melbourne	Yes	No	No	Yes	No	No	No	No
HO486	<i>Shop</i> Part 100-128 Bay Street, Port Melbourne	Yes	No	No	Yes	No	No	No	No
HO487	<i>Three houses</i> 183-187 Rouse Street, Port Melbourne	Yes	No	No	Yes	No	No	No	No
HO488	<i>Tecoma Court</i> 16-20a Wellington Street, St Kilda	Yes	No	No	Yes	No	No	No	No
HO489	<i>Earls Court</i> 44 Wellington Street, St Kilda	Yes	No	No	Yes	No	No	No	No
HO491	<i>Duplex</i> 38-40 Wellington Street, St Kilda	Yes	No	No	Yes	No	No	No	No
HO492	<i>Terraces</i> 56-58 Wellington Street, St Kilda	Yes	No	No	Yes	No	No	No	No

PORT PHILLIP PLANNING SCHEME

PS map ref	Heritage place	External paint controls apply?	Internal alteration controls apply?	Tree controls apply?	Solar energy system controls apply?	Outbuildings or fences not exempt under Clause 43.01-4	Included on the Victorian Heritage Register under the Heritage Act 2017?	Prohibited uses permitted?	Aboriginal heritage place?
HO493	<i>Shops with residences</i> 81-81a Wellington Street, St Kilda	Yes	No	No	Yes	No	No	No	No
HO494	<i>Residence</i> 129 Wellington Street, St Kilda	Yes	No	No	Yes	No	No	No	No
HO503	<i>Maisonettes</i> 21 Redan Street, St Kilda Statement of Significance: 21 Redan Street, St Kilda, "Maisonettes", February 2020	Yes	No	No	Yes	No	No	No	No
HO504	<i>Former church</i> 77 Park Street, South Melbourne	Yes	No	No	Yes	No	No	No	No
HO505	<i>Concrete House</i> 226 Alma Road, St Kilda East Statement of Significance: 226 Alma Road, St Kilda East. "House (Concrete House)", February 2020	Yes	No	No	Yes	No	No	No	No
HO506	<i>Shop and Residence</i> 264-266 St Kilda Road, St Kilda Statement of Significance: 264-266 St Kilda Road, St Kilda "Shops & Residence", February 2020	Yes	No	No	Yes	No	No	No	No
HO507	<i>Houses</i> 110, 112, 114, 116 and 118 Barkly Street, St Kilda and 2, 2A, 4 and 6 Blanche Street, St Kilda	No	No	No	Yes	No	No	No	No

PORT PHILLIP PLANNING SCHEME

PS map ref	Heritage place	External paint controls apply?	Internal alteration controls apply?	Tree controls apply?	Solar energy system controls apply?	Outbuildings or fences not exempt under Clause 43.01-4	Included on the Victorian Heritage Register under the Heritage Act 2017?	Prohibited uses permitted?	Aboriginal heritage place?
HO509	<i>Shops and Residence</i> 588 and 590 City Road, South Melbourne Statement of Significance: 588 and 590 City Road, South Melbourne, "Shops and Residences", February 2021	No	No	No	Yes	No	No	No	No
HO510	<i>Edgewater Towers</i> 12 Marine Parade, St Kilda	No	No	No	Yes	No	No	No	No
HO511	<i>Duplex Houses</i> 152-154 Mitford Street, Elwood	No	No	No	Yes	No	No	No	No
HO512	<i>Flats</i> 58-60 Queens Road, Melbourne	No	No	No	Yes	No	No	No	No
HO561	<i>City Edge</i> 89 & 99 Eastern Road, and 36A and 58A Napier Street, South Melbourne Statement of Significance: 89 & 99 Eastern Road, and 36A and 58A Napier Street, South Melbourne, August 2024	No	No	Yes	Yes	No	No	No	No
HO563	<i>House</i> 110 Bank Street, South Melbourne Statement of Significance: 110 Bank Street, South Melbourne, August 2024	No	No	No	Yes	No	No	No	No
HO564	<i>Terrace row</i> 151-155 Dorcas Street, South Melbourne Statement of Significance: 151-155	No	No	No	Yes	No	No	No	No

PORT PHILLIP PLANNING SCHEME

PS map ref	Heritage place	External paint controls apply?	Internal alteration controls apply?	Tree controls apply?	Solar energy system controls apply?	Outbuildings or fences not exempt under Clause 43.01-4	Included on the Victorian Heritage Register under the Heritage Act 2017?	Prohibited uses permitted?	Aboriginal heritage place?
	Dorcas Street, South Melbourne, August 2024								
HO565	<p><i>Former corner grocery and residence</i></p> <p>108 Bank Street, South Melbourne</p> <p>Statement of Significance: 108 Bank Street, South Melbourne, August 2024</p>	No	No	No	Yes	No	No	No	No
HO566	<p><i>Warehouse</i></p> <p>167-173 Coventry Street, South Melbourne</p> <p>Statement of Significance: 167-173 Coventry Street, South Melbourne, August 2024</p>	No	No	No	Yes	No	No	No	No
HO567	<p><i>Former Coca Cola Factory</i></p> <p>82-86 Clarke Street, South Melbourne</p> <p>Statement of Significance: 82-86 Clarke Street, South Melbourne, August 2024</p>	No	No	No	Yes	No	No	No	No
HO568	<p><i>Former J.E.Searl's Engineers & Coppersmiths</i></p> <p>49-55 York Street, South Melbourne</p> <p>Statement of Significance: 49-55 York Street, South Melbourne, August 2024</p>	No	No	No	Yes	No	No	No	No
HO569	<p><i>Star Hotel</i></p> <p>152 Clarendon Street</p>	Yes	No	No	Yes	No	No	No	No

PORT PHILLIP PLANNING SCHEME

PS map ref	Heritage place	External paint controls apply?	Internal alteration controls apply?	Tree controls apply?	Solar energy system controls apply?	Outbuildings or fences not exempt under Clause 43.01-4	Included on the Victorian Heritage Register under the Heritage Act 2017?	Prohibited uses permitted?	Aboriginal heritage place?
	Statement of Significance: 152 Clarendon Street, South Melbourne, August 2024								
HO570	<i>Alma & Tasma Terraces</i> 68-70 Market Street, South Melbourne Statement of Significance: 68-70 Market Street, South Melbourne	No	No	No	Yes	No	No	No	No

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16/02/2024--/--/---
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PORT PHILLIP PLANNING SCHEME

Name of document	Introduced by:
<i>Acland Courtyard Development Plan</i>	NPS1
<i>Acland Street Upgrade Project Incorporated Document</i> (December 2015)	C124
<i>Albert Park Master Plan</i>	NPS1
<i>Beacon Cove Development, Port Melbourne</i> (Revised 2013) (including <i>Beacon Cove Concept Plan No. 1</i> , <i>Beacon Cove Precinct Plan No. 1</i> , <i>Beacon Cove Residential Component Guidelines No. 1</i> and Plan named <i>Beacon Cove Port Melbourne</i> showing areas subject to an environmental audit)	C104
<i>Becton, Port Melbourne Development Concept Plan and Building Envelope Plan</i>	NPS1
<i>City of Port Phillip Heritage Policy Map</i> - (Adoption Version Amendment G161port Part 2, December 2021 C219port August 2024) - (Part of Port Phillip Heritage Review)	G161port Part 2 C219port
<i>City of Port Phillip Neighbourhood Character Map</i> - (Adoption Version Amendment G161port Part 2, December 2021 C219port August 2024) - (Part of Port Phillip Heritage Review)	G161port Part 2 C219port
<i>Hospital Emergency Medical Services - Helicopter Flight Path Protection Areas Incorporated Document</i> (June 2017)	GC49
<i>Incorporated Plan - Sea Wall and Promenade</i> (September 2008)	C70
<i>Luna Park</i>	NPS1
<i>Major Promotion Signs – Permit Provisions</i> (December 2008)	C100
<i>Melbourne CityLink Project – Advertising Sign Locations</i> (November 2003)	VC20
<i>Melbourne Convention Centre Development, Southbank and North Wharf redevelopment, Docklands</i> (April 2006, Amended May 2016)	GC44
<i>Melbourne Metro Rail Project Incorporated Document</i> (May 2018)	GC82
<i>Melbourne Metro Rail Project – Infrastructure Protection Areas Incorporated Document</i> (December 2016)	GC67
<i>Melbourne Sports & Aquatic Centre, Albert Park</i>	NPS1
<i>Montague Community Park and associated Streetscape Works</i> (August 2017)	C135
<i>Port Melbourne Mixed Use Area Development Contributions Plan</i> (Streetscape Works) (July 1999)	C13
<i>Port Phillip Heritage Review - Volumes 1-6</i> (Adoption Version Amendment G161port Part 2, December 2021 C219port August 2024)	G161port Part 2 C219port
<i>Prince Apartments Stage 2 Development Plans – 29 Fitzroy Street, St Kilda</i> (December 2013)	C94
<i>Shrine of Remembrance Vista Controls</i> (April 2014)	C140
<i>St Kilda Foreshore Urban Design Framework</i> (2002)	C36
<i>St Kilda Seabaths</i>	NPS1
<i>St Kilda Station Redevelopment plans prepared by Billard Leece Partnership</i> (July 1999)	C9
<i>State Sports Facilities Project Albert Park, September 2009</i> (amended May 2012)	C120
<i>Statement of Significance: Montague Commercial Precinct</i> (February 2021)	C186port
<i>Statement of Significance: 21 Redan Street, St Kilda, "Maisonettes"</i> (February 2020)	C142port
<i>Statement of Significance: 226 Alma Road, St Kilda East, "House (Concrete House)"</i> (February 2020)	C142port

PORT PHILLIP PLANNING SCHEME

Name of document	Introduced by:
<i>Statement of Significance: 264-266 St Kilda Road, St Kilda, "Shops & Residence" (February 2020)</i>	C142port
<i>Statement of Significance, 'Houses', 110, 112, 114, 116 and 118 Barkly Street, St Kilda and 2, 2A, 4 and 6 Blanche Street, St Kilda (December 2021)</i>	C161port Part 2
<i>Stokehouse – 30 Jacka Boulevard, St Kilda (July 2014)</i>	C110
<i>Tramway Infrastructure Upgrades Incorporated Document (May 2017)</i>	GC68
<i>Victorian Cricket and Community Centre, St Kilda Cricket Ground (May 2016)</i>	C119
<i>Victorian Pride Centre Incorporated Document (September 2018)</i>	C149port
<i>West Beach Pavilion Precinct Incorporated Plan (2004)</i>	C36
<i>Statement of Significance, 588-590 City Road, South Melbourne "Shops & Residences" (February 2021)</i>	C161port Part 1
<i>Statement of Significance, 324 Esplanade East, Port Melbourne (December 2021)</i>	C161port Part 2
<i>Statement of Significance, 'Duplex Houses', 152 and 154 Mitford Street, Elwood (December 2021)</i>	C161port Part 2
<i>Statement of Significance, 'Edgewater Towers', 12 Marine Parade, St Kilda (December 2021)</i>	C161port Part 2
<i>Statement of Significance, 'Flats', 58-60 Queens Road, Melbourne (December 2021)</i>	C161port Part 2
<i>Statement of Significance, 89 & 99 Eastern Road, and 36A and 58A Napier Street, South Melbourne (August 2024)</i>	C219port
<i>Statement of Significance, 110 Bank Street, South Melbourne (August 2024)</i>	C219port
<i>Statement of Significance, 151-155 Dorcas Street, South Melbourne (August 2024)</i>	C219port
<i>Statement of Significance, 108 Bank Street, South Melbourne (August 2024)</i>	C219port
<i>Statement of Significance, 167-173 Coventry Street, South Melbourne (August 2024)</i>	C219port
<i>Statement of Significance, 82-86 Clarke Street, South Melbourne (August 2024)</i>	C219port
<i>Statement of Significance, 49-55 York Street, South Melbourne (August 2024)</i>	C219port
<i>Statement of Significance, 152 Clarendon Street, South Melbourne (August 2024)</i>	C219port
<i>Statement of Significance, 68-70 Market Street, South Melbourne (August 2024)</i>	C219port

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C213port Proposed C219port

PORT PHILLIP PLANNING SCHEME

Name of background document	Amendment number - clause reference
<i>City of Port Phillip Outdoor Advertising Guidelines 1996</i> (revised 2007) (City of Port Phillip, 2007)	C62 Clause 15.01-1L-01
<i>City of Port Phillip Subdivision Guidelines</i> (City of Port Phillip, 2000)	C62 Clause 15.01-3L
<i>City of Port Phillip Urban Art Strategy</i> (City of Port Phillip, 2002)	C62 Clause 11.03-6L-01 and 15.01-2L-02
<i>City of Port Phillip Urban Iconography Study</i> (City of Port Phillip, 2004)	C62 Clause 11.03-6L-01 and Schedule to Clause 43.02s21
<i>City of Port Phillip Water Sensitive Urban Design Guidelines, 2009</i> (City of Port Phillip, 2009)	C78 Clause 19.03-3L
<i>Compliance Guidelines for Clause 22.12 Stormwater Management</i> (City of Port Phillip, 2017)	C203port Clause 19.03-3L
<i>Design Guidelines 1-7 Waterfront Place, Port Melbourne</i> (City of Port Phillip, 2014)	C104 Schedule to Clause 43.02s23
<i>Don't Waste It! - Waste Management Strategy 2018-28</i> (City of Port Phillip, 2018)	C203port Clause 19.03-5L
<i>Fishermans Bend Framework</i> (Department of Environment, Land, Water and Planning, September 2018)	GC81 Schedule to Clause 45.09s01
<i>Fishermans Bend Additional Heritage Place Assessments</i> (Biosis Pty Ltd, 2016)	C115 Clause 15.03-1L
<i>Fishermans Bend Heritage Review: Montague Commercial Precinct</i> (RBA Architects and Conservation Consultants, October 2019)	C186port Clause 15.03-1L
<i>Fishermans Bend Heritage Study</i> (Biosis Pty Ltd, 2013)	C115 Clause 15.03-1L
<i>Fishermans Bend Integrated Transport Plan 2017</i> (Transport for Victoria Department of Economic Development, Jobs, Transport and Resources, September 2017)	GC81 Schedule to Clause 45.09s01
<i>Fishermans Bend Vision</i> (Department of Environment, Land, Water and Planning, September 2016)	GC81 Schedule to Clause 45.09s01
<i>Foreshore Management Plan</i> (City of Port Phillip, 2012)	C203port Clause 12.02-1L, 17.04-2L and 19.02-6L
<i>Getting Our Community Active – Sport and Recreation Strategy 2015-2024</i> (City of Port Phillip, 2015)	C203port Clause 17.04-2L and 19.06-6L
<i>Greening Port Phillip Strategy – An Urban Forest Approach</i> (City of Port Phillip, 2010)	C203port Clause 12.01-1L
<i>Guidelines for Preparing a Waste Management Plan</i> (City of Port Phillip, 2019)	C203port Clause 19.03-5L
<i>Heritage Appraisal: 16-20A & 44 Wellington Street, St Kilda</i> (Lovell Chen, May 2015)	C122 Clause 15.03-1L
<i>Heritage Assessment, 588-590 City Road, South Melbourne</i> (Context Pty Ltd, May 2017)	C161portPt1 Clause 15.03-1L

PORT PHILLIP PLANNING SCHEME

Name of background document	Amendment number - clause reference
<i>Heritage Design Guidelines</i> (City of Port Phillip, 2022)	C203port Clause 15.03-1L
<i>Heritage Kerbs, Channels and Laneways Guideline</i> (City of Port Phillip, 2006)	C62 Clause 15.01-5L and 15.03-1L
<i>Heritage Overlay 6 St Kilda East Precinct Review Final Report</i> (David Helms Heritage Planning, January 2020)	C142port Clause 15.03-1L
<i>Heritage Review – Wellington Street, St Kilda</i> (Lovell Chen (Revised) March 2017)	C122 Clause 15.03-1L
<i>In Our Backyard: Growing Affordable Housing in Port Phillip 2015-2025</i> (City of Port Phillip, 2016)	C203port Clause 16.01-2L, 16.01-4L and 16.01-5L
<i>Move, Connect, Live: Integrated Transport Strategy</i> (City of Port Phillip, August 2018)	C203port Clause 18.01-1L-01, 18.01-1L-02 and 18.02-4L-01
<i>Ormond Road Urban Design Guidelines</i> (City of Port Phillip, 2007)	C57(part 2) Clause 11.03-1L-01 and Schedule to Clause 43.02s18
<i>Port Development Strategy 2035 Vision</i> (Port of Melbourne Corporation, 2009)	Clause 42.01s4 Clause 43.02s23
<i>Port Melbourne Mixed Use Area Development Contributions Plan (Streetscape Works)</i> (City of Port Phillip, July 1999)	C13 Schedule to Clause 45.06s1
<i>Port Phillip Advertising Signs Policy Review</i> (Hansen Partnership, 2007)	C62 Clause 15.01-1L-01
<i>Port Phillip Design Manual – Chapter 8: Neighbourhood Character Statements – Bay Street Activity Centre Environs</i> (City of Port Phillip, 2000)	C103 Clause 11.03-1L-02
<i>Port Phillip Design Manual</i> (City of Port Phillip, 2000) including: <ul style="list-style-type: none"> <i>Fishermans Bend Estate Guidelines</i> (City of Port Phillip, revised 2021) <i>Garden City Estate Guidelines</i> (City of Port Phillip, revised 2021) <i>Dunstan Estate Guidelines</i> (City of Port Phillip, 2007) 	C5 (original) and C203port (revised) Clause 11.03-1L-03, 15.01-2L-01, 15.01-5L and 15.03-1L
<i>Port Phillip Practice Notes Policy No. 15 – Caretaker's Dwellings</i> (City of Port Phillip, 2011)	C62 Clause 13.07-1L-02
<i>Places for People: Public Space Strategy</i> (City of Port Phillip, 2021)	C203port Clause 19.02-6L
<i>Recreational Boating Facilities Framework</i> (Central Coastal Board, 2014)	C203port Clause 11.03-6L-01
<i>Responsible Gambling Policy</i> (City of Port Phillip, 2011)	C88 Schedule to Clause 52.28s01
<i>Review of Heritage Overlay 1 Port Melbourne – Outcomes and Recommendations</i> (Lovell Chen, July 2011)	C89 Clause 15.03-1L
<i>Review of Heritage Overlay 1 Port Melbourne – Stage 2 Review – Summary Report</i> (Lovell Chen, December 2012)	C103 Clause 15.03-1L

PORT PHILLIP PLANNING SCHEME

Name of background document	Amendment number - clause reference
<i>Review of Heritage Overlay 3</i> (Heritage Alliance, 2009 & Built Heritage, 2010)	C72 Clause 15.03-1L
<i>South Melbourne Central Structure Plan</i> (City of Port Phillip, August 2007 2024)	C52 C219port Clause 11.03-1L-01 and 11.03-1L-06, 11.03-6L-08, 15.01-1L-02, 15.01-5L, 15.03-1L, 17.01-1L, 17.01-2L, 17.02-1L, 17.03-2L, 18.01-3L-01, 18.02-3L-01, 19.02-4L, and Schedule to Clause 43.02s08 43.02s37, 43.02s38, 43.02s39, 43.02s40.
<i>South Melbourne Central Urban Design Framework</i> (David Lock Associates and City of Port Phillip, August 2007)	C52 Clause 11.03-1L and 11.03-6L, and Schedule to Clause 43.02s08
<i>St Kilda Foreshore and Environs Urban Design Guidelines</i> (1991)	Clause 37.02s2
<i>St Kilda Foreshore Urban Design Framework</i> (City of Port Phillip, 2002)	C62 Clause 11.03-6L-01
<i>St Kilda Road North Precinct Plan</i> (City of Port Phillip, 2013)(Updated 2015)	C154 Clause 43.06s26
<i>St Kilda Road South Urban Design and Land Use Framework</i> (Planisphere and City of Port Phillip, 2015)	C122 Clause 11.03-6L-03, and Schedule to Clause 43.02s27, 43.02s34, 43.02s35 and 43.02s36
<i>Sustainable Transport Policy and Parking Rates</i> (Ratio, 2007)	C89 Clause 11.03-1L-01, 11.03-1L-02, 11.03-1L-03, 15.01-3L and 18.02-4L
<i>The Shrine of Remembrance: Managing the significance of the Shrine</i> (Message Consultants Australia, July 2013)	C140 Clause 15.01-1L-01, 15.01-2L-01 and 43.02s26
<i>Tiuna Grove Heritage Assessment</i> (Barrett, 2019)	C173port Clause 15.03-1L
<i>Urban Stormwater Best Practice Environmental Management Guidelines</i> (CSIRO, 1999)	C78 Clause 19.03-3L
<i>Water Sensitive Urban Design – Engineering Procedures: Stormwater</i> (Melbourne Water and CSIRO Publishing, 2005)	C78 Clause 19.03-3L

SCHEDULE TO CLAUSE 74.01 APPLICATION OF ZONES, OVERLAYS AND PROVISIONS**1.0 Application of zones, overlays and provisions**44/04/2023
G203port Proposed C219port

This planning scheme applies the following zones, overlays and provisions to implement the Municipal Planning Strategy and the objectives and strategies in Clauses 11 to 19:

- Mixed Use Zone to:
 - Former industrial areas on the periphery of Major Activity Centres to provide opportunities for new well located residential development at increased densities.
 - Precincts identified in structure plans for Carlisle Street and Bay Street Major Activity Centres.
 - The Kings Way Mixed Use Corridor.
 - Areas transitioning to residential in St Kilda.
- Residential Growth Zone to sites in residential areas identified for Substantial Residential Growth and:
 - Sites that already have high density development at 4 or more storeys:
 - Department of Human Services sites (150 Inkerman Street, Park Towers and Dorcas Street Estate).
 - The block bound by High Street, Raleigh Street and Punt Road, Windsor.
 - Areas where Design and Development Overlays allow 4 or more storeys:
 - Ormond Road (DDO16).
 - Beaconsfield Parade (between Fitzroy Street and Fraser Streets), St Kilda West (DDO6-10).
 - Beacon Cove Towers (DDO20).
 - The Port Melbourne High Rise Areas (block between Esplanade West, Esplanade East, south of Graham Street and north of Rouse Street and the block bound by Liardet Street, Danks Street, Foote Street and Lagoon Reserve) (DDO1).
- General Residential Zone to sites in established suburbs identified for Incremental Residential Growth and:
 - Along Main/Collector roads already characterised by 2-3 storey medium density development (Inkerman Street, between Evelyn Street and Hotham Street and Alma Road, between St Kilda Road and Hotham Street and St Kilda Street, north of Ormond Road).
 - Along Main/Collector roads which have existing DDOs allowing development at 3 or more storeys (Beaconsfield Parade, Port Melbourne (DDO5)).
 - Acland Street, St Kilda (DDO6-5); Marine Parade, St Kilda (DDO6-9); and Marine Parade and Ormond Esplanade, Elwood (DDO7).
 - Selected heritage areas close to the St Kilda Activity Centre which are generally characterised by existing 3 storey development.
- Neighbourhood Residential Zone to established residential areas identified for Limited or Minimal Residential Growth.
- General Residential Zone and Neighbourhood Residential Zone to residential areas in the St Kilda Neighbourhood.
- Industrial 3 Zone to the William Street Industrial area in the Carlisle Street Activity Centre.

- Commercial 1 Zone to established retail and commercial areas where more intensive retail, commercial and residential development is anticipated including activity centres, business precincts, and retail strips.
- Commercial 2 Zone to the ~~Western and Eastern Business Precincts in the South Melbourne Central Activity Centre~~ Western and Eastern Business Precincts in the South Melbourne Enterprise Precinct East and Enterprise Precinct West, adjacent to the South Melbourne Central Activity Centre.
- Special Use Zone to:
 - The St Kilda Triangle site to facilitate renewal as a cultural, recreational and entertainment node.
 - Key destinations within the foreshore.
- Comprehensive Development Zone to:
 - Station Pier and Princes Pier.
 - Major mixed use development sites in Fitzroy Street and Acland Street.
- Capital City Zone - Schedule 1 to the Fishermans Bend Urban Renewal Area.
- Environmental Significance Overlay to the Coroboree Tree in Albert Park Reserve, near St Kilda Junction.
- Heritage Overlay and Environmental Significance Overlay to protect areas and features of special aboriginal cultural heritage significance.
- Heritage Overlay to:
 - Ensure that the scale and heritage qualities of the traditional retail strips within activity centres are maintained, including Bay Street (south of Graham Street) and surrounding streets.
 - Protect all heritage places including buildings, trees, monuments and sites.
- Design and Development Overlay to:
 - Areas of local and regional significance such as the foreshore, the Shrine of Remembrance environs, residential growth areas, and key activity centres to ensure that the design and built form of new development achieves strategic objectives for these areas.
 - Manage the design and built form of new higher density housing, in line with preferred character statements.
 - Guide the intensity of new development within activity centres.
 - Facilitate increased residential ~~and~~ commercial/office and employment densities.
 - Manage the scale and design of development on land within the setting and backdrop area of the Shrine of Remembrance.
- Public Acquisition Overlay:
 - To facilitate the expansion of existing public open space areas.
 - To sites identified in the Carlisle Street Activity Centre Structure Plan 2009, as being required for the future provision of public open space.
- Environmental Audit Overlay to require environmental remediation of land, where necessary, before use or development for a sensitive use on:
 - Potentially contaminated land that may be used for a sensitive use.
 - Former industrial sites that are now within residential and mixed use areas including the Fishermans Bend Urban Renewal Area.

PORT PHILLIP PLANNING SCHEME

- Development Contributions Plan Overlay to the Port Melbourne Mixed Use Growth Area.
- Infrastructure Contributions Overlay to the Fishermans Bend Urban Renewal Area.
- Parking Overlay to the Fishermans Bend Urban Renewal Area.

AUTHORISATION

SCHEDULE TO CLAUSE 74.02 FURTHER STRATEGIC WORK**1.0 Further strategic work**

14/04/2023 --/--/----
G203port Proposed C219port

- Undertake an employment land strategy to identify the City's employment needs and trends.
- Review land use options for the William Street industrial precinct in the medium term (5 plus years) in accordance with the Carlisle Street Activity Centre Structure Plan, 2009; including on-going industrial use; providing for a broader industrial / commercial business mix; or comprehensive renewal to a mixed residential / commercial precinct.
 - Undertake further strategic work to consider how to retain creative industries and urban manufacturers within the municipality, in partnership with the Inner Melbourne Action Plan Councils (IMAP).
 - Develop a new Activity Centres Strategy and Implementation Plan to inform detailed land use policy and structure plans.
 - Develop a future vision and strategic framework to guide the role and function of the St Kilda (Fitzroy/Acland Streets) ~~and South Melbourne Central Activity Centres.~~
 - Prepare a revised Housing Strategy to:
 - Take into consideration current factors and demand influencing housing provision.
 - Update housing policy to account for the new residential zones and Fishermans Bend.
 - Consider the review areas that were not addressed by Amendment C123.
 - Continue to monitor and understand housing trends in the municipality.
 - Review Port Phillip's neighbourhood character policy to better articulate Council's preferred vision in key areas.
 - Undertake further strategic work in response to development opportunities for important tourist destinations such as Waterfront Place and the St Kilda Triangle.
 - Create a city-wide spatial plan to:
 - Better define the City's urban structure and character at both a city-wide and local level.
 - Integrate spatial elements of key strategies such as the Integrated Transport Strategy and Public Spaces Strategy.
 - Identify key features of the City's urban structure and character.
 - Prepare a municipal Aboriginal cultural heritage study/strategy.
 - Develop land use planning controls and adaptation strategies that address coastal inundation, beach erosion, storm surges and sea level rise.
 - Develop a whole-of-catchment approach to flood prevention, including exploring the use of planning mechanisms to deliver appropriate built form outcomes and infrastructure upgrades.
 - Investigate ways to increase impervious surfaces in new development and facilitate onsite stormwater detention to become a more water sensitive City.
 - Apply the Environmental Significance Overlay to sites of biological significance to minimise the loss of significant vegetation.
 - Review public open space policy, planning controls and contributions following completion of Council's Public Space Strategy.

PORT PHILLIP PLANNING SCHEME

- Develop a licensed premises local planning policy to guide the appropriate location and design of licensed premises to ensure they make a positive contribution commensurate to the role of each activity centre and to effectively manage amenity impacts.
- Develop social impact assessment guidelines to set out processes, acceptable scope and methodology and to clarify the types of development where it's required.

AUTHORISATION

Statement of Significance: City Edge

Address:	89 & 99 Eastern Road, and 36A and 58A Napier Street, South Melbourne	Name:	City Edge
Place type:	Residential	Grading:	Significant
PS ref no:	HO561	Constructed:	1971-75
Heritage precinct:	Not applicable	Citation No.:	2465



What is significant?

The apartment complex City Edge in its garden setting including mature trees, designed by Daryl Jackson and Evan Walker and constructed in stages from 1971 to 1975, at 89 & 99 Eastern Road, and 36A and 58A Napier Street, South Melbourne is significant.

Alterations and additions made after 1975 are not significant.

How is it significant?

City Edge is of local historical representative, aesthetic and associative significance to the City of Port Phillip.

Why is it significant?

City Edge is historically significant for its association with the theme of later twentieth century higher density development in the municipality, particularly as a reaction to the earlier slum clearances initiated by the Housing Commission of Victoria in the post-war period. The complex is an outstanding example of a mixed housing development that sought a more contextually and architecturally sensitive local response to the demand for new and higher density modern housing in the municipality in the mid to late twentieth century. City Edge's success helped turn the Housing Commission away from building more high-rise flats and pointed the way towards new forms of housing in the municipality in the 1970s-80s. (Criterion A)

City Edge is significant as an outstanding local example of the work of architects Daryl Jackson and Evan Walker. The development was awarded an RIAA medal for its innovative resolution of social and physical problems. The complex integrates a variety of housing into a large high-density

development for the time, integrating diverse sized residences into the landscape and so creating a more sensitive response to the traditional urban scale of South Melbourne. Architecturally, the complex is notable for the way building mass is stacked to optimise the site limitations and orientations towards views and sunlight, balancing the competing demands for privacy and sociability. It incorporates the traditional functions of front gardens, balconies and footpaths into a brutalist/functionalist styled modern complex of interconnected internal landscapes, causeways and terraces in order to create its own 'neighbourhood' over and above the parking below. The complex is a significant and pioneering representative of the higher density mixed housing type in the municipality. (Criterion D & E)

City Edge is significant for its association with Daryl Jackson and Evan Walker. The development was a highlight and defining project of the professional pairing of Jackson and Walker. Daryl Jackson went on to become a renowned and influential Australian architect and an associate professor at the University of Melbourne. Evan Walker took his ideas in a more political direction, entering Victorian parliament and becoming minister for planning in the 1980s. In City Edge, the combination of their political and architectural influences is demonstrated by the dedication to mixed development and modern design ideas as the pair translated their overseas experiences and influences into the local Victorian context. (Criterion H)

Primary source

Trethowan Architecture, *South Melbourne Structure Plan Heritage Review* (2023)

Statement of Significance: House

Address:	110 Bank Street, South Melbourne	Name:	House
Place type:	Residential: House	Grading:	Significant
PS ref no:	HO563	Constructed:	1875-1888
Heritage precinct:	Not applicable	Citation No.:	2458



What is significant?

The house at 110 Bank Street, South Melbourne, is significant. Labourer James Donovan originally occupied the site in a three-room timber house c.1875. The house was enlarged c.1888 for shipwright James Grant. The verandah and balcony were added after 1895.

Alterations and additions, other than specified above, are not significant.

How is it significant?

The house at 110 Bank Street, South Melbourne, is of local historical and representative significance to the City of Port Phillip.

Why is it significant?

It is historically significant as it demonstrates how the housing in this area evolved over time, particularly as a house first built in 1875 and transformed by the 1880s enlargement. Its rarity reflects historical changes to municipal building regulations to prevent building fires that effectively limited the building of two-storey timber dwellings. f. The house was built in stages, first as a three-room timber house c.1875 when this section of Bank Street was initially developed, with the second storey added by shipwright James Grant, who lived here from 1888. (Criterion A)

The house is an example of a timber two-storey terrace styled house. This type of house is rare in the municipality with only a small number of examples extant. (Criterion B)

It is of representative significance as a nineteenth century two-storey timber house, representing the different building requirements and economic profile of South Melbourne in the late nineteenth century. It demonstrates characteristics of a timber two-storey terrace comprising timber block clad façade, hipped iron roof and rendered Italianate chimney. (Criterion D)

Primary source

Trethowan Architecture, *South Melbourne Structure Plan Heritage Review* (2023)

AUTHORISATION

Statement of Significance: Terrace

Address:	151, 153 & 155 Dorcas Street, South Melbourne	Name:	Terrace
Place type:	Residential: Terrace House	Grading:	Significant
PS ref no:	HO564	Constructed:	1885
Heritage precinct:	Not applicable	Citation No.:	2459



What is significant?

The terrace row built by 1881 at 151, 153 & 155 Dorcas Street, South Melbourne is significant.

Alterations and additions are not significant.

How is it significant?

The terrace row built by 1881 at 151, 153 & 155 Dorcas Street, South Melbourne is of local historical and representative significance to the City of Port Phillip.

Why is it significant?

The terrace row at 151, 153 & 155 Dorcas Street, South Melbourne is historically significant for its association with the typical development of this area of Dorcas Street late in the nineteenth century. (Criterion A)

The terrace row at 151, 153 & 155 Dorcas Street, South Melbourne is significant as a representative example of a double storey terrace development without an upper-level verandah. The three large houses demonstrate refined matching detailing including Italianate corbelled course and chimneys. The matching facades of rendered brick include large arched doorways with arched transom windows and Italianate pilasters, two tall sash windows with chamfered edges on the upper level and single wide sash windows with triple panes within raised pointed architraves on the ground level. The line of the former verandah is demonstrated by the extant side boundary walls with decorative moulded ends. (Criterion D & E)

Primary source

Trethowan Architecture, *South Melbourne Structure Plan Heritage Review* (2023)

AUTHORISATION

Statement of Significance: Corner shop & residence

Address:	108 Bank Street, South Melbourne	Name:	Corner shop & residence
Place type:	Retail and Residential: Corner Shop & Residence	Grading:	Significant
PS ref no:	HO565	Constructed:	1889
Heritage precinct:	Not applicable	Citation No.:	2460



What is significant?

The two storey late Victorian corner shop and residence built c.1889 at 108 Bank Street, South Melbourne, is significant.

The twentieth century alterations and additions are not significant.

How is it significant?

The corner shop and residence at 108 Bank Street, South Melbourne is of local historical and representative significance to the City of Port Phillip.

Why is it significant?

The corner shop is historically significant as a former grocery associated with the growth of South Melbourne in the late nineteenth century, in particular the pattern of corner shops accompanying new residential areas during the late Victorian boom period. (Criterion A)

The corner shop and residence is of representative significance as a good and largely intact example of a corner shop with residence above. It demonstrates important characteristics of corner shops in residential areas, notably chamfered corner addressing the street with corner entry and space for signage above, with street windows and separate side entry for the residence above. The roofline is distinguished from other examples by its prominent hipped slate roof also chamfered at the corner. Other detailing is restrained with cornice and sash windows above, with Italianate chimney. (Criterion D)

Primary source

This is an incorporated document in the Port Phillip Planning Scheme pursuant to section 6(2)(j) of the *Planning and Environment Act 1987*

Trethowan Architecture, *South Melbourne Structure Plan Heritage Review* (2023)

AUTHORISATION

This is an incorporated document in the Port Phillip Planning Scheme pursuant to section 6(2)(j) of the *Planning and Environment Act 1987*

Statement of Significance: Patons Brake Replacements (former)

Address:	167-173 Coventry, South Melbourne	Name:	Patons Brake Replacements (former)
Place type:	Commercial: Workshop / Showroom	Grading:	Significant
PS ref no:	HO566	Constructed:	1958
Heritage precinct:	Not applicable	Citation No.:	2461



What is significant?

The single-storey post-war Patons Brake Replacements workshop and showroom (former) designed by D. Graeme Lumsden and constructed c.1958 at 167-173 Coventry Street, South Melbourne is significant.

Later alterations are not significant.

How is it significant?

The Patons Brake Replacements workshop and showroom (former) at 167-173 Coventry Street, South Melbourne is of local historical and aesthetic significance to the City of Port Phillip.

Why is it significant?

The Patons Brake Replacements workshop and showroom (former) is historically significant as it demonstrates the development of local-enterprise industrial / commercial development in South Melbourne in the post-war period. In contrast to the larger industrial complexes set up for national businesses, the Patons Brakes Replacement building demonstrates smaller-scale industry for the local community. The function for automotive parts and repair demonstrates the post-war rise in private motor vehicle ownership. (Criterion A)

The Patons Brake Replacement workshop and showroom (former) is aesthetically significant as a good example of a post-war modernist industrial/commercial building in a functionalist mode with a restrained material palette and zoned façade. The building features a simple form with expressed

structure and horizontal emphasis offset by delicate vertical elements. Ornamentation is achieved through material placement and form, rather than application. It is a relatively rare example of a post-war industrial building at a small scale in the municipality. (Criterion E)

Primary source

Trethowan Architecture, *South Melbourne Structure Plan Heritage Review* (2023)

AUTHORISATION

Statement of Significance: Coca Cola Factory / Bottling Plant (former)

Address:	82-86 Clarke Street, South Melbourne	Name:	Coca Cola Factory / Bottling Plant (former)
Place type:	Factory	Grading:	Significant
PS ref no:	HO567	Constructed:	c1938-41
Heritage precinct:	Not applicable	Citation No.:	2462



What is significant?

The inter-war period Coca-Cola factory / bottling plant (former) at 82-86 Clark Street, South Melbourne, built c1938-41 is significant.

How is it significant?

The Coca-Cola factory / bottling plant (former) at 82-86 Clark Street, South Melbourne is of local historical, representative and aesthetic significance to the City of Port Phillip.

Why is it significant?

The Coca-Cola factory / bottling plant is historically significant for its association with the inter-war phase of industrial development in South Melbourne. It demonstrates smaller-scale industries that emerged prior to large national and international industrial complexes that developed north of City Road and at Fishermans Bend. (Criterion A)

The Coca-Cola factory / bottling plant is of representative significance as a fine and externally intact example of a functional modernist industrial building. The building fabric demonstrates the characteristics of a concrete structural grid frame, cuboid form, limited material palette, lack of ornamentation, use of clinker brick, and expansive areas of steel-framed glazing. The building is notable as an early example of the style, and for demonstrating modernist characteristics more typically associated with the post-war period. (Criterion D)

The Coca-Cola factory / bottling plant is of aesthetic significance as a fine example of early functional, industrial modernist design. The building demonstrates a stylistic purity across all elevations and is

rigorous in the application of functionalist principles. The Coca-Cola factory / bottling plant is important in exhibiting these aesthetic characteristics because it is an early and intact example. (Criterion E)

Primary source

Trethowan Architecture, *South Melbourne Structure Plan Heritage Review* (2023)

AUTHORISATION

Statement of Significance: J.E. Searls Engineers & Coppersmiths (Former)

Address:	49-55 York Street, South Melbourne	Name:	J.E. Searls Engineers & Coppersmiths (Former)
Place type:	Factory / warehouse	Grading:	Significant
PS ref no:	HO568	Constructed:	1943-45
Heritage precinct:	Not applicable	Citation No.:	2463



What is significant?

The late inter-war factory / warehouse at 49-55 York Street, South Melbourne, built 1943-45 for J.E Searls Engineers & Coppersmiths is significant.

Alterations and additions are not significant.

How is it significant?

The former J.E. Searls Engineers & Coppersmiths factory /warehouse at 49-55 York Street, South Melbourne is of local historical and representative significance to the City of Port Phillip.

Why is it significant?

The former J.E. Searls Engineers & Coppersmiths factory / warehouse is of historical significance as an early factory in the area and as an example of local manufacturing in support of the war effort. The building demonstrates the rapid expansion of manufacturing industries for military purposes which shaped the direction of post-war economic recovery. The site also demonstrates the importance of women in the workforce during the war, and the expansion of work opportunities to roles traditionally unavailable to women. (Criterion A)

The former J.E. Searls Engineers & Coppersmiths factory / warehouse is of representative significance as a good example of a late inter-war to post-war light industrial building with associated warehouse and offices. The building demonstrates a restrained, functional aesthetic with ornamentation achieved through brick colour and placement representative of the material and financial constraints during World War II. The building is representative of a phase of industrial /

commercial development in this part of South Melbourne during the early part of the twentieth century.
(Criterion D)

Primary source

Trethowan Architecture, *South Melbourne Structure Plan Heritage Review* (2023)

AUTHORISATION

Statement of Significance: Star Hotel (Former)

Address:	152 Clarendon Street, South Melbourne	Name:	Star Hotel (Former)
Place type:	Commercial: Hotel	Grading:	Significant
PS ref no:	HO569	Constructed:	c.1877, Remodelled 1941
Heritage precinct:	Not applicable	Citation No.:	2466



What is significant?

The former Star Hotel at 152 Clarendon Street South Melbourne, a c.1877 Victorian hotel remodelled in 1941 to a design by D.F. Cowell Ham, is significant.

Alterations and additions made after 1941 are not significant. The current paint colour scheme is not significant.

How is it significant?

The former Star Hotel is of local historical, representative and aesthetic significance to the City of Port Phillip.

Why is it significant?

The former Star Hotel is historically significant as the site has been a hotel continuously since 1854 when the Phoenix was opened as one of four original hotels in Emerald Hill. The current building was rebuilt in 1877 and expanded to incorporate shopfronts along Clarendon Street. The hotel is representative of the modernisation of Victorian-era hotels during the Interwar period, whereby earlier buildings were typically remodelled, upgraded and expanded in response to liquor license regulations. The corner address is reflective of the traditional practice of prominently locating hotels at prominent intersections. (Criterion A)

The former Star Hotel is of representative significance as a Victorian hotel remodelled in the 1940s. (Criterion D)

The former Star Hotel is of aesthetic significance as an example of a hotel remodelled in the Streamline Art Deco style. (Criterion E)

Primary source

Trethowan Architecture, *South Melbourne Structure Plan Heritage Review* (2023)

Statement of Significance: Alma & Tasma Terrace

Address:	68 & 70 Market Street, Southbank	Name:	Alma & Tasma Terrace
Place type:	Residential	Grading:	Significant
PS ref no:	HO570	Constructed:	c.1905
Heritage precinct:	Not applicable	Citation No.:	2467



What is significant?

Alma and Tasma Terrace, built c.1905 at 68 and 70 Market Street, Southbank is significant.

Alterations and additions are not significant.

How is it significant?

Alma and Tasma Terrace is of local representative and aesthetic significance to the City of Port Phillip.

Why is it significant?

Alma and Tasma Terrace is significant as a representative of single-storey terrace pair from the turn of the twentieth century. (Criterion D)

Alma and Tasma Terrace is significant aesthetically as a very late example of the transitional Victorian to Federation style. It demonstrates the survival of the Victorian Italianate terrace form into the early twentieth century, with Italianate elements including rendered chimneys, decorative parapet and party walls with rendered details. The rounded parapets flanked by small pilasters demonstrate the simpler Arts & Crafts style of the Federation period. (Criterion E)

Primary source

Trethowan Architecture, *South Melbourne Structure Plan Heritage Review* (2023)

This document is an extract for the purposes
of Exhibition

Port Phillip Heritage Review

Volume I – ~~Version 36, December 2021~~

Version 37, August 2024

~~December 2021~~ August 2024



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Prepared for the City of Port Phillip by Andrew Ward, Architectural Historian

1. Introduction

1.1 Port Phillip Heritage Review¹

The initial Review of the cultural heritage of the City of Port Phillip was commissioned in October, 1996. The consultant team was lead by Andrew Ward, architectural historian and supported by Francine Gilfedder, horticulturalist and garden historian. Data base management and map generation was undertaken by Ian Perry of Big Picture Software Pty. Ltd. and research assistance was provided by Jenny Dalrymple of Andrew Ward's office. The project was directed by Jim Holdsworth, manager, Urban Design and Strategic Planning at the City of Port Phillip and he was assisted during the early stages of the Study by Peter Boyle, architect, as project officer.

The project was undertaken simultaneously with the City of Port Phillip Urban Character Study and a joint project steering committee was established to give support to the consultant teams. The membership of the committee was as follows:

- Councillor Dick Gross (chair),
- Councillor Pat Browne,
- Councillor Liz Johnstone
- Jim Holdsworth, manager Urban Design and Strategic Planning,
- David Spokes, general manager, Community Planning,
- Steve Dunn, manager Business and Industry,
- Geoff Austin, Department of Infrastructure,
- Dale Wardlaw, Department of Infrastructure,
- Kim Dovey, University of Melbourne,
- Lyn Harrison, community representative,
- Adair Bunnett, community representative,
- Andrew Heslop, community representative, and
- David Brand, community representative.

The support of the members of this committee throughout the course of the Study is acknowledged with thanks, their knowledge of the Municipality, their expertise and commitment to the task being highly valued.

¹ The Port Phillip Heritage Review (Version 2) was gazetted as a part of Amendment C5 to the Port Phillip Planning Scheme in December 2000.

The City of Port Phillip, having been formed by the amalgamation of the Cities of Port Melbourne, South Melbourne and St. Kilda, had inherited several urban conservation studies. They had been prepared at different times, commencing in 1975 and using different criteria for the evaluation of the significance of places. Furthermore, the municipal boundaries represented discontinuities in the identified urban conservation areas, placing the integrity of the new City's conservation strategy at risk. The studies were as follows:

- Yuncken Freeman Ashton Wilson: South Melbourne Conservation Study (1975),
 - Jacobs Lewis Vines: Port Melbourne Conservation Study (1979),
 - Allom Lovell Sanderson Pty. Ltd.: South Melbourne Conservation Study (1987),
 - Nigel Lewis and Associates: St. Kilda Conservation Study Area 1 (1982),
 - David Bick: St. Kilda Conservation Study Area 2 (1985),
 - Robert Peck von Hartel Trethowan with Henshall Hansen Associates: City of St. Kilda Twentieth Century Architectural Study (1992), and
 - Allom Lovell and Associates: Port Melbourne Conservation Study Review (1995).
-

1.2 Updating the Port Phillip Heritage Review

Since the gazettal of the original Port Phillip Heritage Review in 2000, additional assessments of places and areas of heritage significance have been completed. The following heritage studies were commissioned:

- Graeme Butler and Associates: *Swallow Street* (2004)
- Heritage Alliance: *East St Kilda Heritage Study* (2004)
- Heritage Alliance: *Elwood Heritage Review* (2005)
- Heritage Alliance: *Nightingale Street Heritage Study* (2008)
- Heritage Alliance & Built Heritage: *Review of Heritage Overlay 3* (2009 / 2010)
- Lovell Chen: *Review of Heritage Overlay 1* (2011).
- Lovell Chen: *Review of Heritage Overlay 1 Port Melbourne – Stage 2 Review* (2012)
- Peter Andrew Barrett: *Tiuna Grove, Elwood* (2019)
- RBA Architects and Conservation Consultants: *Fishermans Bend Heritage Review: Montague Commercial Precinct* (2019)
- David Helms Heritage Planning: *Port Phillip Heritage Review Update* (2019)
- David Helms: *HO6 St Kilda East Precinct Review* (2020)
- Context: *Heritage Assessment 588-590 City Road South Melbourne* (2017)
- Context: *Heritage Assessment 2-6 Blanche Street and 110-118 Barkly Street, St Kilda* (December 2017)
- Peter Andrew Barrett: *Heritage Assessment 58-60 Queens Road, Melbourne* (November 2017)
- David Helms Heritage Planning: *Port Phillip Heritage Review Update* (2019)
- [Trethowan Architecture, *South Melbourne Structure Plan Heritage Review* \(2023\)](#)

The resultant findings have been included in the Port Phillip Heritage Review through the Planning Scheme Amendment process.²

The assessment framework used in each of the above heritage assessments is based upon the framework developed by Andrew Ward as outlined in the following chapters.

AUTHORISATION

² The Port Phillip Heritage Review is an incorporated document in the Port Phillip Planning Scheme. Planning Scheme Amendments C24, C29, C32, C46, C52, C54, C70, C72, C74, C89 and C103 enabled new information on heritage places and heritage areas within the municipality to be included in the Port Phillip Heritage Review.

City of Port Phillip Heritage Review

Place name: City Edge
Other names: -

Citation No:
2456



INSERT MAP

Address: 89 & 99 Eastern Road, and 36A and 58A Napier Street, South Melbourne

Heritage Precinct: None

Category: Residential

Heritage Overlay: HO561

Period/Style: Late Twentieth Century/Brutalist-Functionalism

Graded as: Significant

Constructed: 1971-75

Victorian Heritage Register: No

Designer: Daryl Jackson & Evan Walker

Amendment: TBC

Comment: New Citation

Significance

What is significant?

The apartment complex City Edge in its garden setting including mature Australian native trees, designed by Daryl Jackson and Evan Walker and constructed in stages from 1971 to 1975, at 89 & 99 Eastern Road, and 36A and 58A Napier Street, South Melbourne is significant.

Alterations and additions made after 1975 are not significant.

How is it significant?

City Edge is of local historical representative, aesthetic and associative significance to the City of Port Phillip.

Why is it significant?

City Edge is historically significant for its association with the theme of later twentieth century higher density development in the municipality, particularly as a reaction to the earlier slum clearances initiated by the Housing Commission of Victoria in the post-war period. The complex is an outstanding example of a mixed housing development that sought a more contextually and architecturally sensitive local response to the demand for new and higher density modern housing in the municipality in the mid to late twentieth century. City Edge's success helped turn the Housing Commission away from building more high-rise flats and pointed the way towards new forms of housing in the municipality in the 1970s-80s. (Criterion A)

City Edge is significant as an outstanding local example of the work of architects Daryl Jackson and Evan Walker. The development was awarded an RIAA medal for its innovative resolution of social and physical problems. The complex integrates a variety of housing into a large high-density development for the time, integrating diverse sized residences into the landscape and so creating a more sensitive response to the traditional urban scale of South Melbourne. Architecturally, the complex is notable for the way building mass is stacked to optimise the site limitations and orientations towards views and sunlight, balancing the competing demands for privacy and sociability. It incorporates the traditional functions of front gardens, balconies and footpaths into a brutalist/functionalist styled modern complex of interconnected internal landscapes, causeways and terraces in order to create its own 'neighbourhood' over and above the parking below. The complex is a significant and pioneering representative of the higher density mixed housing type in the municipality. (Criterion D & E)

City Edge is significant for its association with Daryl Jackson and Evan Walker. The development was a highlight and defining project of the professional pairing of Jackson and Walker. Daryl Jackson went on to become a renowned and influential Australian architect and an associate professor at the University of Melbourne. Evan Walker took his ideas in a more political direction, entering Victorian parliament and becoming minister for planning in the 1980s. In City Edge, the combination of their political and architectural influences is demonstrated by the dedication to mixed development and modern design ideas as the pair translated their overseas experiences and influences into the local Victorian context. (Criterion H)

Thematic context

Victoria's framework of historical themes

6. Building towns, cities and the garden state: 6.3 Shaping the suburbs

Port Phillip thematic environmental history

5. Buildings & Cultural Landscapes: 5.3.5 Higher-density housing

History

Contextual history

The suburb of South Melbourne, known as Emerald Hill between 1855 and 1883, is part of the traditional Country of the Yalukit Willam clan of the Boon Wurrung people, part of the broader Kulin nation, whose name means 'people of the river'.

South Melbourne, initially called Emerald Hill by British colonists, was hemmed in on all sides by the swampy lowlands. The colonists treated the 'island' as a wasteland for grazing, dumping, camping and recreation in the 1830s-40s. To the east of the future Kings Way, settlement was not only constrained by the swamplands but also by the desire to keep free the area around the military reserve west of St Kilda Road. The first sixty-seven allotments of the 1852 Subdivision of Emerald Hill were auctioned on 18 August. (Lemon, p.7) Many buyers were speculators, none of whom planned to reside in the area. Development in the 1850s was rapid, with 1,149 houses assessed for rates by the Melbourne Corporation by 1855. (Lemon, p.8) The Sandridge Road was improved to make it an acceptable path through the marshes. Toll gates were established on the major roads through the district to raise funds up until 1878. The Emerald Hill municipality was created in 1855 and a railway line through from Melbourne to St Kilda opened in 1857. Emerald Hill was free from the building restrictions of Melbourne municipality that had been introduced to reduce the risk of fire. This led to the rapid construction of timber buildings in the suburb.

From the 1850s-1860s the gold rush period saw the rapid formation of a new suburb, 'out of the canvas' as new arrivals sought new residences and services close to their arrival point in the colony. While land northwards was eventually reclaimed for industrial use, expansion eastwards was stymied by the military reserve (later Victoria Barracks). The military reserve extended as far west as Moray Street, but was reduced to Hanna Street (King's Way) in 1862. Lowlands to the south would eventually be reclaimed for recreation (Albert Park). In the 1870s-80s the area became known for its cheap and well-located land for factories and worker housing. The population of South Melbourne grew from 17,101 in 1871, to 25,374 in 1881. In 1881 most of the land between the Barracks and Emerald Hill was sold, requiring filling and leveling. As a result, population saw an even greater increase to 41,724 by 1891. After the 1890s Depression, the area reached a new peak in industrial and residential development in the early twentieth century, but population growth slowed to 46,190 in 1911. In the Interwar and early post-war period South Melbourne fell into relative decline as population fled the inner city.

By 1947 the population fell to 43,452 and saw further declines in the post-war period. The remnant inner urban 'slums' became a concern of new government interventions in the 1960s, with significant interventions by the Victorian Housing Commission to demolish what was considered substandard housing and move tenants into modern tower developments. Nonetheless, the late twentieth century saw a resurgence in appreciation for the communities and identity of the inner city, including South Melbourne. This led to movements to conserve historic areas as well as experiments in new forms of medium density housing. New processes of gentrification concurrently led to fresh development pressures and an economic transformation of the area away from its industrial past.

From Thematic History 5. Buildings & Cultural Landscapes: 5.3.5 Higher-density housing:

After significant community backlash, the Housing Commission of Victoria abandoned high-rise flat development in the early 1970s and began to explore alternatives. In 1975, the Commission erected a three-storey block of modern terrace houses in Raglan Street, South Melbourne, which won the Royal Australian Institute of Architects (RAIA) Bronze Medal for housing that year – the first time the Commission had ever received a state architectural award. A similar block was erected on the former Loreto Convent site in Beaconsfield Parade. The Commission later employed several known or emerging architectural practices to design small-scale medium density or infill housing throughout South Melbourne and Port Melbourne in a sympathetic fashion that paid homage to the traditional architecture and streetscapes of these areas. After the initial burst of development in the 1960s and early 1970s, very few private high-rise apartments were built.

At the same time the Commission was ending its high-rise program, some architects also began exploring an alternative to the typical suburban house and high-rise apartment block. The City Edge development, designed by Daryl Jackson and Evan Walker and constructed in four stages from 1971 to 1975, is a pioneering example comprising a complex of 175 apartments across three-storey and five-storey buildings in a landscaped setting. It was one of several medium density housing developments in inner Melbourne built on land originally identified by the Commission for slum reclamation. According to one account, the scheme:

... found a balance between private and public space in a large scaled housing project. How to live close together and gain all the benefits of the single family house was the problem to be solved by the architects.

Following the success of the Housing Commission's Raglan Street development in 1975, City Edge won the equivalent RAIA Bronze Medal in 1976.

City Edge

The City Edge complex was built in stages starting in 1972. Architects Daryl Jackson and Evan Walker were influenced by the debates at the time around the renewal of the inner cities. Post-war suburban drift had hollowed out the centres of Australian cities and created new suburban landscapes decried by Modernist

architects for their banality and uniformity. At the same time, authorities such as the Housing Commission of Victoria had set about clearing 'slums' and transforming social housing along a more Corbusier-inspired high-rise vision.

Evan Walker's political activism began in the 1970s, a decade in which he led several activist oriented organisations including the Collins Street Defence Movement and Australian Frontier. Australian Frontier was a social research organisation whose remit included the issue of social housing in Australian cities. Walker's involvement in the Labor Party also began at this time (Evan Walker, 'If a Thing's Worth Doing'. Ch.7).

Writing in his memoirs, Evan Walker recalled:

[...] both Daryl and I had a very clear idea of the social value of architecture and the responsibilities of architects. Daryl drew a link to Finnish concepts of appropriate architecture. You can see this in the City Edge development (to which Andrew McCutcheon also contributed at the conceptual level). There was strong disappointment at the time that the Victorian Ministry of Housing was not able to take up such an idea and run with it, but rather continued to build projects that were doomed to become ghettos. City Edge was a mixed housing project in South Melbourne where families of varying income status would live side-by-side. (Evan Walker, 'If a Thing's Worth Doing')

Constructed by Breschi Constructions, these low-rise high-density flats and townhouses were envisioned as alternatives to both suburban houses and high-rise apartments. The development contained a variety of dwelling types – apartments and townhouses of varying sizes. It took design cues from both Modernism and the more traditional function of historical street level neighbourhoods:

Mass is stacked to optimise the site limitations and orientations towards views and sunlight, balancing the competing demands for privacy and sociability. The result is an architecture of aggregation, with windows punched into walls separating dwelling and court, courts connecting gardens and footpaths, and footpaths leading to entrances. These hierarchies clarify function and what is communal. Landscape and streetscape were most carefully considered. Although it is an individual design, it still acknowledges the neighbouring nineteenth-century terrace houses with their front gardens, balconies and street colours. (Daryl Jackson 1984)

Writing a retrospective of Jackson's oeuvre, Patrick McCaughey located City Edge as the first of a series of inner urban schemes by the architect that opened up a 'curious dialogue' within the practice and a new era for Jackson's architecture. Other subsequent projects were the North Melbourne hospital housing project (1978), Southport housing scheme in South Melbourne (1980-81) and a 'quasi urban scheme' Emu Ridge in Canberra (McCaughey, p.6). McCaughey wrote:

The first full-scale project, City Edge, was designed as an alternative to the high rise Housing Commission blocks of flats – the pits of Australian architecture – which had been foisted on almost every inner Melbourne suburb. There had to be another way to develop the inner city suburbs and supply high density housing. The search for an alternative took Jackson back to a free adaptation of the Victorian terrace house with its verandah, narrow frontage, and street rhythm. To this concept he added a version of early modernism's classic planning, with its internal pedestrian street and sense of a garden enclave. Together they provided as an obtainable goal, a new image for high density housing. The result, especially with the careful planting of Australian native trees and shrubs, was both satisfying and human. It may have lacked the architectural pyrotechnics of early Jackson, but in addressing himself to critical social and urban problems Jackson renewed his claim to the 'high seriousness' of an engaged architect (McCaughey, p.7).

The design was recognised by the Royal Australian Institute of Architects in 1976:

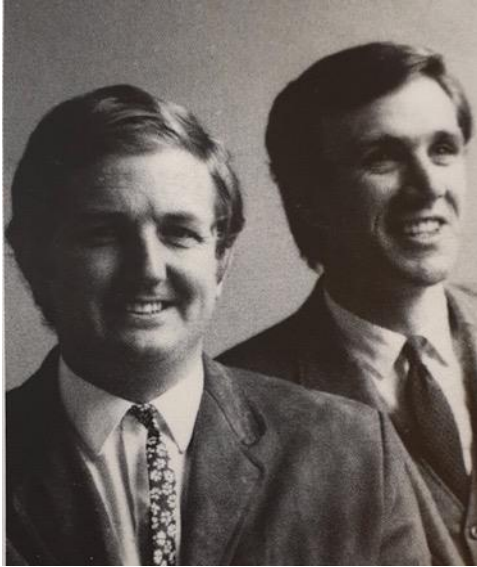
The young firm of Daryl Jackson and Evan Walker dominated the awards, winning one bronze (first prize) and four citations.

Jackson and Walker was awarded the Housing Projects bronze medal for their massive 'City Edge' development on the corner of Kingsway and Napier Street, in South Melbourne.

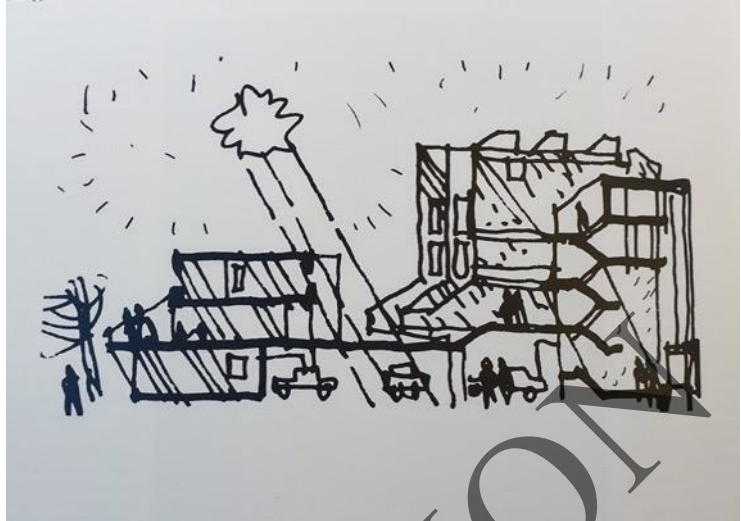
City Edge, which packs 175 low-rise units into a site of 3.5 acres, was described by the jury as providing an innovative resolution of the social and physical problems. The land was originally acquired by the Housing Commission for slum reclamation. City Edge's success had helped turn the commission away from building more high-rise flats, said the jury. (Top award to 'slum project' (The Age, 20 October 1976, by Tim Colebatch)

In 'Sensibility and Structure beyond Earth and Sky', Daryl Jackson described the project a decade later in his own words:

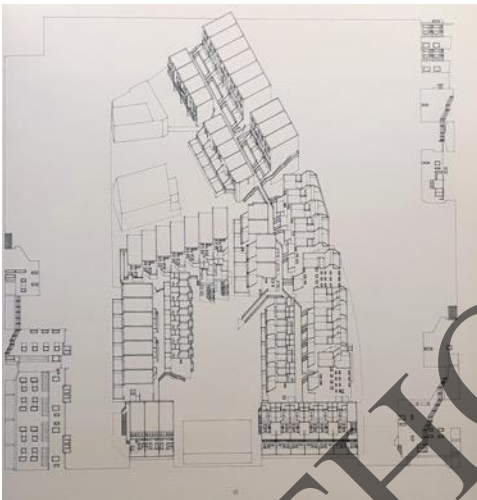
Even in the 1980's high density living is an exception in Melbourne. This set of low-rise high density flats/town houses are intended as alternatives to both suburban house and high rise apartments. The upper level footpath, and ramp system subjugate the automobile. Other important aims were an identity for each unit, a variety of unit types, landscape quality and aspects of streetscape encounter. The architecture sets up its own conditions, acknowledging its debt to nineteenth century terrace houses, with their own front gardens, balconies and street doors. Mass is stacked to accord with site limitation and opportunity, orientation toward view and sun, and the perennial demands for 'privacy' as well as 'sociability, the inherent and conflicting duality of neighbourhood. The result is an architecture of aggregation with windows punched into walls, walls separating dwelling and attached court, courts connecting gardens and footpaths, footpaths leading to entrances. These hierarchies clarify function, ensure legibility, and underpin individual and communal consciousness. The vernacular typology describes how the combine to form the whole. Ten years after completion, only the residents have changed.



'The Partners' Evan Walker & Daryl Jackson.
Source: Evan Walker.



A sketch by Daryl Jackson outlining the concept of City Edge's emphasis on light and human scale and activity. Source: Daryl Jackson *Drawings and Photographs* 1984.



View of City Edge from Daryl Jackson's retrospective portfolio 1984.



Model of City Edge made by Rob Wallace in 1989. It was donated to Museum Victoria for the exhibition 'Home Sweet Home: Changes in Victorian Domestic Architecture 1839 - 1989'. Source: Museums Victoria.

References

Andrew Lemon, 'A History of South Melbourne', p.4/1 of Allom Lovell Sanderson, *City of South Melbourne Urban Conservation Study* 1986

Daryl Jackson, *Architecture, Drawings and Photographs* 1984.

McCaughey 'The Architecture of Daryl Jackson', 1984.

Evan Walker, *If a Thing's Worth Doing*, unpublished memoir held by State Library of Victoria.

The Age, as cited.

Museums Victoria.

Way Back When, *City of Port Phillip Thematic Environmental History*, 2021.

Description

The City Edge residential complex comprises a group of buildings of varying scale connected via concrete pedestrian causeways and Australian native garden landscape with entries and street frontages along Eastern Road, Park Street and Napier Street.

The taller buildings are along Napier Street, with frontages oriented north facing the sunlight with terraces onto an internal open walkway. Ground level car parking entered off Napier Street is situated underneath the apartments and causeways. Four levels rise sheerly above the car park level, making five storeys including car park along Napier Street. A series of stairs join each individual block from the central raised causeway at the first level. Other external concrete staircases connect the causeway to the carpark and street level.

The central rows are attached three-storey townhouses situated to the north to maximise light to the southern blocks, with their own individual north-facing terraces and gardens. These blocks have their own individual entrances opening on the central walkway.

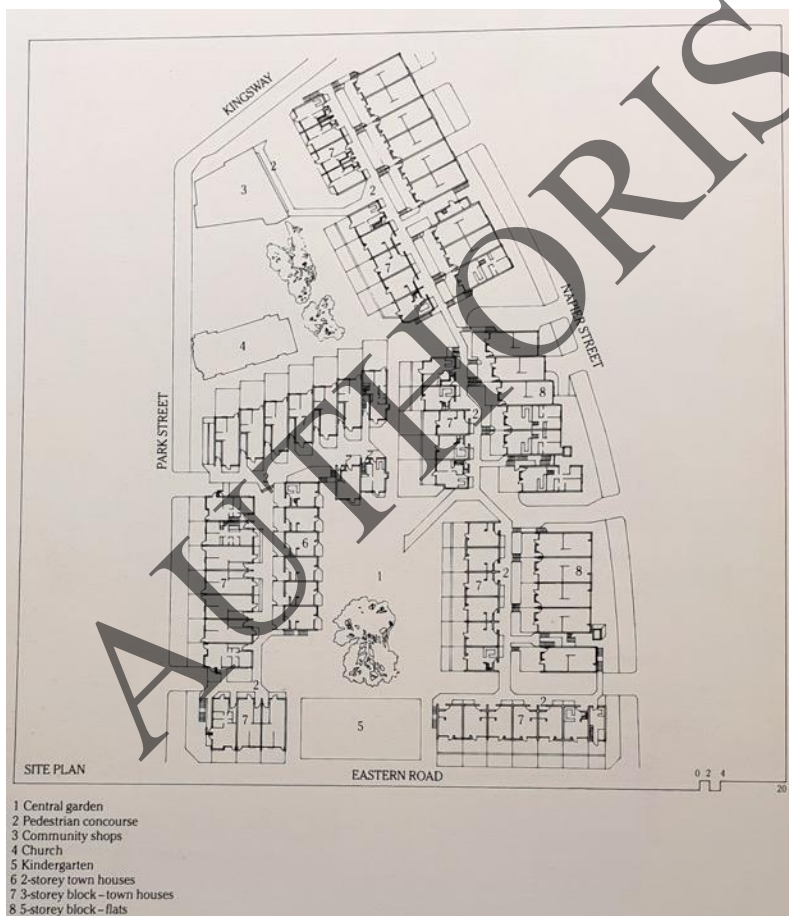
The northern residential blocks are three and two storey townhouses arranged around a central garden area with open pedestrian walkways above car parking entered off Park Street. A row of attached townhouses is staggered to capture light along the centre-east boundary.

The complex is unified by the network of causeways and pedestrian paths, the internal garden landscape, and by uniform design features characteristic of a brutalist and functionalist style with strongly rectangular block form. Defining characteristics include the material palette of sheer brick walls, concrete causeways, stairs and terraces with timber balustrades. The external facades are dominated by plain brick walls and repeated sets of rectangular glazed windows with brick soldier course sills and plain concrete lintels. The internal facing facades are characterised by concrete terraces with timber balustrades and horizontal rectangular balconies and glazed windows above continuing the pattern of rectangular timber frame windows with brick sills and concrete lintels. Balconies have concrete balustrades with timber planter boxes. Sheer brick walls punctured with vertical windows tend to dominate side or rear facades.

The internal garden landscape comprises paths and lawns, with plantings of mature native trees concentrated in and around the central garden. Mature native trees also flank some pedestrian causeways and along the boundaries of the complex.



Aerial view of City Edge, outlined in red. Source: Google Maps.



Plan of City Edge. Source: Daryl Jackson 1984



View of City Edge along Napier Street.



View of the townhouses with garden and terraces.



View of a side staircase on the Napier Street carpark entry.



A Napier Street pedestrian entry.



View of the western townhouse block with causeway over carpark.

Comparative analysis

Late twentieth century residential architecture is not well represented on the Port Phillip Heritage Overlay. Comparable examples of higher density residential development and Brutalist/Functionalist design include:

- 169 Hotham Street, Balaclava (Significant within HO7). This is one of the most notable of St Kilda's conventional 1960s blocks of flats, designed by Michael R.E. Feldhagen and noted for its series of articulated forms and surfaces with the ground floor masonry walls extending from the building to form courtyard gardens and boundary walls. The flats at 169 Hotham Street are comparable architecturally as sharing a more functionalist style emphasising clean lines, block forms and brick textured walls, but on a far more modest scale than the subject property.
- 20 Shirley Grove, St Kilda East (HO399) is significant as a fine and intact example of a post-war flat complex in the International style. It is significant less for the individual buildings that comprise the complex than for the planning concept of flats arranged around a large garden courtyard and its generous provision of open space. The subject site compares favourably to Shirley Grove in terms of its mature garden landscape, but in a Brutalist/Functionalist rather than International style.

Stylistically comparable brutalist or functionalist residential architecture is also rare on the Port Phillip Heritage Overlay. Examples include:

- 2 Crimea Street, St Kilda (Significant within HO6). These maisonettes, designed by Michol Design and constructed in 1976-77, are of aesthetic significance as a distinctive block of townhouses that show the influence of the Brutalist style. This is demonstrated by the distinctive first floor composition consisting of jutting battered walls cantilevered in bays over the ground floor garages to the south, which creates a bold sculptural composition. The setting of the townhouses is complemented by the original front fence and landscaping scheme. The maisonettes at 2 Crimea Street are comparable in terms of period, but far more modest in scale and with greater emphasis on Brutalist concrete sculptural form as opposed to brick when compared to the subject site.
- 21 Redan Street, St Kilda (HO503). These maisonettes are of local historic, architectural and aesthetic significance as a fine and intact example of the work of John & Phyllis Murphy who are remembered for their contribution to the residential architecture of Melbourne in the post-war era. The maisonettes at 21 Redan Street are earlier and also more modest than the subject property but display a more comparable concern for functional planning around light and emphasise similarly European and Scandinavian concepts of liveable multi-unit design. Both demonstrate bold and functional planning emphasising access to north light, plain brick walls and well-proportioned windows.

By comparison to these other significant post-war and late twentieth century higher density housing complexes, City Edge is notable for its strong functionalist emphasis on material and design simplicity around light and humanistic principles borrowed from northern Europe. These include the integration with landscape and garden setting, the dominance of the pedestrian causeway over the parking, the creation of spaces and opportunities for human interaction in an attempt to translate traditional neighbourhood functionality into a modern context and Brutalist style in the 1970s.

Both historically and architecturally, City Edge may also be compared to the HCV Raglan Street estate. This estate has been graded Contributory to HO440 Emerald Hill Residential Precinct. This estate was designed by the Chief Architect's Branch and includes split level maisonettes and single level apartments. Each unit includes enclosed courtyard or balcony, with courtyards connected to communal open space. Like the subject property, the units sit above an undercroft carpark. The blocks are constructed of dark brown brick with tiled gabled roofs. The distinctive elevation is broken into bays by projecting wing walls and comprises rows of windows separated by rendered spandrels. The development was completed by 1974 and in 1975 won the Royal Australian Institute of Architects Bronze Medal for Housing, the first Commission development to be so honoured at the state level. While City Edge demonstrates similar

elements of design in terms of materiality and layout, it is distinguished by its larger scale and special architectural pedigree. In terms of aesthetics, the subject site demonstrates purer Brutalist design aesthetic, including use of exposed concrete and flat roofs. Historically, the subject site is distinguished as a mixed private development on previous HCV slum clearance land. The native garden setting of the subject property is also more extensive and intact.



169 Hotham Street, Balaclava



Shirley Grove



2 Crimea Street, St Kilda



21 Redan Street, St Kilda

Assessment

This place has been assessed in accordance with the processes and guidelines outlined in the Australia ICOMOS *Charter for Places of Cultural Heritage Significance (The Burra Charter) 2013*, using the Hercon criteria.

Recommendations

Recommended for inclusion in the Heritage Overlay as an individual place.

Extent of HO: Whole of property as defined by the title boundaries.

Tree Controls are recommended for the Schedule to the Heritage Overlay. No other specific HO controls are required.

Primary source

Trethowan Architecture, *South Melbourne Structure Plan Heritage Review*, 2023

Other studies

Not applicable.

Other images

AUTHORISATION

City of Port Phillip Heritage Review

Place name: House
Other names: -

Citation No:
2458



Address: 110 Bank Street, South Melbourne

Heritage Precinct: None

Category: Residential: House

Heritage Overlay: HO563

Style: Victorian

Graded as: Significant

Constructed: c.1875-1888

Victorian Heritage Register: No

Designer: Unknown

Amendment: TBA

Comment: New citation

Significance

What is significant?

The house at 110 Bank Street, South Melbourne, is significant. Labourer James Donovan originally occupied the site in a three-room timber house c.1875. The house was enlarged c.1888 for shipwright James Grant. The verandah and balcony were added after 1895.

Alterations and additions, other than specified above, are not significant.

How is it significant?

The house at 110 Bank Street, South Melbourne, is of local historical and representative significance to the City of Port Phillip.

Why is it significant?

The house is historically significant as it demonstrates how the housing in this area evolved over time, particularly as a house first built in 1875 and transformed by the 1880s enlargement. Its rarity reflects historical changes to municipal building regulations to prevent building fires that effectively limited the building of two-storey timber dwellings. f. The house was built in stages, first as a three-room timber house c.1875 when this section of Bank Street was initially developed, with the second storey added by shipwright James Grant, who lived here from 1888. (Criterion A)

The house is an example of a timber two-storey terrace styled house. This type of house is rare in the municipality with only a small number of examples extant. (Criterion B)

It is of representative significance as a nineteenth century two-storey timber house, representing the different building requirements and economic profile of South Melbourne in the late nineteenth century. It demonstrates characteristics of a timber two-storey terrace comprising timber block clad façade, hipped iron roof and rendered Italianate chimney. (Criterion D)

Thematic context

Victoria's framework of historical themes

6. Building towns, cities and the garden state: 6.3 Shaping the suburbs

Port Phillip thematic environmental history

5. Buildings and cultural landscapes: 5.1 Early dwellings; 5.2 Shaping the suburbs

History

Contextual history

The suburb of South Melbourne, known as Emerald Hill between 1855 and 1883, is part of the traditional Country of the Yalukit Willam clan of the Boon Wurrung people, part of the broader Kulin nation, whose name means 'people of the river'.

South Melbourne, initially called Emerald Hill by British colonists, was hemmed in on all sides by the swampy lowlands. The colonists treated the 'island' as a wasteland for grazing, dumping, camping and recreation in the 1830s-40s. To the east of the future Kings Way, settlement was not only constrained by the swamplands but also by the desire to keep free the area around the military reserve west of St Kilda Road. The first sixty-seven allotments of the 1852 Subdivision of Emerald Hill were auctioned on 18 August. (Lemon, p.7) Many buyers were speculators, none of whom planned to reside in the area. Development in the 1850s was rapid, with 1,149 houses assessed for rates by the Melbourne Corporation by 1855. (Lemon, p.8) The Sandridge Road was improved to make it an acceptable path through the marshes. Toll gates were established on the major roads through the district to raise funds up until 1878. The Emerald Hill municipality was created in 1855 and a railway line through from Melbourne to St Kilda opened in 1857. Emerald Hill was free from the building restrictions of Melbourne municipality that had been introduced to reduce the risk of fire. This led to the rapid construction of timber buildings in the suburb.

From the 1850s-1860s the gold rush period saw the rapid formation of a new suburb, 'out of the canvas' as new arrivals sought new residences and services close to their arrival point in the colony. While land northwards was eventually reclaimed for industrial use, expansion eastwards was stymied by the military

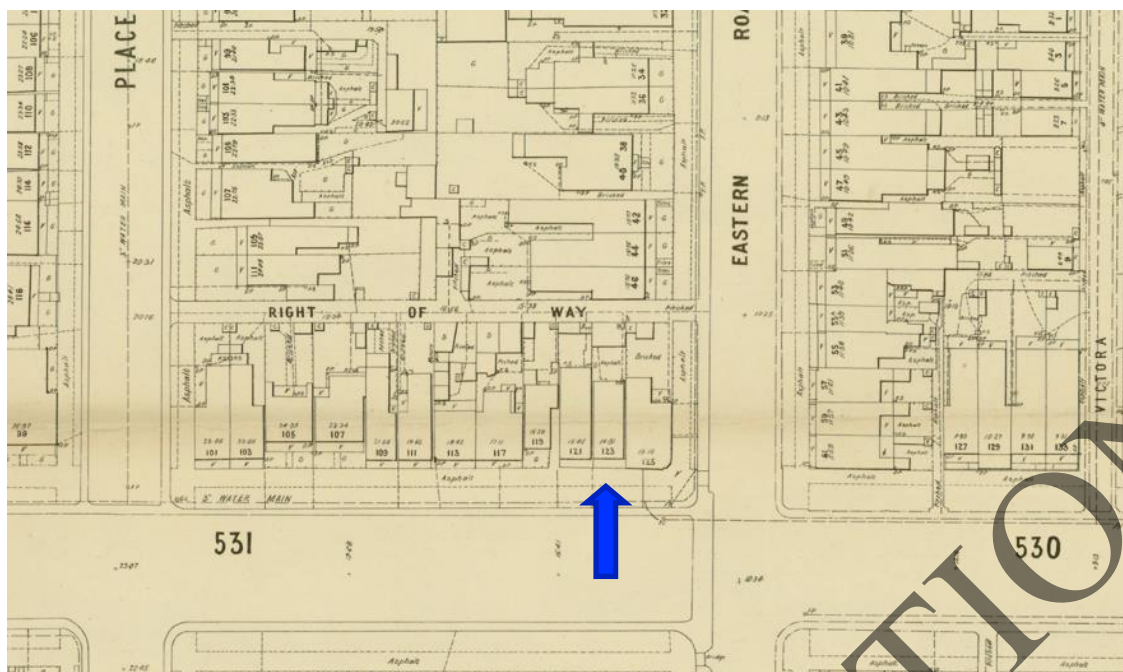
reserve (later Victoria Barracks). The military reserve extended as far west as Moray Street, but was reduced to Hanna Street (King's Way) in 1862. Lowlands to the south would eventually be reclaimed for recreation (Albert Park). In the 1870s-80s the area became known for its cheap and well-located land for factories and worker housing. The population of South Melbourne grew from 17,101 in 1871, to 25,374 in 1881. In 1881 most of the land between the Barracks and Emerald Hill was sold, requiring filling and leveling. As a result, population saw an even greater increase to 41,724 by 1891. After the 1890s Depression, the area reached a new peak in industrial and residential development in the early twentieth century, but population growth slowed to 46,190 in 1911. In the Interwar and early post-war period South Melbourne fell into relative decline as population fled the inner city.

By 1947 the population fell to 43,452 and saw further declines in the post-war period. The remnant inner urban 'slums' became a concern of new government interventions in the 1960s, with significant interventions by the Victorian Housing Commission to demolish what was considered substandard housing and move tenants into modern tower developments. Nonetheless, the late twentieth century saw a resurgence in appreciation for the communities and identity of the inner city, including South Melbourne. This led to movements to conserve historic areas as well as experiments in new forms of medium density housing. New processes of gentrification concurrently led to fresh development pressures and an economic transformation of the area away from its industrial past.

This section of Bank Street (East – North Side) was first taken up by purchasers after 1865, and by 1870 there were nine houses on the block between what was then Moray Place and Eastern Road. These were chiefly working-class residences such as labourer Charles Hine who owned and occupied an iron shack at then No.99. There were also several small timber houses, and the brick duplex on the corner of Moray Place that saw a variety of tenants. Gradually houses were upgraded or added to, some by small owner-occupiers. James Grant shipwright extended his house to five rooms with a stables in 1888 and next door the new resident James Mather, ironmonger, saw the iron shack upgraded to three-room timber house. The block was fully occupied by 1889 with the construction of the corner brick shop and residence, which was operated as a local grocery by John Windle through the 1890s.

I 10 Bank Street

A timber three room house was first built on the site by 1875, when it was occupied by labourer James Donovan. At that time, it was No.101 Bank Street, one of nine properties that had been built along this block after 1865 when the area was still known as Emerald Hill. The Donovans also owned the adjacent, then vacant, corner block. In 1888, shipwright James Grant moved into the enlarged 5 room timber house with stable on the site. Before moving to Bank Street, James Grant had lived in Cecil Street with his wife until her death in 1880, and before then in Clarendon Street (1877). Grant was still living in the house as owner-occupier at what was then 123 Bank Street in 1895 when he gave evidence at the inquest into the wreck of the 'Alert', a bay steamer he had helped refurbish. By that time, he was a shipwright of forty years' experience, having attained a certificate in navigation in 1875. During his career he had been chief officer of the 'Omeo', trading between Melbourne and Adelaide. He was also second mate and carpenter on board of a vessel trading to New Zealand, and on the 'Royal Bride.' By 1895 Grant had also purchased the neighbouring property at No.123 Bank Street. In 1899, Grant continued to own both properties. In this year however, Grant resided in the neighbouring cottage at No.121 while an Eleanor Graham resided at the enlarged 6 room house at No.123.



The house, then No.123 Bank Street, indicated on the 1895 MMBW Plan. Source: State Library of Victoria.

References

Andrew Lemon, 'A History of South Melbourne', p.4/1 of Allom Lovell Sanderson *City of South Melbourne Urban Conservation Study* 1986

Census of Victoria

Way Back When, *City of Port Phillip Thematic Environmental History* 2021

South Melbourne Rate Books (RB)

Sands and McDougall's Street Directories (SM), 1870-1920

The Age, as cited.

Andrew Ward & Associates, *Port Phillip Heritage Review*, 1998

Melbourne & Metropolitan Board of Works (MMBW) Detail Plans

Description

The house at 110 Bank Street is a two-storey single fronted Victorian-era timber house. It has timber block cladding to the façade. At ground level is a timber door with a transom window above. Beside this is a timber framed sash window with sidelights and timber architraves. At the first floor are a pair of tall timber framed double sash windows set within timber architraves, opening onto a timber verandah. The verandah has a lacework balustrade, with lacework and scalloped detailing to the verandah frieze. To the sides of the porch and verandah are low timber nib walls.

A rendered Italianate chimney rises from the central ridge of the metal sheet hipped roof. A second rendered Italianate chimney is visible to the rear on the east side of the house. The hipped roof extends to the rear of the house. The rear walls are weatherboard, with a smaller balcony with verandah behind.

Some alterations are evident such as the non-original timber picket front fence and new verandah posts, however it is largely intact in appearance.

The house is adjacent to a nineteenth century corner shop.

Comparative analysis

The house at 110 Bank Street can be compared to other double storey timber houses of the nineteenth century in the municipality, particularly around Port Melbourne and South Melbourne. These buildings are relatively rare as a building type, with most timber houses being smaller single storey cottages. While single-storey timber houses continued to be built in large numbers in South and Port Melbourne and elsewhere through the nineteenth century and into the twentieth century, two-storey timber houses are rare after the 1870s due to changes to municipal building regulations to prevent building fires that effectively stopped the building of two-storey timber dwellings with no fire separation.

- The house at 64 Cruikshank Street, Port Melbourne (Citation 634) is a comparably scaled two-storey single-fronted timber-framed house with similar timber block fronted cladding to the façade. It was built in the last decade of the nineteenth century, making it only slightly more recent than the subject property.
- The house at 9 Church Street, South Melbourne (Citation 968) is another double story timber house apparently constructed and enlarged in phases from 1856, 1873 and the 1880s. Like the subject property it is also clad in weatherboards to represent ashlar blocks to the front façade, with a more recent verandah addition to the front.
- The house at 225 Cecil Street, South Melbourne (Contributory in HO440 precinct) is another two-storey timber house with block cladding, hipped metal roof with centrally located render Italianate chimney but with a smaller verandah at ground level only. By comparison to the subject house, it is less intact due to the replacement of original windows with more horizontally expressed interwar windows.
- The house at 99 Raglan Street, South Melbourne (Citation 2220), built c.1885 for the clerk, James Muir has a comparable presentation as a surviving timber house. However, it is wider with the possible addition of timber balcony and French doors to the upper level. The subject site compares well as an example of remnant timber housing in an otherwise brick streetscape.

Overall, the house at 110 Bank Street compares well as a relatively intact example of remnant timber housing among a small group of extant timber houses in the municipality. It demonstrates key features such as the timber block pattern, setback from the street, and modest Italianate decorative elements such as the rendered chimney and timber window and door architraves. It demonstrates a similar history of addition over time, such as the front balcony and verandah.



64 Cruikshank Street, Port Melbourne



9 Church Street, South Melbourne



225 Cecil Street, South Melbourne.



99 Raglan Street, South Melbourne.

Assessment

This place has been assessed in accordance with the processes and guidelines outlined in the Australia ICOMOS *Charter for Places of Cultural Heritage Significance (The Burra Charter) 2013*, using the HERCON criteria.

Recommendations

Add to the Heritage Overlay as an individual place.

No specific HO controls are required.

Extent of HO: Whole of property as defined by the title boundaries.

Primary source

Trethowan Architecture, *South Melbourne Structure Plan Heritage Review*, 2023.

Other studies

-

Other images



City of Port Phillip Heritage Review

Place name: Terrace

Other names: Houses

Citation No:

2459

INSERT MAP



Address: 151, 153 & 155 Dorcas Street, South Melbourne

Heritage Precinct: None

Category: Residential: Terrace Houses

Heritage Overlay: HO564

Period/Style: Victorian: Italianate

Graded as: Significant

Constructed: 1881

Victorian Heritage Register: No

Designer: -

Amendment: TBC

Comment: New Citation

Significance

What is significant?

The terrace row built by 1881 at 151, 153 & 155 Dorcas Street, South Melbourne is significant.

Alterations and additions are not significant.

How is it significant?

The terrace row built by 1881 at 151, 153 & 155 Dorcas Street, South Melbourne is of local historical and representative significance to the City of Port Phillip.

Why is it significant?

The terrace row at 151, 153 & 155 Dorcas Street, South Melbourne is historically significant for its association with the typical development of this area of Dorcas Street late in the nineteenth century. Built by 1881, they are relatively early survivors in this part of South Melbourne of the pre-boom period. (Criterion A)

The terrace row at 151, 153 & 155 Dorcas Street, South Melbourne is significant as a representative example of a double storey terrace development without an upper-level verandah. The three large houses demonstrate refined matching detailing including Italianate corbelled course and chimneys. The matching

facades of rendered brick include large arched doorways with arched transom windows and Italianate pilasters, two tall sash windows with chamfered edges on the upper level and single wide sash windows with triple panes within raised pointed architraves on the ground level. The line of the former verandah is demonstrated by the extant side boundary walls with decorative moulded ends. (Criterion D)

Thematic context

Victoria's framework of historical themes

6. Building towns, cities and the garden state: 6.3 Shaping the suburbs

Port Phillip thematic environmental history

5. Buildings & Cultural Landscapes: 5.2: Shaping the suburbs, 5.3 Diverse housing

History

Contextual history

The suburb of South Melbourne, known as Emerald Hill between 1855 and 1883, is part of the traditional Country of the Yalukit Willam clan of the Boon Wurrung people, part of the broader Kulin nation, whose name means 'people of the river'.

South Melbourne, initially called Emerald Hill by British colonists, was hemmed in on all sides by the swampy lowlands. The colonists treated the 'island' as a wasteland for grazing, dumping, camping and recreation in the 1830s-40s. To the east of the future Kings Way, settlement was not only constrained by the swamplands but also by the desire to keep free the area around the military reserve west of St Kilda Road. The first sixty-seven allotments of the 1852 Subdivision of Emerald Hill were auctioned on 18 August. (Lemon, p.7) Many buyers were speculators, none of whom planned to reside in the area. Development in the 1850s was rapid, with 1,149 houses assessed for rates by the Melbourne Corporation by 1855. (Lemon, p.8) The Sandridge Road was improved to make it an acceptable path through the marshes. Toll gates were established on the major roads through the district to raise funds up until 1878. The Emerald Hill municipality was created in 1855 and a railway line through from Melbourne to St Kilda opened in 1857. Emerald Hill was free from the building restrictions of Melbourne municipality that had been introduced to reduce the risk of fire. This led to the rapid construction of timber buildings in the suburb.

From the 1850s-1860s the gold rush period saw the rapid formation of a new suburb, 'out of the canvas' as new arrivals sought new residences and services close to their arrival point in the colony. While land northwards was eventually reclaimed for industrial use, expansion eastwards was stymied by the military reserve (later Victoria Barracks). The military reserve extended as far west as Moray Street, but was reduced to Hanna Street (King's Way) in 1862. Lowlands to the south would eventually be reclaimed for recreation (Albert Park). In the 1870s-80s the area became known for its cheap and well-located land for factories and worker housing. The population of South Melbourne grew from 17,101 in 1871, to 25,374 in 1881. In 1881 most of the land between the Barracks and Emerald Hill was sold, requiring filling and leveling. As a result, population saw an even greater increase to 41,724 by 1891. After the 1890s Depression, the area reached a new peak in industrial and residential development in the early twentieth century, but population growth slowed to 46,190 in 1911. In the Interwar and early post-war period South Melbourne fell into relative decline as population fled the inner city.

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and move tenants into modern tower developments. Nonetheless, the late twentieth century saw a resurgence in appreciation for the communities and identity of the inner city, including South Melbourne. This led to movements to conserve historic areas as well as experiments in new forms of medium density housing. New processes of gentrification concurrently led to fresh development pressures and an economic transformation of the area away from its industrial past.

Terrace, 151-155 Dorcas Street

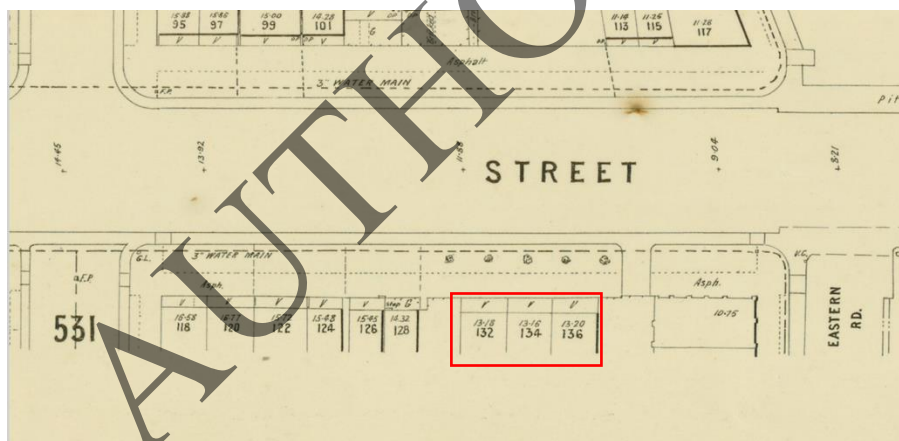
This area of South Melbourne was developed relatively late, in the 1880s and was mostly built up by 1894. According to the municipal directories, in 1885 the street on this side was fully developed, the terraces were Nos.124-128, occupied by Thomas Kemp, George Arthur Whannell, and Miss Mary Hall. At this time, Dorcas Street ended at Eastern Road. By 1895, the properties at 151-155 Dorcas Street were numbered 132-136 in the 1895 MMBW plan. At this time the Sands & McDougall Municipal Directories indicate that they were occupied by Thomas Farrell, James O'Brien, and Christopher Mars. In 1890, Henry Francis, Mrs. Susan Andrews, and William Rose lived there. The row of three terraces were shown set back with verandahs attached to the street boundary.

Rate book extracts show that these houses were built by 1881. The land was recorded as vacant in the October 1880 rate book and three vacant houses are listed in the rate book for the following year.

In October, 1880 Thomas Stead was the owner of vacant land (V.L) 48' x 82' (South Melbourne Rate Book no. in rate 2392). He also owned the adjoining site, which contained a brick store, which is vacant (2391).

In the 1881 South Melbourne Rate Book Thomas Stead was listed as the owner of three vacant ('V') '5B' houses at 124, 125 & 128 Dorcas Street (nos.2421, 2422 & 2433 in the rate book). By 1884, the three houses at 124, 126 & 128 Dorcas Street were occupied by Mary Hall, George Whannel ('C.S.'), and Charles Grey, engineer (nos. 2553, 2554 & 2555 in the rate book)..

Thomas Stead the owner appears to have owned many parcels of land in South Melbourne. A search of Trove shows him to have been involved with several different building societies from the 1870s to the 1890s. He was also a South Melbourne councillor during the 1890s. Joseph Stead, a possible relative, was also a councillor and mayor.



Detail from the 1895 MMBW Plan showing the properties highlighted in red. Source: State Library of Victoria.

References

South Melbourne Rate Books

Sands & McDougall Municipal Directories

State Library of Victoria

The Age, as cited.

Andrew Lemon, 'A History of South Melbourne', p.4/1 of Allom Lovell Sanderson *City of South Melbourne Urban Conservation Study* 1986

Way Back When, *City of Port Phillip Thematic Environmental History*, 2021

Description

This row comprises a matching set of three double storey terraces with transverse gable roof. The walls are rendered brick. The facades comprise pairs of tall sash windows with chamfered edges on the upper level, single broader windows on the ground level set within a raised and pointed architrave. The ground level windows are triple paned sash windows with narrow panes to the sides. The doorways are large with arches flanked by pilasters with decorative Italianate capitals and arched transom windows above. The original doors are timber, four panelled doors with bluestone steps. The terrace is united at the parapet level by an ornate course with decorative moulded brackets and ends. The boundary side walls at Nos 151 and 155 demonstrate the extent of the former verandah and have decorative moulds.

The houses are substantially intact with minor alterations. While the verandah has been removed, the former verandah extent is still legible. There are the ruins of an urn that indicates the former positioning and design of decorative urns along the top of the parapet. Overpainted bluestone footings are visible at No.155.

No.151 is painted white, No.153 is painted dark charcoal, and No.155 has been painted white with charcoal highlights. Shutters have likely been added to the exterior of Nos.153 and 155. The door at No.153 has been replaced with a modern glazed timber framed door. The lower window pane in the ground window at No.155 appears to have been replaced with bars. The front setbacks have new paving and planters, with reproduction fences at Nos 151 and 155.



View of the three terrace houses together. Source: Trethowan Architecture.



Detail of one of the arched doorways. Source: Trethowan Architecture.

Comparative analysis

There are many individually significant terrace rows in South Melbourne. Individually significant terraces of similar design (with ground floor verandah only) include Pembroke Terrace at 351-53 Moray Street, South Melbourne (Citation 1026); terrace at 141-47 Bank Street, South Melbourne (Citation 1025); Alfred Terrace at 160-62 Station Street, Port Melbourne (Citation 706); and Laywell Terrace at 20-24 Stokes Street, Port Melbourne (Citation 711).

Pembroke Terrace (1865) demonstrates similar basic form with a triple set of substantial houses with pairs of windows above and broader single window and door at ground level, with decorative parapet and Italianate chimney. Only two of the three however are fully intact, with architraves and verandahs. 'Pembroke Terrace' is of significance as one of the earliest and most substantial buildings to have been built in Moray Street and for retaining a substantial degree of its original detailing on two of the houses. By comparison, the subject property retains the detailing across all three despite the removal of the verandah.

Alfred Terrace (c.1868) is of local significance for its historical associations with its original owner, the Port Melbourne councillor William Weatherstone, and with its subsequent owner, and possible designer, the locally prominent architect Frederick Williams. It appears to be the earliest surviving double storey terrace row in Port Melbourne. The refined quality of the facade design, particularly the ground floor rustication and the surrounds to the upper floor windows, is exceptional for the date in Port Melbourne residences. One of the terraces has been altered by the addition of a balcony. The subject property is much later, but demonstrates comparable form and scale, illustrating the longevity of this style through the Victorian period.

Laywell Terrace (1890) is of local significance for the refined detailing of the facade and the relatively large size of the houses. The terrace is of a similar period to the subject property, demonstrating once more the basic form of the double storey triple set of terrace houses with unifying parapet detailing, pairs of windows above and pattern of broader ground level window and door below, set back from the street. While Laywell Terrace has retained its verandah, the subject property is distinguished by the way the verandah extended to the street boundary and this is identifiable by the extant side walls. Laywell Terrace also has altered fences comparable to the subject property.

In summary, the subject property demonstrates the key features of the Victorian double-storey terrace row, particularly those without a balcony. The three houses form a legible matching set unified by consistent decorative features and layout of the windows and doors. The subject property is distinguished by its large arched doorways with pilasters, pointed architraves to the lower windows, fine parapet detailing, and transverse gable roof with pair of Italianate chimneys. While the original verandah is missing, its former extent is clearly legible in the side walls to the street boundary.



Alfred Terrace, 160-62 Station Street, Port Melbourne.
Source: David Helms.



Laywell Terrace at 20-24 Stokes Street, South Melbourne.
Source: David Helms.

Assessment

This place has been assessed in accordance with the processes and guidelines outlined in the Australia ICOMOS *Charter for Places of Cultural Heritage Significance (The Burra Charter)* 2013, using the Hercon criteria.

Recommendations

Recommended for inclusion in the Heritage Overlay as an individual place.

Extent of HO: Whole of three properties as defined by the title boundaries.

Paint controls are recommended to retain or restore the visual unity of the set.

Primary source

Trethowan Architecture, *South Melbourne Structure Plan Heritage Review*, 2023.

Other studies

Not applicable.

Other images

None

City of Port Phillip Heritage Review

Place name: Corner shop & residence
Other names: Former Windle's Grocery

Citation No:
2460

INSERT MAP



Address: 108 Bank Street, South Melbourne

Heritage Precinct: None

Category: Retail and Residential: Corner Shop & Residence

Heritage Overlay: HO565

Period/Style: Victorian: Italianate

Graded as: Significant

Constructed: c.1889

Victorian Heritage Register: No

Designer: Unknown

Amendment: TBC

Comment: New Citation

Significance

What is significant?

The two storey late Victorian corner shop and residence built c.1889 at 108 Bank Street, South Melbourne, is significant.

The twentieth century alterations and additions are not significant.

How is it significant?

The corner shop and residence at 108 Bank Street, South Melbourne is of local historical and representative significance to the City of Port Phillip.

Why is it significant?

The corner shop is historically significant as a former grocery associated with the growth of South Melbourne in the late nineteenth century, in particular the pattern of corner shops accompanying new residential areas during the late Victorian boom period. (Criterion A)

The corner shop and residence is of representative significance as a good and largely intact example of a corner shop with residence above. It demonstrates typical characteristics of corner shops in residential areas, notably chamfered corner addressing the street with corner entry and space for signage above, with

street windows and separate side entry for the residence above. The roofline is distinguished from other examples by its prominent hipped slate roof also chamfered at the corner. Other detailing is restrained with cornice and sash windows above, with Italianate chimney. (Criterion D)

Thematic context

Victoria's framework of historical themes

5. Building Victoria's industries and workforce: 5.3 Marketing and retailing

Port Phillip thematic environmental history

4. Commerce, trade and work: 6.2 Markets, shops and retail

History

Contextual history

The suburb of South Melbourne, known as Emerald Hill between 1855 and 1883, is part of the traditional Country of the Yalukit Willam clan of the Boon Wurrung people, part of the broader Kulin nation, whose name means 'people of the river'.

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From the 1850s-1860s the gold rush period saw the rapid formation of a new suburb, 'out of the canvas' as new arrivals sought new residences and services close to their arrival point in the colony. While land northwards was eventually reclaimed for industrial use, expansion eastwards was stymied by the military reserve (later Victoria Barracks). The military reserve extended as far west as Moray Street, but was reduced to Hanna Street (King's Way) in 1862. Lowlands to the south would eventually be reclaimed for recreation (Albert Park). In the 1870s-80s the area became known for its cheap and well-located land for factories and worker housing. The population of South Melbourne grew from 17,101 in 1871, to 25,374 in 1881. In 1881 most of the land between the Barracks and Emerald Hill was sold, requiring filling and leveling. As a result, population saw an even greater increase to 41,724 by 1891. After the 1890s Depression, the area reached a new peak in industrial and residential development in the early twentieth century, but population growth slowed to 46,190 in 1911. In the Interwar and early post-war period South Melbourne fell into relative decline as population fled the inner city.

By 1947 the population fell to 43,452 and saw further declines in the post-war period. The remnant inner urban 'slums' became a concern of new government interventions in the 1960s, with significant interventions by the Victorian Housing Commission to demolish what was considered substandard housing and move tenants into modern tower developments. Nonetheless, the late twentieth century saw a

resurgence in appreciation for the communities and identity of the inner city, including South Melbourne. This led to movements to conserve historic areas as well as experiments in new forms of medium density housing. New processes of gentrification concurrently led to fresh development pressures and an economic transformation of the area away from its industrial past.

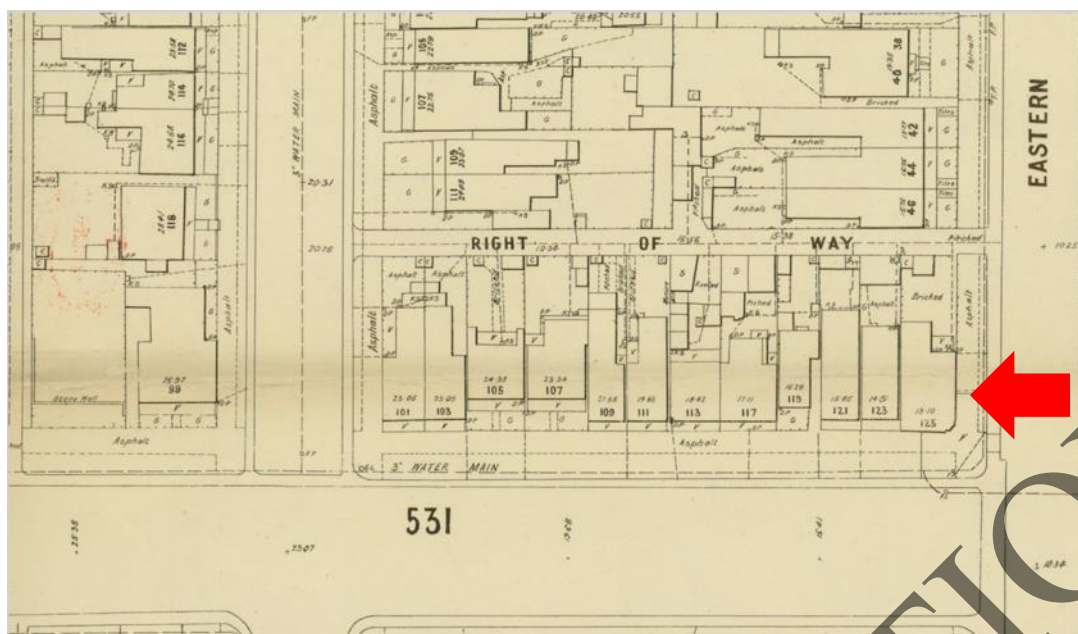
From Thematic Environmental History (p.107): Corner Shops

It was not only retail precincts that developed during the late nineteenth and early twentieth century. Small, local corner stores were once dotted around in different neighbourhoods, within residential precincts. They provided residents with easy access to basic grocery items. One fine example that remains is 186 Buckhurst Street, South Melbourne. This grand, two-story Victorian corner shop and residence was built around 1885. It was a grocery store with a private residence upstairs from the time it was built until around 1974. Many of these former corner stores have been lost, demolished or converted into residential properties, as shopping habits changed, and larger retail chains and supermarkets developed. Others, however, have been adapted. Jerry's Milk Bar on Barkly Street in Elwood, for example, has become a local institution in the last four decades and continues to offer residents a taste of the 1950s in its heritage building, a late Edwardian red brick corner shop. Many small businesses struggled through the extremely challenging years of the Great Depression in the early 1930s. Shop owners applied for extra time to pay their rates, and premises became dilapidated, with owners unable to keep up with maintenance costs. Conversely, retail precincts boomed with the increase in population as flats and apartments became popular. The Village Belle in Acland Street, St Kilda became one of the busiest shopping centres in Melbourne, with new shops opening frequently.

Shop and residence, 108 Bank Street

This section of Bank Street (East – North Side) was first taken up by purchasers after 1865, and by 1870 there were nine houses on the block between what was then Moray Place and Eastern Road. These were chiefly working-class residences such as labourer Charles Hine who owned and occupied an iron shack at then No.99. There were also several small timber houses, and the brick duplex on the corner of Moray Place that saw a variety of tenants. Gradually houses were upgraded or added to, some by small owner-occupiers. James Grant shipwright extended his house next door to five rooms with a stables in 1888 and further west the new resident James Mather, ironmonger, saw the iron shack upgraded to three-room timber house. The block was fully occupied by 1889 with the construction of the corner brick shop and residence, which became the local grocery.

The vacant site on the corner of Eastern Road and Bank Street was owned by Patrick Donovan in 1888. By 1890 however, a five room brick building was listed on the site, owned and occupied by John Navish, a 'mariner'. Not long after, by 1893 the property was occupied by John Windle, a grocer. Windle continued to operate the grocery through the 1890s. *The Age* reported a burglary at Windle grocers on this corner in 1898.



The corner shop appeared on the 1895 MMBW Plan. Source: State Library of Victoria.

References

Andrew Lemon, 'A History of South Melbourne', p.4/1 of Allom Lovell Sanderson *City of South Melbourne Urban Conservation Study* 1986

Census of Victoria

Way Back When, *City of Port Phillip Thematic Environmental History*, 2021

South Melbourne Rate Books (RB)

Sands and McDougall's Street Directories (SM), 1870-1920

The Age

Andrew Ward & Associates, *Port Phillip Heritage Review*, 1998

Melbourne & Metropolitan Board of Works (MMBW) Detail Plans

Description

The building is double-storey, comprising a corner shop with residence above. It is chamfered on the street corner with a shop entry and a blank wall for signage above. The corner entry is flanked on both streets by large square shop windows.

On Dorcas Street is an intact original doorway with Roman numerals in the transom and with transom window and side light both with new glazing. A cornice runs the length of the building indicating the line of the former verandah, with timber framed sash windows above. The prominent hipped roof is slate, chamfered to match the corner wall. An Italianate chimney is visible to the rear.

The building has been rendered and overpainted. The verandah has been removed and a post-war addition has been added to the rear along Eastern Road.



The former shop viewed from the corner of Bank Street and Eastern Road.



Residential entry at 108A Bank Street.

Comparative analysis

108 Bank Street compares to a number of significant corner shops around South Melbourne, where such places often developed in tandem with residential growth in the Victorian period. As a type, the buildings often address the corner through a chamfered corner with entry point and either signage or window above. Levels of detailing vary, from restrained Victorian cornice and window detailing through to ornate Greek Revival or Art Nouveau parapets and arched windows. The residence is usually entered through a

separate side door. 108 Bank Street demonstrates these characteristics of chamfered corner entry with signage space above, separate side door which is unusually intact, restrained cornice detail and sash windows above. It is distinguished by its prominent hipped slate roof, also chamfered to the corner. The verandah has been removed, but this is true of most of the comparable corner shops. Most also have altered shop front windows and doors.

Corner shop (outside of retail centres) comparisons include:

- 186 Buckhurst Street, South Melbourne is a two-storey late Victorian corner shop and residence of typical parapeted form and detailing that is built to both street boundaries with a splayed corner. It has Greek Revival detailing, cornices and parapet. The corner entrance is flanked by large windows, with some modified ground level openings (Citation 2414). 108 Bank Street compares favourably in terms of the corner street presentation and separate residential entry, with a more restrained Italianate rather than Greek Revival style. Both are missing the original verandah as indicated by the cornice and have some modification to street level windows but the subject site has a more intact residential entry.
- 242 Bank Street, South Melbourne (Citation 1083). This rendered and brick corner store has been restored to expose the original brick and render. It has arched door and windows and decorative Italianate cornice. 108 Bank Street has comparable street presentation with chamfered corner entry and signage space above, but is distinguished by its square windows and prominent slate roof.
- 229 York Street, South Melbourne (Citation 1084) is another double storey shop but an unusual one in its squarer form oriented to York Street. It is more decorative and with an intact shopfront window but lacks the chamfered corner presentation compared to the subject site.
- 406 Park Street, South Melbourne (Citation 1064) is unusually intact and demonstrates Greek Revival detailing to the parapet and arched windows and doorways. 108 Bank Street again shares the typical corner street entry but has more restrained Italianate style. Both have comparably altered shop windows.
- 315 Moray Street, South Melbourne (Citation 1057): This example has altered windows/doors on ground floor, rendered, restrained ornamentation, blank corner wall, altered residential entry. 108 Bank Street is comparable in terms of restraint of detailing, rectangular window forms and level of alteration overall, but is distinguished by a more intact residential entry.



242 Bank St, South Melbourne



406 Park Street South Melbourne



315 Moray St, South Melbourne



186 Buckhurst Street

Assessment

This place has been assessed in accordance with the processes and guidelines outlined in the Australia ICOMOS *Charter for Places of Cultural Heritage Significance (The Burra Charter)* 2013, using the Hercon criteria.

Recommendations

Recommended for inclusion in the Heritage Overlay as an individual place.

Extent of HO: Whole of property as defined by the title boundaries.

No specific HO controls are required.

Primary source

Trethowan Architecture, *South Melbourne Structure Plan Heritage Review*, 2023.

Other studies

Not applicable.

Other images



City of Port Phillip Heritage Review

Place name: Patons Brake Replacements
(former)
Other names: Workshop / showroom

Citation No:



Address: 167-173 Coventry, South Melbourne

Heritage Precinct: None

Category: Workshop / Showroom

Heritage Overlay: HO proposed

Period/Style: Post-War

Graded as: Significant

Constructed: c1958

Victorian Heritage Register: No

Designer: D. Graeme Lumsden

Amendment: TBC

Comment: New Citation

Significance

What is significant?

The single-storey post-war Patons Brake Replacements workshop and showroom (former) designed by D. Graeme Lumsden and constructed c.1958 at 167-173 Coventry Street, South Melbourne is significant.

Later alterations are not significant.

How is it significant?

The Patons Brake Replacements workshop and showroom (former) at 167-173 Coventry Street, South Melbourne is of local historical and aesthetic significance to the City of Port Phillip.

Why is it significant?

The Patons Brake Replacements workshop and showroom (former) is historically significant as it demonstrates the development of local-enterprise industrial / commercial development in South Melbourne in the post-war period. In contrast to the larger industrial complexes set up for national businesses, the Patons Brakes Replacement building demonstrates smaller-scale industry for the local community. The function for automotive parts and repair demonstrates the post-war rise in private motor vehicle ownership. (Criterion A)

The Patons Brake Replacement workshop and showroom (former) is aesthetically significant as a good example of a post-war modernist industrial/commercial building in a functionalist mode with a restrained material palette and zoned façade. The building features a simple form with expressed structure and horizontal emphasis offset by delicate vertical elements. Ornamentation is achieved through material placement and form, rather than application. It is a relatively rare example of a post-war industrial building at a small scale in the municipality. (Criterion E)

Thematic context

Victoria's framework of historical themes

5. Building Victoria's industries and workforce: 5.3 Markets and retailing, 5.8 Working

Port Phillip thematic environmental history

6. Commerce, trade and work: 6.3 Industry, 6.4 Work culture

History

Contextual history

The suburb of South Melbourne, known as Emerald Hill between 1855 and 1883, is part of the traditional Country of the Yalukit Willam clan of the Boon Wurrung people, part of the broader Kulin nation, whose name means 'people of the river'.

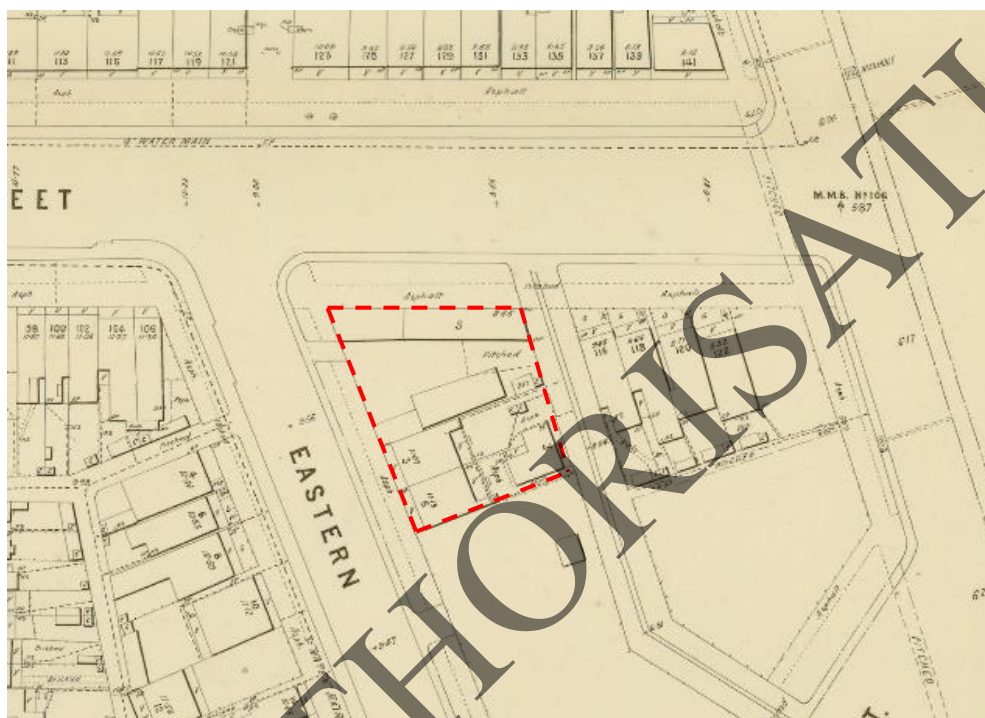
South Melbourne, initially called Emerald Hill by British colonists, was hemmed in on all sides by the swampy lowlands. The colonists treated the 'island' as a wasteland for grazing, dumping, camping and recreation in the 1830s-40s. To the east of the future Kings Way, settlement was not only constrained by the swamplands but also by the desire to keep free the area around the military reserve west of St Kilda Road. The first sixty-seven allotments of the 1852 Subdivision of Emerald Hill were auctioned on 18 August. (Lemon, p.7) Many buyers were speculators, none of whom planned to reside in the area. Development in the 1850s was rapid, with 1,149 houses assessed for rates by the Melbourne Corporation by 1855. (Lemon, p.8) The Sandridge Road was improved to make it an acceptable path through the marshes. Toll gates were established on the major roads through the district to raise funds up until 1878. The Emerald Hill municipality was created in 1855 and a railway line through from Melbourne to St Kilda opened in 1857. Emerald Hill was free from the building restrictions of Melbourne municipality that had been introduced to reduce the risk of fire. This led to the rapid construction of timber buildings in the suburb.

From the 1850s-1860s the gold rush period saw the rapid formation of a new suburb, 'out of the canvas' as new arrivals sought new residences and services close to their arrival point in the colony. While land northwards was eventually reclaimed for industrial use, expansion eastwards was stymied by the military reserve (later Victoria Barracks). The military reserve extended as far west as Moray Street, but was reduced to Hanna Street (King's Way) in 1862. Lowlands to the south would eventually be reclaimed for recreation (Albert Park). In the 1870s-80s the area became known for its cheap and well-located land for factories and worker housing. The population of South Melbourne grew from 17,101 in 1871, to 25,374 in 1881. In 1881 most of the land between the Barracks and Emerald Hill was sold, requiring filling and leveling. As a result, population saw an even greater increase to 41,724 by 1891. After the 1890s Depression, the area reached a new peak in industrial and residential development in the early twentieth century, but population growth slowed to 46,190 in 1911. In the Interwar and early post-war period South Melbourne fell into relative decline as population fled the inner city.

By 1947 the population fell to 43,452 and saw further declines in the post-war period. The remnant inner urban 'slums' became a concern of new government interventions in the 1960s, with significant interventions by the Victorian Housing Commission to demolish what was considered substandard housing and move tenants into modern tower developments. Nonetheless, the late twentieth century saw a resurgence in appreciation for the communities and identity of the inner city, including South Melbourne. This led to movements to conserve historic areas as well as experiments in new forms of medium density housing. New processes of gentrification concurrently led to fresh development pressures and an economic transformation of the area away from its industrial past.

Patons Brake Replacements, 167-173 Coventry Street

167-173 Coventry Street comprises what was originally a series of smaller properties facing to both Coventry Street and Eastern Road. MMBW map, dated 1895, shows that the site was, at that time, within a residential area. The site was unoccupied to the Coventry Street frontage, with some properties to the rear at the Eastern Road frontage.

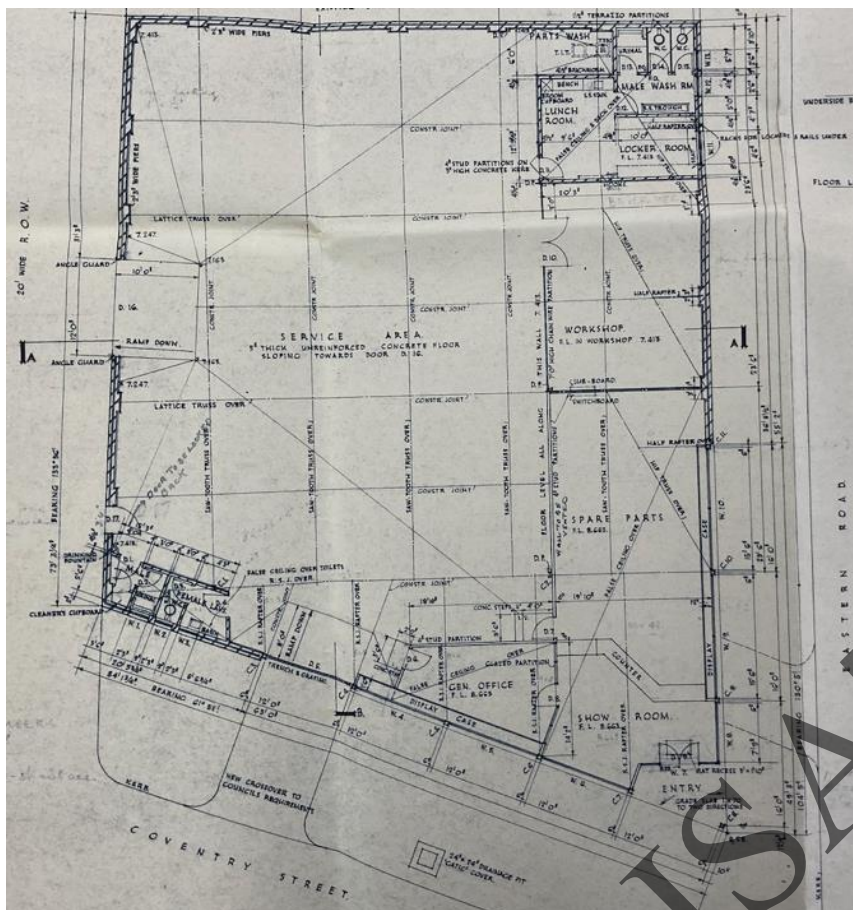


MMBW Detail Plan South Melbourne (nos 520 and 521), dated 1895. The subject site was unoccupied at that time, but with structures to the back at numbers 3 and 5 Eastern Road. Source: State Library of Victoria, pictures collection

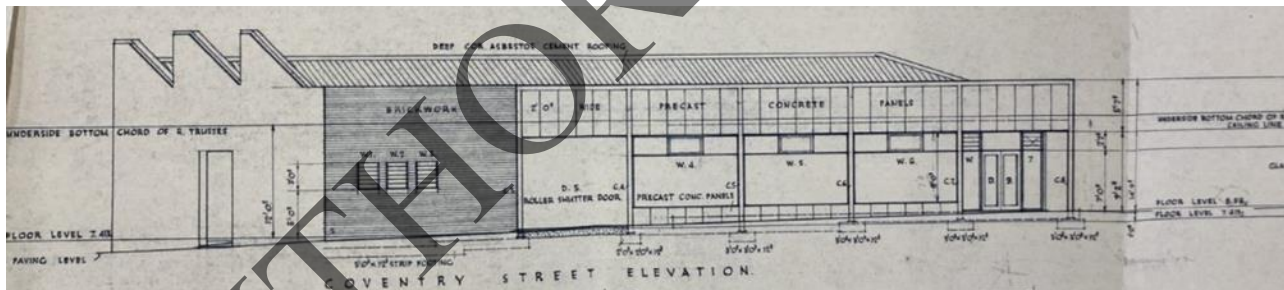
The current building on site was constructed c1958 as Patons Brake Replacements Pty Ltd. Sands & McDougall directories give the address as number 171 Coventry Street, with numbers 161-165 occupying the land to the east up to King's Way (formerly Hanna Street). From 1960 onwards, the adjacent properties are variously occupied by Australian Cement Ltd (depot), Rosenhain & Lipmann Pty Ltd (importers) and AF Bambach Pty Ltd (electrical goods). This demonstrates the post-war change in this part of South Melbourne from a dominant residential character to a mixed-use area with a variety of commercial and light industrial premises. Patons Brake replacements occupied the premises until at least 1974. The site was associated with motor vehicle parts and servicing into the twenty-first century.

Original drawings show the design intent of the building, which included vehicle access on Coventry Street and Hanna Lane, as well as offices, service area, workshop and showroom. The original floor plan shows that the main customer entrance was designed to be inset at the Coventry Street and Eastern Road corner, decorated with glass mosaic tiles. The design included smaller panes at the top of the glazed section, some operable. Small pre-cast concrete panels, matching those above, formed a base to the glazing.

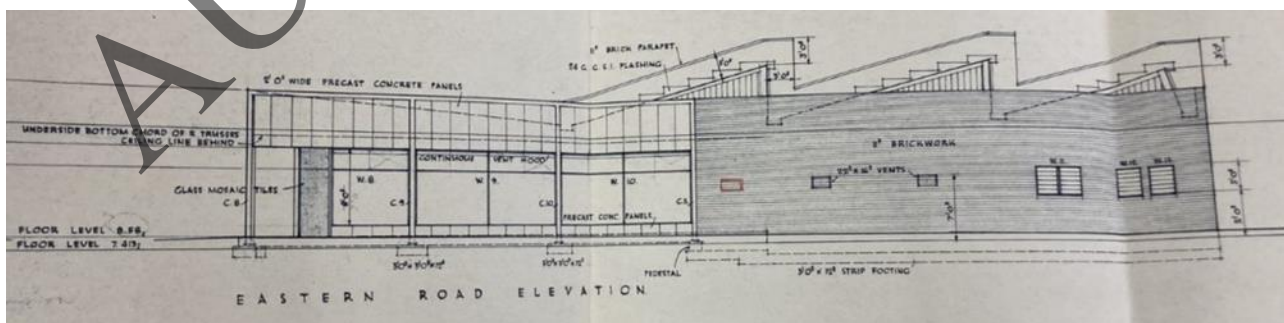
Vehicle access was via Coventry Street and Hanna Lane.



Original floor plan – note the corner entrance inset behind a column, and vehicle entrance at Coventry Street. Source: Public Records Office of Victoria, VPRS10150 P0 3539



Design of Coventry Street elevation. The two elements of the building – warehouse and showroom – are evident, and the inset entrance at the corner. Note the legible sawtooth roof along Hanna Lane at left of image. Source: Public Records Office of Victoria, VPRS10150 P0 3539



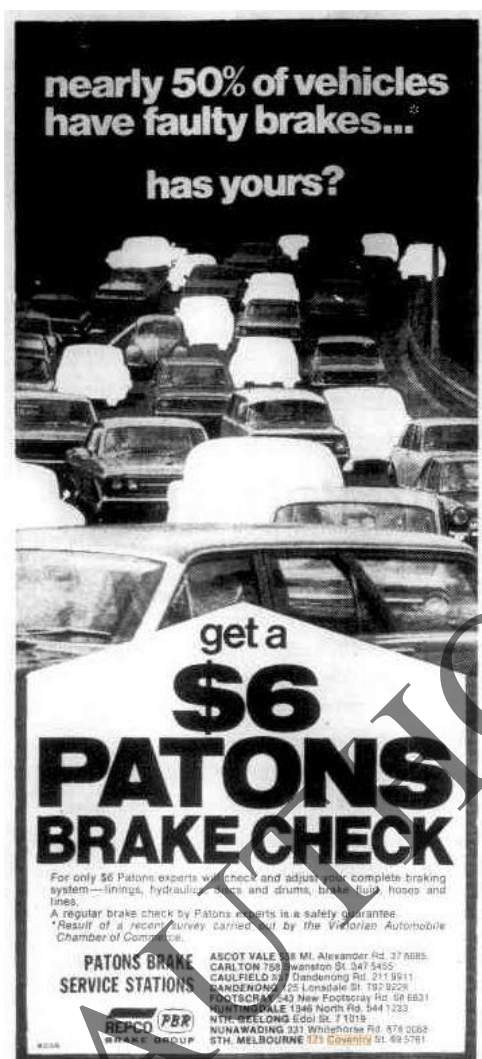
Design of the Eastern Road elevation. Note the plain brickwork to the rear and parapet. Source: Public Records Office of Victoria, VPRS10150 P0 3539.

Patons Brake Replacements

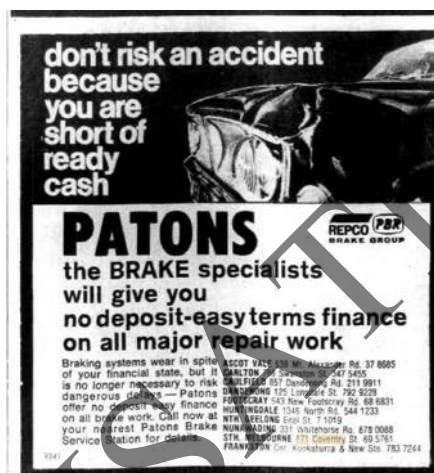
Patons Brake Replacements Pty Ltd (PBR) was established in the 1920s by Mr John G Paton for manufacturing, distributing, and servicing brakes and parts.¹ PBR began by manufacturing brake drums, brake bands, motor cables and various other parts and fluids from the East Bentleigh factory.² The company expanded to operate service stations around Melbourne. The company also provided brake repair machinery to other service stations around Australia.³

In 1947 Patons was wholly acquired by Repco Limited and became a subsidiary brake manufacturing division.⁴ In 1988 Repco Brake and Clutch, Patons Brakes and Girlock amalgamated to form Checkpoint.⁵

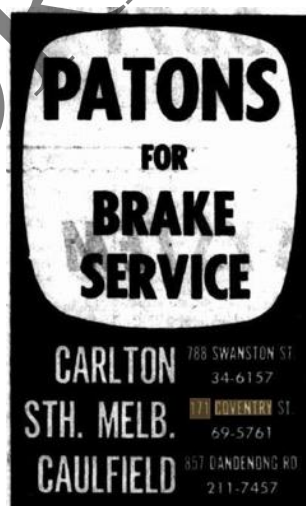
Repco still operates to this day retailing in automotive parts and accessories, however the Patons name is no longer used.



Australian Jewish News, 'Advertising', Friday 25 January 1974.



Australian Jewish News, 'Advertising', Friday 11 January 1974.



Australian Jewish Herald, 'Advertising', Friday 28 May 1965

¹ Herald, Thursday 30 October 1947: 6.

² Examiner (Launceston), Saturday 16 November 1935: 10.

³ Victor Harbour Times (SA), Wednesday 16 May, 1984: 30.

⁴ Argus, Friday 31 October 1947: 16.

⁵ Canberra Times, Friday 19 August, 1988: 5.

D Graeme Lumsden

Donald Graeme Lumsden was born in 1915 and studied architecture at Melbourne Technical College and the University of Melbourne. Lumsden completed his articles with A & K Henderson. Lumsden practiced Launceston in the late 1940s⁶ and established his own practice around 1948. He initially undertook mostly industrial and factory commissions in Tasmania and Victoria. Lumsden was later joined by Ted Ashton and Bill Hale and their office came to specialise in large industrial projects through the 1950s to the 1980s. The practice also worked on houses, offices and commercial projects. Notable projects include the Petersville factory administration building (1962-64) in Mulgrave (VHR H2394), Royal Mail House (1963) in Bourke Street, Melbourne (HO1305), the James Nelson Ltd factory (1948) in Launceston, Tasmania, and the Mitchelton Winery (1974) in Nagambie.

References

Andrew Lemon, 'A History of South Melbourne', p.4/1 of Allom Lovell Sanderson, *City of South Melbourne Urban Conservation Study* 1986

Built Heritage, 'D Graeme Lumsden' in Dictionary of Unsung Architects, www.builtheritage.com.au/dua_lumsden.html

Census of Victoria

City of Port Phillip Thematic Environmental History 2021

Philip Goad and Julie Willis (eds), *Encyclopedia of Australian Architecture*, Melbourne: Cambridge University Press, 2012.

South Melbourne Rate Books

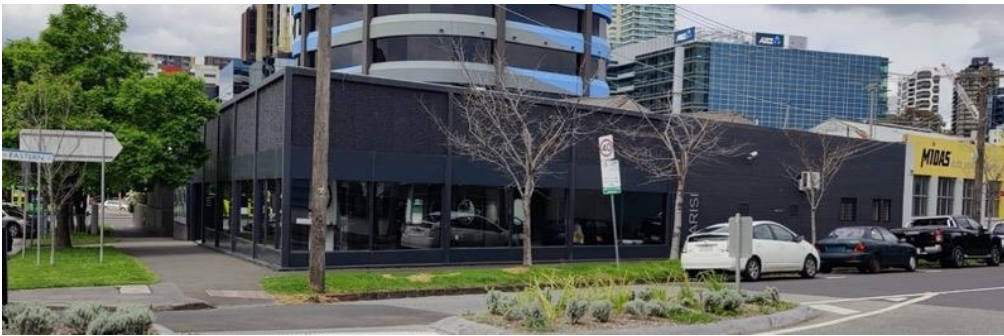
Sands & McDougall Melbourne Street Directory.

Description

167-173 Coventry Street is occupied by a single-storey brick workshop and showroom building that addresses the corner with Eastern Road. The building has three frontages (Coventry Street, Eastern Road and Hanna Lane) and is clearly divided into two sections – the glazed 'showroom' to the north-west corner, and the plain brick 'warehouse' to the east and south. The showroom section is divided horizontally at mid-height by a thick lintel with glazing below and textured pre-cast concrete panels above. The façade is further divided into vertical bays by steel framing which continues around the parapet edge. Behind the showroom, the warehouse element is plain and constructed of brick with few openings.

The building is designed in a functionalist modern aesthetic with a regular façade rhythm formed by strong horizontal bands and delicate vertical divisions. The aesthetic is driven by the use of glass, concrete and steel framing to the showroom. The pre-cast concrete panels provide an unusual textural element. The standard face-brick to the workshop areas contrasts against the showroom aesthetic and delineates the building's two functions.

⁶ Philip Goad, 'D Graeme Lumsden' in Goad & Willis (eds) *Encyclopedia of Australian Architecture*, 2012



View of 167-173 Coventry Street looking east across Eastern Road. The glazed 'showroom' component of the building is visible at the front, and the plain brick 'workshop' is visible behind.



View of the corner at Coventry Street and Eastern Road showing the original entrance location. Note also the textured pre-cast concrete panels in vertical divisions, and the continuous form of the column to the parapet edge.



Coventry Street elevation looking west. Note the plain brick element and the crossover leading to the former vehicle entrance.



View of the corner of Coventry Street and Hanna Lane. Note the roof form and vehicle entrance.

Alterations include removing the corner pedestrian entrance and the vehicle entrance at Coventry Street. It is not clear whether or not the entrance was constructed to be inset as designed, or if it was altered later. Either way, the public entrance is now located at the reconfigured vehicle entrance and crossover remains as evidence of the former arrangement. The vertical division of the façade remains evident at the pre-cast concrete panels and the glazing, despite alterations. The upper, smaller panes of glazing have been covered, as has the pre-cast concrete base. The condition of these elements is unknown. Google streetview imagery shows the earlier condition of the building, prior to the recent alterations.

Openings in the brickwork to Eastern Road remain as per the original drawings; those at Coventry Street are covered by signage and their condition is unknown. Original drawings show a sawtooth roof projecting above the parapet line. While a sawtooth roof was constructed, it is largely concealed behind the parapet and only minimally evident along Hanna Lane.

The building has been overpainted dark grey and there is business signage relating to the current occupants.

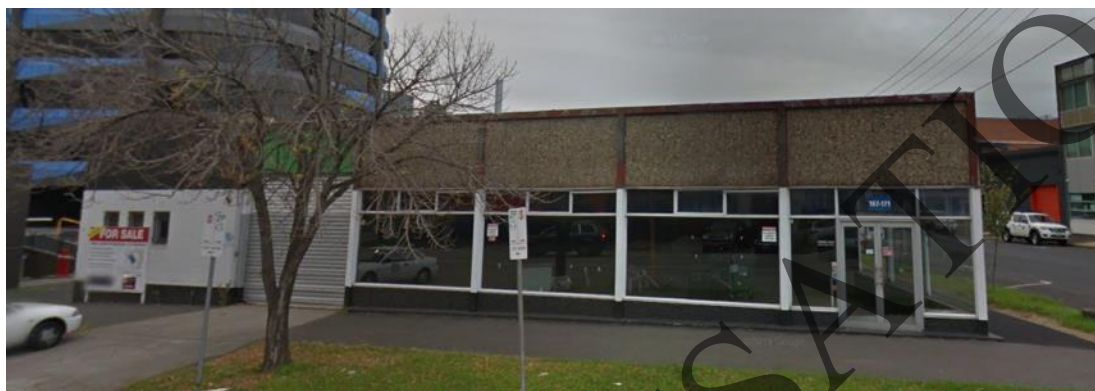


Image dated 2014, showing the Coventry Street elevation prior to moving the entrance, overpainting, and covering the upper panes of glass and the concrete base. Source: Google Maps.



View of the Eastern Road elevation dated 2014, prior to the recent changes. Source: Google Maps.

Comparative analysis

The subject site is characteristic of the industrial and commercial character of parts of South Melbourne from the late-nineteenth century and into the twentieth century. In contrast to the larger industrial complexes that dominated Port Melbourne and Fishermans Bend, the subject site represents smaller scale local commercial enterprise. As a workshop and showroom related to automotive parts and repair, the subject site is indicative of industries related to the growth in manufacturing, including automotive and machinery production, up to the late 1950s. It also demonstrates the increase in motor car ownership during the post-war period.

The architect D Graeme Lumsden was a celebrated post-war designer specialising in industrial and factory buildings and their associated offices. Lumsden was specialised and prolific in this idiom, however, no other

properties designed by his office have been identified within Port Phillip and, as such, comparative analysis against work by Lumsden is not pursued.

Comparable sites include those that relate to post-war industry such as warehouses, factories and shops; particularly those designed in a modernist aesthetic. Modernism filtered through to Australia in the early part of the twentieth century, particularly from the 1920s. By the post-war period, modernism was firmly entrenched and the preferred aesthetic approach for commercial and institutional buildings particularly factories.⁷ The style is characterised by a futuristic outlook and faith in progress. This translates to architecture that is abstracted, eschews historic references, features crisp detailing and planar forms, and a celebration of new materials such as glass, steel and concrete.⁸

- KL Ballantyne & Co Pty Ltd Factory & Office, 39 Ballantyne Street, Southbank (HO4, individually listed) – constructed 1945 to a design by Cedric Ballantyne. A two-storey clinker brick factory with a domestic character.



- Semak Electrics (former), 147-149 Chapel Street, St Kilda (within HO7, graded significant) – built 1954 to a design by the Office of Harry Norris Architects. A three-storey, symmetrical, red-brick modernist factory that is relatively intact. It replaced, and partly incorporated, an earlier single storey factory. The façade is distinguished by steel-framed windows and green tiles to the central section surrounded by a projecting hood.
- Telephone Exchange, 62-78 Inkerman Street, St Kilda (HO160, individually listed) – constructed 1949 to a design by the Commonwealth Department of the Interior. It is a multi-storey, yellow brick building with a prominent corner tower with vertical glazing. It is one of the largest buildings of its type and is a good example of a well-resolved modernist building.

The subject site compares favourably against these comparators. All properties demonstrate the post-war industrial growth within Port Phillip, particularly related to smaller-scale local enterprise in contrast to the major complexes of large national and international companies that proliferated around Fishermans Bend. The subject site is similar to Semak Electrics and the Telephone Exchange in their adoption of functional, industrial modern characteristics. All of these examples use clean, orthogonal lines; simple, abstracted forms; restrained material palette; and minimal ornamentation. The Ballantyne factory stands out for the adoption of a more domestic design idiom in contrast with the forward-looking design approaches of the other buildings. Compared with the three example buildings, the subject site stands out for combining a retail showroom alongside the workshop and for the zoned façade that delineates function through varied levels of articulation.

⁷ Philip Goad "Modernism" in Phillip Goad and Julie Willis (eds), *Encyclopedia of Australian Architecture*, Cambridge University Press, 2012. 464-467.

⁸ Ibid.

Assessment

This place has been assessed in accordance with the processes and guidelines outlined in the Australia ICOMOS *Charter for Places of Cultural Heritage Significance* (The Burra Charter) 2013, using the Hercon criteria.

Recommendations

Recommended for inclusion in the Heritage Overlay as an individual place.

No specific HO schedule controls are required.

Extent of HO: Whole of property as defined by the title boundaries.

Primary source

Trethowan Architecture, *South Melbourne Structure Plan Heritage Review*, 2023

Other studies

Not applicable.

Other images

None

AUTHORISATION

City of Port Phillip Heritage Review

Place name: Coca Cola Factory / Bottling Plant (former)

Citation No:
2462

Other names: -



Address: 82-86 Clarke Street, South Melbourne

Heritage Precinct: None

Category: Manufacturing and processing: Factory

Heritage Overlay: HO567

Period/Style: Interwar, Modern

Graded as: Significant

Constructed: c1938-41

Victorian Heritage Register: No

Designer: Unknown

Amendment: TBC

Comment: New Citation

Significance

What is significant?

The Coca-Cola factory / bottling plant (former) at 82-86 Clark Street, South Melbourne, built c1938-41 is significant. Alterations and additions are not significant.

How is it significant?

The Coca-Cola factory / bottling plant (former) at 82-86 Clark Street, South Melbourne is of local historical, representative and aesthetic significance to the City of Port Phillip.

Why is it significant?

The Coca-Cola factory / bottling plant (former) is historically significant for its association with the interwar phase of industrial development in South Melbourne. It demonstrates smaller-scale industries that emerged prior to large national and international industrial complexes that developed north of City Road and at Fishermans Bend. (Criterion A)

The Coca-Cola factory / bottling plant is of representative significance as a fine and externally intact example of a functional modernist industrial building. The building fabric demonstrates the characteristics of a concrete structural grid frame, cuboid form, limited material palette, lack of ornamentation, use of clinker brick, and expansive areas of steel-framed glazing. The building is notable as an early example of the style,

and for demonstrating modernist characteristics more typically associated with the post-war period. (Criterion D)

The Coca-Cola factory / bottling plant is of aesthetic significance as a fine example of early functional, industrial modernist design. The design has a strong tectonic character through the expressed structural grid frame, offset by a sense of lightness is achieved by the extensive glazing. The purity of the design is strengthened through the limited material and colour palette with no ornamentation. The building demonstrates a stylistic purity across all elevations and is rigorous in the application of functionalist principles. The Coca-Cola factory / bottling plant is important in exhibiting these aesthetic characteristics because it is an early and intact example. (Criterion E)

Thematic context

Victoria's framework of historical themes

5. Building Victoria's industries and workforce: 5.2 Developing a manufacturing capacity

Port Phillip thematic environmental history

6. Commerce, trade and work: 6.3 Industry: 6.3.2 Manufacturing in the twentieth century

History

Contextual history

The suburb of South Melbourne, known as Emerald Hill between 1855 and 1883, is part of the traditional Country of the Yalukit Willam clan of the Boon Wurrung people, part of the broader Kulin nation, whose name means 'people of the river'.

South Melbourne, initially called Emerald Hill by British colonists, was hemmed in on all sides by the swampy lowlands. The colonists treated the 'island' as a wasteland for grazing, dumping, camping and recreation in the 1830s-40s. To the east of the future Kings Way, settlement was not only constrained by the swamplands but also by the desire to keep free the area around the military reserve west of St Kilda Road. The first sixty-seven allotments of the 1852 Subdivision of Emerald Hill were auctioned on 18 August. (Lemon, p.7) Many buyers were speculators, none of whom planned to reside in the area. Development in the 1850s was rapid, with 1,149 houses assessed for rates by the Melbourne Corporation by 1855. (Lemon, p.8) The Sandridge Road was improved to make it an acceptable path through the marshes. Toll gates were established on the major roads through the district to raise funds up until 1878. The Emerald Hill municipality was created in 1855 and a railway line through from Melbourne to St Kilda opened in 1857. Emerald Hill was free from the building restrictions of Melbourne municipality that had been introduced to reduce the risk of fire. This led to the rapid construction of timber buildings in the suburb.

From the 1850s-1860s the gold rush period saw the rapid formation of a new suburb, 'out of the canvas' as new arrivals sought new residences and services close to their arrival point in the colony. While land northwards was eventually reclaimed for industrial use, expansion eastwards was stymied by the military reserve (later Victoria Barracks). The military reserve extended as far west as Moray Street, but was reduced to Hanna Street (King's Way) in 1862. Lowlands to the south would eventually be reclaimed for recreation (Albert Park). In the 1870s-80s the area became known for its cheap and well-located land for factories and worker housing. The population of South Melbourne grew from 17,101 in 1871, to 25,374 in 1881. In 1881 most of the land between the Barracks and Emerald Hill was sold, requiring filling and leveling. As a result, population saw an even greater increase to 41,724 by 1891. After the 1890s

Depression, the area reached a new peak in industrial and residential development in the early twentieth century, but population growth slowed to 46,190 in 1911. In the interwar and early post-war period South Melbourne fell into relative decline as population fled the inner city.

The early twentieth century saw a boom in industrial development in South Melbourne with an emphasis on new industries and manufacturing. In contrast with areas north of City Road and Fishermans Bend, the southern part of South Melbourne experienced growth in smaller and more locally focused industries with small-scale factories and warehouses. Growth declined during World War I before rapidly expanding again through the interwar period. The post-war period saw another phase of industrial growth fuelled by migration, and repurposing factories set up for the war effort to encourage economic development. By the late 1950s Victoria led the nation in a variety of secondary industries including food and beverage.

By 1947 the population fell to 43,452 and saw further declines in the post-war period. The remnant inner urban 'slums' became a concern of new government interventions in the 1960s, with significant interventions by the Victorian Housing Commission to demolish what was considered substandard housing and move tenants into modern tower developments. Nonetheless, the late twentieth century saw a resurgence in appreciation for the communities and identity of the inner city, including South Melbourne. This led to movements to conserve historic areas as well as experiments in new forms of medium density housing. New processes of gentrification concurrently led to fresh development pressures and an economic transformation of the area away from its industrial past.

Coca-Cola Factory / Bottling Plant, 82-86 Clarke Street

The MMBW plan of 1895 shows that the subject site was occupied by houses at that time and the surrounding area was residential in character. The subject site was auctioned by EM Purdy & Co in 1937 and described as a factory site with two timber cottages.¹ In 1938 the Coca Cola Co registered to erect a factory at the site.² The building on site was constructed in late 1938 as a bottling plant for Coca-Cola Cordial manufacturers, with production commencing soon after.³ No architect has been established, however the building was constructed by W.G. Rothwell who was likely also the designer. Rothwell constructed a factory around the same time at Chetwynd Street, North Melbourne (now demolished) that was very similar to the subject site in form, material, and details.⁴ Both factories were constructed for the same owner, ES Brown (investor) of Brighton.

The Coca-Cola brand dates to 1886 and made its first foray into Australia in 1937, when a branch of the *Coca-Cola Export Corporation* opened a branch in Waterloo, Sydney. The subject site soon followed in 1938, as well as factories in Adelaide and Brisbane. As sales of the beverage increased dramatically during and post-World War II, Coca-Cola moved their Melbourne operation to a larger premises at Brunswick in 1952, before expanding again to a complex in Moorabbin, designed by Stuart Calder, in 1955.⁵ Coca-Cola remains in operations at the Moorabbin factory. The Brisbane factory, constructed 1942 at 39-45 Balaclava Street Woolloongabba, is the only other surviving factory from Coca-Cola's initial expansion into Australia during the interwar period.

Sands & McDougall record that Coca-Cola occupied the site until at least 1960, despite the company having moved to larger premises by 1952. Sands & McDougall records indicate that by 1965 the site was occupied by Trading & Agency Co Pty Ltd (merchants), 1970 it was M & D Steel Riggers P/L steel fabricators, 1974 it was Porzelack Car Polishing Service, motor car washing and polishing. The building continued to be used for a variety of industrial / commercial purposes, and is currently occupied by 'Lumber Punks' axe throwing.

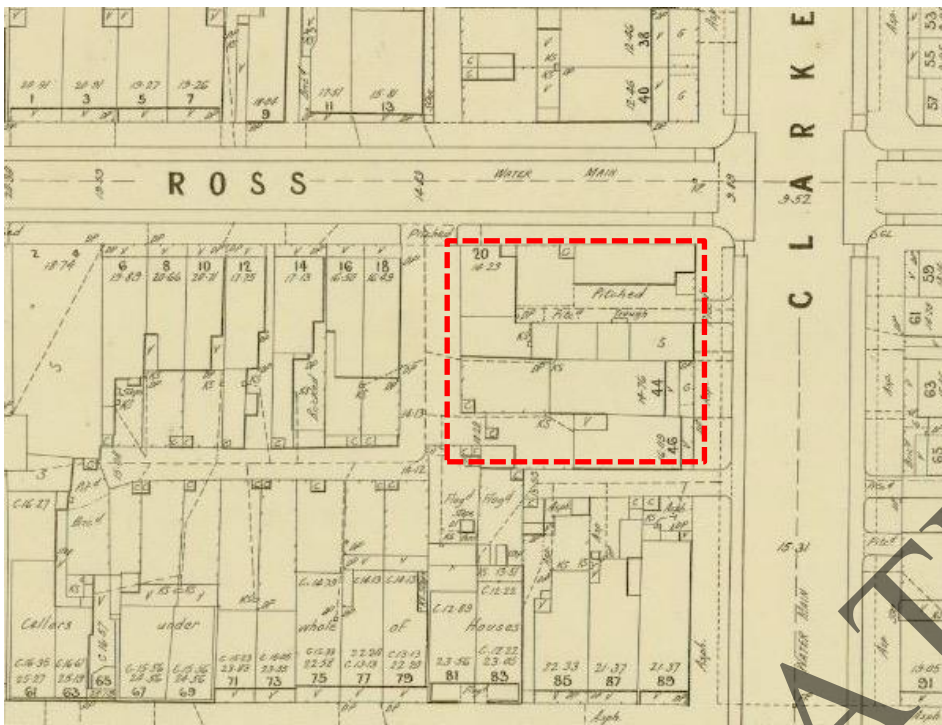
¹ "Forthcoming auctions" *Argus*, 8 July 1937: 6, and "Property market: Investment properties in keen demand" *Argus*, 16 July 1937: 7.

² "The week's events in financing and markets" *The Herald*, 22 October 1938: 2.

³ "Coca Cola", *Australian Cordial Maker, Brewers & Bottlers Gazette*, 9 September 1938: 7 as referenced in *B3465 Classification Report*, National Trust of Australia (Victoria).

⁴ *B3465 Classification Report*, National Trust of Australia (Victoria).

⁵ Simon Reeves, 'A Refreshing Classification' in *Victorian News: National Trust*, National Trust of Australia (Vic), August 2011: 5.



MMBW Detail Plan South Melbourne (nos 518 and 519), dated 1895. The subject site (indicated), and surrounding context, was occupied by single residences at that time. Source: State Library of Victoria, pictures collection



Extract of aerial view of South Melbourne, by Charles Daniel Pratt dated 1950-1960, showing the subject site (indicated) and surrounding context. Source: State Library of Victoria, pictures collection.

References

Andrew Lemon, 'A History of South Melbourne', p.4/1 of Allom Lovell Sanderson, *City of South Melbourne Urban Conservation Study* 1986

Census of Victoria

Way Back When, *City of Port Phillip Thematic Environmental History* 2021.

Philip Goad and Julie Willis (eds), *Encyclopedia of Australian Architecture*, Melbourne: Cambridge University Press, 2012.

National Trust of Australia (Victoria), building file for 82-86 Clark Street, South Melbourne.

Simon Reeves, 'A Refreshing Classification', in *Victorian News: National Trust*, August 2011, p5.

South Melbourne Rate Books

Sands & McDougall Melbourne Street Directory.

Description

The subject site at 82-86 Clarke Street, South Melbourne, is located at the west side of Clarke Street, at the corner with Ross Street. The site is occupied by a concrete grid framed, cuboid building dominated by extensive steel-framed glazing with clinker bricks to the base and parapet. The building is single storey but double height, with a large open span space internally. The building is designed in a functionalist modern industrial mode with a strong tectonic character through the expression of the structural grid frame. The concrete frame is painted white and the steel frame for the glazing is a mix of green and white. The glazing is stepped in sections to provide gaps for ventilation. The windowsills are sloped and finished in brown tiles to match the brick colour. The building is characterised by a very limited material palette and no ornamentation. There are large openings for vehicle access at the south end of Clarke Street and the west end of Ross Street; and a pedestrian doorway at Clarke Street near the vehicle opening. The site slopes down towards the north-east corner with the internal floor aligned to the lowest point, creating a step down into the building at the south end of Clarke Street and the west end of Ross Street.

There is a purity of expression through the functional form, and a sense of lightness is achieved by the extensive glazing despite the heavy concrete frame and bands of brickwork. The building does not present a primary frontage, rather the aesthetic approach is carried across all four elevations (the southern elevation is now concealed by a later building constructed to the boundary).

The building is externally largely intact. Internally the building has been substantially altered with just the structure remaining as evidence of the original appearance.



View of the building from Clarke Street, looking west up Ross Street.



View of the Ross Street elevation; note the unified design style through to the rear laneway.



Detail views of the glazing and clinker brick base. Note the glazing steps in and out to provide gaps for ventilation.

Comparative analysis

The subject site is characteristic of the industrial redevelopment of parts of South Melbourne in the early twentieth century. The development pattern for important national and international companies in the area relates to post-war redevelopment and expansion, often to large industrial complexes. While the subject site relates to the growth of international manufacture and distribution, it stands out for its relatively small scale and its early construction date.

The Coca-Cola Factory is an early example of functional modernist industrial building influenced by European Examples, related to an important development phase in South Melbourne. Functionalist industrial buildings express efficiency and are shaped by the needs of the processes they house. This resulted in buildings that eschewed architectural expression and ornament in favour of practical expressions of spatial arrangements and operational requirements.⁶ Examples constructed during the interwar period often incorporated Art Deco elements, rather than the pure modernism of the Coca-Cola Factory. The subject site's cubic form expressed structural grid frame infilled with large expanses of steel-framed glazing, and crisp detailing reflects the influence of European industrial architecture, in particular Walter Gropius' Fagus Factory. By the post-war period the inspiration for industrial buildings shifted away from Europe and towards the United States and much larger factory complexes. At the same time, modernism became the standard preferred aesthetic for factories and industrial buildings.

Comparable sites include interwar factories, modernist industrial buildings, and factories related to international companies. The comparative analysis has expanded to review other Coca-Cola factories in Australia of a similar period.

⁶ Bruce Trethowan "Industrial Architecture" in Phillip Goad and Julie Willis (eds), *Encyclopedia of Australian Architecture*, Cambridge University Press, 2012.

Interwar Industrial Buildings

- **BALM Paints Factory, 2 Salmon St Port Melbourne (Citation 8, HO282)** – constructed 1937 in a European Modernist manner with influences of Art Deco and Moderne ornament. The building features a plain brick and stucco façade with fluted Art Deco parapet treatment and projecting hood across the windows, which is punctured by the brick piers. There is an overall horizontal emphasis to the composition, which is offset by a tower with a streamline moderne expression. The building has been altered, including unsympathetic additions.



- **Former Union Can Company, 4 Douglas St, Southbank (Citation 2368)** – a factory complex with buildings constructed in 1913-15, 1932 and 1941. The subject site is contemporaneous with, and best compares with, the third factory at the Union Can Complex which was constructed in 1941 to a design by AJ Ainslie. The 1941 factory is two-storey brick building with a parapet and simple form. Both elevations (to Douglas and Meaden streets) have a horizontal emphasis with contrasting verticality to the corner. Windows are square, multi-paned and steel-framed. Horizontal mouldings across the façades provide ornamentation, offset by vertical fluting to the corner, and the whole has been overpainted in dark grey. The building presents a functionalist Moderne aesthetic, typical of the interwar period, and is largely intact externally.
- **Herald & Weekly Times Warehouse (former), 108-120 York Street and 89-97 Market Street (Contributory within HO4 precinct,)** – constructed between 1937 and 1940, designer unknown. The building is a large, double height warehouse constructed of brick with a high parapet that features a decorative capping. The building addresses both York Street and Market Street. The brickwork gradually varies in colour from dark brown at the base up to red at the parapet. Both street frontages feature a decorative band of glazing divided by angled piers in yellow brick with a rendered panel below and a central patera moulding. Reveals to original window and door openings are curved, and there is a large porthole window on the market Street elevation. The building has been heavily altered and redeveloped as residential apartments.
- **Marrickville Margarine Pty Ltd (former), 7-19 Ballantyne Street, Southbank (Contributory within HO4 Precinct)** – constructed c1935, designer unknown. The building is a two-storey brick warehouse / factory constructed of polychrome brick in horizontal bands, with steel-framed multi-paned windows and an asymmetrically positioned tower with render details. The building takes a box-like form with a high parapet, but features a curved corner to the side laneway. The architectural expression of the façade is not carried through to the side elevation. The building appears to be fairly intact externally.



By comparison with these other buildings the subject site stands out for its cohesive modernist aesthetic and as an avant garde example that avoids historic references. The other examples properties include Moderne detailing, applied ornament and traditional materials such as brick and render. The subject site eschews ornament and uses a modern material palette dominated by glass, steel and concrete. The subject site is further notable for the purity of its form and expression across all four elevations which remain largely intact and in good condition. All of the properties demonstrate a phase of industrial development in Port Phillip through the interwar period.

Coca-Cola Factories

Other examples of Coca-Cola factories from a similar period can provide context for the subject site and its role in the expansion of the company into Australia. As noted above, of the four factories constructed during Coca-Cola's initial expansion into Australia, only one other survives – at 39-45 Balaclava Street, Woolloongabba, Brisbane. The Moorabbin factory, constructed in 1955, remains in use to this day.

- Coca-Cola Amatil factory complex, 20 Levanswell Road, Moorabbin (HO84: Coca Cola Factory, individually listed Kingston Planning Scheme) – constructed 1955 to a design by Stuart Calder, as the Coca-Cola Bottlers plant. The complex has been expanded and altered over time however the original portion remains evident on Levanswell Road. The complex originally comprised an expansive, rectangular, gable roofed factory with clerestory windows running east-west and set back from the street behind the entrance wing. Facing Levanswell Road, the entrance wing is a two-storey brick building with a cuboid form. The façade is horizontally divided in half by a concrete hood/awning with the top part featuring large brand signage, and the lower part divided into seven bays, six of steel-framed glazing and one (at west) forming the entrance. The glazing bays are separated by deep concrete fin reveals. An additional window is located at the top part of the façade above the entrance. Notably, new wings have been added to the east and west ends of the entrance building, concealing the bulk of the factory wing from the street.
- Coca-Cola factory (former), 39-45 Balaclava Street, Woolloongabba Brisbane – constructed 1942, designer unknown. This two-storey brick building is expressed with a six-bay façade separated by fluted fins / pilasters that extend above the parapet. Two bays feature a gentle curve to the parapet, which conceals a saw-tooth roof behind. There are upper-level windows at some of the bays, and a mix of pedestrian and vehicle entrances at ground level.

All the remaining Coca-Cola factories in Australia demonstrate the introduction and rapid growth of this popular product, and the development of local manufacturing capacity. As the company rapidly expanded

locally, the number and size of factories grew to support the increased demand. This rapid growth also reflects the popularity of American brands and products amongst the Australian public in the post-war period. Of the three extant Coca-Cola factories, the subject site is the most intact to its original form. However, the Moorabbin factory remains associated with the company and provides a more tangible link to the Coca-Cola company. There is no stylistic continuity amongst the three examples, and they do not form a consistent understanding of Coca-Cola in Australia.

Assessment

This place has been assessed in accordance with the processes and guidelines outlined in the Australia ICOMOS *Charter for Places of Cultural Heritage Significance* (The Burra Charter) 2013, using the Hercon criteria.

Recommendations

Recommended for inclusion in the Heritage Overlay as an individual place.

No specific HO schedule controls are required.

Extent of HO: Whole of property as defined by the title boundaries.

Primary source

Trethowan Architecture, *South Melbourne Structure Plan Heritage Review*, 2023

Other studies

Not applicable.

Other images

None

City of Port Phillip Heritage Review

Place name: J.E. Searls Engineers & Coppersmiths (Former)
Other names: Factory

Citation No:
2463



Address: 49-55 York Street, South Melbourne

Heritage Precinct: None

Category: Factory / warehouse

Heritage Overlay: HO568

Period/Style: Late Inter-War / Post War

Graded as: Significant

Constructed: 1943-45

Victorian Heritage Register: No

Designer: Unknown

Amendment: TBC

Comment: New Citation

Significance

What is significant?

The late inter-war factory / warehouse at 49-55 York Street, South Melbourne, built 1943-45 for J.E Searls Engineers & Coppersmiths is significant.

Alterations and additions are not significant.

How is it significant?

The former J.E. Searls Engineers & Coppersmiths factory /warehouse at 49-55 York Street, South Melbourne is of local historical and representative significance to the City of Port Phillip.

Why is it significant?

The former J.E. Searls Engineers & Coppersmiths factory / warehouse is of historical significance as an early factory in the area and as an example of local manufacturing in support of the war effort. The building demonstrates the rapid expansion of manufacturing industries for military purposes which shaped the direction of post-war economic recovery. (Criterion A)

The former J.E. Searls Engineers & Coppersmiths factory / warehouse is of representative significance as a good example of a late inter-war to post-war light industrial building with associated warehouse and offices. The building demonstrates a restrained, functional aesthetic with ornamentation achieved through brick

colour and placement representative of the material and financial constraints during World War II. The building is representative of a phase of industrial / commercial development in this part of South Melbourne during the early part of the twentieth century. (Criterion D)

Thematic context

Victoria's framework of historical themes

5. Building Victoria's industries and workforce: 5.2 Developing a manufacturing capacity

Port Phillip thematic environmental history

6. Commerce, trade and work: 6.3 Industry: 6.3.2 Manufacturing in the twentieth century

History

Contextual history

The suburb of South Melbourne, known as Emerald Hill between 1855 and 1883, is part of the traditional Country of the Yalukit Willam clan of the Boon Wurrung people, part of the broader Kulin nation, whose name means 'people of the river'.

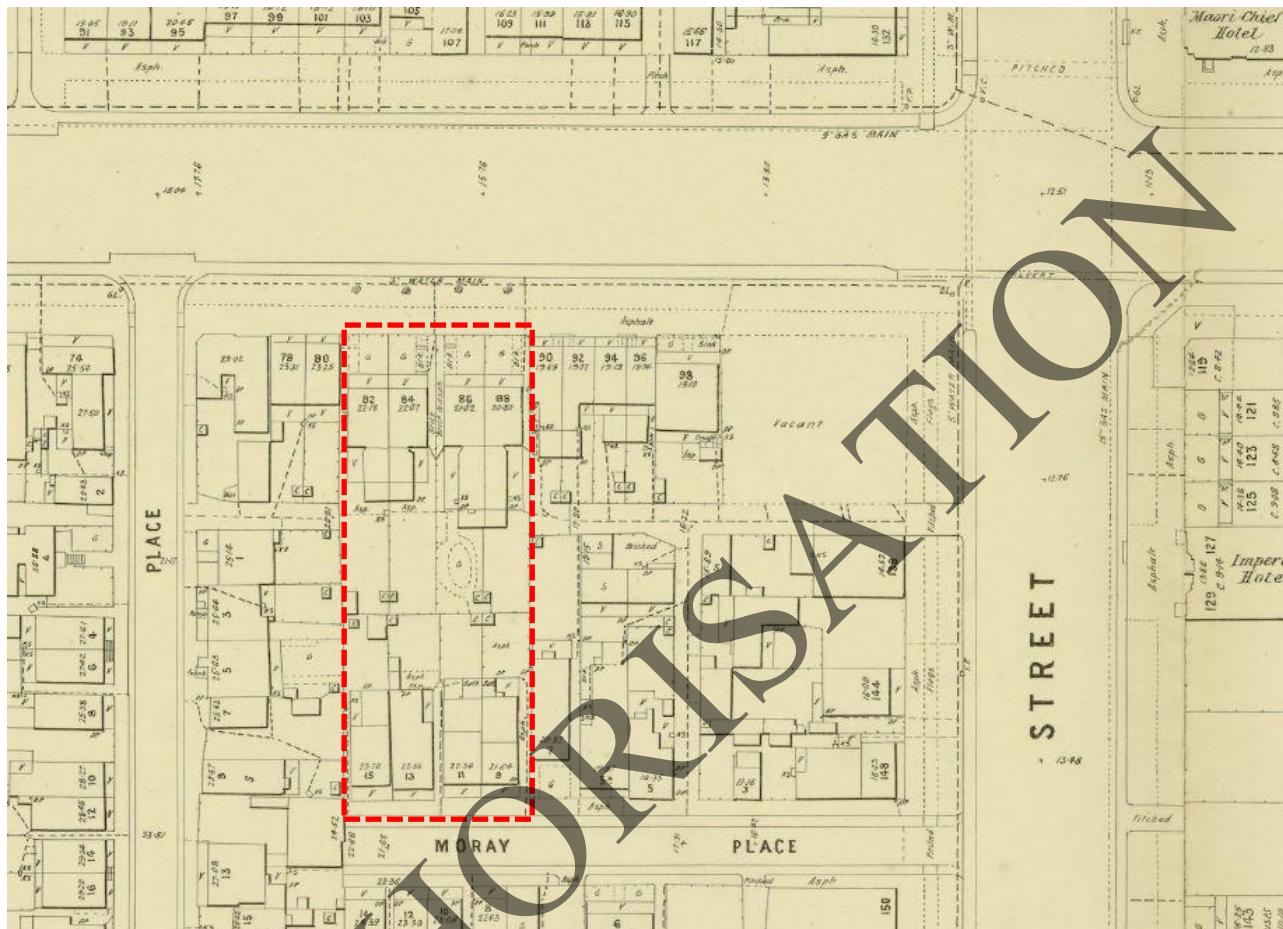
South Melbourne, initially called Emerald Hill by British colonists, was hemmed in on all sides by the swampy lowlands. The colonists treated the 'island' as a wasteland for grazing, dumping, camping and recreation in the 1830s-40s. To the east of the future Kings Way, settlement was not only constrained by the swamplands but also by the desire to keep free the area around the military reserve west of St Kilda Road. The first sixty-seven allotments of the 1852 Subdivision of Emerald Hill were auctioned on 18 August. (Lemon, p.7) Many buyers were speculators, none of whom planned to reside in the area. Development in the 1850s was rapid, with 1,149 houses assessed for rates by the Melbourne Corporation by 1855. (Lemon, p.8) The Sandridge Road was improved to make it an acceptable path through the marshes. Toll gates were established on the major roads through the district to raise funds up until 1878. The Emerald Hill municipality was created in 1855 and a railway line through from Melbourne to St Kilda opened in 1857. Emerald Hill was free from the building restrictions of Melbourne municipality that had been introduced to reduce the risk of fire. This led to the rapid construction of timber buildings in the suburb.

From the 1850s-1860s the gold rush period saw the rapid formation of a new suburb, 'out of the canvas' as new arrivals sought new residences and services close to their arrival point in the colony. While land northwards was eventually reclaimed for industrial use, expansion eastwards was stymied by the military reserve (later Victoria Barracks). The military reserve extended as far west as Moray Street, but was reduced to Hanna Street (King's Way) in 1862. Lowlands to the south would eventually be reclaimed for recreation (Albert Park). In the 1870s-80s the area became known for its cheap and well-located land for factories and worker housing. The population of South Melbourne grew from 17,101 in 1871, to 25,374 in 1881. In 1881 most of the land between the Barracks and Emerald Hill was sold, requiring filling and leveling. As a result, population saw an even greater increase to 41,724 by 1891. After the 1890s Depression, the area reached a new peak in industrial and residential development in the early twentieth century, but population growth slowed to 46,190 in 1911. In the Interwar and early post-war period South Melbourne fell into relative decline as population fled the inner city.

By 1947 the population fell to 43,452 and saw further declines in the post-war period. The remnant inner urban 'slums' became a concern of new government interventions in the 1960s, with significant interventions by the Victorian Housing Commission to demolish what was considered substandard housing and move tenants into modern tower developments. Nonetheless, the late twentieth century saw a

resurgence in appreciation for the communities and identity of the inner city, including South Melbourne. This led to movements to conserve historic areas as well as experiments in new forms of medium density housing. New processes of gentrification concurrently led to fresh development pressures and an economic transformation of the area away from its industrial past.

J.E. Searl Engineers & Coppersmiths Factory / warehouse (former)



Extract of MMBW Detail Plan South Melbourne (nos 520 and 521), dated 1895. The subject site (indicated in red) was then occupied by multiple residences numbered 82-88 York Street and 9-15 Moray Place. Source: State Library of Victoria.

MMBW plans indicate that around 1895 the land that now forms the subject site was occupied by eight residences, four facing to York Street and four to Moray Place. The surrounding area had a residential character at that time, with hotels and corner shops serving the local community. This area of South Melbourne was redeveloped for industrial and commercial purposes through the inter-war and post-war periods, and now has a mixed-use character. Industrial development boomed in South Melbourne through the early part of the twentieth century. World War II saw existing factories transition to supplying the war effort, and new factories purpose-built for military purposes – particularly around Fishermans Bend. Sands & McDougall directories indicate that the current building on site was constructed c1943 for JE Searls Engineers and Coppersmiths Pty Ltd. It is probable that this factory was constructed for the war effort. Additional industrial capacity built up during the war combined with post-war migration to rapidly expand manufacturing after World War II and shape economic recovery.

JE Searls was originally known as Smith and Searls engineers and began operations in Spencer Street around 1915¹, later expanding to factories at 171 Wells Street, South Melbourne and 235 Church Street, Richmond

¹ "Compact Plant: Smith and Searls' Methods" *Herald*, 22 July 1930: 30.

as well as the subject site. News articles from 1944 indicate that the Searls factory in South Melbourne was producing parts for munitions projects as well as labour-saving machinery to improve production efficiency. The South Melbourne factory was operated by women and managed by Miss T Searls who joined the family business.² It is not clear, however, if the newspaper articles are referring to the factory at 171 Wells Street or the subject site at York Street.

An aerial image dated c1950s-1960s shows that the subject site was one of the earliest industrial buildings in the area. JE Searls remained at the site until the late 1960s and by 1970 the property was occupied by Traffic Engineering Equipment Australasia P/L engineers and manufacturing. The 1974 directory does not list the property, indicating that it may have been unoccupied at that time. It is currently occupied by a staging and events business.



Extract of aerial view of South Melbourne, by Charles Daniel Pratt dated 1950-1960, showing the subject site (indicated) and surrounding context. Source: State Library of Victoria, pictures collection.

References

Age, as cited.

Andrew Lemon, 'A History of South Melbourne', p.4/1 of Allom Lovell Sanderson, *City of South Melbourne Urban Conservation Study* 1986.

Census of Victoria.

Way Back When, *City of Port Phillip Thematic Environmental History* 2021.

Herald, as cited.

² "Factory Operated and managed by Women" *Argus* Wednesday 27 September 1944: 8. And, "The Housewives Paradise: Woman Industrialist's Visit to United States" *Age*, Tuesday 31 October 1944: 4.

South Melbourne Rate Books.

Sands & McDougall Melbourne Street Directory.

Description

The subject site at 49-55 York Street, South Melbourne is located on the south side of York Street between Moray and Clarendon streets, extending to Little Moray Street at the rear. The site is occupied by a two-storey polychrome brick factory/warehouse and office with sawtooth roof behind a parapet to the street frontage. The façade is symmetrically arranged around a central pedestrian entrance with steel-framed rectangular windows, and vehicle access to a loading bay at the east. The recessed entrance is accessed via white terrazzo steps and covered by a white concrete hood. The vehicle entrance opening is detailed with stepped brickwork, and narrow windows either side.

The building is largely red brick with brown bricks to the base, entrance, window sills and parapet; and yellow bricks above the entrance, at the parapet, and forming a stringcourse at first floor level. Additional detail is created by recessing or projecting the different coloured bricks. There is an overall horizontal emphasis created by the bands of different brick colours and the rectangular windows, with a contrasting vertical emphasis at the central entrance. The side and rear elevations have no ornament or architectural detail, but the sawtooth roof is visible.

The building is currently occupied by a staging systems company, retaining its original use for light industry / commercial purposes with related office space. The building is intact externally with only minor alterations evident such as business signage.



View of the building façade on York Street. Note the polychrome brick and vertical emphasis at the entrance.



View of the recessed front entrance. Note the projecting brickwork pattern.



View of the vehicle entrance. Note the stepped brickwork and the recessed stringcourse above.



East side view, note the sawtooth roof behind.

Comparative analysis

The subject site is characteristic of small-scale industrial / commercial properties constructed during the first half of the twentieth century. It demonstrates the industrial redevelopment of parts of South Melbourne from the late-nineteenth through to the mid twentieth century. The subject site was used for engineering and copper smithing, demonstrating the growth in manufacturing and machinery production as new industries emerged in the twentieth century and particularly through World War II.

Comparable sites include those related to inter-war and post-war industry and manufacturing, such as warehouses and factories, that have already been assessed and included on the Heritage Overlay. Of particular relevance are properties designed in a restrained, inter-war style.

- **BALM Paints Factory, 2 Salmon Street Port Melbourne (HO282, Citation 8)** – constructed 1937 in a European Modernist manner. The façade is of face brick and stucco with steel-framed multi-pane windows and projecting concrete window hood. The design details demonstrate Art Deco influence through the rotated and stacked bricks to the pilasters, fluting to the parapet, and the streamline moderne tower and flagpole. The building has been altered but the original design intent is legible.

The subject site compares favourably against the BALM Paints Factory. The subject site was constructed later, and in a more restrained and functional aesthetic. Ornamentation is achieved through colour placement and subtle projection / recession; whereas the BALM Paints Factory is bolder with Art Deco inspired decorative features. This demonstrates stylistic shifts during the period, but may also be a result of designing within wartime constraints. The subject site retains a high level of intactness to the exterior compared with the altered BALM Paints Factory.



- **Herald & Weekly Times Warehouse (former), 108-120 York Street and 89-97 Market Street (Contributory within HO4 precinct,)** – constructed between 1935 and 1940, designer unknown. The building is a large, double height warehouse constructed of brick with a high parapet that features a decorative capping. Both elevations (to York and Market streets) feature brickwork that gradually varies in colour from dark brown at the base up to red at the parapet, and a decorative band of glazing divided by angled piers in yellow brick with a rendered panel below and a central patera moulding. The building has been heavily altered and adapted for a new use, but the original design intention is legible at the street frontages.

The subject site compares favourably against the HWT Warehouse. Both properties demonstrate polychrome brickwork and orthogonal, symmetrical elevations. However, the HWT Warehouse is more decorative and incorporates applied ornament and articulation through diagonal pilasters and rendered mouldings. The subject site, on the other hand, eschews ornament and presents a more functional, restrained aesthetic. The subject site is largely intact externally, and retains a commercial use, in contrast to the heavily altered HWT Warehouse which is now part of an apartment building development.

- **Marrickville Margarine Pty Ltd (former), 7-19 Ballantyne Street, Southbank (Contributory within HO4 Precinct,)** – constructed c1935, designer unknown. It is a two-storey, polychrome brick factory with steel-framed, multi-pane windows. The façade has a horizontal emphasis offset by an asymmetrically positioned flagpole and rendered entrance, and a curve to the corner.

The subject site and the Marrickville Margarine building are similar in their use of polychrome brick, and a simple, box-like form with vertical emphasis to the entrance. However, the subject site was constructed later and is a more functional example without rendered ornament or curves that

reference Art Deco and moderne styles. Both buildings are externally intact and demonstrate characteristics of light industrial / commercial buildings from the inter-war period. The subject site compares favourably against the Marrickville Margarine building.



Compared with these examples, the subject site is notable for its restrained design that achieves an expressive quality through simple gestures of material and colour placement, rather than applied ornament. This more functional design approach was likely influenced by wartime constraints but still creates architectural interest. The buildings are each horizontally arranged with a contrasting vertical element that emphasises part of the composition.

The subject site retains its industrial / commercial use and is notable for its external intactness.

Assessment

This place has been assessed in accordance with the processes and guidelines outlined in the Australia ICOMOS *Charter for Places of Cultural Heritage Significance* (The Burra Charter) 2013, using the Hercon criteria.

Recommendations

Recommended for inclusion in the Heritage Overlay as an individual place.

No specific HO schedule controls are required.

Extent of HO: Whole of property as defined by the title boundaries.

Primary source

Trethowan Architecture, *South Melbourne Structure Plan Heritage Review*, 2023

Other studies

Not applicable.

Other images

None

AUTHORISATION

City of Port Phillip Heritage Review

Place name: Star Hotel (Former)
Other names: Former Phoenix Hotel, Market Hotel

Citation No:
2466



INSERT MAP

Address: 152 Clarendon Street, South Melbourne
Category: Commercial: Hotel
Period/Style: Interwar: Streamline Moderne
Constructed: c.1877, Remodelled 1941
Designer: D. F. Cowell Ham (1941)
Amendment: TBC
Comment: New citation

Heritage Precinct: Not applicable
Heritage Overlay: HO571
Graded as: Significant
Victorian Heritage Register: No

Significance

What is significant?

The former Star Hotel at 152 Clarendon Street South Melbourne, a c.1877 Victorian hotel remodelled in 1941 to a design by D.F. Cowell Ham, is significant.

Alterations and additions made after 1941 are not significant. The current paint colour scheme is not significant.

How is it significant?

The former Star Hotel is of local historical, representative and aesthetic significance to the City of Port Phillip.

Why is it significant?

The former Star Hotel is historically significant as the site has been a hotel continuously since 1854 when the Phoenix was opened as one of four original hotels in Emerald Hill. The current building was rebuilt in 1877 and expanded to incorporate shopfronts along Clarendon Street. The hotel is representative of the modernisation of Victorian-era hotels during the Interwar period, whereby earlier buildings were typically remodelled, upgraded and expanded in response to liquor license regulations. The corner address is reflective of the traditional practice of prominently locating hotels at prominent intersections. (Criterion A)

The former Star Hotel is of representative significance as a Victorian hotel remodelled in the 1940s, demonstrating prominent corner site with frontage on two streets, chamfered corner entry, and a façade that has incorporated a growing hotel over time. (Criterion D)

The former Star Hotel is of aesthetic significance as an example of a hotel remodelled in the Streamline Moderne style, demonstrating horizontal window canopies on the lower level, arched windows on the

upper level, and stepped parapet with restrained horizontal line detailing and vertical corner tower element at the corner. (Criterion E)

Thematic context

Victoria's framework of historical themes

5. Building Victoria's Industries and Workforces: 5.6 Entertaining and socialising

Port Phillip thematic environmental history

Theme 3.5 Expressing identity and culture

Theme 8.5 Gathering places

Theme 9.3.1 Hotels, cafes and restaurants

History

Contextual history

The suburb of South Melbourne, known as Emerald Hill between 1855 and 1883, is part of the traditional Country of the Yalukit Willam clan of the Boon Wurrung people, part of the broader Kulin nation, whose name means 'people of the river'.

South Melbourne, initially called Emerald Hill by British colonists, was hemmed in on all sides by the swampy lowlands. The colonists treated the 'island' as a wasteland for grazing, dumping, camping and recreation in the 1830s-40s. To the east of the future Kings Way, settlement was not only constrained by the swamplands but also by the desire to keep free the area around the military reserve west of St Kilda Road. The first sixty-seven allotments of the 1852 Subdivision of Emerald Hill were auctioned on 18 August. (Lemon, p.7) Many buyers were speculators, none of whom planned to reside in the area. Development in the 1850s was rapid, with 1,149 houses assessed for rates by the Melbourne Corporation by 1855. (Lemon, p.8) The Sandridge Road was improved to make it an acceptable path through the marshes. Toll gates were established on the major roads through the district to raise funds up until 1878. The Emerald Hill municipality was created in 1855 and a railway line through from Melbourne to St Kilda opened in 1857. Emerald Hill was free from the building restrictions of Melbourne municipality that had been introduced to reduce the risk of fire. This led to the rapid construction of timber buildings in the suburb.

From the 1850s-1860s the gold rush period saw the rapid formation of a new suburb, 'out of the canvas' as new arrivals sought new residences and services close to their arrival point in the colony. While land northwards was eventually reclaimed for industrial use, expansion eastwards was stymied by the military reserve (later Victoria Barracks). The military reserve extended as far west as Moray Street, but was reduced to Hanna Street (King's Way) in 1862. Lowlands to the south would eventually be reclaimed for recreation (Albert Park). In the 1870s-80s the area became known for its cheap and well-located land for factories and worker housing. The population of South Melbourne grew from 17,101 in 1871, to 25,374 in 1881. In 1881 most of the land between the Barracks and Emerald Hill was sold, requiring filling and leveling. As a result, population saw an even greater increase to 41,724 by 1891. After the 1890s Depression, the area reached a new peak in industrial and residential development in the early twentieth century, but population growth slowed to 46,190 in 1911. In the Interwar and early post-war period South Melbourne fell into relative decline as population fled the inner city.

By 1947 the population fell to 43,452 and saw further declines in the post-war period. The remnant inner urban 'slums' became a concern of new government interventions in the 1960s, with significant interventions by the Victorian Housing Commission to demolish what was considered substandard housing and move tenants into modern tower developments. Nonetheless, the late twentieth century saw a

resurgence in appreciation for the communities and identity of the inner city, including South Melbourne. This led to movements to conserve historic areas as well as experiments in new forms of medium density housing. New processes of gentrification concurrently led to fresh development pressures and an economic transformation of the area away from its industrial past.

From Thematic History:

Providing food, refreshments, entertainment and accommodation, hotels – or pubs – have been popular places to eat, socialise and stay in the City of Port Phillip since the arrival of European settlers. Wilbraham Liardet was the first to open a hotel in the area, the Brighton Pier Hotel, in October 1840. During the gold rush period from 1851 to 1876, 51 new hotels were opened in Port Melbourne alone.

Before the creation of civic buildings, hotels were often used as meeting places for early councils, social groups and committees, as well as general socialising. Some of the oldest buildings in the City of Port Phillip are hotels that have stood the test of time, such as the Golden Gate Hotel (1853), Prince of Wales Hotel (1863), the George (which began as the Terminus in 1857), the Esplanade (which began as the New Bath Hotel in 1856), the Golden Fleece Hotel (1872) and the Balaclava Hotel (formerly Carlisle Hotel 1869). Some, like the Hotel Victoria built in 1888 on Beaconsfield Parade, Albert Park, remain but no longer used as hotels. Others, like the Greyhound Hotel (1853) have been demolished. Hotels have been particularly associated with working-class people as centres of Australian drinking culture, and continue to provide vital social venues for the community today.

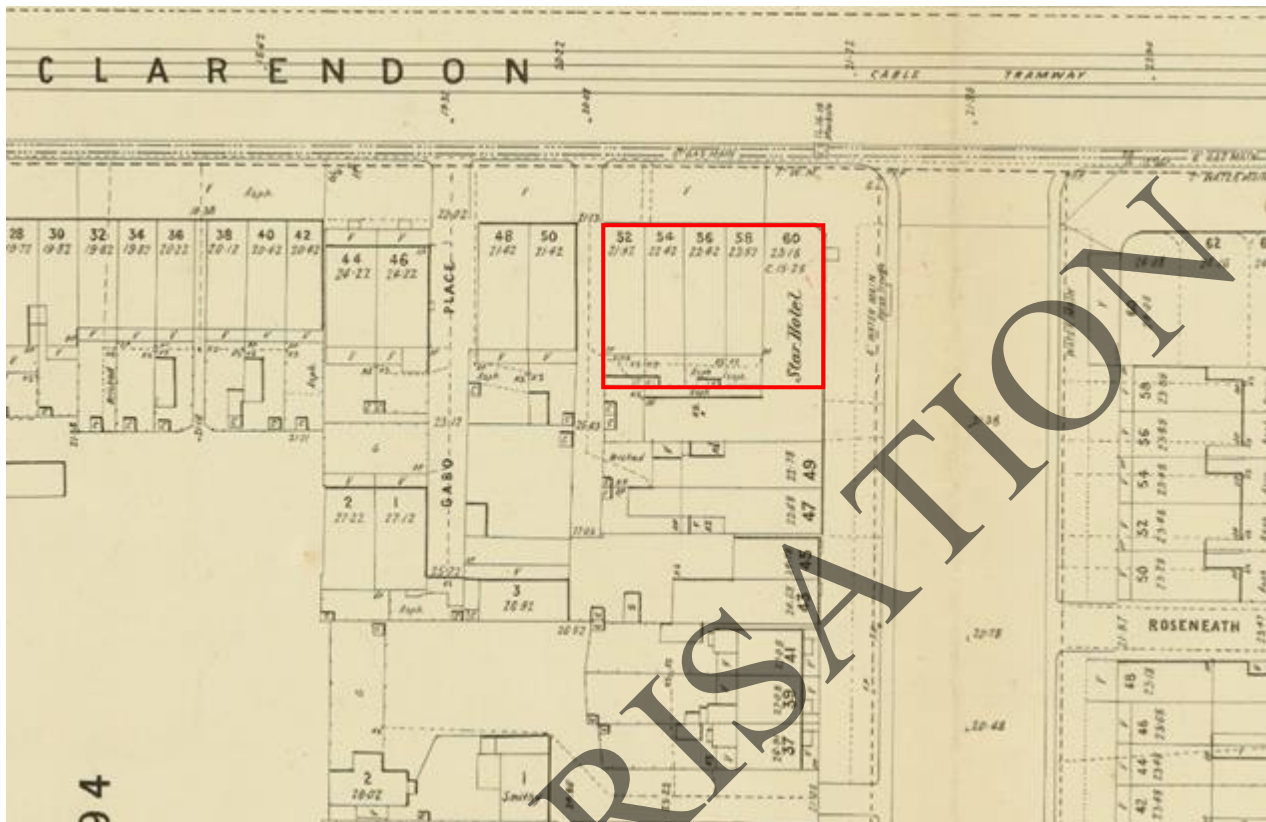
Star Hotel (former)

The property at 152 Clarendon Street was one of the original four hotel sites purchased in the first land sales of Emerald Hill in 1852. A hotel named the Phoenix opened on the site in 1854. It was a ten room brick hotel with slate roof licensed by Thomas Dollard. This hotel was used as the headquarters of Emerald Hill's first local paper, the *Weekly Times*. The hotel was renamed the Star Hotel by proprietor Christian Henry Heier in 1862. Heier was previously the publican of the Star Hotel in Swanston Street. In 1871, architects W.H. Ellerker & Co. called for tenders for alterations to the hotel. It was auctioned on 1 June 1874, and subsequently rebuilt in 1877 by Thomas Cowen, formerly of the Market Hotel. (Aizen, St Kilda Historical Society 2004) The hotel was throughout this time a prominent social feature of the community, hosting various events and meetings. In 1894, one such 'crowded meeting' included an address by the local candidate J.E. Mackey (member of the Legislative Assembly) to discuss the financial depression at a time when the community was facing particular hardship (Age, 10 September 1894:6).

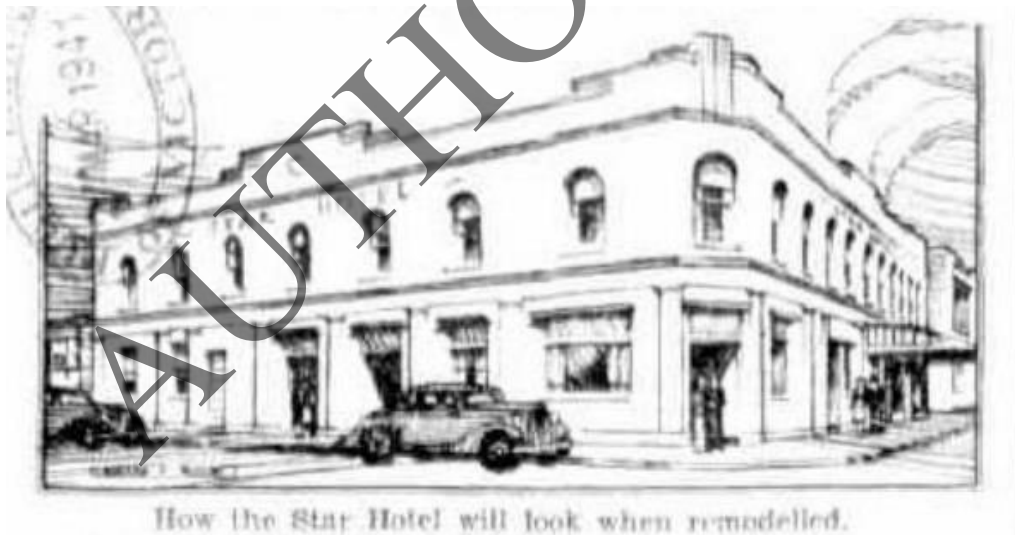
In 1906, the Victorian government established a Licensing Reduction Board with set ratios, different for rural and urban Victoria, for the closing of hotels. The Board investigated one licensing district after another and recommended closure of hotels, principally those older and smaller establishments typical of the inner suburbs and the older mining towns (Queens Arms Hotel Citation 927). During the 1920s the proprietors of the Star Hotel were repeatedly in trouble with the board for 'sly grog selling' outside of permitted hours. Licensee Mary Daisy Scullion and her husband were fined in 1929 for trading after hours, with the Chairman of the Licensing Court declaring that 'for twenty years this hotel had been a source of trouble to the court.' (Age, 22 November 1929:13) Older hotels that survived the new licensing regime often did so through extending the building or adding second or third storeys of accommodation (McConville).

In 1941 the architect D.F. Cowell Ham designed a remodelled façade for the 'well known South Melbourne hotel' in the popular Streamline Moderne style (*Herald*, 26 March 1941:12). At this time, the owner was F.W. Jefferson. The remodelling extended the hotel along both Clarendon and Market Streets and refurbished the interior to include a new private entrance, hall, office, dining room, kitchen and server. The hotel was still being used for accommodation, with eight bedrooms, bathrooms and lavatories on both floors. A new refrigerated cool room was built under the bar. The bar was rearranged to serve a women's parlor, bar parlor and bottle department. The refurbishment cost £3400 and was carried out by the Reynolds Brothers. (*Herald*, 26 March 1941:12)

During the 1990s, the Star Hotel converted to a nightclub and was considered one of Melbourne's premiere live entertainment venues featuring Australia's leading cover bands, DJs and special events (Aizen, St Kilda Historical Society 2004).



MMBW Plan 1895, showing the former extent of the Star Hotel before the amalgamation of the neighbouring properties, with subject site indicated in red. Source: State Library of Victoria.



Artist's sketch of the Star Hotel's remodeled façade in 1941.

References

Becky Aizen, Pots, Punks and Punters: A History of the Hotels of St Kilda and South Melbourne, Kilda Historical Society 2004.

South Melbourne Rate Books.

Sands & McDougall Municipal Directories.

State Library of Victoria.

The Age, as cited.

The Herald, as cited.

Andrew Lemon, 'A History of South Melbourne', p.4/1 of Allom Lovell Sanderson *City of South Melbourne Urban Conservation Study* 1986.

Chris McConville & Associates, 'Hotels in Victoria thematic typology', prepared for Historic Building Council, n.d.

Way Back When, *Thematic Environmental History of the City of Port Phillip* 2021.

Description

The building is located at the northwest corner of the intersection of Clarendon and Market streets. The rectangular parcel of land has frontage to both streets, with a chamfered corner with recessed entry. A square recessed doorway enters off Clarendon Street, and a series of doors off Market Street.

The ground floor façade consists of regular square recessed window openings defined by pilasters. Above the ground floor window openings are rectangular window canopies impressed with parallel line motifs.

The first-floor façade has seven regularly spaced arched windows with lintels and sashes on the Market Street elevation. A similar row of nine arched windows present to the Clarendon Street elevation. The corner entry has a matching arched window above. Rendered courses run horizontally along the façade between the levels with parallel line motifs.

Typical of the Streamline Moderne style, ornamentation is concentrated on the upper part of the building and is most evident in the parapet where the parallel line motif is joined by a stepped skyline motif that repeats at the corners and in the middle of each street façade. At the street corner the horizontal expression is broken by rendered vertical lines, again characteristic of the Streamline Moderne style.

The building appears largely intact to its 1941 Streamline Moderne remodelling, with alterations including the replacement of ground level window infill with glazing, new corner door, and new recessed entry off Clarendon Street.



View of the façade from the corner along Clarendon Street.
Source: Trethowan Architecture



View of the corner elevation including Moderne parapet details. Source: Trethowan Architecture

Comparative analysis

There are many hotels included in the Heritage Overlay either individually or as part of a precinct. The majority of hotels remodelled during the interwar period adopted either a Stripped Classical/Greek Revival (particularly those owned by CUB and designed by Sydney Smith & Ogg) or Streamline Moderne (sometimes with Jazz Moderne details). Historically, the former Star Hotel can be compared to other surviving hotels, many of which were replaced or remodelled in the early twentieth century. These include:

- The Golden Gate Hotel at 238 Clarendon Street, South Melbourne (Contributory within HO440), established in 1854, is another Victorian era corner hotel, built in at least two stages, and altered during the interwar period with Stripped Classical elements added to earlier Renaissance Revival character. It has typical form and detailing including splayed corner entry, square top or arched windows and doors, and remnants of tiled dado at ground floor level.
- Other corner hotels include the Hotel Rex at 145 Bay Street, Port Melbourne (Citation 724). This was probably the largest hotel built in Port Melbourne, its exterior is relatively intact and is a refined Renaissance revival design, representative of hotel design in the 1870s and '80s. It is a prominent element in the Bay Street streetscape. While different in style to the subject property, it demonstrates comparable characteristics of the local corner hotel in terms of scale and corner street presentation. The Limerick Arms at 364-8 Clarendon Street, South Melbourne (Contributory within HO440) was among the first hotels in Emerald Hill, established in 1855. It is a three-storey corner hotel, remodelled and rebuilt during the interwar period (1926 and 1939) in the Stripped Classical style including the addition of a third storey. The ground level has been

substantially altered. The former Queens Arms Hotel (Citation 927) was similarly rebuilt in the 1920s in response to the changing Licensing Board regime, but to a Greek Revival style.

The former Star Hotel is one of a number of Victorian-era hotels in the City of Port Phillip that were remodelled in the Streamline Moderne style in the interwar period. These include:

- The Pier Hotel at 1 Bay Street, Port Melbourne (Citation 608), site of a hotel since 1840, dates to the 1860s though its appearance like that of the subject property reflects the interwar remodelling in the popular Moderne or Streamline Moderne style. The hotel is also significant as an example of a Victorian hotel remodelled using these Art Deco styles, including enclosed first floor lounge with corner glazing and combination of strong horizontal parallel lines punctured by its understated vertical tower. By comparison, the former Star Hotel demonstrates a more consistent design distinguished by the rhythm of its upper level arched windows and its stepped silhouette parapet.
- The Town Hall Hotel at 135-7 Bank Street, South Melbourne (Contributory within HO440), a two-storey hotel, established in 1868 and substantially remodelled in 1936 to a design by architect Frederick Moresby. The hotel is notable for the Moderne style facade with some Art Deco details. By comparison, the former Star Hotel demonstrates stronger corner site orientation and more intact streamline features at ground level such as the horizontal line window canopies.
- The former Royal Mail Hotel (HO485) at 22 Bay Street, Port Melbourne was similarly built in stages, starting in 1873 and remodelled in 1937 to a design by architect Richard Le Poer Terry. It demonstrates a similar stepped silhouette parapet and horizontal parallel lines characteristic of the Streamline Moderne style. By comparison the former Star Hotel again demonstrates stronger corner orientation and distinctive combination of arched windows, stepped parapet at the corners, and intact window canopies at ground level.
- The Rose & Crown Hotel at 309 Bay Street, Port Melbourne, which was remodelled in 1939 to a design by the same architect D.F. Cowell Ham. The place is Significant within HO1. It demonstrates comparable Streamline Moderne detailing combining horizontal and vertical ornamentation and patterned stepped parapet. It is smaller in scale than the subject site and is located with a regular street frontage rather than on a corner. Overall, by comparison to other remodelled hotels in the municipality, the former Star Hotel demonstrates a high level of relative intactness and strong aesthetic characteristics of a Streamline Moderne remodelling of the earlier Victorian building.

AUTHORISED



Limerick Arms Hotel. Source: David Helms.



Hotel Rex. Source: Google Maps.



Pier Hotel. Source: David Helms.



Town Hall Hotel. Source: Trethowan Architecture.

Social Value

A Social Significance survey was conducted by Council in March 2023. The survey sought community input and asked a number of questions relating to the potential social significance of the former Star Hotel to residents and customers. A total of six responses were generated in this process. The low number of responses may reflect the fact that the hotel has not been in operation for some time. The responses rated the historical significance of the hotel the most highly. The respondents were from diverse social backgrounds and no particular pattern of social affiliation or usage emerged. From this, it can be suggested that the hotel is not significant to a particular social or community group above others. Rather, its usage has been by the community and residents more broadly. Its significance is encapsulated in its historical use as a hotel/pub serving the local community in Albert Park, South Melbourne and Port Melbourne.

Respondents were asked whether they belonged to particular social groups, clubs or teams. None were members of particular groups. Asked the reason for visiting the hotel, visiting to socialize, to eat, and out of convenience were the common reasons. In terms of duration of association, four respondents had been frequenting the hotel over ten years, while two began frequenting it more recently. In terms of aspects of the hotel that were valued, the most frequently nominated value was its location (five) while four respondents also nominated its architecture and façade, history, live music scene and outdoor atmosphere. Of the six respondents, five felt connected to the history of South Melbourne when they visited, while one disagreed. An identical pattern emerged from responses to the question of its importance as a meeting

place. Respondents were more ambivalent in relation to its importance to the live music scene, with three agreeing and three neutral or not knowing about this value. When given a list of attributes, four respondents picked History as its most important attribute, while one selected Gentrified.

Respondents were given the opportunity to write in their own words what the Star Hotel meant to them and/or their community and five respondents wrote:

'Hotels have been an important community resource historically. They remain an important community resource, although often under utilised as the degree to which they remain under community control has been diminished greatly by the corporate influence of the breweries and capitalism generally. That historical association with community needs to be defended and we look to the Council to achieve this.'

'Memories, sense of community and essence of South Melbourne.'

"One of the significant buildings for its historic value. A reminder of South's working class years"

'Does not mean anything to me- I work very close by and have visited a few times. I heard that it has a history of live music but never experienced it. Recently, I found it to have a bit of a dead atmosphere. It is in a great location and was all renovated inside but no one ever went there.'

"Easily identifiable; For public use; Gathering meeting spot; Remains after many pubs demolished or facades kept and developed into apartments and shops; Function space available; Restaurant"

'Social value' is a collective attachment to a place that embodies meanings and values that are important to a community. Based on the responses to the survey, the social value of the Star Hotel is demonstrated against the key indicators below. The indicators derive from the *Victorian Heritage Register Criteria and Threshold Guidelines* (Heritage Council Victoria, Dec. 2020).

- **The existence of a current community/group by whom the place is valued.**

The survey responses suggest an informal community of local people who have sporadically patronized the hotel and who appreciate it as a symbol of the area's local history.

- **The strength of the community/group's attachment to the place.**

The survey responses suggest a fairly weak attachment to the place as a local gathering place providing hospitality to the local community and surrounds.

- **Whether the social values resonate across the broader community as part of a story that contributes to the municipality's identity.**

Hotels have been an important social institution in the municipality throughout its history and continue to provide important social venues for the community today.

In conclusion, the former Star Hotel's value is adequately encapsulated in its historical significance. The community survey has not provided a strong basis for identifying a particular community or group by whom the place is valued, or a particularly strong attachment to the place over time when compared to other hotels. This does not preclude the future social significance of the place should it resume trading.

Assessment

This place has been assessed in accordance with the processes and guidelines outlined in the Australia ICOMOS *Charter for Places of Cultural Heritage Significance* (The Burra Charter) 2013, using the Hercon criteria.

Recommendations

Add to the Heritage Overlay as an individual place.

Apply external paint controls in the Schedule to the Heritage Overlay (to ensure complementary colour schemes are employed). No other specific HO schedule controls are required.

Extent of HO: Whole of property as defined by the title boundaries.

Primary source

Trethowan Architecture, *South Melbourne Structure Plan Heritage Review*, 2023.

Other studies

Not applicable.

Other images

None

AUTHORISATION

City of Port Phillip Heritage Review

Place name: Alma & Tasma Terrace
Other names: Houses

Citation No:
2467



INSERT MAP

Address: 68 & 70 Market Street, Southbank

Heritage Precinct: None

Category: Residential

Heritage Overlay: HO572

Period/Style: Federation/ Transitional

Graded as: Significant

Constructed: c.1905

Victorian Heritage Register: No

Designer: -

Amendment: TBC

Comment: New Citation

Significance

What is significant?

Alma and Tasma Terrace, built c.1905 at 68 and 70 Market Street, Southbank is significant.

Alterations and additions are not significant.

How is it significant?

Alma and Tasma Terrace is of local historical, representative and aesthetic significance to the City of Port Phillip.

Why is it significant?

The pair of terrace houses are historically important for their capacity to demonstrate the attraction of Market Street as an area of residential development during the Federation period. (Criterion A)

Alma and Tasma Terrace is significant as a representative of single-storey terrace pair through their demonstration of symmetry and matching design including shared parapet, side walls and verandah, matching chimneys and ornamentation with complementary names written in the parapets. (Criterion D)

Alma and Tasma Terrace is significant aesthetically as a very late example of the transitional Victorian to Federation style. It demonstrates the survival of the Victorian Italianate terrace form into the early twentieth century, with Italianate elements including rendered chimneys, decorative parapet and party walls with rendered details. The rounded pediments flanked by small pilasters are distinctive for the type and demonstrate the simpler Arts & Crafts style of the Federation period. (Criterion E)

Thematic context

Victoria's framework of historical themes

6. Building towns, cities and the garden state: 6.3 Shaping the suburbs

Port Phillip thematic environmental history

5. Buildings & Cultural Landscapes: 5.3 Diverse housing

History

Contextual history

The suburb of South Melbourne, known as Emerald Hill between 1855 and 1883, is part of the traditional Country of the Yalukit Willam clan of the Boon Wurrung people, part of the broader Kulin nation, whose name means 'people of the river'.

South Melbourne, initially called Emerald Hill by British colonists, was hemmed in on all sides by the swampy lowlands. The colonists treated the 'island' as a wasteland for grazing, dumping, camping and recreation in the 1830s-40s. To the east of the future Kings Way, settlement was not only constrained by the swamplands but also by the desire to keep free the area around the military reserve west of St Kilda Road. The first sixty-seven allotments of the 1852 Subdivision of Emerald Hill were auctioned on 18 August. (Lemon, p.7) Many buyers were speculators, none of whom planned to reside in the area. Development in the 1850s was rapid, with 1,149 houses assessed for rates by the Melbourne Corporation by 1855. (Lemon, p.8) The Sandridge Road was improved to make it an acceptable path through the marshes. Toll gates were established on the major roads through the district to raise funds up until 1878. The Emerald Hill municipality was created in 1855 and a railway line through from Melbourne to St Kilda opened in 1857. Emerald Hill was free from the building restrictions of Melbourne municipality that had been introduced to reduce the risk of fire. This led to the rapid construction of timber buildings in the suburb.

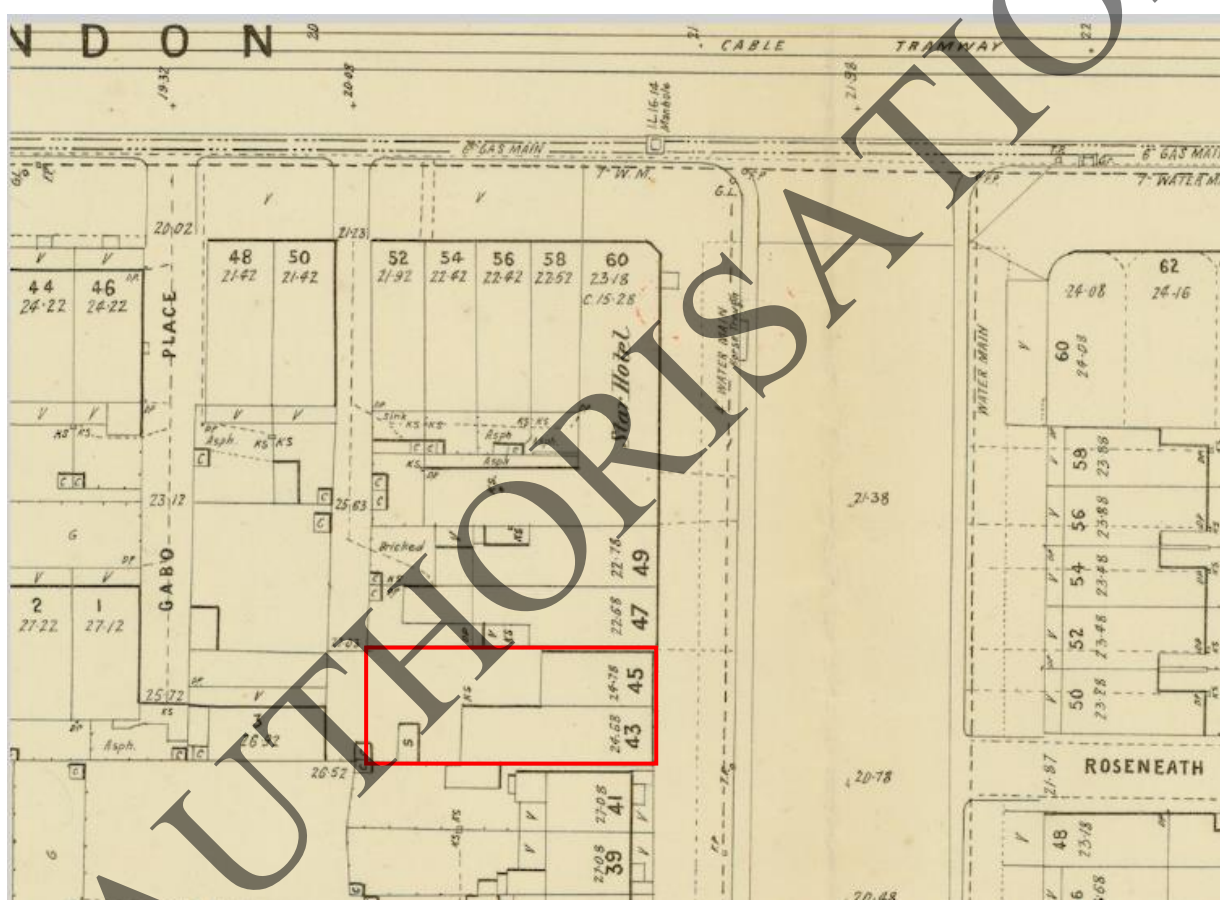
From the 1850s-1860s the gold rush period saw the rapid formation of a new suburb, 'out of the canvas' as new arrivals sought new residences and services close to their arrival point in the colony. While land northwards was eventually reclaimed for industrial use, expansion eastwards was stymied by the military reserve (later Victoria Barracks). The military reserve extended as far west as Moray Street, but was reduced to Hanna Street (King's Way) in 1862. Lowlands to the south would eventually be reclaimed for recreation (Albert Park). In the 1870s-80s the area became known for its cheap and well-located land for factories and worker housing. The population of South Melbourne grew from 17,101 in 1871, to 25,374 in 1881. In 1881 most of the land between the Barracks and Emerald Hill was sold, requiring filling and leveling. As a result, population saw an even greater increase to 41,724 by 1891. After the 1890s Depression, the area reached a new peak in industrial and residential development in the early twentieth century, but population growth slowed to 46,190 in 1911. In the Interwar and early post-war period South Melbourne fell into relative decline as population fled the inner city.

By 1947 the population fell to 43,452 and saw further declines in the post-war period. The remnant inner urban 'slums' became a concern of new government interventions in the 1960s, with significant interventions by the Victorian Housing Commission to demolish what was considered substandard housing and move tenants into modern tower developments. Nonetheless, the late twentieth century saw a resurgence in appreciation for the communities and identity of the inner city, including South Melbourne. This led to movements to conserve historic areas as well as experiments in new forms of medium density housing. New processes of gentrification concurrently led to fresh development pressures and an economic transformation of the area away from its industrial past.

Alma & Tasma Terrace

The site was first occupied in 1871, when the property was listed in the Sands & MacDougall Directories. In 1871 the lot at No 45 was occupied by Thomas Warren and operated as a store. A neighbouring lot was occupied by Michael Lynch. By 1895 the properties were shown as Nos.43 and 45 on the MMBW Plan, backing on to the alley. In the 1895/7 Ratebook, Nos. 43/45 appear as a single entry 6 room wooden building occupied by basket maker William Pritchard and owned by Thomas Carter, Stationer. By 1900, No.43 was occupied by Pritchard, with No.45 appearing vacant.

By 1905, the street numbers had changed, with William Pritchard moved to No.64 and L. Bird, engineer, residing at No.66. The fluctuating numbers of occupancies and changing street numbers indicate a significant degree of change in the vicinity at the turn of the century. This, combined with the transitional late Victorian to Federation style of the terrace makes this the likely construction date for the residences. By 1910, street numbers had changed slightly, with L. Bird continuing at No.68, and No.70 occupied by George Sheppeck.



MMBW Plan 1895 with the subject site indicated in red. Source: State Library of Victoria.

References

South Melbourne Rate Books

Sands & McDougall Municipal Directories

State Library of Victoria

The Age, as cited.

Andrew Lemon, 'A History of South Melbourne', p.4/1 of Robert Peck Van Hartel Trethowan *City of South Melbourne Urban Conservation Study* 1986

Way Back When, *Thematic Environmental History of the City of Port Phillip* 2021.

Description

Alma and Tasma are a pair of matching terraces that present symmetrically to the street, with each having a single door and sash windows. The doors and windows are tall and arched at the top. Originally exposed brick with bluestone banding and bluestone sills, the walls have been painted over white.

The verandah with iron lacework is divided by a brick party wall, with side walls to the boundaries with moulded ornaments (vermiculated panels supported by scroll brackets) at the ends. The verandah tiles and fences have been replaced with new but sympathetic tiles and fences.

The houses are joined by matching parapets with the name 'Alma' and 'Tasma' rendered under an arched pediment flanked by pilasters in a simple Federation/Arts & Crafts style and decorative scrolls and cornice framed by vermiculated posted with fluted undersides in a more Italianate style.

A pair of Italianate rendered chimneys are visible behind the parapet. The pitched roofs are corrugated iron. Hipped roofs with simple brick chimneys are visible at the rear, above a skillion roofed rear addition.



View of the terrace façade. Source: Trethowan Architecture.

Comparative analysis

In terms of the late Victorian transitional style continuing into the twentieth century, the terrace pair compares to the terrace houses at 231 & 233 Bank Street, South Melbourne (Contributory within HO30). Built in 1901, the double storey terraces evoke the form of the Victorian terrace while incorporating elements of the Art Nouveau more popular in the Edwardian period home. The subject property is more modest in scale by comparison but compares well in terms of demonstrating a transitional style in a smaller scale terrace.

Some comparisons of terrace pairs/rows with shared parapets within precincts include the terraces at 310-16 & 322-28 Bank Street, South Melbourne (HO440). Dalkeith Terrace displays more purely Victorian period Italianate exuberance in terms of detail. By comparison the subject property demonstrates similar fundamental form with verandahs separated by party walls, surmounted by matching symmetrical parapets and prominent chimneys. In the subject site however the evolution to the simpler Federation style is more evident in the contrasting simplicity of the arch and pilaster forms to the parapet.

In terms of intactness, the subject property compares well to significantly graded terraces within HO440. The terrace houses at 194-98 Ferrars Street, South Melbourne (HO440) while earlier in period, are similarly overpainted and have new, taller fences to the front. The subject property also compares well to these in terms of integrity and the front fences and tiled verandahs are sympathetic. The small group of terraces at 388-92 Park Street, South Melbourne (HO440) also display varying degrees of overpainting to the brickwork, and new front fences. The subject property compares favourably as a discrete grouping of terrace houses, in this case a matching pair from a later period, demonstrating a distinctive mixture of aesthetic elements combining Victorian terrace form with more unusual transitional parapet design.

152-3 Evans Street is of local significance (Significant within HO1). A relatively early pair of houses in this part of Port Melbourne, the simple detailing of the cornice, parapet and window surrounds is representative of domestic architecture of the 1870s. They are unusual in Port Melbourne, particularly for the early date, for their brick construction.

81 & 82 Evans Street, Port Melbourne (Significant within HO1). Pair of bi-chromatic brick cottages with undivided hip roof indicating an early date (c.1880). The bi-chrome brickwork includes decorative quoining around the openings and at wall corners and chimneys (chimneys also have rendered cornices), and diaper work below the windows and between the eaves brackets. Cast iron verandah with palisade fence. The pair have tiled verandah floor and altered fences.

Overall, Alma & Tasma Terrace compares well to these various examples as a representative of the terrace type, demonstrating elements of the unified design such as shared parapet, verandah, symmetrical design and place naming. It is distinguished by its incorporation of simpler Federation design elements such as the rounded pediments, demonstrating the historical adaptation of the terrace pair type and style into the early twentieth century.



231 & 233 Bank Street, South Melbourne (1901).



Victorian terraces at 322-28 Bank Street, South Melbourne.



152-3 Evans Street, Port Melbourne

Assessment

This place has been assessed in accordance with the processes and guidelines outlined in the Australia ICOMOS *Charter for Places of Cultural Heritage Significance (The Burra Charter)* 2013, using the Hercon criteria.

Recommendations

Recommended for inclusion in the Heritage Overlay as a significant place.

Extent of HO: Whole of property as defined by the title boundaries.

No specific HO controls are required.

Primary source

Trethowan Architecture, *South Melbourne Structure Plan Heritage Review*, 2023.

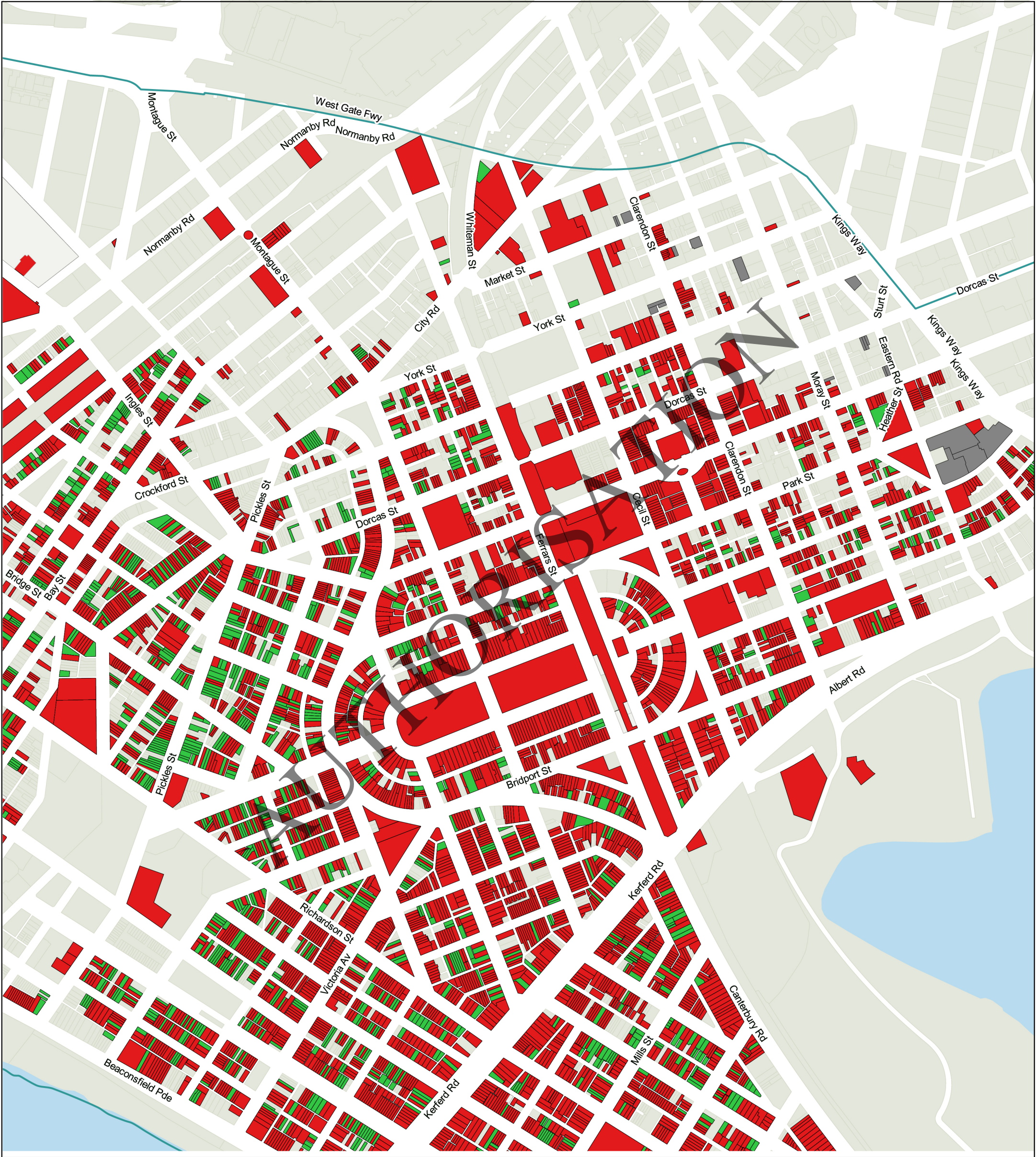
Other studies

Not applicable.

Other images

AUTHORISATION

City of Port Phillip Heritage Policy Map: Proposed updates for South Melbourne (July 2024)



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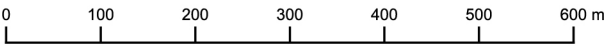
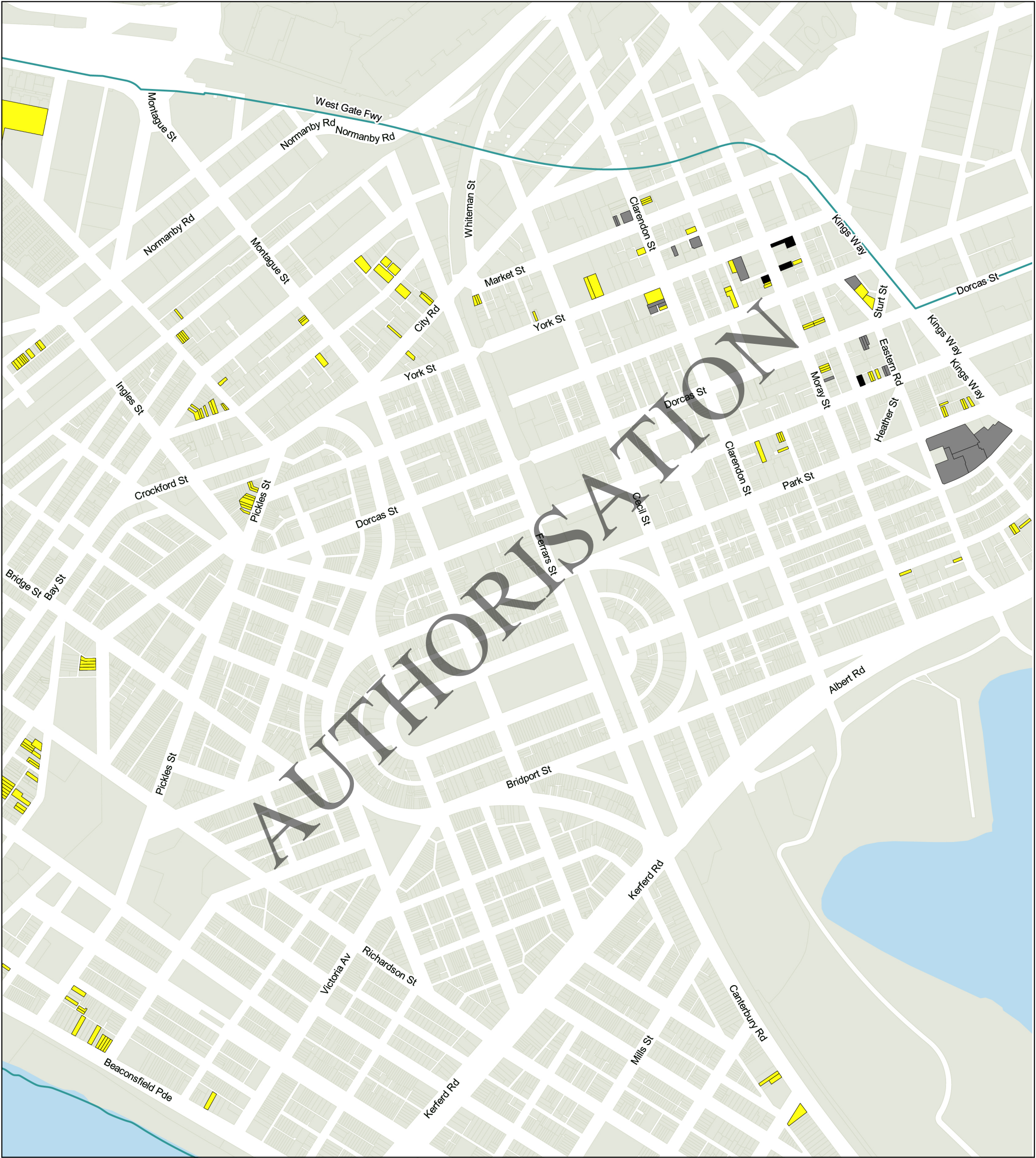
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Amendment C219 - Proposed

- Significant Heritage Place - inside HO
- Contributory Heritage Place - inside HO
- Check Statement of Significance for grading



City of Port Phillip Neighbourhood Character Map: Proposed updates for South Melbourne (July 2024)



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- Amendment C219 - Proposed
- Contributory Heritage Place - outside HO
 - Check Statement of Significance for grading
 - No grading

