
The Port Phillip Design Manual, Version 3, 2000 was included as a Reference Document in the Port Phillip Planning Scheme as a part of Amendment C5 (Neighbourhood Amendment).

In 2007 the Dunstan Estate Guidelines were adopted and included into this Design Manual.

In 2010 the Garden City Estate & Fishermans Bend Guidelines were reviewed, updated and adopted.

The Port Phillip Design Manual remains as a Reference Document to the Port Phillip Planning Scheme.

PREAMBLE

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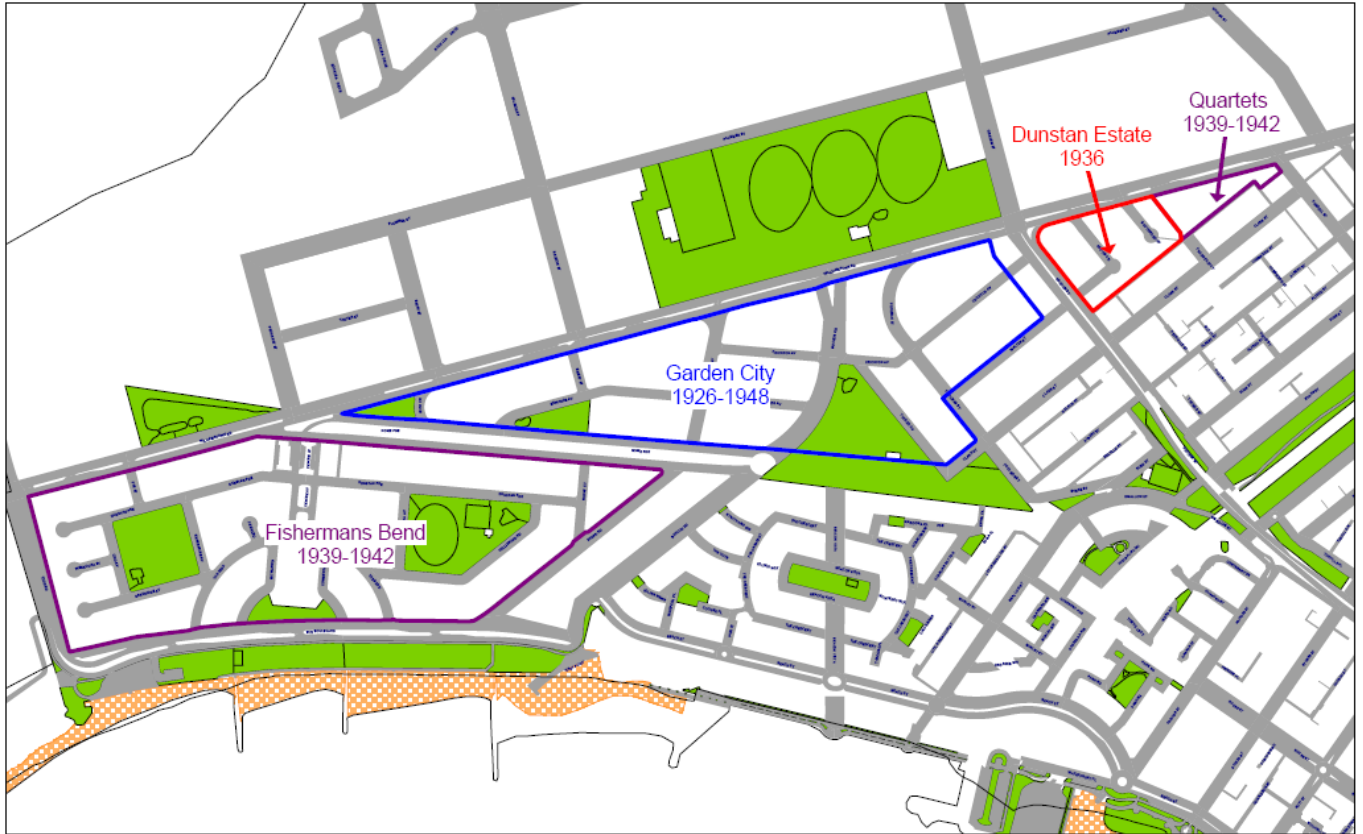
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Map of the housing estates within the Garden City neighbourhood: Fishermans Bend (including the Quartets), Garden City & Dunstan Estate.

CONSERVATION details

The following guidelines summarise the main elements of a building in each of the architectural eras predominant in the City of Port Phillip.

The Glossary attached to these Guidelines provides an explanation of some of the specialist architectural terms used.

i) Early Victorian (1850s)

ITEM	DESCRIPTION
<i>Materials / Colours</i>	
Roof	Slate or unpainted corrugated iron with galvanised sheet metal cover flashings; possibly split shingles covered or replaced by corrugated iron later.
Walls	Lime washed, brick work (locally made bricks), with flush struck lime mortar joints, some natural finish cement rendered, often since painted; or timber weather boards (very occasionally shaped edges) with timber stops and corners. Bluestone footings. Bluestone.
Verandah Floor	Timber. Chequered slate and marble. Terra-cotta.
Colours	Walls: yellow cream, ochre wash, or natural brick, stone or cement render. Joinery and trim: semi gloss finish in cream, light stone, light brown, rich brown, Indian Red, olive and deep Brunswick Green (Prussian Blue or dark tints should be used only in restricted situations on large buildings), and limited use of graining to doors and windows.
Elements	
Roof Form	Hipped roof generally 20 to 30 degrees, pitch with some simple parapets.
Chimneys	Multiple or single and symmetrically placed.
Eaves	Small or no eaves overhang, occasionally with simple brackets or simple parapet.
Verandah	Skillion or hipped, convex/concave corrugated galvanised iron roof to verandah; generally to ground floor only, and not always included. Verandah roof form always separate from main roof; timber post generally.
Facade Form	Generally simple, quiet planar facade (with or without verandah).
Windows	Double hung sash windows with simple forms and arrangements; often multi-plane sashes.

Doors	Four panel bolection moulded doors
Gutters	Ogee or half-round profile with timber mould below.
Fence	Types of fences used include early timber picket, cast iron palisade, corrugated galvanised iron sheet (for side fence) and hooped metal overlapping palisade.
Other	Single or double storey. Double storey either with or without verandah or to ground floor only. Single storey either with or without verandah.
Ornamentation	
Roof	Nil
Eaves	None or narrow sometimes with bracket and simple cornices.
Chimneys	Simple corbelled brick detail to brick chimneys. Cornices to rendered brick chimneys.
Parapets	Simple and rendered.
Verandahs	Timber posts, sometimes openwork in timber.
Window/door framing	Simple mouldings/architraves or rendered quoins on substantial houses.
Glazing	Toplights in front doors, multi-pane sashes and occasional use of French doors.
Proportions	
Facade Composition	Rectangular box.
Facade Break Up	Divisions vertically with setbacks and fenestration. Division horizontally with eaves, string coursing and verandahs.
Openings	Vertical rectangles, generally symmetrically arranged.
Siting and Plan Form	Generally either built to street alignment or with minimal setbacks, generally quite uniform, villas in large grounds, terraces, semi-detached and detached examples. The plan generally consists of two front rooms with a central passage (double fronted width) or side passage (single fronted width) leading to two back rooms. The front rooms are roofed by a hipped roof and the back rooms are roofed by a skillion attachment or a hipped roof.

ii) Italianate (1850s to 1880s)

ITEM	DESCRIPTION
<i>Materials / Colours</i>	
Roof	"M" shaped hipped slate roof to generally detached villas. Corrugated iron to verandah. Gables used in various houses.
Walls	Rendered brick, sometimes on a bluestone plinth with mouldings and cast cement ornament.
Verandah Floor	Timber or tiles
Colours	Walls: Rendered walls generally unpainted. Framings: darker colours (including black). Sashes: lighter colours (including white). Window/door framing: graining or paint. Woodwork: brown, dark red, dark green.
Elements	
Roof Form	15 to 25 degrees pitch. Hipped, occasionally gabled.
Chimneys	Most commonly symmetrically placed, generally multiple.
Eaves	Narrow eaves with brackets and occasional use of wide eaves.
Verandah	Concave corrugated galvanised iron roof, cast iron or timber columns, often with return portion; verandah roof separate from main roof.
Facade Form	Multi-planar. Occasionally a square tower at the front of the roof and a projecting front room.
Windows	Double hung, sometimes with curved head. Complexity of types, heights and usage. Upper floors with some full length sashes, often to verandahs.
Doors	Four panel doors with sidelights and toplights.
Gutters	Ogee profile.
Fence	Fence types include cast iron palisade, timber picket and corrugated galvanised iron (for side fence).
Other	String coursing often continues over curved opening beads.

Ornamentation

Roof	Slate, corrugated galvanised iron, often with lead ridge capping.
Eaves	Mouldings and brackets.
Chimneys	Cornices, panels and mouldings.
Parapets	None or balustraded.
Verandahs	Timber bressumer or cast iron (more common) valances.
Window/door framing	Mouldings to head and surrounds, very occasionally with attached pilasters.
Glazing	Coloured and etched glass often used to sidelights.

Proportions

Facade Composition	<p>Rectangular. Classical, balanced, asymmetrical arrangement. Generally detached villas.</p> <p>Projecting section has square to vertical rectangles grouped horizontally. Facade divided horizontally by string courses, eaves, plinth and verandah form.</p>
Openings	Windows as vertical rectangles (some triple lights).
Siting and Plan Form	<p>Varying setbacks to street, generally detached.</p> <p>Plan form based on a front section containing four rooms with a central passage, with a valley/hip roof structure above. This era also generally included a projecting section of the front facade with an octagonal bay window element often included.</p>

iii) Mid-Late Victorian (1860s to 1880s)

ITEM	DESCRIPTION
<i>Materials / Colours</i>	
Roof	"M" shaped roof with patterned or plain slate (hip exposed or behind parapet); corrugated iron to verandah.
Walls	Face brick (Hawthorn brick, later red brick) with flush struck lime mortar joints or smooth rendered. Tuck pointing. Limited polychrome in 1880's. Square edged timber weatherboards with timber stops to corners. The fronts of many weatherboard buildings used imitation ashlar boards. Bluestone fittings. Bluestone.
Verandah Floor	Timber. Terra-cotta tiles or encaustic tiles.
Colours	<p>Walls: Sandstone, ochre, salmon pink, natural brick or cement render.</p> <p>Joinery: Gloss finish in cream, light stone, light brown, rich brown, Indian Red, and Brunswick Green.</p> <p>Window/door framing: Graining to doors and windows. Major doors generally dark colour, with lighter colour for panels.</p>
Elements	
Roof Form	20 to 30 degrees pitch. "M" shaped, hipped.
Chimneys	Symmetrically placed, generally multiple.
Eaves	Tall parapets often to front facade. Narrow eaves.
Verandah	Concave or convex corrugated galvanised iron to roof. verandah roof separate from main roof, often with gabled porch roof built-in.
Facade Form	Quite flush, often with projecting masonry wing walls. Some arcading and projecting verandahs.
Windows	Double hung, sometimes with sidelights of varying vertical proportions.
Doors	Four panel, with basalt threshold (as for window sills).
Gutters	Ogee profile galvanised iron with timber mould below.
Fence	Fence types include cast iron palisade, timber picket and corrugated galvanised iron (for side fence) or hedges.
Other	Both single and double storey with verandahs/balconies to each level common.

Ornamentation

Roof	Cast iron cresting and finials sometimes used.
Eaves	Panelled entablature to eaves on front facade, particularly on later examples.
Chimneys	Corbelled brick detail to face brick chimneys and cement render cornices to face brick or rendered brick chimneys.
Parapets	Increasingly elaborate decoration, deep cornicing, decorative parapets with urns and pediments and rendered/timber brackets.
Verandahs	Cast iron columns, balustrading, friezes, brackets (1870s/80s); or timber verandah posts and frieze, often to weatherboard house.
Window/door framing	Wood graining popular, mostly to doors.
Glazing	Coloured and etched glass sometimes used to front door (1870's/80s).

Proportions

Facade Composition	Strong rectangles and verandah elements and a formal arrangement of elements.
Horizontal verandah lines, string coursing and parapet. Columns divide facade into smaller horizontal and vertical rectangles.	
Openings	Vertical rectangles, generally symmetrically arranged.
Siting and Plan Form	Generally setback from street alignment including terraces, semi-detached and detached examples. Plan form similar to Italianate. To increase the internal space and articulate the facade, gabled or hipped wings were sometimes added.

iv) Boom Style (1880s to early 1890s)

ITEM	DESCRIPTION
<i>Materials / Colours</i>	
Roof	Patterned or plain slate or corrugated galvanised iron either hipped, gabled or behind parapet; Corrugated iron concaves or bullnose roof to verandah.
Walls	Tuck pointed brickwork, elaborate render work, face brick work and polychromatic brickwork distinguished this style from earlier eras.
Verandah Floor	Terra-cotta tiles, encaustic tiles or, for more expensive houses, marble.
Colours	<p>Walls: Stone, ochre, dark colours, natural brick or cement render.</p> <p>Joinery: Gloss finish in cream, light stone, light and middle browns, rich brown, Indian Red, various greens including Brunswick Green (Prussian Blue or dark tints used in restricted situations).</p> <p>Window/door framing: Graining to main door.</p>
Elements	
Roof Form	Low pitched "M" shaped, hipped roof behind parapet, up to 30 degrees.
Chimneys	Commonly symmetrically placed, generally multiple.
Eaves	Narrow eaves, heavily decorated parapets and deep cornices.
Verandah	Generally included concave or bull nosed corrugated galvanised iron roof to verandah, often to both floors on double storey residences. Verandah roof separate to main roof.
Facade Form	Some arcaded verandahs and some projecting bay windows.
Windows	Double hung, sometimes with sidelights of varying vertical proportions.
Doors	Four or six panels with basalt threshold for outside doors.
Gutters	Ogee profile galvanised iron with timber mould below.
Fence	Fence types include cast iron palisade, timber picket and corrugated galvanised iron (for side fence) or hedges.
Other	Single and double storey with single storey forms often similar in height to earlier double storey dwellings due to their parapet height and higher ceilings.

Ornamentation

Roof	Cast iron cresting and finials sometimes used.
Eaves	Panelled entablature to eaves on front facade.
Chimneys	Heavy corbelling and cement render cornices to face brick or rendered brick chimneys with shaft decoration.
Parapets	Stucco, moulds, brackets, scrolls, balusters, shell motifs, urns, balls and acroteria.
Verandahs	Cast iron friezes. Cast-iron columns with fluted or decorated shafts, floral cast-iron lacework and masonry arcading (cast-iron lacework was made locally and so new designs were quite frequent).
Window/door framing	Window head moulding (occasionally attached spiral pilasters) and heavy door panelling.
Glazing	Etched and coloured glass to door sidelights and occasionally some leadlight windows to main rooms.

Proportions

Facade Composition	Similar to mid-Victorian period but with the forms standing out more boldly.
Openings	Vertical rectangles, generally symmetrically arranged.
Siting and Plan Form	Generally setback from street alignment to various degrees, with some groups of dwellings of uniform setback; terraces, semi-detached and detached examples. Plan form as for mid-late Victorian period.

v) Edwardian (1900s to 1910s)

ITEM	DESCRIPTION
<i>Materials / Colours</i>	
Roof	Terra-cotta 'Marseilles' tiles for brick dwellings or corrugated galvanised iron or slate.
Walls	Pressed red face brick (decorative moulded bricks also available), tuck pointing; rough cast render as bands and to gable ends. Half timber gable ends or square edged timber weather-boards often with notched patterns to resemble shingles. Brick work usually cavity brick.
Verandah Floor	Timber verandah floor, occasionally tiles.
Colours	Cream, yellow ochre, reds, dark brown, with dark and softer green trims.
Elements	
Roof Form	High, 30 to 40 degree pitch, asymmetrical roof form. In larger houses roof forms became more complex with the provision of steep roof planes and an array of gables, vents, dormer windows, turrets and spires projecting through the roof.
Chimneys	Asymmetrically placed, tall, ornate and brick.
Eaves	Narrow or with exposed rafters and lining boards.
Verandah	Low verandahs often extend from the main roof, clad in tiles.
Facade Form	Gabled windows project at right angles with square or oblong upper lights with obscure pattern glazing in pastel shades or leaded coloured glass patterns. Bay windows or curved square double hung sash windows and circular windows to principle rooms.
Doors	Three or four panels. Narrow vertical panels with large (often gabled) top panel.
Gutters	Ogee or later quadrant profile galvanised iron.
Fence	Fence types include timber picket, woven wire, chain link mesh, corrugated galvanised iron (for side fences) or hedges.

Ornamentation

Roof	Terra-cotta roof ridge capping and finials including dragons. On larger houses, towers and spires often came out of the roof covered with terra-cotta tiles. Decorative gable ends, often with pressed metal or roughcast infill panels and timber valances. Half timbering to gable ends.
Eaves	Timber fretwork sometimes.
Chimneys	Red brick chimney with brick or stucco banding and stucco ornamentation; often tall with capping blocks and chimney pots.
Parapets	Rarely used, sometimes at rear or side. Turned timber posts, elaborate timber fretwork, friezes and valances with little iron used. Hoods to window openings.
Verandahs	Leadlight to principle rooms.
Openings	Vertical and horizontal rectangles, often in an asymmetrical arrangement.
Siting and Plan Form	Generally setback from street to various degrees. Generally semi-detached and detached; some groups of semi-detached dwellings of uniform setback.

vi) Californian Bungalow (1920s)

ITEM	DESCRIPTION
<i>Materials / Colours</i>	
Roof	Terra-cotta or coloured concrete tiles.
Walls	Red brick, rough cast render detailing. Occasional pebble dash finish to verandah piers. Timber shingles to gables over bay windows.
Verandah Floor	Timber.
Colours	Woodwork/weather boards: brown, ivory and green (a popular colour)
Elements	
Roof Form	20 to 30 degree pitch, large gable ends, (perhaps with porch roof form a minimum 15 degree pitch); low, wide over hung roof.
Chimneys	Generally one large chimney, either to front or side elevation, assymetrically placed.
Eaves	Overhanging (450mm) rafters exposed below gutter line.
Verandah	Deep verandahs with heavy pilons as supports.
Facade Form	One room forward generally.
Windows	Casement/double hung. Sometimes a simple bay window to main room. Diamond paneled leadlight on upper sashes of windows often to either side of chimney.
Doors	Often glazed.
Fence	Fence types woven and cyclone wire mesh with timber posts, brick, rendered masonry, timber picket or hedges.
Ornamentation	
Roof	Simple form with gabled ends.
Eaves	Deep barge boards with projecting rafters.
Chimneys	Little or no corbelling.
Parapets	Nil
Verandahs	Heavy tapered pylons with brick or rendered capping to verandah wall.
Window/door frames	Simple framing.
Note	A more restrained approach than earlier eras.

Proportions

Facade	Informal horizontal rectangles with a simple triangular roof form.
Siting and Plan Form	Setback significantly from the street alignment, to a fairly uniform degree in each street. Generally detached. Always asymmetrical plan form but with strong central facade element and central hall component.

vii) Spanish Mission (Late 1920s to Early 1930s)

ITEM	DESCRIPTION
<i>Materials / Colours</i>	
Roof	Cordova or Marseilles pattern terra-cotta tiles.
Walls	Stucco, sometimes with a parge effect
Verandah Floor	Tiled verandah floor.
Colours	Off white, yellow ochre, terra-cotta, salmon pink, beige and dark brown.
Elements	
Roof Form	20 to 30 degree pitch.
Chimneys	Assymmetrically placed, generally to side elevation and capped with cordova tiles.
Eaves	Boxed eaves.
Verandah	Parapet with a scrolled edge, arch opening with intermediate columns.
Facade Form	Planer front rooms and verandah.
Windows	Double hung curved heads.
Fence	Fence types include woven and cyclone wire mesh with timber posts, brick, rendered masonry, timber picket or hedges.
Ornamentation	
Roof	Cordova tiled ridges.
Eaves	Nil.
Chimneys	Tile Caps.
Parapets	Scrolled edge.
Verandahs	Baroque style intermediate columns.
Window/door frames	Plain framing with black wrought iron grilles sometimes.
Glazing	Sometimes bevelled edge glazing.
Other	Decorative lanterns sometimes.
Proportions	
Siting and Plan Form	Setback a considerable distance from the street alignment but to a fairly uniform degree in each street. Generally detached villas. Generally asymmetrical plan form with strong facade elements. Also apartment blocks.

GLOSSARY

of terms

Arcade	A range of arches carried on piers or columns, either freestanding or blind, i.e., attached to a wall.
Architrave	The lowest of the three main parts of an entablature; also more loosely, the moulded frame surrounding a door or window (if this frame turns away at the top at right angles, rises vertically and returns horizontally, it is called a shouldered architrave).
Ashlar	Hewn blocks of masonry wrought to even faces and square edges and laid in horizontal courses with vertical joints, as opposed to rubble or unhewn stone straight from the quarry.
Attic Storey	The space within the sloping roof of a house or the upper storey of a building if less high than the other floors.
Balcony	A platform projecting from a wall, enclosed by a railing or balustrade, supported on brackets or columns or cantilevered out.
Balustrade	A short post or pillar in a series supporting a rail or coping and thus forming a balustrade.
Barge board	A board fixed to the verge of a pitched roof.
Bay	A vertical division of the exterior or interior of a building.
Bay window	An angular or curved projection of a house front filled by fenestration. If curved, also called a bow window. If on an upper floor only, called an oriel or oriel window.
Bead moulding	A small cylindrical moulding enriched with ornament resembling a string of beads; used in the Romanesque period.
Blocking course	In classical architecture, the plain course of stone surmounting the cornice at the top of a building. Also a projecting cornice of stone or brick at the base of a building.
Bolection Moulding	A moulding used to cover the joint between two members with different surface levels. It projects beyond both surfaces.
Bracket	A small opening supporting a piece of stone or other material, often formed of scrolls or volutes, to carry a projecting weight.

Bressumer	A massive horizontal beam, sometimes carved, spanning a wide opening such as a fireplace. Also the principal horizontal rail in a timber-framed house.
Brickwork	A header is a brick laid so that the end only appears on the face of the wall, while a stretcher is a brick laid so that the side only appears on the face of the wall.
Bungalow	A single storey house.
Capital	The head or crowning feature of a column.
Casement window	A metal or timber window with the sash hung vertically and opening outwards or inwards.
Cast iron	A hard alloy of iron, carbon and silicon cast in a mould.
Chimney shaft	A high chimney with only one flue.
Chimney stack	Masonry or brickwork containing several flues, projecting above the roof and terminating in chimney pots.
Column	An upright member, circular in plan and usually slightly tapering; in classical architecture it consists base, shaft and capital. It is designed to carry an entablature or other load, but is also used ornamentally in isolation.
Coping	A capping or covering to a wall, either flat or sloping to throw off water.
Corbel	A projecting block, usually of masonry, supporting a beam or other horizontal member.
Corbelling	Brick or masonry courses, each built out beyond the one below. A common device on the brickwork of Edwardian chimneys.
Cornice	In classical architecture, the top, projecting section of an entablature; also any projecting ornamental moulding along the top of a building, wall or arch, finishing or crowning it.
Cresting	An ornamental finish along the top of a screen, wall or roof; usually decorated and sometimes perforated.
Dormer window	A window placed vertically in a sloping roof and with a roof of its own. The name derives from the fact that it usually serves sleeping quarters.
Eaves	The part of a roof which overhangs beyond the line of the wall.
Encaustic Tiles	Earthenware flooring tiles that are glazed and decorated, commonly used in the late Victorian period.

Etched glass and Patterned clear glass	Patterned clear glass common in the early and mid Victorian periods.
Fascia	A dressed timber member fixed at the end of a roof rafter that usually supports a gutter.
Fenestration	The arrangement of windows in a building.
Finial	Ornament applied to the apex of a roof, pediment or gable.
Fluting	Shallow, concave grooves running vertically on the shaft of a column, pilaster, or other surface.
French doors/windows	A long window reaching to floor level and opening in like a pair of doors.
Frieze	The middle division of an entablature, between the architrave and cornice; usually decorated but may be plain or the decorated band along the upper part of an internal wall, immediately below the cornice.
Gable	The triangular upper portion of a wall at the end of a pitched roof. It is sometimes finished with a decorated barge board or rough cast render.
Graining	The imitation of natural woodgrain involving the application of a light undercoat and then a darker paint and varnish.
Half timber (also Nogging)	A method of construction where walls are built of timber framework with the spaces filled in by plaster or brickwork (known as nogging). Sometimes the timber is covered with plaster or boarding laid horizontally.
Leadlight	A window having small panes of clear, coloured or painted glass connected with strips of lead.
Mouldings	The contours given to projecting members.
Nogging	See Half timber.
Mullion	A vertical post or other upright dividing a window or other opening into two or more lights.
Ogee	A double-curved line made up of a convex and a concave part, resembling an "s" shape.
Oriel	See Bay window.
Palisade	A fence of pales or of iron railings.

Parapet	The extension of a wall above what would otherwise be the eaves line. Common in Victorian architecture, usually between 500 and 1500mm in height and decorated with ornamentation such as moulded cornices, pediments and nameplates.
Pediment	Derived from Greek architecture. An element used in Victorian architecture ornamentally over doors or windows, or surmounting a parapet, usually triangular or curved in shape.
Pier	A solid masonry support, as distinct from a column. The solid mass between doors, windows, and other openings in buildings. A name often given to Romanesque and Gothic pillars varying from a square to a composite section.
Pilaster	A shadow pier or rectangular column projecting only slightly from a wall.
Pillar	A free standing upright member which, unlike a column, does not need to be cylindrical or conform with any of the classical orders.
Plinth	The projecting base of a wall or column pedestal, generally chamfered or moulded at the top.
Pointing	In brickwork, the strong mortar finishing given to the exterior of the joints.
Polychromatic brickwork	Exposed brickwork in at least three colours ranging from creams to terra-cotta to dark brown and combined to form bold patterns, usually on the public facade of the building.
Quoins	A stone or brick used to reinforce or decoratively distinguish an external corner edge of a wall from adjacent masonry.
Render	The plastering of an outer wall.
Ridge	The horizontal line formed by the junction of two sloping surfaces of a roof.
Roughcast	An external rendering, the top coat of which contains gravel, crushed stone or pebbles.
Sash window/ double hung window	A window formed with sashes, ie sliding glazed frames running in vertical grooves; imported from Holland into England in the late C17.
Shingles	Wooden tiles for covering roofs and spires.

Sill	The lower horizontal part of a window frame
Soffit	The underside of any architectural element.
String Course	A continuous projecting horizontal band set in the surface of an exterior wall and usually moulded.
Stucco	Plasterwork.
Tessellated	Tiles of different shapes, sizes and colours laid on a path, verandah or floor to form a mosaic pattern.
Tuckpointed	A method of finishing the joints between face brickwork in which mortar coloured to match the brick is used and onto which a lime putty bead is run to form a crisp white delineation of the bonding of the brickwork.
Verandah	An open gallery or balcony with a roof supported by light (usually metal) supports.
Weatherboard	Overlapping horizontal boards covering a timber framed wall, the boards are wedge shaped in section, the upper edge being thinner.

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GARDEN CITY ESTATE GUIDELINES

(Formerly known as, and referenced in the Port Phillip Planning Scheme as the
Design & Development Guidelines for the Conservation of Garden City)

**THESE ARE ADVISORY GUIDELINES TO ASSIST RESIDENTS IN RENOVATING AND
MAINTAINING THEIR HOUSES**



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**Prepared by the City of Port Phillip in association with Heritage Advisers
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PART ONE:

BACKGROUND TO THE GUIDELINES

1. INTRODUCTION

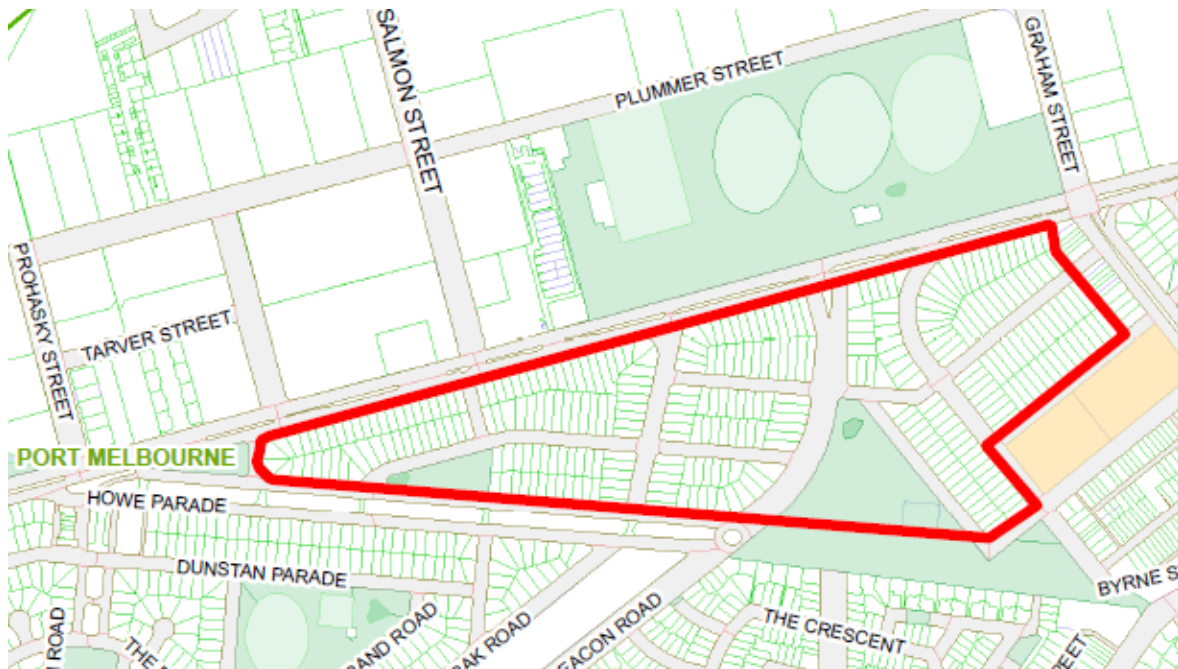
The Bank House Estate at Port Melbourne, commonly known as Garden City, is bounded by the lane west of Graham Street, Williamstown Road, Howe Parade, Poolman Street and Walter Street (see Figure 1). The area contains 322 dwellings, which were constructed for the State Savings Bank of Victoria by WA Henderson of Henderson and Haddow Architects between 1926 and 1948. The dwellings are built in pairs to six standard designs. Garden City includes three recreational reserves, however the small commercial precinct on Graham Street was not constructed along with the development of Garden City (despite it initially being designed as a part of the Estate).

The Bank House Estate was a unique experiment in mass housing, quite unlike anything else in Australia. Initiated by the State government through the State Savings Bank, it was Victoria's first attempt to provide low-cost housing on a single estate. Garden City was influential on later State public housing policies as implemented through the Housing Commission of Victoria and the construction of many other public housing estates around the State.

The estate was designed according to the Garden City town planning philosophy popular in England earlier this century. The housing style within the estate and the general layout of the estate were extremely innovative for their time and were very different from the typical housing developments of the 1920s and 1930s.

Garden City is a significant heritage place. It is essential that the City of Port Phillip and its residents maintain and enhance its unique character. It is also important that the needs of individual property owners are recognised. These guidelines are designed to strike a balance between these objectives. They describe the features that make Garden City Estate significant and offer practical suggestions to help property owners maintain its significance, whilst allowing for renovations and development to occur.

Figure 1: Garden City



Garden City, Port Melbourne

2. THE SIGNIFICANCE OF GARDEN CITY

Garden City was originally recognised as an area of special heritage significance in the *Port Melbourne Conservation Study* (1979) commissioned by the former Port Melbourne City Council and the Australian Heritage Commission. Garden City was also classified by the National Trust of Australia (Victoria) in 1987. In 1995 Port Phillip City Council reaffirmed the significance of the Estate through the Port Melbourne Conservation Study Review (1995) and in 2000 recognised the estate as Significant on the Heritage Policy Map within the Port Phillip Planning Scheme

Both the conservation study review and the National Trust classification report conclude that Garden City is an area of State (and probably national) significance due to its:

- Contribution to the development of public housing policies and practices in Australia;
- Integrity and uniqueness as a residential environment
- Relationship to the Garden City movement in Britain (see Appendices A and B).

The estate is also significant as an example of experimental building technology, residential planning and streetscape design.

Garden City owes its special character to the unusual combination of social, administrative, planning and architectural factors that influenced its development over more than two decades. The consistent application of a single development philosophy over such a long period is unusual in itself. The layout of the sites and the design and orientation of the dwellings express the vision policy-makers had of working-class housing during the inter-war years. They also tell us much about the tastes and lifestyles of the time.

The Statement of Significance for Port Melbourne – The Garden City Housing Estates (HO2), is contained within the Port Phillip Heritage Review.

3. IMPORTANT FEATURES

The most important feature of the Bank House Estate is the uniform character of the streetscape. A streetscape is made up of all those aspects of the built and natural environment, which are visible from the street. It includes both public areas (the road reserve and street trees) and private areas (the visible parts of people's houses and gardens).

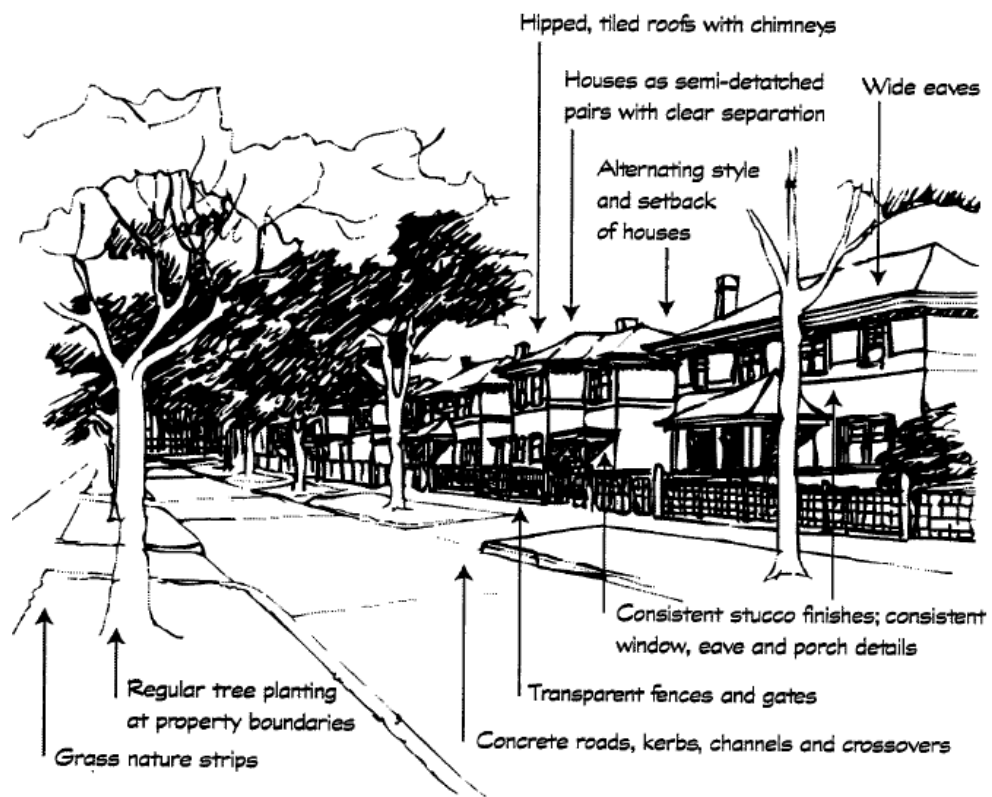
In Garden City, the main streetscape elements are the dwellings, which are all two storey, attached houses on sites with a similar front and side setback. The consistent and prominent use of features like unglazed terracotta roof tiles, stucco walls (both rough-cast and smooth), woven wire fences and multi-paned windows contribute to the area's distinctive streetscape character.

The use of concrete for paving roads, footpaths and garden paths is another unifying aspect of the streetscape, as is the regular planting of only a few selected species of vegetation.

Most residential areas combine many different (and sometimes incompatible) approaches to design. In the case of Garden City, faithful adherence to a single design theme has produced an unusually harmonious and integrated urban environment – this is the key to the area's architectural and aesthetic significance. Figure 2 shows the important key features of the Garden City streetscape.

All these features make Garden City a residential environment which is unique in Australia.

Figure 2: Important features of the Garden City streetscape



4. CONDITION OF THE ESTATE, MAY 1997

These guidelines were initially produced in 1997. At that time the description of the Estate was detailed below. Much of this is still relevant today (2010)

The layout of the Bank House Estate and the architectural form of most buildings has not changed greatly since the 1940s; the area looks much the same from the street as it did two generations ago.

However, numerous small changes have occurred, and these are now starting to erode Garden City's special streetscape character. Front fences have been replaced, original street trees have been removed, and visually obtrusive alterations and additions have been made to houses, including bay windows, window shutters, planter boxes, pergolas, balconies and garages in the front garden. Front porches have been enclosed and stucco surfaces have been painted. Unsympathetic modifications like these are slowly but surely undermining the integrity of the area. Figure 3 provides a pictorial description of the six standard housing designs found within Garden City.

The major concern for Garden City is that piecemeal change will destroy or seriously diminish the special character which makes it such an attractive place and such an important part of Australia's heritage.

Figure 3: The six standard house designs

Type 1



Type 2



Type 3



Type 4



Type 5



Type 6



5. THE NEED FOR GUIDELINES

The population of Garden City remained relatively stable for many years after the estate was completed in 1948. However, during the 1990s this began to change due to an aging population and the attraction of a new demographic to the area. Large-scale residential developments nearby (such as Beacon Cove) have also drawn attention to the area.

With the demographics of the population within the estate changing, the demand to renovate and extend houses has increased. In the absence of detailed guidelines, there is a real danger that these alterations would be inappropriate.

Since December 1998, Garden City has been subject to new planning provisions in the Port Phillip Planning Scheme, including policy, heritage, amenity and design and development provisions. The heritage provisions are as follows:

Clauses 21 and 22 – Local Planning Policy Framework

The Municipal Strategic Statement (Clause 21.05-5) and the Port Phillip heritage policy (Clause 22.04) outline objectives, strategies and policies that apply to all land within a heritage overlay. These aim to conserve identified significant heritage places and manage new development so that it respects the heritage significance of an area.

Clause 43.01 - Heritage Overlay

In a heritage overlay, a planning permit is required to:

- Subdivide or consolidate land.
- Demolish or remove a building.
- Construct a building.
- Externally alter a building.
- Construct or carry out works.
- Externally paint a building.

Note: No planning permit is required for internal alterations to a dwelling in Garden City.

Before deciding on an application in a heritage overlay, the responsible authority must consider various specified matters, including the Port Phillip heritage policy and the Port Phillip heritage review.

Clause 81 - Port Phillip Heritage Review

The heritage review contains a statement of significance for the garden city neighbourhood and its various estates.

Clause 81 - City of Port Phillip Heritage Policy Map

The heritage policy map identifies all of the buildings and open spaces in Garden City as significant heritage places.

The heritage overlay is a standard Victorian Planning Provision. While it offers general protection of heritage places, it does not provide specific guidance on the implementation of its objectives. In particular, highly consistent areas such as Garden City, require definition and interpretation of the heritage overlay objectives in order to respond to their particular significance and built form. Similarly, the Port Phillip heritage policy offers general policies and performance measures for all heritage overlay areas and does not provide specific guidance for highly consistent heritage overlay areas.

These guidelines assist the assessment of planning permit applications in the Garden City area. An applicant is encouraged to use these guidelines when preparing a development application for a property in the area.

A development proposal in Garden City:

- should satisfy the heritage objectives in the Port Phillip planning scheme as well as the
- objectives of the guidelines; and
- should respond to the guidelines where appropriate.

PART TWO: THE GUIDELINES

The following guidelines apply to the area bounded by the lane to the west of Graham Street, Williamstown Road, Howe Parade, Poolman Street and Walter Street, which is included in the Garden City Estate. (see Figure 1).

6. OBJECTIVES OF THE GUIDELINES

- To protect the unique residential and architectural character of Garden City, which is recognised as being of State and national heritage importance.
- To help the residents of Garden City protect the character of the area.
- To recognise the changing needs of people living in the Garden City estate and the potential to make alterations and additions to houses that do not adversely affect the character of the area.
- To increase community awareness about the special qualities and significance of Garden City.
- To provide a clear and consistent basis for the assessment of planning permit applications having regard to the heritage overlay and heritage policy provisions in the Port Phillip Planning Scheme that applies to the area.

7. WORKS WITHIN THE ROAD RESERVE

- Works within the road reservation should be designed to be consistent with the form and materials of the original works and kept within the original design and street layout, but to modern-day standards. This applies to:
 - Roads and road surfaces
 - Kerbs and channels, footpaths and crossovers
 - Nature strips
- Any new works which are not part of the original design (such as kerb extensions) should match the form and materials of comparable existing works within the estate.
- Speed humps should be constructed from asphalt.
- Roundabouts should not alter any original kerb lines.

8. EXTENSIONS TO DWELLINGS

8.1 SINGLE STOREY EXTENSIONS

Single-storey extensions may be permitted at the side of the house if:

- They are more than 600mm back from the front of the house;
- They match the house's original form, materials and character (with a cement render or similar textured finish, and a flat or pitched roof with terracotta tiles).
- Single-storey extensions at the side of houses with two street frontages (corner blocks) may be permitted provided the extended boundary wall is rendered consistent with the finish of the original house.
- Single-storey extensions will generally not be permitted in other areas, including the front of the house.

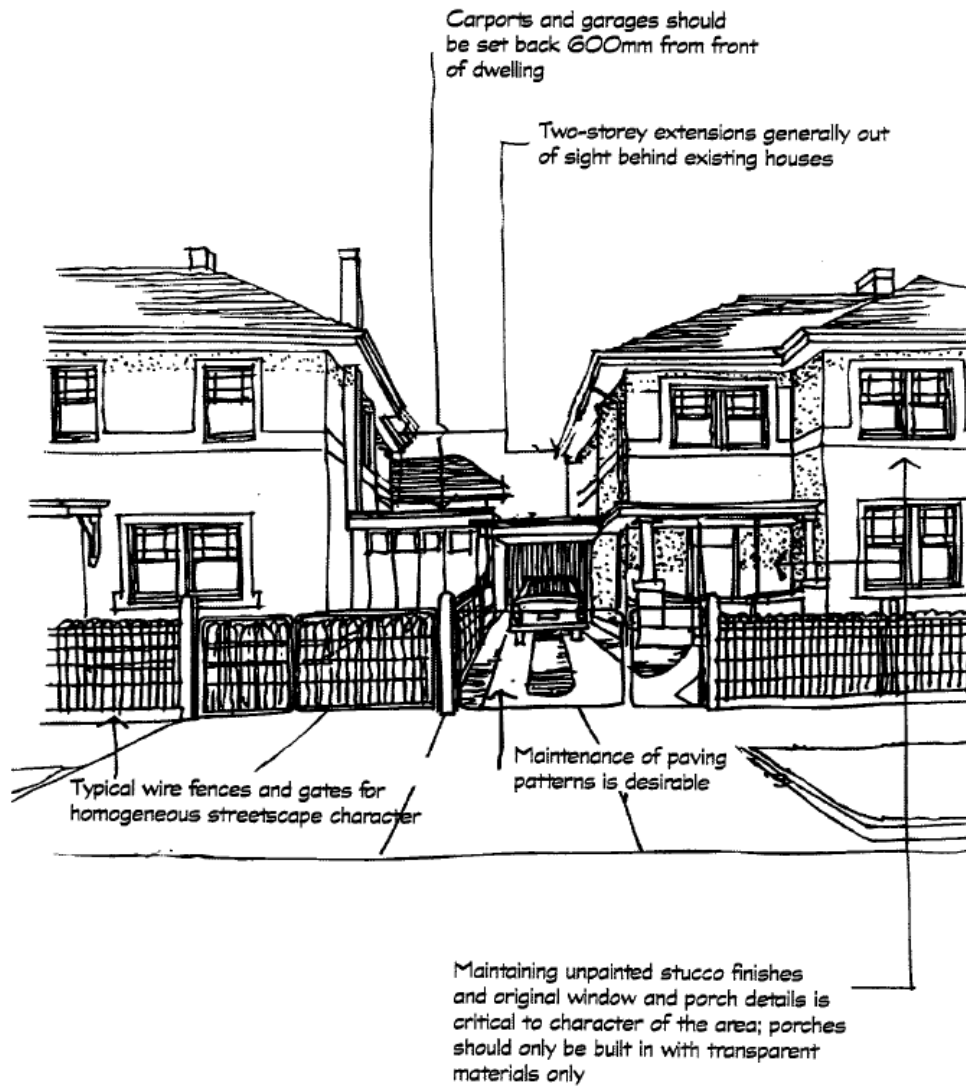
8.2 TWO-STOREY EXTENSIONS

Two-storey extensions may be permitted at the back of the house if:

- The roof of the extension is no higher than the roof of the original dwelling
- The walls of the extension are no higher than the walls of the original dwelling
- The extension matches the house's original form, materials and character (with a cement render or similar textured finish, and a flat or pitched roof with terracotta tiles).
- Two-storey extensions will generally not be permitted in other areas, including the front of the house and the side of the house.

Figure 4 shows a range of acceptable options for alterations and additions to a building in Garden City.

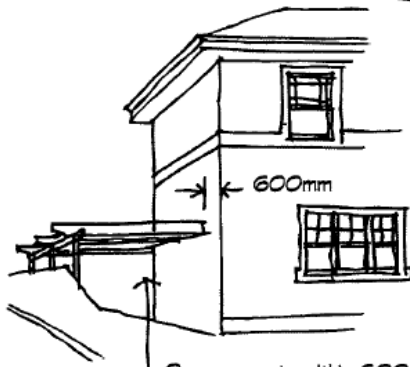
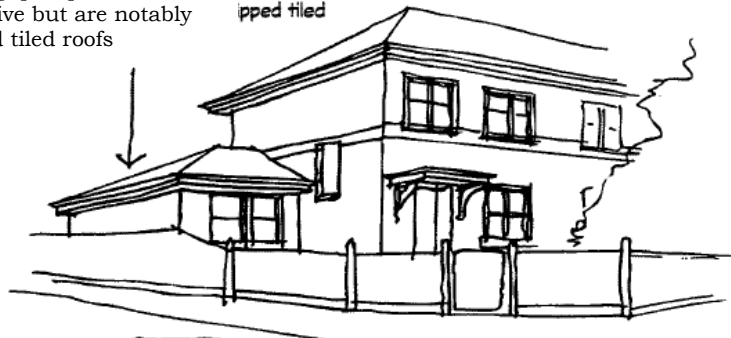
Figure 4: Acceptable alterations and additions





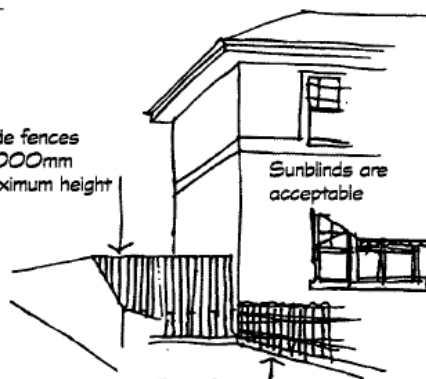
Single storey extensions should be set back 600mm from front of dwelling: parapet walls at corners are not highly intrusive but are notably less sympathetic than hipped tiled roofs

intrusive
pped tiled



Open carports within 600mm of the front of the house are not intrusive

Side fences
2,000mm
maximum height



Front fences
1,500mm
maximum height

Sunblinds are
acceptable

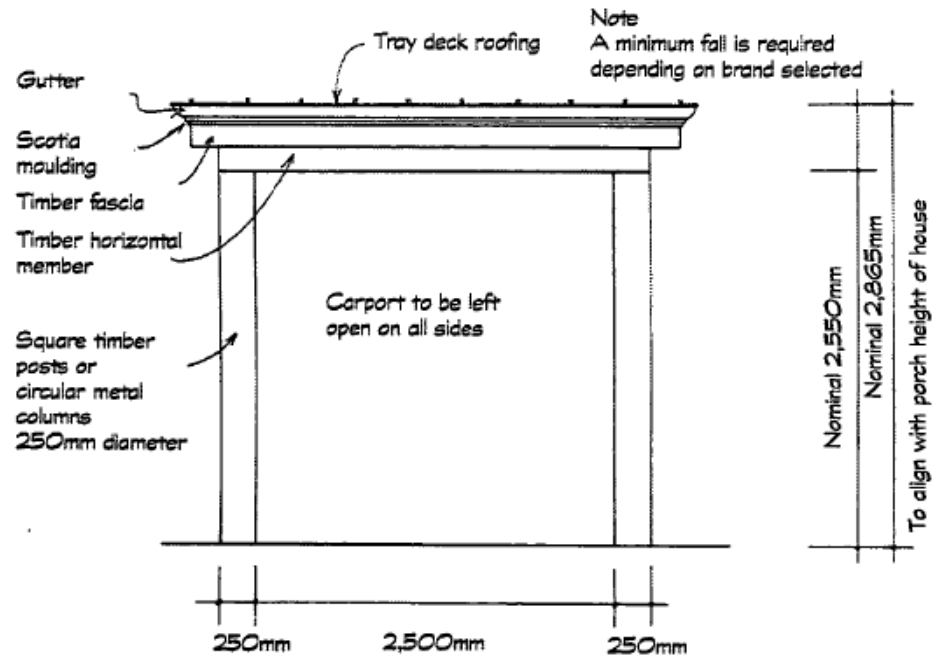
9. GARAGES AND CARPORTS

- New garages and carports should be built at the side of the house and be consistent with the house's original form, materials and character (with a cement render or similar textured finish, and a flat or hipped roof with terracotta tiles).
- The front wall of a new garage or carport should not extend beyond the front wall of the house. Where a new garage or carport has eaves, the eaves should not extend beyond the front wall of the house; and
- New garages and carports should be positioned to retain the original staggered line of houses along the street, but in no circumstance should they extend beyond the front of the porch.

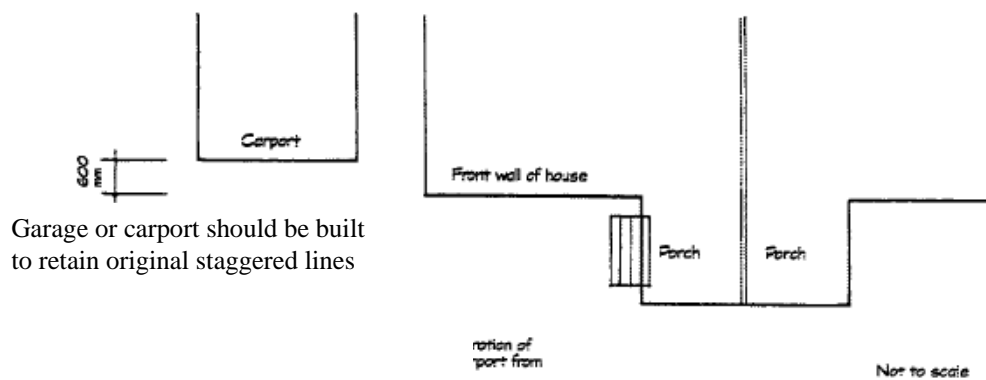
Acceptable garage and carport types are shown at Figure 5.

Figure 5: Acceptable garages and carports

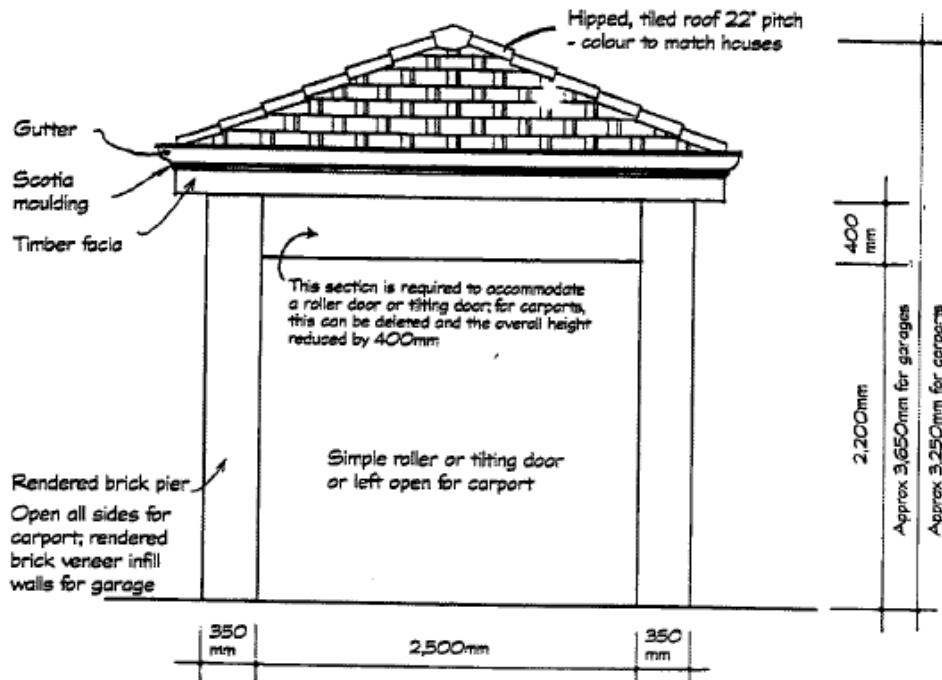
Carport with flat roof



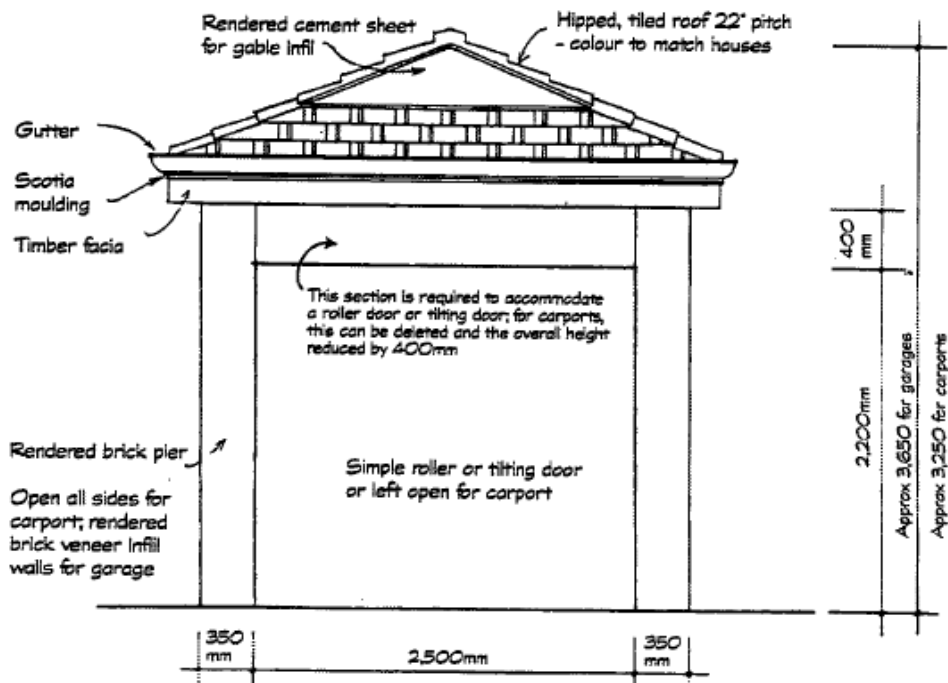
Site plan for garage or carport



Garage or carport with hipped roof



Garage or carport with hipped and gabled roof



10. FENCES

Front fences and side fences from the front boundary to where the house starts should be:

- no more than 1000 mm high
- 75% transparent
- made from appropriate materials
- Some examples of acceptable fence types are shown in Figure 6.
- The requirement for 75 percent transparency does not apply to picket fences; however, picket fences may not exceed 900mm in height.
- Residents seeking more visual privacy are encouraged to use a combination of acceptable fencing and screen plantings such as hedges or bushes.

Back fences and side fences from where the house starts to the back boundary should be:

- No more than 2 metres high
- Made from timber palings or other suitable materials as agreed between the neighbours.

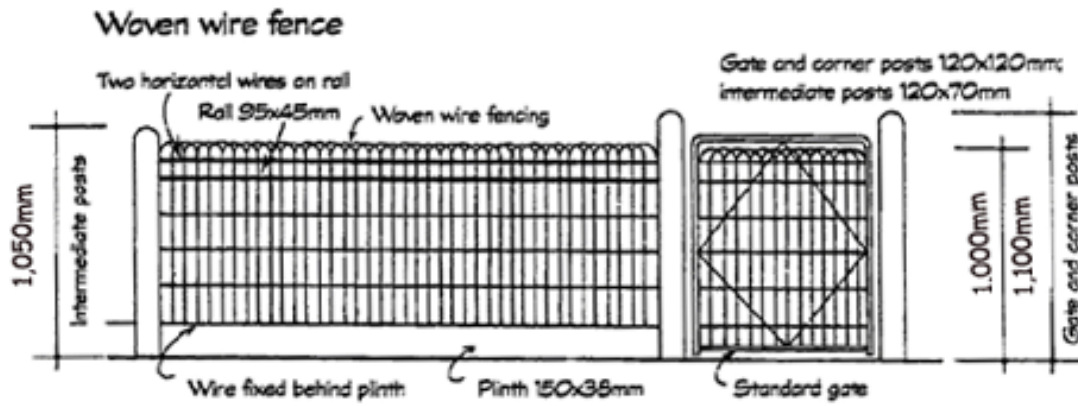
This applies to all properties, including corner blocks.

- Council may use its discretion when considering front fence height and fence materials on any designated secondary or main road. This discretion shall only be applied in order to provide adequate acoustic protection to residential properties abutting any designated secondary or main road, to the satisfaction of Council.

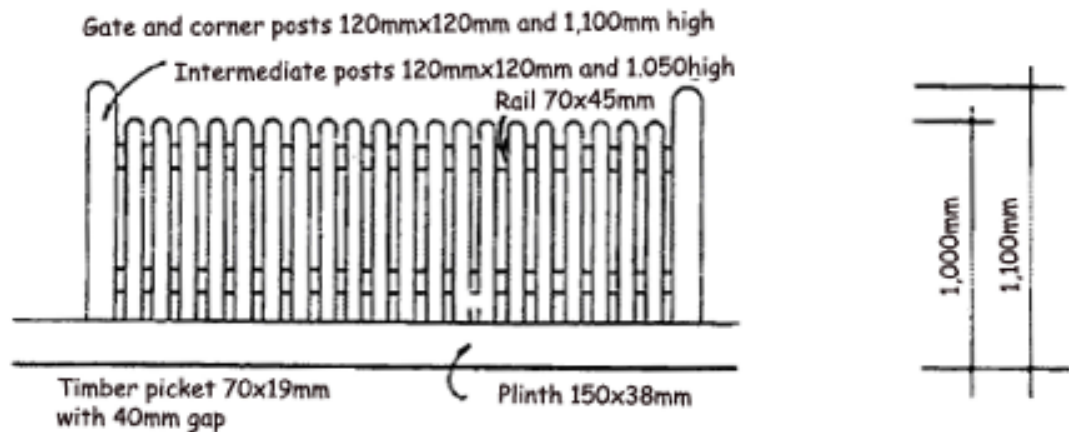
Acceptable fence designs are shown at Figure 6.

Figure 6: Acceptable Front fence designs
Front fence:

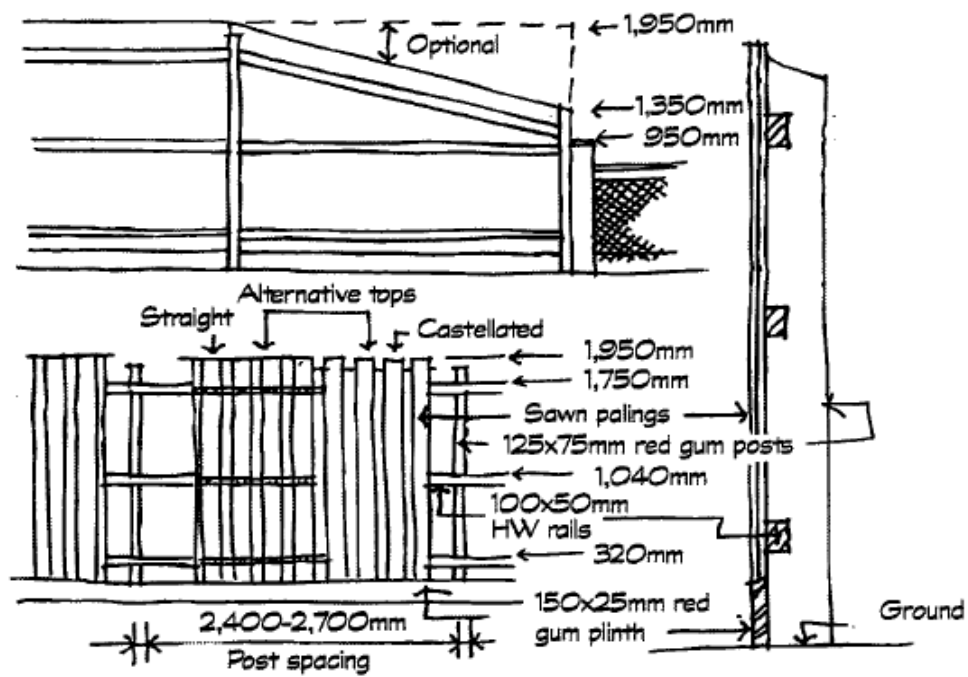
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Timber fence 1,000mm high



Side and rear fences: (all areas, unless other suitable materials are agreed by neighbours):



11. ROOFS

- The original roof form and materials should be retained and, if necessary, restored all the way along the front and side of the house.

Chimneys

- Original chimneys should be retained.

Downpipes

- Downpipes at the front of the house should be kept or returned to their original location where practical.
- Consideration will be given to removal or concealment of downpipes.

12. EXTERNAL WALLS AND RENDERED SURFACES

- External walls which are visible from the street should be kept in their original form, with the original materials and finishes.
- Rendered surfaces should be restored by cleaning with an appropriate solution or by applying a cement-and-sand wash of the same colour and texture. The work should be undertaken by a qualified tradesperson.
- Repairs to rendered surfaces should match the colour, texture and composition of the original render.
- Rendered surfaces which have not been painted can be left unpainted or painted in colour matching the original render or cement.
- Rendered surface which have been painted should be restored where possible. Paint should be removed by an approved method (not sandblasting) and the surface should be treated with a cement-and-sand wash in the original colour (at least three different render colours were used on the estate).

13. WINDOWS AND DOORS

- Any of the original window designs used on the estate may be applied to any house. If a window must be replaced, the new one should match the form and materials of the original.
- New or enlarged window and door openings will not be permitted at the front of the house.
- New window openings may be permitted at the side of the house. They should be set as far back from the front of the house as possible and should match the existing windows in form and materials.
- Double glazing of windows is encouraged and does not require approval from the Council.
- Original windows and doors should be retained and, where possible, the reinstatement of windows and doors in the original form is encouraged.

Repairing cracked lintels

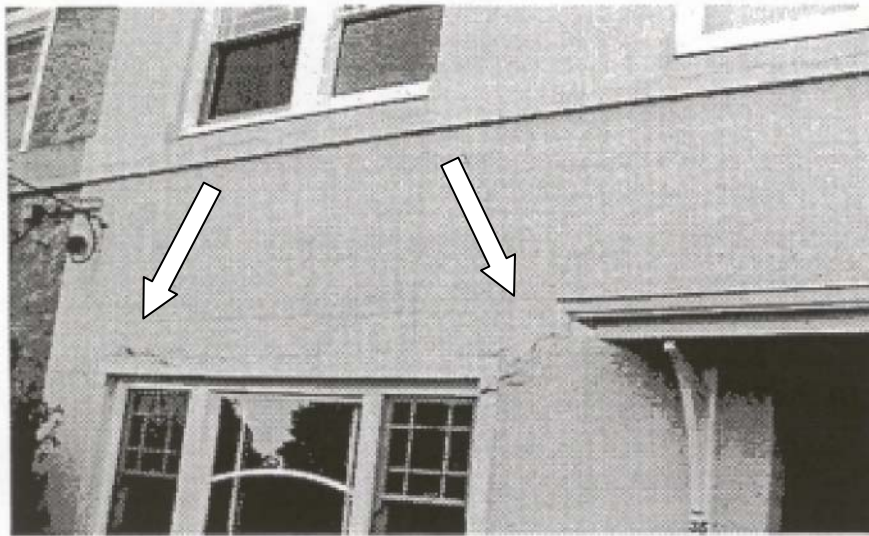
The lintels above many windows in Garden City are badly cracked (see Figure 7). The cracking is usually caused by moisture seeping through the external skin of the building and corroding the steel reinforcing rods inside the lintel. It is worst on windows that face the weather (south and west). Windows on north and east facing walls and upper-storey windows protected by eaves are less likely to be affected.

The best way to tackle severe cracking is to replace the lintel with a new one manufactured using dense concrete (at least 50Mpa). There should be at least 40mm of cover over the new lintel's steel reinforcing rods.

Less seriously damaged lintels can be repaired using one of several patching systems. These involve removing the external concrete to expose the corroded reinforcing rods, treating the rods, concreting over them again, and restoring the external finish. This may seem like a cheaper option, but it is important to remember that patches typically last only about five years – replacing the lintel may be more economical in the long term.

For more information about replacing lintels, it is recommended that you talk to a builder experienced in this field.

Figure 7: A cracked lintel



Air Conditioners

- Air conditioners should not be visible from the street.

Porches and entries

- Porches and entries may be enclosed with transparent screens that are at least 75 percent transparent or with plain glazing with the minimum of framing and glazing bars.
- New porches should not be located at the front of a house, or where it is visible from the street.

Sunblinds and awnings

- Sunblinds and awnings (temporary and permanent) should complement the character of the house.

Shutters

- Security screens may be installed:
 - At the back of the house
 - At the side of the house (as long as they are more than halfway back from the front of the house)
 - Inside the windows at the front of the house
- External screens and timber shutters over front windows are discouraged.

Front doors

- Doors and screen doors at the front of the house should be painted in a suitable colour.

14. LANDSCAPING

- Residents are encouraged to retain early, established plantings in good condition (including trees, hedges and specimen plants) and to plant new, historically appropriate plant species.
- Residents are encouraged to select drought tolerant trees and plantings, and to utilise water sensitive urban design measures where possible.
- The space in front of a dwelling should be retained as lawn or garden and not used as a space to park vehicles.
- The original uniformity of street trees should be retained.

15. DECORATION AND PAINTING

- Window and door frames, fences and gutters should be painted in the original colours, or a colour scheme typical of the area or the period. The original colours can usually be determined by scraping back to the wood one layer at a time. Figure 9 shows a range of possible colour schemes, although other schemes will be considered.
- Neighbours are encouraged to agree on one colour scheme for both houses in each pair.

An appropriate colour scheme is shown at Figure 8

Figure 8: External colour schemes

An article in *The Herald* of 6 July 1927 loosely describes the colours used as:

- green and buff
- deep orange and buff
- yellow and buff.

No physical investigation of the colours used has been made to date, but the following options would be appropriate; many other combinations may also be appropriate.

The colours follow a common scheme, which can be clearly seen in the early photographs and which is typical of the period.

Colour options

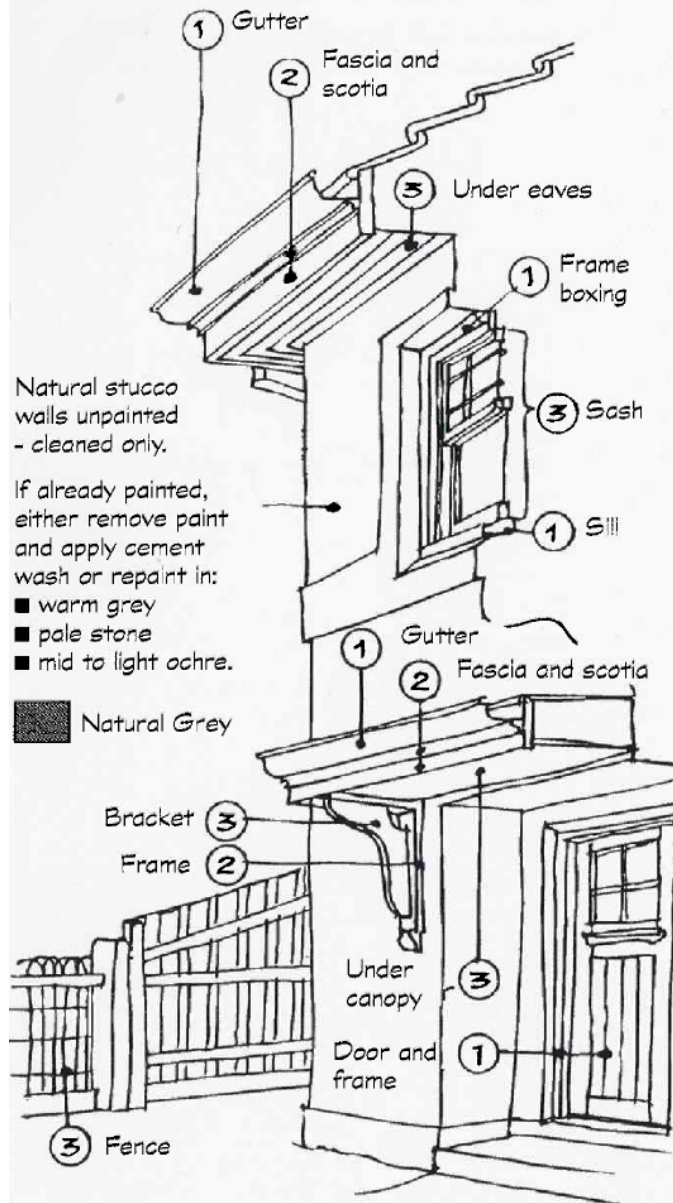
- ① Is a dark trim
- ② Is a mid-range colour
- ③ Is a light trim

- 1 Mid Brunswick Green
- 2 Biscuit
- 3 Off White

- 1 Leaf Brown
- 2 Biscuit
- 3 Off White

- 1 Deep Brunswick Green
- 2 Deep Buff
- 3 Cream

- 1 Deep Indian Red
- 2 Deep Buff
- 3 Manilla



Typical colour system

16. DEMOLITION

- Demolition of an entire dwelling will not be permitted.
- Demolition of the rear section of some dwellings may be permitted where it will not affect the heritage significance of the building, subject to any new addition being sympathetic to the scale and form of the original building and respecting the character of the area.
- Demolition of outbuildings may be permitted.
- Any decision regarding demolition will be assessed against the provisions of the Port Phillip Planning Scheme, particularly Clause 22.04 Heritage Policy

17. MULTI-UNIT DEVELOPMENT

- Multi-unit development (more than one residential dwelling per lot) will not be permitted as this is contrary to the significance of the estate.
- Subdivision of original lots will not be permitted.
- Single-storey granny flats located at the rear of the property may be permitted.
- Any proposed granny flat must be in sympathy and consistent with these guidelines.

Appendix A

Extract From

Port Melbourne Conservation Study Review

Statement of Significance

4.2.5 Garden City Precinct (UC1)

History

Until 1926, most of Fisherman's Bend, including the Garden City area, was open sandy wasteland. In the nineteenth century, small areas of the Bend have been used sporadically for a range of activities, for example, by the 1860's, a manure depot and slaughter yards had been located there, and the foreshore area was dotted with small fishermen's huts.¹³ A great deal of sand mining was also undertaken at the Bend. By the early twentieth century, however, the potential use and development of such a large tract of land so close to Melbourne had become the subject of widespread discussion. In 1925, the report of the Melbourne Town Planning Commission proposed that the area be redeveloped following the principles of the Garden City movement in England. The proposed redevelopment was to incorporate 340 acres for residential development, 420 acres for industry and 80 acres of open space and playing fields.¹⁴

Though this specific scheme did not eventuate, the findings of the Commission paved the way for the partial redevelopment of land at the Bend for housing undertaken by the State Savings Bank of Victoria in 1926 – 7. The bank had long been involved in the provision of housing to ordinary Victorians. As early as 1894, it had established a system through which long term loans could be provided at low interest on the security of freehold land. In the period following World War 1, after the passing of the Housing and Reclamation Act and in the wider context of a widespread housing shortage in Victoria, the Bank's housing activities were concentrated in the provision of assistance to returned servicemen and low income earners.¹⁵ The Bank established a housing department under chief architect G Burridge Leith. A variety of standard house designs were developed, simply as a means of providing housing more economically, and these were adapted and revised over time. The houses themselves were built by private contractors. Unlike the later Housing Commission of Victoria houses, which remained in public ownership and were rented to their occupiers, the State Bank houses were purchased outright. The system was intended to encourage home ownership, an important element in the conservative political culture of the 1920's.¹⁶

The Bank purchased 10 acres of reclaimed land at Fishermen's Bend in 1926, another 20 in 1927 and a further 14 in 1928. With its radial network of intersection roads, the layout of the subsequent development accorded to a degree with that proposed by the Town Planning Commission in 1925, which in turn appears to have been inspired by the ideas of the originator of the "Garden City" movement, Ebenezer Howard.¹⁷ Indeed, generally speaking, the form of the Garden City estate owed much to similar developments in Britain. In 1925, the General Manager of the Bank, G.E Emery, had visited England to examine the low income housing schemes there. The subdivision layout of the Garden City estate also incorporated a range of ideas from the broader new town planning movement, including a curvilinear hierarchy of concrete roads, reservations for shops and landscaped open space, and the replacement of the usual rear service lanes by grassed nature strips in front of the houses. The first houses were constructed in 1927 in Walter and Poolman Streets and in Tucker and Crichton Avenues. Further houses were also constructed in Beacon and Williamstown Roads. One hundred and fifty four houses were complete by 1929.¹⁸

Statement of Significance

Both the road layout and most of the houses at Garden City were designed by W A Henderson of Henderson and Haddon, architects, under contract to the Bank.¹⁹ Although the Bank's Chief Architect G Burridge Leith, was later to claim that the planning of Garden City was not influenced by the English or European Garden City movement, but rather emerged from functional and economic necessity²⁰, the houses are very similar to post World War 1 council housing in Britain, and have little in common with typical residential architecture in Australia at the time. Their design appears to have been influenced directly by the British Local Government Board's Manual on the *Preparation of State-Aided Housing Schemes*, published in 1919, as well as by observation of Council housing estates in England and Scotland by Emery, in 1925 – 1926.²¹ Emery appears to have been particularly impressed with the semi-detached houses at the Kelvin Dale estate, Glasgow, and argued in favour of two-storey houses of this form to economise on land-use, to maximise open space and to minimise the expense of constructing foundations. In addition, two-storey houses would have a "fine imposing appearance" and it was argued that upper storey bedrooms were more healthy.²²

Possibly because the house designs were so radically different from the Australian norm of individual single storey houses, considerable opposition to the houses was expressed in 1926 by some groups, notably Trades Hall Council and Port Melbourne City Council, on the grounds that they were "dog boxes" unfit for human habitation. Ironically, in Britain the early post War council housing standards, on which the Garden City houses appear to have been modelled, were by the mid-1920's considered to be over-generous and too costly and subsequent council housing was constructed to significantly smaller spatial standards.²³

The State Savings Bank Garden City housing project at Fisherman's Bend marked not only the first large-scale intensive development at the Bend, it also marked a significant shift in government housing policies, and a partial undertaking on the part of the State to provide low-cost housing. The particular manner in which the scheme was set up, however, meant that houses in the Garden City estate were never particularly affordable. David Harris has remarked, "because of the high cost of land and the absence of any government subsidy to cover the costs, houses at Garden City were often more expensive than other available houses."²⁴ None of the houses were intended for the low income rental market, and the deposit and fee requirements of the deal effectively excluded many working class people. Robert Freestone has noted that the development came too late to substantially influence the operations of private builders and local government authorities in Melbourne, and that it was 'too small and perhaps too expensive to satisfy the demand for good housing near the waterfront'. Freestone also comments, however, that the development provided a model for local supporters of the Garden City movement.²⁵

Statement of Significance

Description

Garden City, a broadly triangular area bounded by Graham Street, Williamstown Rd, Howe Parade, Poolman Street and Walter Street, contains 322 dwellings constructed in pairs by the State Savings Bank of Victoria. The row of shops facing Graham Street, although included in the original State Savings Bank plan, was developed privately and was not included in the then Urban Conservation area. The area includes three recreational reserves, including the small reserve at the corner of Williamstown Road and Howe Parade. The street layout, based on the 1925 Metropolitan Town Planning Commission's masterplan for the Fisherman's Bend area, adopted many features current in 'garden suburb' planning in Britain in the 1920's, including curvilinear roads of different widths according to traffic usage, generous provision of wide 'nature strips' planted with trees, and reservation of specific areas for shops and recreation.²⁶

All of the dwellings were constructed as semi-detached two-storey pairs in similar style using roughcast rendered walls (originally unpainted), hipped roofs with wide overhanging eaves and sash windows with multi-paned upper sashes. Because of difficult ground conditions on the reclaimed land at Fisherman's Bend, the houses were built on deep piled concrete foundations. The external walls were constructed from "Cindcrete" concrete blocks, chosen for their lightness, to minimise foundation costs. Roofs were covered with terracotta Marseilles pattern tiles.

Six basic house types were designed initially and continued to be used, with minor variations, throughout the twenty-two year construction period from 1926 – 1948.

All of the houses are rectangular in form without projecting wings except to the rear and to the front of Type 4 houses. Variations between the different types including locating front doors to the centre or the outer corners of the facades, provision of ground floor bay windows in Type 1 houses, various forms of entrance canopies or porches, with generous flat roofed porches to Type 4 houses and hipped roofed front porches to Type 6 houses. Roof forms are varied subtly, with gambrelled hips to Type 1 and small gablets to Type 5. While visual continuity throughout the estate was ensured by use of this limited range of design types, monotony was avoided by random distribution of the different types, varying set backs of the houses from the street and use of different coloured renders.

While most of the houses remain largely intact to the extent of the basic external form of their front elevations, the integrity and visual coherence of the estate has been significantly eroded by numerous, often small, alterations. Many houses have been extended to the rear, usually with relatively little impact as viewed from the street. Probably the most common alterations have been to front fences and windows. Many of the original Cyclone woven wire fences have been replaced by a variety of masonry walls and picket and other types of fencing. In a few houses the original timber-framed sash windows have been replaced using other materials including aluminium. In many more houses, while timber sashes remain, the original distinctive margin glazing bars have been removed. The roughcast render on many houses has been painted, and most joinery has been painted white instead of the original dark green and other colours.

Statement of Significance

Among other changes that have occurred is the replacement of the original rainwater goods, with distinctive curved offsets to the down pipes, with standard modern gutters and down pipes with angled offsets.

Among the more intact houses are 42-4 Edwards Ave (Type 1), 396 Williamstown Rd (Type 2) (the windows of the adjoining house at 394 have been replaced), 17-9 Edwards Ave (Type 3), 13-5 Crichton Ave (Type 4), 2-4 Page Ave (Type 5) and 49-51 and 62-4 Edwards Ave (Type 6). 62-4 Edwards Ave in particular appears to retain the original joinery paint colours and front fence. Interiors have not been inspected as part of the conservation study, and the extent to which intact interiors survive is not known.

Garden City has a distinctive landscape quality deriving from the relatively dense planting of street trees along the nature strips and the landscaping of the reserves and private gardens. The landscape has been analysed in detail and conservation policies formulated by John Hawker.²⁷ Street planting comprises a number of native and exotic species, including predominantly *fraxinus* (Ash) and specimens of *Lophostemon* (Brush (Queensland) Box), *Malaleuca* (Paperbark), *Populus x camadesis* "A urea" (Golden Poplar), *Platanus x acerifolia* (London Plane) and various *Eucalyptus* species. Hawker notes in particular an outstanding avenue of *Acmena Smithii* (Lillipilli) in Crichton Ave between Page St and Beacon Rd, and comments that much of the recent replacement of original trees is with inappropriate species which detract from the uniformity of the original planting. The reserves contain a variety of specimen trees, including recent planting of native species. The private gardens are diverse in character, typically with lawns and flower beds to the front and separated from the street by shrubbery or hedges.

Statement of Significance

Garden City is of state significance. Construction of the estate from 1926 was the first experiment in Victoria in mass provision of housing on a single estate by the State Savings Bank of Victoria, acting as a public agency on behalf of the Victorian Government. It marked the beginning of a progressively increasing direct involvement by the Victorian Government in provision of housing, culminating in the formation of the Housing Commission in 1938 and the construction of large housing estates at Fisherman's Bend and other areas in the 1940's and the following decades. Garden City contrasted with the State Savings Bank's more general role in the 1920's and '30 in provision of housing through cheap loans for constructions of individual houses on suburban sites to the Bank's own standard designs.

The layout of the estate was one of a number of manifestations of interest in Victoria in garden city planning principles in the 1920's, seen also in some speculative suburban estates involving cul-de-sacs and crescents constructed at the time, but was largest in scale and the most publicly prominent of such developments in Victoria. The uniform design of the semi-detached two-storey houses, based closely on post World War I government-funded council housing in Great Britain, contrasted strongly in style and planning with the Australian norm of individual single storey detached houses, reinforcing the unique character of the estate. Conversely, the policy adopted from the outset of sale, rather than rental of the houses, and the absence of subsidies contrasted with the British policy of subsidised council houses for rental.

Appendix B

Citation of Significance from National Trust of Australia

Garden City, Port Melbourne

The significance of the Garden City area, Port Melbourne, is that it represents a unique experiment in the provision of low cost housing utilising the physical aspects (rather than social aspects) of the British Garden City movement: it occurred on the initiative of a statutory authority; it was based on the development of a total housing estate, including dwellings; it was a subdivision based on the English Garden City suburb concept; its two-storeyed housing was based on current (in the 1920's) English "working mans" cottages and it involved experimentation with building technology. Garden City was influential on later State public housing policies as implemented through the Housing Commission of Victoria.

Description

The Garden City Fisherman's Bend Area, is all that area bounded by Graham St, Williamstown Rd, Howe Pde, Poolman St and Walter St, Port Melbourne as shown on the accompanying map.

The area includes the 322 dwellings, constructed in pairs, which were erected by the State Savings Bank of Victoria between the years 1926 and 1948. It also includes the three recreation reserves which have a total area of approximately four acres, and the commercial zone on Graham St which, although developed privately, was designated as shop sites in the original scheme.

There are six designs of housing on the estate. All housing is built in two storey semi-detached pairs. Dwellings have terracotta-tiled roofs. The external walls are of cindcrete unit blocks, which are cement rendered. Because the land was reclaimed, foundations consist of reinforced concrete piers resting on concrete rafts at a depth of five feet and spanned by reinforced concrete beams at ground level. House allotments have a 33 feet frontage, and houses are set back 35 feet from the frontage, with each alternate pair of houses set back 30 feet. Each house is separated by a minimum of 18 feet from the neighbouring dwelling on one side. The variation in design, setback, and colour of cement render allowed the house pairs to be specially grouped to avoid monotony.

Some houses retain their original cement render, and there are also a number of original wood and woven wire fences remaining on the estate.

Historical Evolution

Many schemes have been proposed for Fisherman's Bend over the years, including a shipping channel (1851), a plantation (1879), and a storm water drain (1883).¹ By the 1920's it lay undeveloped however – an extensive sand quarry excavated to a depth of three to six feet.

As Melbourne's suburban development gradually resumed in the new century, the incongruously fallow expanse at the Bend attracted increasing attention. Being in Crown ownership and close to the city, the land seemed to provide an opportunity to relieve the great problem of slum housing. In 1912 the Port Melbourne Council asked the Government to make provision for the reclamation, subdivision and development of the land.² Also in that year it suggested that Municipalities be empowered to provide workingmen's housing.³ At the 1913 Royal Commission into metropolitan housing various witnesses expressed the increasingly popular idea that any housing development at the Bend should be preceded by 'proper planning to include street layout, parks and playing fields'⁴ and one prominent public figure envisaged a "small garden suburb" there.⁵ Although another government enquiry authorised the commencement of reclamation in 1913, hopes were thwarted when in that same year the Harbour Trust claimed that the land was required for its own purposes and had it withdrawn from sale.⁶

Interest in urban issues, particularly housing and town planning, revived after the War. By the mid 1920's the idea was emerging that some large-scale development was necessary to redress the slum housing problem. Newspapers considered the merits of grand projects being undertaken elsewhere, in particular Letchworth and Welwyn Garden Cities, Britain's massive municipal housing schemes, and the "thousand homes" project at Adelaide's Colonel Light Gardens.⁷ The unions efforts to provide workingmen's housing by a co-operative guild had failed⁸ and even the Port Melbourne Council found itself unequal to the task of developing the small parcels of land which had been reclaimed at the Bend by 1923.⁹

In 1925 several events occurred which spurred the development of "Garden City". Unemployment was rife, which favoured the persistent efforts of advocates for the scheme.¹⁰ The Metropolitan Town Planning Commission released a report which proposed an industrial garden suburb at Fishermen's Bend, with 340 acres for residential development, 420 acres for industry, and 80 acres of open space and playing fields.¹¹ Mr Emery, General Manager of the State Savings Bank of Victoria conducted a tour of Britain intended to ascertain more economical methods of providing low-income housing, and returned an advocate of "mass construction".¹² Since 1920, the Bank had been designing houses, and occasionally assembling land packages, for people of small means¹³ and the task of developing Melbourne's "dream city of Fisherman's Bend"¹⁴ consequently fell to it.

The Bank purchased 10 acres of reclaimed land in 1926, 20 acres in 1927, and a further 14 acres in 1928.¹⁵ The streets on this land were laid out as far as possible to accord with the Town Planning Commission's comprehensive plan for the Bend. This layout, a radical web of residential road circumscribed firstly by open space and then Ebenezer Howard's influential Garden Cities of Tomorrow (1900). All semblance of this coherent design was foregone however when in 1929 the Lands Department continued to sell sand from a further 119 acres which the Bank had been negotiating to buy, rendering this extension economically prohibitive.¹⁶

Nevertheless the estate did incorporate many progressive features of the town planning movement, such as curvilinear hierarchical roads constructed in concrete, a reservation for shops, no rear service lanes, nature strips, and the contemporary fashion for "garden suburbs"¹⁷ these features were sufficient for the estate to assume the popular title "Garden City".¹⁸

The houses built were semi-detached two storey pairs. They were designed under the influence of the Manual on the Preparation of State-Aided Housing Schemes (1919) distributed by the British Local Government Board.^{18B} Houses being built at Kelvin Dale, near Glasgow, which Mr Emery considered to be of a better appearance than the multi-unit terraces¹⁹ observed in other parts of Britain, inspired the adoption of this standard. The newspapers agreed with his assessment that tenements were inappropriate for Australia, and that houses here should be owned rather than leased.²⁰ Double storey was necessary to economise on land (the building frontages were reduced to 33 feet), to maximise open space, and to minimise the expense of constructing foundations on the silt.²¹ Two storey dwellings would have a “fine imposing appearance” argued Mr Emery, adding that “the health of the occupants will be improved by having bedrooms on the upper floors”²² At that time the Bank was experimenting with concrete as a means of providing cheap and durable housing and it was decided “cindcrete” (cinder concrete) walls would be lighter and better suited to the elaborate foundations which were required at the Bend.²³

Despite its distinctiveness, the two storey semi-detached housing represented a compromise of the single storey detached cottage, which was the Australian ideal.²⁴ The Bank recognised this from the beginning²⁵ and attempted to avoid the appearance of dull similarity by providing six variations of designs for the pairs, by alternating setbacks 30 and 35 feet from the street, and by providing variation in the colour of the cement render.²⁶ Nevertheless there arose a great opposition, primarily from the Trades Hall Council and Port Melbourne Council, to the erection of the houses on the grounds that they were “dog boxes” unfit for human habitation.²⁷ Agitation focused upon the ceiling height being eight, rather than ten feet high. A compromise of nine feet was eventually reached²⁸ and after some diplomatic leadership from local MLA and long-time advocate of the scheme, Mr Murphy, the development was able to proceed, and was officially opened on 9th March 1927.²⁹

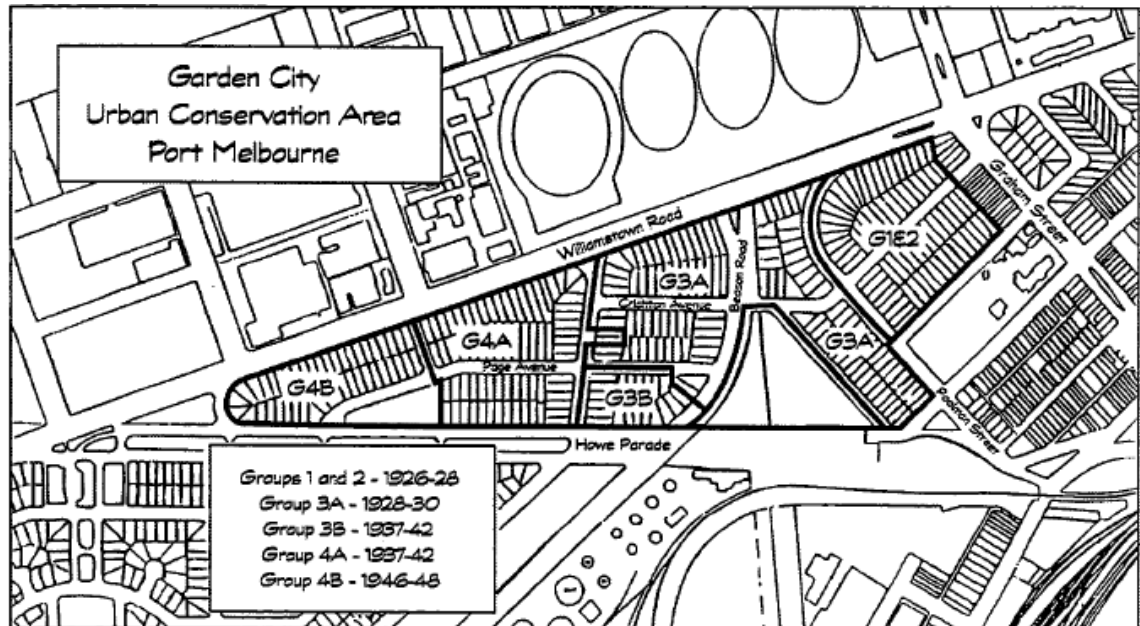
To counter the lingering prejudice the Bank planted trees, encouraged owners to keep neat gardens, planned community facilities, and sponsored the formation of the “Garden City Progress Association”³⁰

The houses were constructed in groups.³¹ The first two groups, of 72 dwellings (36 pairs), were built 1926-8 between Walter St, Crichton Avenue, Williamstown Rd and Poolman St, with allotments along Graham St left as shop sites. A further 112 dwellings were erected in the vicinity of Beacon Rd before the depression halted construction in 1930. Between recommencement of building in May 1937 and cessation in June 1942 due to wartime restrictions, a further 94 dwellings were built. The final 44 dwellings were constructed in 1946-8, bringing the total number of houses built by the Bank at Garden City to 322 (161 pairs). In 1937 the Bank proposed six new designs (Types 9-14) which incorporated contemporary façade elements, but these were never built. Some minor alterations were incorporated into the standard designs, including rearrangement of stoves, back porches and upstairs bedrooms, and the deletion of upstairs fireplaces (eg Type 7 & 8). However, retention of the six basic façade designs over the twenty-year construction span preserved the visual continuity of the estate.

“Garden City” represented a distinctive stage in the development of welfare housing in Victoria. Previously it was considered that control of subdivision and building and the improvement of transport to the outer suburbs would improve housing conditions³² but in the 1920’s the role of Government became more positive. The Victorian Government eschewed the grand schemes being undertaken elsewhere and opted for minimal intervention through the agency of the State Bank. Housing would be made more accessible to low income people by making finance more liberally available (according to the provisions of the 1920 Housing and Reclamation Act)³³ and by reducing the cost of housing. Thus the Bank produced its own house designs, experimented with cheap building materials (especially concrete),³⁴ attempted to assemble land at low prices,³⁵ and finally, as in the Fisherman’s Bend case, constructed group housing en masse in the quest for economies of scale.

The high price of land (£5 per foot), the lack of any subsidy, and the requirement that housing be purchased rather than rented, meant that the cost of housing at “Garden City” was comparable to, and often more expensive than, other available housing.³⁶ Many people could not raise the deposit or the weekly repayments. It was not until the Depression of the 1930’s that “welfare” housing was introduced in the form of the Housing Commission of Victoria.

Stages in the Development of Garden City



Type 1



Type 2



Type 3



Type 4



Type 5



Type 6



Character & Significance

The estate of two storey semi-detached houses represents a unique reproduction of British Council Housing. It was linked more closely to the European State housing schemes of the early 20th century than to the norm of the detached cottage which prevailed in the Australian urban expansion of the 1920's.

The imposing and uniform nature of the architecture, the regular siting of houses in relation to one another, the narrow concrete roads, wide nature strips and spacious parks all contribute to the unique feel of this distinctive precinct.

Many of the latest town planning and "garden suburb" features were incorporated in this scheme. These include the curvilinear roads of different width according to traffic usage, the reservation of a specific precinct for shops, the deletion of rear service lanes, provision of nature strips with tree plantation, and generous public reserves. Physical services were inclusive in the development, and provision was made for a community hall and recreational facilities.

For social activists, Garden City marked the beginning of the fulfilment of long held hopes for Fisherman's Bend. By virtue of its "garden suburb" features, its status as Victoria's first major state housing scheme, and of the prestige associated with the misnomer "Garden City", the estate became a showplace of the Welfare State's latest projects. In 1936, the Public Works Department built 44 houses to the immediate northeast of the estate along Southward, Griffin Crescent, Graham St and Williamstown Rd as an experiment.³⁷ In 1938, the Housing Commission extended the garden suburb concept by creating its first "model settlement" in the area to the south-west of the Bank's estate.³⁸

The post-war community development was particularly active in establishing neighbourhood facilities and groups at Fisherman's Bend.



Management

To ensure that the character and significant of Garden City is conserved and enhanced, the following management recommendations are made.

Statutory Protection

The existing designation of Garden City as an area of special significance in the Melbourne Metropolitan Planning Scheme should be maintained.

Demolition and Removal of Original Elements

The demolition of buildings or works is currently subject to the grant of a planning permit by the Responsible Authority.

The demolition of any of the State Savings Bank houses, or the removal of original external elements (such as fences) or detailing, should not be permitted.

Alterations

Proposals to alter the appearance of the State Savings Bank houses, particularly where they involve changes to the fabric of building, should not generally be permitted.

New Buildings & Works

It is important that the bulk, location and external appearance (including colours and materials) of the new buildings and works (including garages and carports) is in harmony with the character and appearance of the houses and the general area.

Owners should be encouraged to locate new buildings or works so that they are not visible from the street (including garages and carports).

Public Works

Local government and State government instrumentalities when undertaking those activities for which they responsible (e.g. road and drainage works, traffic improvements; tree planting etc) must be sympathetic to the characteristics and qualities of Garden City.

Much of the character and significance of Garden City derives from the width and proportions of nature strips, footpaths and roadways, and the general layout of the streets. Extreme care should be taken by public authorities undertaking roadworks, traffic improvements etc, which might alter the characteristics of these elements.

Assistance and Advice

Every effort should be made to assist and encourage homeowners who wish to remove unsympathetic additions or alterations, or to restore original features (such as fences) which are missing.

This could be partly achieved through the preparation of detailed guidelines. Detailed guidelines should address issues such as the siting and design of new additions and works (eg. garages); the restoration of the existing building stock and other elements (eg. fences); and landscaping of private and public open space.

There are a number of publications which may provide assistance to property owners undertaking new works or restoration. A small collection of these publications could be obtained and held by the Council and/or the Port Melbourne Library for the use of the community.

Footnotes

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- ¹ Jacobs Lewis Vines Port Melbourne Conservation Study pp71-2; The Age, 12 March 1982
- ² U'Ren Turnbull A History of Port Melbourne, p227
- ³ Ibid, p229
- ⁴ Ibid; p231
- ⁵ Freestone "Garden Suburbs of Melbourne" in RHSV p32
- ⁶ U'Ren Turnbull op.cit. pp227,230
- ⁷ E.g. The Herald 5/11/25, 29/12/25, 11/5/26, 15/7/26; The Age 21/12/26, 26/1/26, 29/1/26, The Argus 21/12/25
- ⁸ The Age 29/1/26
- ⁹ The Age 10/3/27; U'Ren Turnbull op.cit. p233
- ¹⁰ The Age 10/3/27
- ¹¹ Metropolitan Town Planning Commission, Plan of General Development 1929, p254
- ¹² Emery G. General Manager's report to the Commissioners of the State Savings Bank of Victoria on his visit to Great Britain to enquire into Housing, Dec 1925, p4, F.N 563.1 Series 746; Route D. "Creating the Classing Home: The Role of the SSB Housing Scheme" SAHANZ Conference, May 1986, p7
- ¹³ SSB Archives, Statements & Returns, Series 327-2-2 Annual Reports 1920-1 931; Cooch, The SSB of Victoria, pp102-7; Craddock Cavena, 125 Years: The Story of the SSB of Victoria 1842 – 1966, pp22-3
- ¹⁴ The Herald 2/2/26, 11/5/26
- ¹⁵ Cooch op.cit. p 109
- ¹⁶ MTPC op.cit pp 256-7; U'Ren Turnbull op.cit p241
- ¹⁷ See eg. Freestone's articles on the garden city/garden suburb idea in Australia, and Burke's paper on the garden suburb idea in NSW.
- ¹⁸ Before the first sod was turned, newspaper reports conferring this title eg, The Herald 30/12/25, 3/3/26, 11/5/26, The Argus 18/2/26; a 1928 broadsheet advertised a meeting to form the "Garden City Progress Association", S.S.B F.N.440.26
- ^{18B} Tibbits G. "The Garden City Idea in Australia" SAHANZ Conference, 1986 p6
- ¹⁹ Emery G. Report to Commissioners op.cit p3
- ²⁰ Eg; The Herald 29/12/25, The Age 10/3/27
- ²¹ "Memo DC Webb (architect) to General Manager" 27 1/26, S.S.B F.N 566.121, Series 746; MTPC op.cit p249
- ²² Report from General Manager re proposal to build at Fisherman's Bend Port Melbourne 2/2/26, SSB FN 566.121, Series 746
- ²³ Memo Webb to General Manager, op.cit.
- ²⁴ Eg MTPC op.cit. pp248, 250-1
- ²⁵ Memo Webb to General Manager, op.cit
- ²⁶ SSB Fisherman's Bend Housing Scheme Group No 3, Specification Contract No H5554, Series 438-4-1; The Age 26/10/26; Teng Ooi Chi "Development of Garden City in Port Melbourne" Melbourne University, 1965, p10
- ²⁷ Various newspaper reports: May, June 1026; Also Mercury (Hobart) 8/7/26, The Sun 21/1/27, The Age 21,1,27
- ²⁸ The Herald 16/6/26
- ²⁹ The Age 10/3/27
- ³⁰ The Age 21/1/27; SSB FN 440.26, 566.104
- ³¹ Info. In this para. From SSB Archives Series 746; FN 566.121 Misc. notes & documents inc. Memo GB Leith to Gen Manager 20/4/37; Sec. To Gen Manager 12/5/38; Gen. Manager to GB Leith 4/10/46; GB Leith to Gen Manager 7/5/47; FN 566.123 Miscellaneous file notes; FN 566.101 final amendment map 13/4/39; Cooch op.cit. p101- 3; U'Ren Turnbull op.cit p236; Tend et al op.cit p.12
- ³² Freestone "The Condition of Cities & the Response...." In Burnley Forest Living in Cities p20; Tibbits G. "he Garden City Idea in Victoria" op.cit pp2,3; Burke S. "he Evolution of the Garden City Idea in NSW before WWI" op.cit pp2,4
- ³³ Cooch op.cit p95-1 12; Craddock Cavenagh op.cit p5

³⁴ The Herald 26/1/26; The Age 3/3/26

³⁵ General Managers Report to Commissioners 1925, op.cit p5

³⁶ Eg; SSB Annual Statements & Returns 1929, pp12-13; The Herald 15/7/26; The Sun 27/10/26; Labour Call 15/4/26; The Argus 26/7/26, 11/1/27; The Age 26/10/26

³⁷ U'Ren Turnbull op.cit p246-8; Tibbits op.cit p6

³⁸ U'Ren Turnbull op.cit p249; Tibbits op.cit p7; Butler, Heidelberg Conservation Study pp1870-2

³⁹ Maunders "An Historical Look at Community Centres in Australia" paper, passim; Allport "Women & Suburban Housing; Post War Planning in Sydney, 1943-61" passim.

GARAGE AND/OR CARPORT STRUCTURES

The following guidelines offer design suggestions in relation to the siting and design of garage and/or carport structures.

Objective

Car parking structures should not dominate the development or street frontage and should contribute to the character of the streetscape.

Guidelines

If a roller door is proposed, it should be demonstrated by the applicant that it contributes to the character of the area and that the overhead drum enclosure is designed so as to be fully enclosed and screened from view from surrounding streets/laneways.

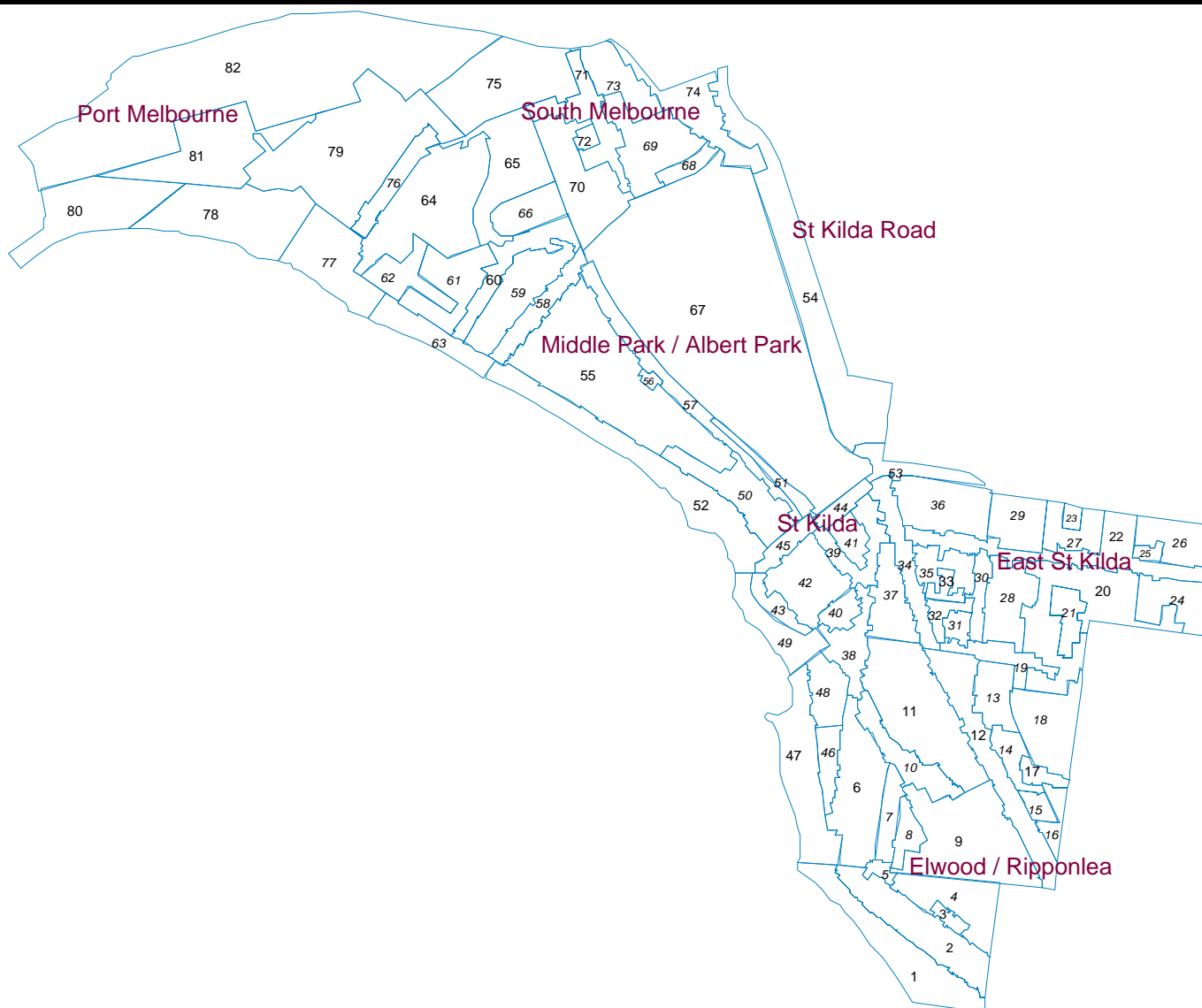
In a Heritage Overlay Area, hinged, horizontal sliding, concertina or panel lift garage doors are preferred, as they are generally complimentary to the main architectural styles predominant in Port Phillip.

Garage/carport structures should be located at least 1 metre behind the line of the front wall of the building (excluding projections such as verandahs, balconies and bay windows).



City of Port Phillip

NEIGHBOURHOODS AND URBAN CHARACTER AREAS





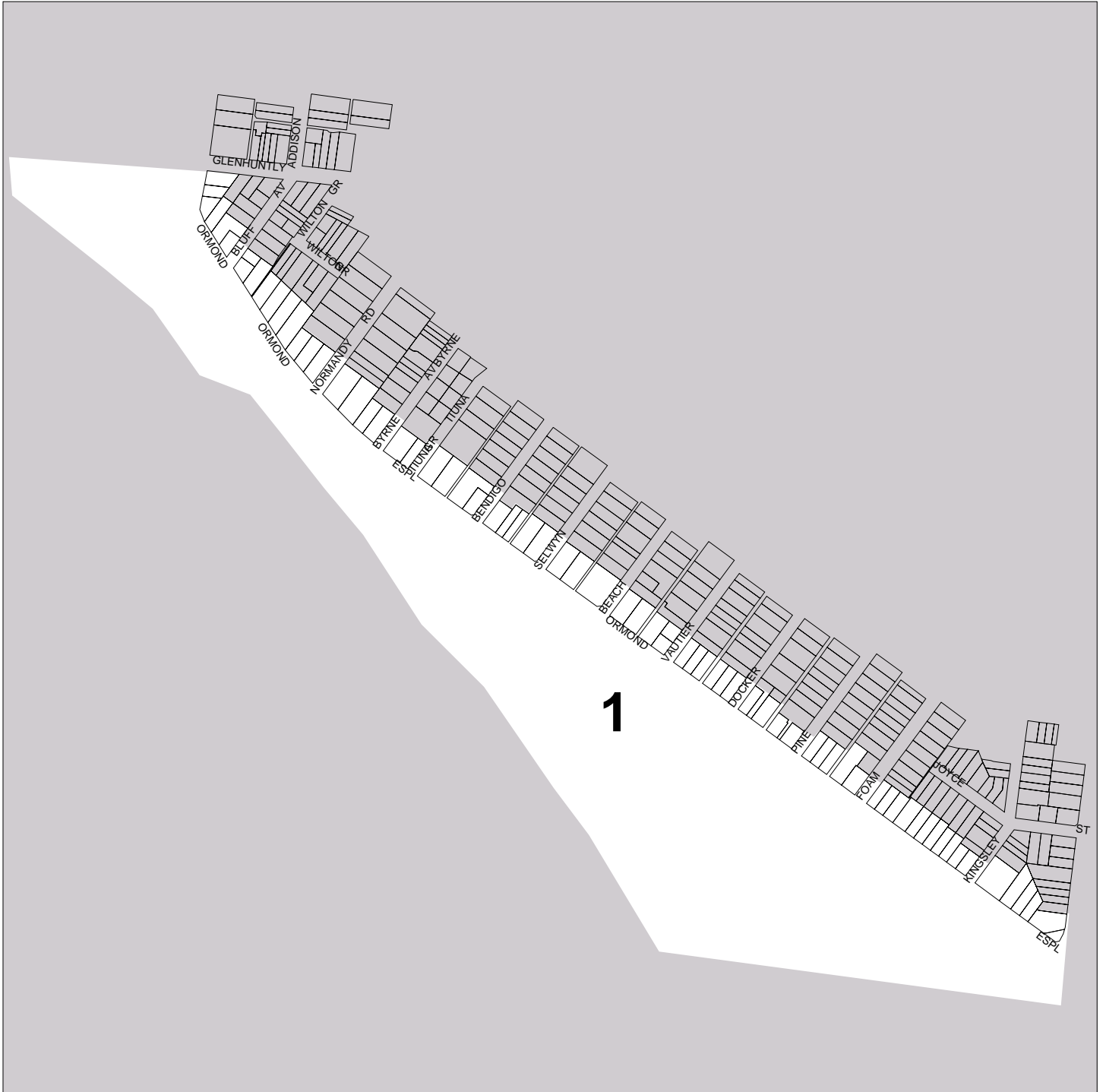
City of Port Phillip

URBAN CHARACTER DESCRIPTION

AREA 1: ORMOND ESPLANADE, ELWOOD

The following description should be used in carrying out the site analysis and design response to your site.

Pay attention to the very important and important character elements listed here, as well as the actual conditions observed at the site analysis stage.



Foreshore, St Kilda St, Ormond Esplanade, Glenhuntly Rd.

Streets in the area:

Head St, Ormond Esp, Point Ormond Rd,

Description of the character of the area:

The area has been developed with dwellings and apartments fronting Ormond Esplanade opposite Point Ormond Reserve and Elwood Park.

Lot sizes are predominantly greater than 750 square metres and buildings constructed in a range of styles are generally large.

Buildings remain largely intact although renovations, fencing, maintenance and occasional replacements reduce its coherence.

The foreshore open space, views and heavy traffic are major determinants of the character of this area distinguishing it from the adjoining residential area to the north.

<i>Element</i>	<i>Description</i>
Dominant style	1920s - 1940s
Dominant scale	one and two storey
Dominant material	stucco and render/minor variation
Dominant roof	no dominance/terracotta
Setback	5-9 metres/minor variation
Heritage	more than 50% of sites identified as heritage places
Gardens	visible/hidden/small
Fences	medium height/various materials

The existing urban character of the City of Port Phillip has been assessed using 24 character elements. The design should especially respond to the important and very important elements, which in this area are:

<i>Element</i>	<i>Importance</i>
Views	Very important
Traffic	Very important
Building style	Very important
Foreshore	Very important
Lot size	Important
Street dimensions	Important
Setbacks	Important
Building scale	Important
Heritage	Important
Open space	Important

A comprehensive urban character assessment and policy statement for the area is available from the Council .



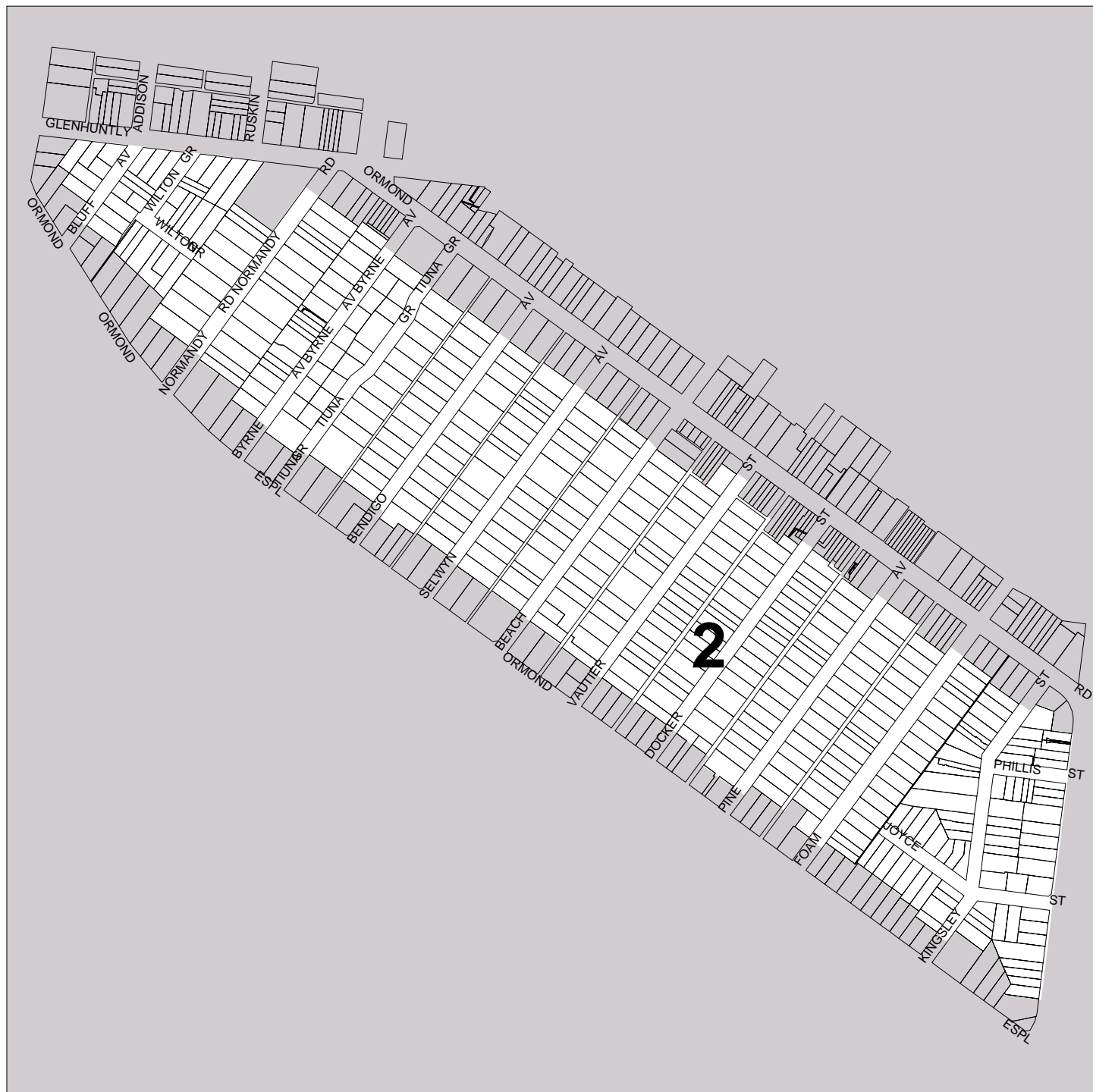
City of Port Phillip

URBAN CHARACTER DESCRIPTION

AREA 2: LAND GENERALLY BOUNDED BY ORMOND ROAD, GLENHUNTLY ROAD AND ORMOND ESPLANADE, ELWOOD

The following description should be used in carrying out the site analysis and design response to your site.

Pay attention to the very important and important character elements listed here, as well as the actual conditions observed at the site analysis stage.



Ormond Esplanade, Glenhuntly Road, St Kilda St, Ormond Rd.

Streets in the area:

Beach Av, Bendigo Av, Bluff Av, Byrne Av, Docker St, Foam St, Joyce St, Kingsley St, Normandy Rd, Phyllis St, Pine Av, Selwyn Av, Tiuna Gv, Vautier St, Wilton Gv,

Description of the character of the area:

This area is located on a low ridge providing views to open space and Port Phillip Bay from the southern ends of most streets. The predominant lot size is greater than 750 square metres and the street pattern is fairly consistent with rear lanes constructed in bluestone.

The area retains a limited number of large Victorian houses but has been largely developed during the inter-war period with detached dwellings and walk-up apartment buildings. The area also has a limited number of more recent flats and houses.

The large lots occupied by single dwellings often have generous gardens however, privacy is reduced by the high proportion of two and three storey houses and flats.

Most streets are lined with Plane trees and this aspect together with the relatively rich and varied architectural style and scale are important factors contributing to the character of the area.

The area has excellent access to open space, the foreshore and local shops and limited through traffic.

<i>Element</i>	<i>Description</i>
Dominant style	1920s - 1940s
Dominant scale	one and two storey
Dominant material	stucco and render/minor variation
Dominant roof	pitched/terracotta
Setback	less than 5 metres/minor variation
Heritage	less than 50% of sites identified as heritage places
Gardens	visible/hidden/medium/small
Fences	medium height/various materials

The existing urban character of the City of Port Phillip has been assessed using 24 character elements. The design should especially respond to the important and very important elements, which in this area are:

<i>Element</i>	<i>Importance</i>
Pattern	Very important
Building style	Very important
Open space	Very important
Lot size	Important
Street trees	Important
Street detailing	Important
Setbacks	Important
Building scale	Important
Heritage	Important
Foreshore	Important

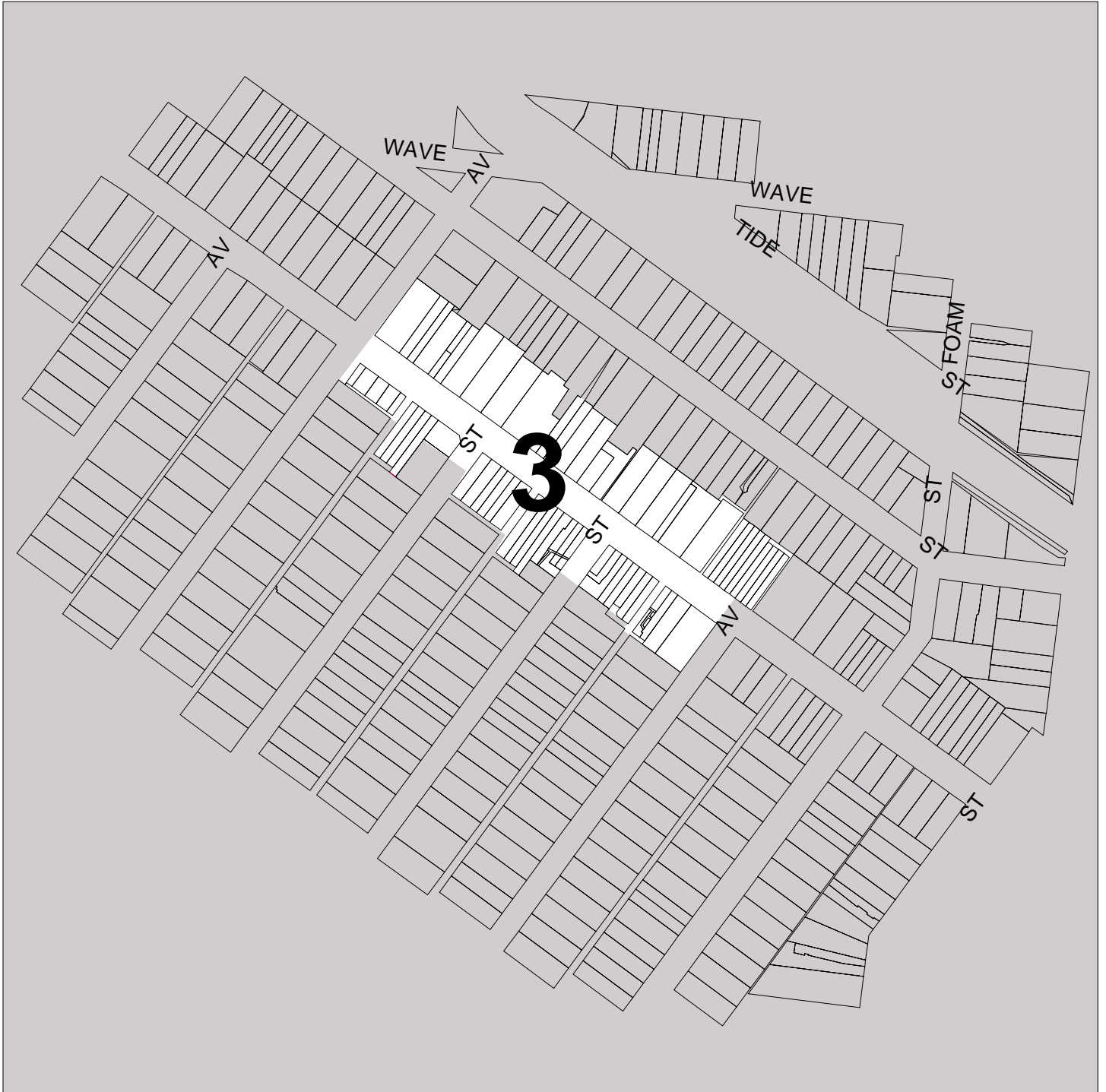
A comprehensive urban character assessment and policy statement for the area is available from the Council .



City of Port Phillip
URBAN CHARACTER DESCRIPTION

AREA 3: ORMOND ROAD BETWEEN BEACH AVENUE AND PINE AVENUE, ELWOOD

The following description should be used in carrying out the site analysis and design response to your site.
Pay attention to the very important and important character elements listed here, as well as the actual conditions observed at the site analysis stage.



Beach Ave, Pine Ave, Ormond Rd.

Streets in the area:

Ormond Rd,

Description of the character of the area:

This area is distinct from its surroundings because it is a commercial shopping centre serving adjacent residential areas.

Largely developed during the inter-war period, it still retains most of the original buildings. These include a mix of single and double storey terraced shops with a few free standing buildings on larger lots.

The design of the street has been modified with traffic control devices, landscaping, brick feature paving and walls.

<i>Element</i>	<i>Description</i>
Dominant style	1920s - 1940s
Dominant scale	one and two storey
Dominant material	stucco and render/minor variation
Dominant roof	concealed by parapets/metal
Setback	no setback/minor variation
Heritage	shops on north side of street identified as heritage places
Gardens	invisible
Fences	none

The existing urban character of the City of Port Phillip has been assessed using 24 character elements. The design should especially respond to the important and very important elements, which in this area are:

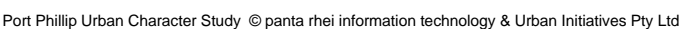
<i>Element</i>	<i>Importance</i>
Setbacks	Very important
Land use	Very important
Parking	Important
Footpaths	Important
Street detailing	Important
Signage	Important
Building style	Important
Building scale	Important
Access	Important

A comprehensive urban character assessment and policy statement for the area is available from the Council .



AREA 4: LAND GENERALLY BOUNDED BY GLENHUNTLY ROAD, ST KILDA STREET AND ORMOND ROAD, ELWOOD

The following description should be used in carrying out the site analysis and design response to your site. Pay attention to the very important and important character elements listed here, as well as the actual conditions observed at the site analysis stage.



Glenhuntly Rd, St Kilda St, Ormond Rd.

Streets in the area:

Beach Av, Foam St, Kingsley St, Ormond Rd, Spray St, Wave St,

Description of the character of the area:

The area has been subdivided into lots with a predominant size of 650 - 750 square metres around the Elwood Canal drainage reservation which provides a focus for the area.

Developed for residential purposes, buildings consist largely of detached dwellings and walk-up flats from the inter-war period. The flats tend to be concentrated on the perimeter main roads.

Streets are lined with deciduous trees and much of the original detailing is still intact.

The area has excellent access to shops, services and transport.

<i>Element</i>	<i>Description</i>
Dominant style	1920s -1940s
Dominant scale	one, two and three storey
Dominant material	brick or masonry
Dominant roof	pitched/terracotta
Setback	less than 5 metres/mixed
Heritage	some heritage qualities/no protection
Gardens	visible/medium
Fences	low height/various materials

The existing urban character of the City of Port Phillip has been assessed using 24 character elements. The design should especially respond to the important and very important elements, which in this area are:

<i>Element</i>	<i>Importance</i>
Building style	Very important
Street trees	Important
Fences	Important
Building scale	Important
Housing type	Important
Land use	Important
Access	Important
Open space	Important

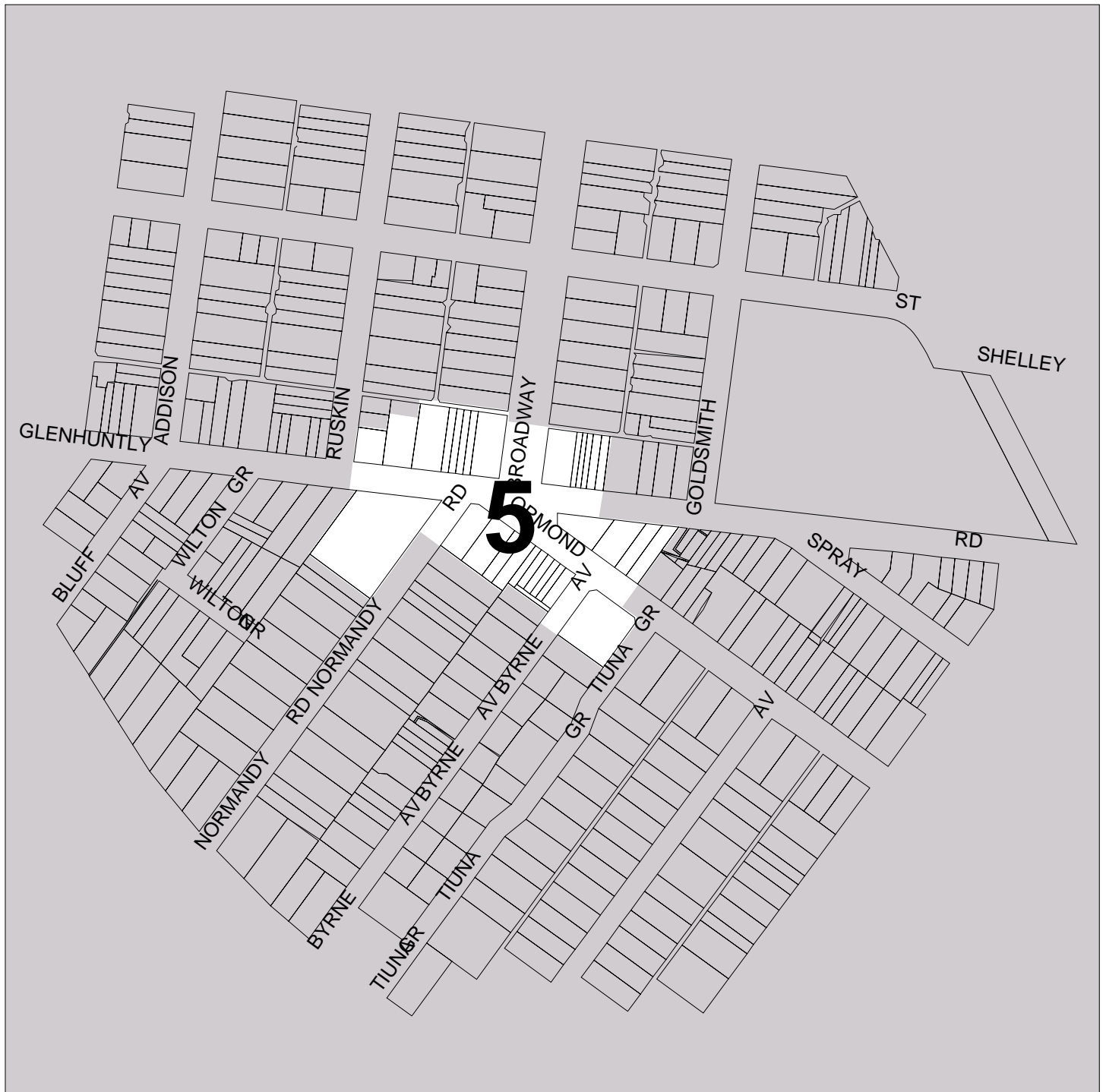
A comprehensive urban character assessment and policy statement for the area is available from the Council .



City of Port Phillip
URBAN CHARACTER DESCRIPTION

AREA 5: INTERSECTION OF GLENHUNTLY ROAD AND ORMOND ROAD, ELWOOD

The following description should be used in carrying out the site analysis and design response to your site.
Pay attention to the very important and important character elements listed here, as well as the actual conditions observed at the site analysis stage.



Boundary streets:

Ruskin St, Tiuna Gve,
Glenhuntly - Ormond Rd junction.

Streets in the area:

Glenhuntly Rd, Ormond Rd,

Description of the character of the area:

This small commercial area has been developed with buildings dating back to the 1900's. These buildings contribute to the character of the area due to their scale, styles and prominent location. Shop top housing also contributes to the viability and life of this area.

Whilst the large roundabout with mature gums contributes to the character of the area, street detailing and other landscaping is inconsistent with the heritage qualities of the area.

<i>Element</i>	<i>Description</i>
Dominant style	no dominance
Dominant scale	two storey
Dominant material	stucco and render/minor variation
Dominant roof	concealed by parapets/terracotta
Setback	no setback/minor variation
Heritage	identified heritage place
Gardens	very few/visible/medium/small
Fences	none

The existing urban character of the City of Port Phillip has been assessed using 24 character elements. The design should especially respond to the important and very important elements, which in this area are:

<i>Element</i>	<i>Importance</i>
Building style	Very important
Heritage	Very important
Pattern	Important
Street dimensions	Important
Traffic	Important
Street trees	Important
Parking	Important
Footpaths	Important
Street detailing	Important
Setbacks	Important
Building scale	Important
Land use	Important

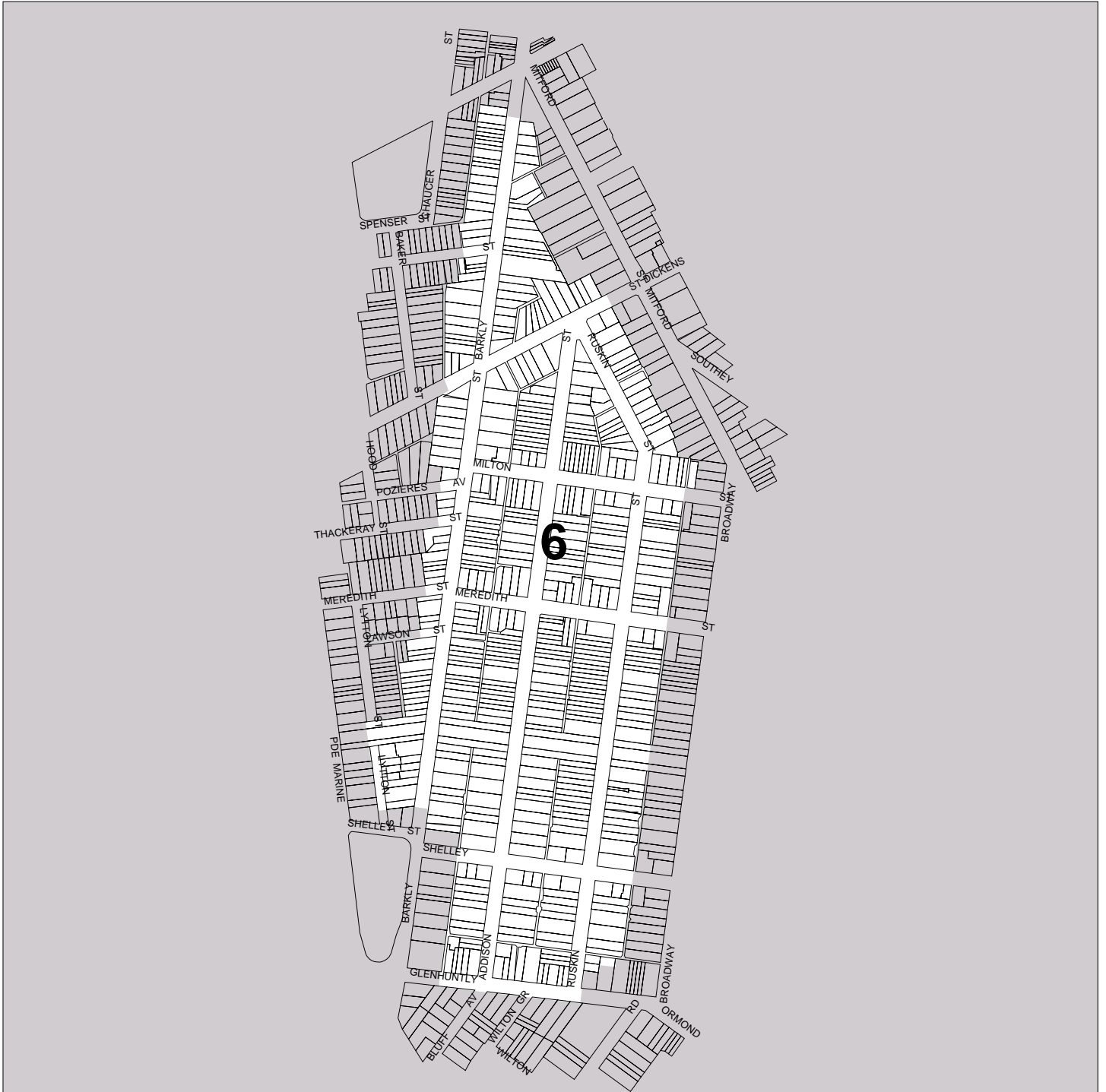
A comprehensive urban character assessment and policy statement for the area is available from the Council .



City of Port Phillip
URBAN CHARACTER DESCRIPTION

**AREA 6: LAND GENERALLY BOUNDED BY BARKLY STREET, MITFORD STREET, BROADWAY
AND GLENHUNTLY ROAD, ELWOOD**

The following description should be used in carrying out the site analysis and design response to your site.
Pay attention to the very important and important character elements listed here, as well as the actual conditions
observed at the site analysis stage.



Barkly, Ruskin, Glenhuntly Rds.

Streets in the area:

Addison St, Barkly St, Dickens St, Glenhuntly Rd, Meredith St, Milton St, Ruskin St, Shelley St, Rosetti La,

Description of the character of the area:

The area has a consistent pattern of wide streets flanked by plane trees providing access to reasonably deep lots, mostly between 300-650 square metres.

It has been developed for residential purposes during Edwardian and inter-war periods with a mixture of duplex housing and flat complexes. There has been some further development of flats during the post-war period with little replacement of single dwellings.

Elwood Canal passes through the southern part of the area creating a distinct character for the immediate environment. The character is influenced by the broad expanse of water, a cycle path and associated landscaping.

With the exception of Barkly Street, most streets are free of through traffic. The area is close to neighbourhood shopping centres, community facilities and open space.

<i>Element</i>	<i>Description</i>
Dominant style	1920s - 1940s
Dominant scale	one and two storey
Dominant material	brick or masonry/mixed
Dominant roof	pitched/terracotta
Setback	less than 5 metres/minor variation
Heritage	identified heritage places adjacent to southern boundary
Gardens	visible/medium/small
Fences	low height/various materials

The existing urban character of the City of Port Phillip has been assessed using 24 character elements. The design should especially respond to the important and very important elements, which in this area are:

<i>Element</i>	<i>Importance</i>
Street trees	Very important
Building style	Very important
Views	Important
Pattern	Important
Street dimensions	Important
Street detailing	Important
Fences	Important
Front gardens	Important
Housing type	Important
Land use	Important

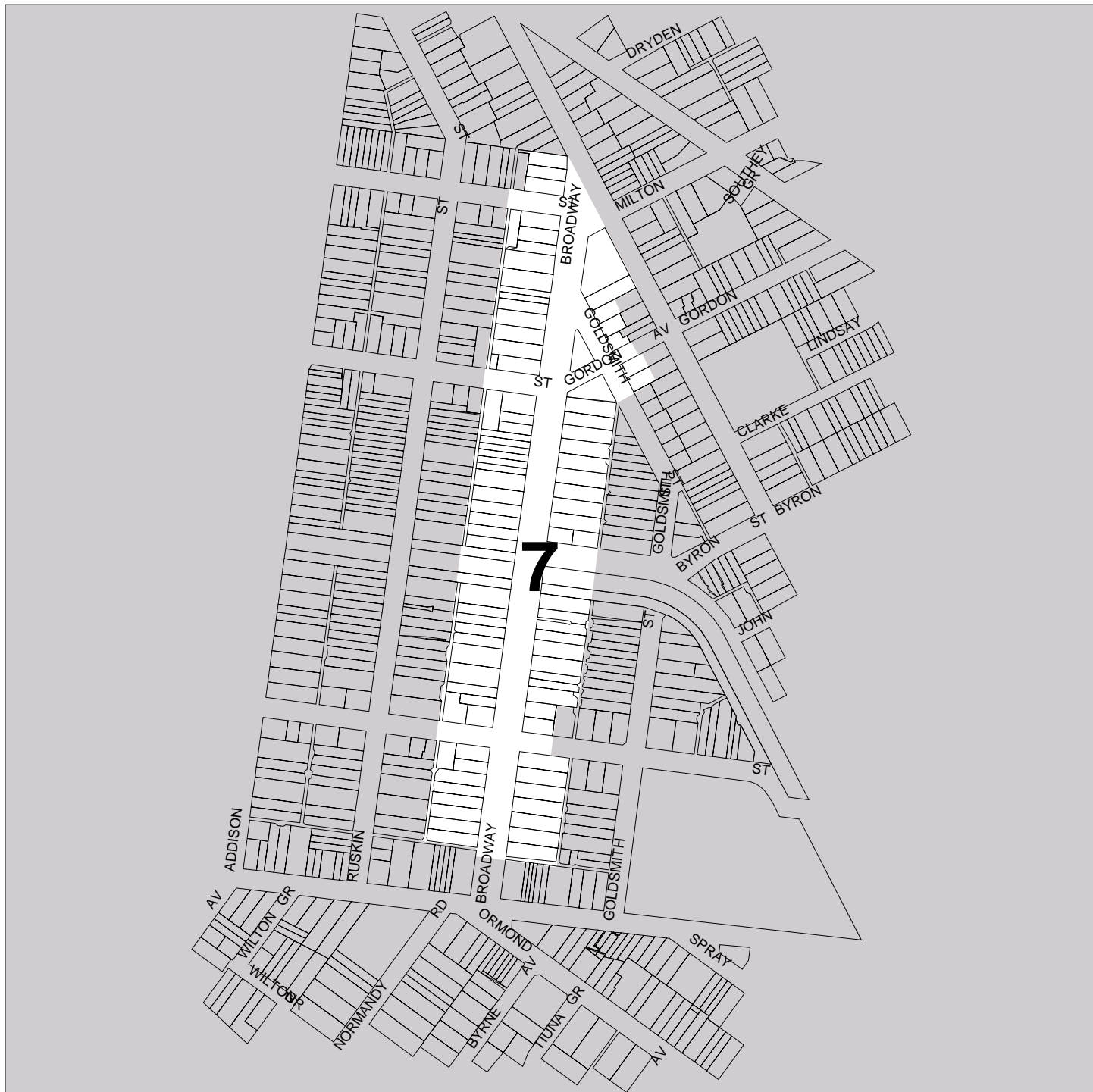
A comprehensive urban character assessment and policy statement for the area is available from the Council .



City of Port Phillip
URBAN CHARACTER DESCRIPTION

AREA 7: BROADWAY

The following description should be used in carrying out the site analysis and design response to your site. Pay attention to the very important and important character elements listed here, as well as the actual conditions observed at the site analysis stage.



Broadway.

Streets in the area:

Broadway, Byron St, Goldsmith St, Gordon Av,

Description of the character of the area:

This area is distinguished from adjoining areas by the exceptionally wide road reservation and the slightly larger lot sizes.

Development consists of Edwardian and inter-war houses and apartment buildings together with a number of post-war flat blocks. At the northern end of the area, original period apartment buildings dominate the street corners providing views over the small triangular parks.

Broadway is a local collector street that carries reasonable volumes of through traffic during peak periods. The street retains original edge detailing however the centre has been modified to incorporate roundabouts and planter tree islands.

The area is more distant from local shopping, community and transport facilities.

<i>Element</i>	<i>Description</i>
Dominant style	1920s - 1940s
Dominant scale	one and two storey
Dominant material	stucco and render/minor variation
Dominant roof	pitched/terracotta
Setback	less than 5 metres/minor variation
Heritage	identified heritage places adjacent to northern boundary
Gardens	visible/small
Fences	low/various materials

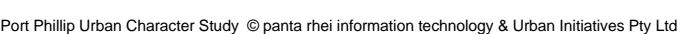
The existing urban character of the City of Port Phillip has been assessed using 24 character elements. The design should especially respond to the important and very important elements, which in this area are:

<i>Element</i>	<i>Importance</i>
Street dimensions	Very important
Street trees	Very important
Building style	Very important
Views	Important
Lot size	Important
Street detailing	Important
Front gardens	Important
Building scale	Important
Housing type	Important
Heritage	Important
Land use	Important

A comprehensive urban character assessment and policy statement for the area is available from the Council .



The following description should be used in carrying out the site analysis and design response to your site. Pay attention to the very important and important character elements listed here, as well as the actual conditions observed at the site analysis stage.



Mitford, Broadway, Glenhuntly Rds.

Streets in the area:

Goldsmith St, Shelley St,

Description of the character of the area:

The area has a pattern of streets with consistent detailing and mature Plane trees that form a full canopy over the road spaces throughout the area. The subdivision layout is influenced by the presence of the Elwood Canal and lot sizes are predominantly 300 - 650 square metres.

The area was developed in the inter-war period with detached houses and flats of various styles. It remains largely intact with the exception of one mock Edwardian replacement dwelling and minor renovation changes. Fences are also notable for their consistency.

The area is free from through traffic and close to schools and open space.

<i>Element</i>	<i>Description</i>
Dominant style	1920s - 1940s
Dominant scale	one storey
Dominant material	stucco and render/consistent
Dominant roof	pitched/terracotta
Setback	5 - 9 metres/minor variation
Heritage	identified heritage places adjacent to southern boundary
Gardens	visible/medium
Fences	low height/masonry/consistent

The existing urban character of the City of Port Phillip has been assessed using 24 character elements. The design should especially respond to the important and very important elements, which in this area are:

<i>Element</i>	<i>Importance</i>
Street trees	Very important
Fences	Very important
Building style	Very important
Heritage	Very important
Street detailing	Important
Setbacks	Important
Front gardens	Important
Building scale	Important
Housing type	Important
Land use	Important

A comprehensive urban character assessment and policy statement for the area is available from the Council .



AREA 9: LAND GENERALLY BOUNDED BY GLENHUNTLY ROAD, MITFORD STREET, GOLDSMITH STREET, BRIGHTON ROAD, GORDON STREET AND MASON STREET, ELWOOD

Pay attention to the very important and important character elements listed here, as well as the actual conditions observed at the site analysis stage.



Glenhuntly Rd, Mitford St, Goldsmith St, Brighton Rd, Gordon St, Mason St.

Streets in the area:

Austin Av, Burns St, Byron St, Coleridge St, Cyril St, Daley St, Glenhuntly Rd, Goldsmith St, Greig St, Heaton Av, John St, Keats St, Kendall St, Knight St, May St, McCrae St, Mitford St, Moore St, Poets Gv, Rainsford St, Robert St, Rothesay Av, Scott St, Shelley St, Tennyson St North, Tennyson St North, Florence La,

Description of the character of the area:

The area includes low lying areas near Elwood Canal and land sloping upwards toward Brighton Road. Within a distorted grid pattern, the predominant lot size is 300 - 650 square metres and frontages vary in width.

The area was largely developed during the inter-war period however the eastern half has been substantially redeveloped with flat blocks during the 1960's and 1970's.

Tennyson, Mitford and Byron Streets are wide streets, but the area also includes a network of smaller streets, lanes and cul-de-sacs, which add diversity to the area.

The small shopping area in Tennyson Street is a viable local shopping centre and community focus. The grouping of schools and open space along Elwood Canal is another important focus for this area.

<i>Element</i>	<i>Description</i>
Dominant style	1920s -1940s
Dominant scale	one and two storey
Dominant material	stucco and render/mixed
Dominant roof	pitched/terracotta
Setback	less than 5 metres/mixed
Heritage	identified heritage places restricted to south-east corner
Gardens	visible/medium/small
Fences	low/various materials

The existing urban character of the City of Port Phillip has been assessed using 24 character elements. The design should especially respond to the important and very important elements, which in this area are:

<i>Element</i>	<i>Importance</i>
Building style	Very important
Building scale	Very important
Pattern	Important
Setbacks	Important
Housing type	Important
Land use	Important
Access	Important



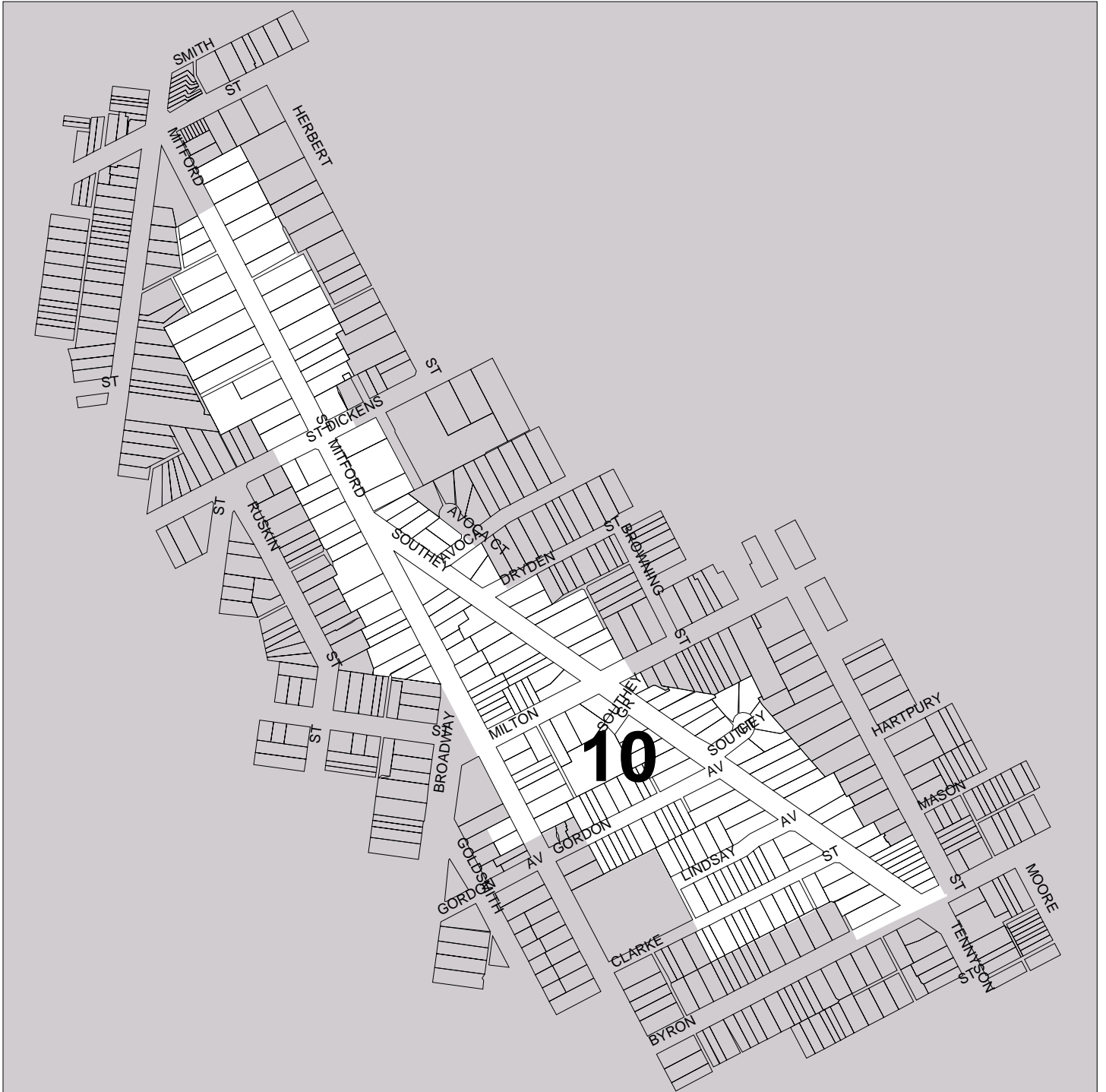
City of Port Phillip

URBAN CHARACTER DESCRIPTION

AREA 10: SOUTHEY STREET, ELWOOD

The following description should be used in carrying out the site analysis and design response to your site.

Pay attention to the very important and important character elements listed here, as well as the actual conditions observed at the site analysis stage.



Southey St, Mitford St, Byron St.

Streets in the area:

Avoca Ct, Bayview St, Clarke St, Gordon Av, Lindsay Av, Milton St, Mitford St, Southey Ct, Southey Gv, Southey St,

Description of the character of the area:

A diversity of housing scale and type as well as the relatively high density is a strong attribute of this area.

Walk-up apartment blocks dating from the inter-war period display outstanding design quality, and these together with apartment buildings from the post- Second World War period, tend to create a sense of enclosure even in the wide streets due to the relatively small setback and large building scale.

Cross streets in the area tend to have smaller scale housing on smaller lots, and more intimately proportioned streets.

Substantial changes have been made to the design of Southey Street with the introduction of road narrowings and sections of angled parking. Street trees are mostly deciduous but quite mixed within some streets.

<i>Element</i>	<i>Description</i>
Dominant style	1920s -1940s
Dominant scale	one, two and three storey
Dominant material	stucco and render/mixed
Dominant roof	pitched/terracotta
Setback	less than 5 metres
Heritage	@50% identified heritage places
Gardens	visible/small
Fences	low height/various materials

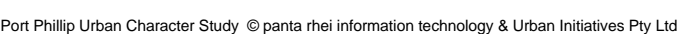
The existing urban character of the City of Port Phillip has been assessed using 24 character elements. The design should especially respond to the important and very important elements, which in this area are:

<i>Element</i>	<i>Importance</i>
Building style	Very important
Building scale	Very important
Housing type	Very important
Land use	Very important
Landform	Important
Street trees	Important
Parking	Important
Street detailing	Important
Fences	Important
Setbacks	Important
Heritage	Important

A comprehensive urban character assessment and policy statement for the area is available from the Council .



The following description should be used in carrying out the site analysis and design response to your site. Pay attention to the very important and important character elements listed here, as well as the actual conditions observed at the site analysis stage.



Boundary streets:

Brighton Rd, Carlisle St, Barkly St, Southey St, Milson Ave.

Streets in the area:

Alfriston St, Avoca Av, Blessington St, Browning St, Bundalohn Ct, Byron St, Carlisle St, Dickens St, Dryden St, Foster Av, Garden Ct, Hartpury Av, Hennessy Av, Herbert St, Irymple Av, Jacka St, Mason Av, Milton St, Mitchell St, Mozart St, Smith St, Tennyson St North, Tennyson St North, Wimbledon Av,

Description of the character of the area:

The area was subdivided into large allotments and developed with a number of large houses during the 1850's. Further subdivision to create house allotments and apartment buildings occurred during Edwardian and inter-war periods with the most recent redevelopment during the 1960's and 1970's replacing many houses with two and three storey flat blocks.

The succession of development phases has created a rich mixture of housing type, style and scale which is an important element of its present character. However, the post-Second World War flat buildings are less sympathetic.

In addition, the St Kilda Botanic Gardens is a central focus and activity centre that adds greatly to the character of the area.

The streets are quite varied in their proportions and landscape treatment although deciduous trees predominate.

<i>Element</i>	<i>Description</i>
Dominant style	1920s - 1940s
Dominant scale	one and two storey
Dominant material	stucco and render
Dominant roof	pitched/terracotta
Setback	less than 5 metres/minor variation
Heritage	significant area identified as heritage places
Gardens	visible/medium/small
Fences	low height/various materials

The existing urban character of the City of Port Phillip has been assessed using 24 character elements. The design should especially respond to the important and very important elements, which in this area are:

<i>Element</i>	<i>Importance</i>
Building style	Very important
Building scale	Very important
Heritage	Very important
Open space	Very important
Lot size	Important
Street dimensions	Important
Street trees	Important
Parking	Important
Street detailing	Important
Fences	Important
Setbacks	Important
Housing type	Important
Access	Important

A comprehensive urban character assessment and policy statement for the area is available from the Council .



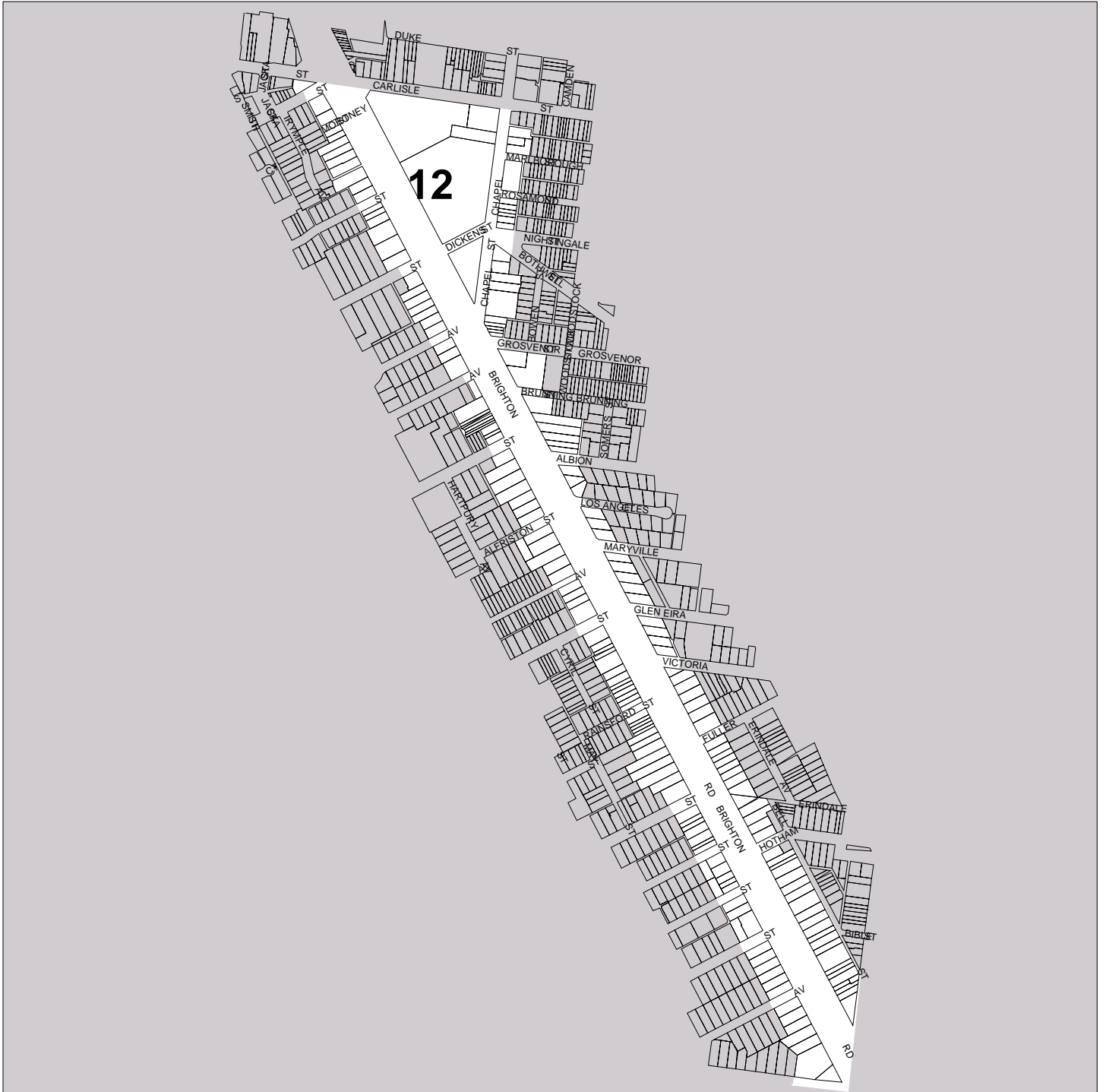
City of Port Phillip

URBAN CHARACTER DESCRIPTION

AREA 12: BRIGHTON ROAD, ST KILDA

The following description should be used in carrying out the site analysis and design response to your site.

Pay attention to the very important and important character elements listed here, as well as the actual conditions observed at the site analysis stage.



Boundary streets:

Carlisle St, Glenhuntly Rd, Chapel St, Brighton Rd.

Streets in the area:

Bell St, Brighton Rd, Chapel St, Dickens St, Moroney St,

Description of the character of the area:

The qualities that distinguish the area from the adjoining areas are the road, scale, landscape treatment and trees.

Development along Brighton Road is mixed in scale and style from Victorian and inter-war houses and flats, through to post-war flat blocks, which dominate much of the western edge of the road.

The area also has a limited number of commercial sites with a strong visual presence. Some of these, particularly the car yards in Chapel Street detract from the quality of the area.

The town hall triangle is a significant collection of public buildings and associated landscaping with identified heritage value and Brighton Road is one of Melbourne's more attractive boulevards with a unique plantation of flowering gums.

The area has excellent access to shops, services and public transport including the Brighton Road tram route.

<i>Element</i>	<i>Description</i>
Dominant style	1920s - 1940s
Dominant scale	one and two storey
Dominant material	stucco and render/mixed
Dominant roof	pitched/terracotta
Setback	less than 5 metres/varied
Heritage	entire area identified as heritage place
Gardens	visible/hidden/medium/small
Fences	medium height/varied materials

The existing urban character of the City of Port Phillip has been assessed using 24 character elements. The design should especially respond to the important and very important elements, which in this area are:

<i>Element</i>	<i>Importance</i>
Street dimensions	Very important
Traffic	Very important
Street trees	Very important
Access	Very important
Views	Important
Heritage	Important

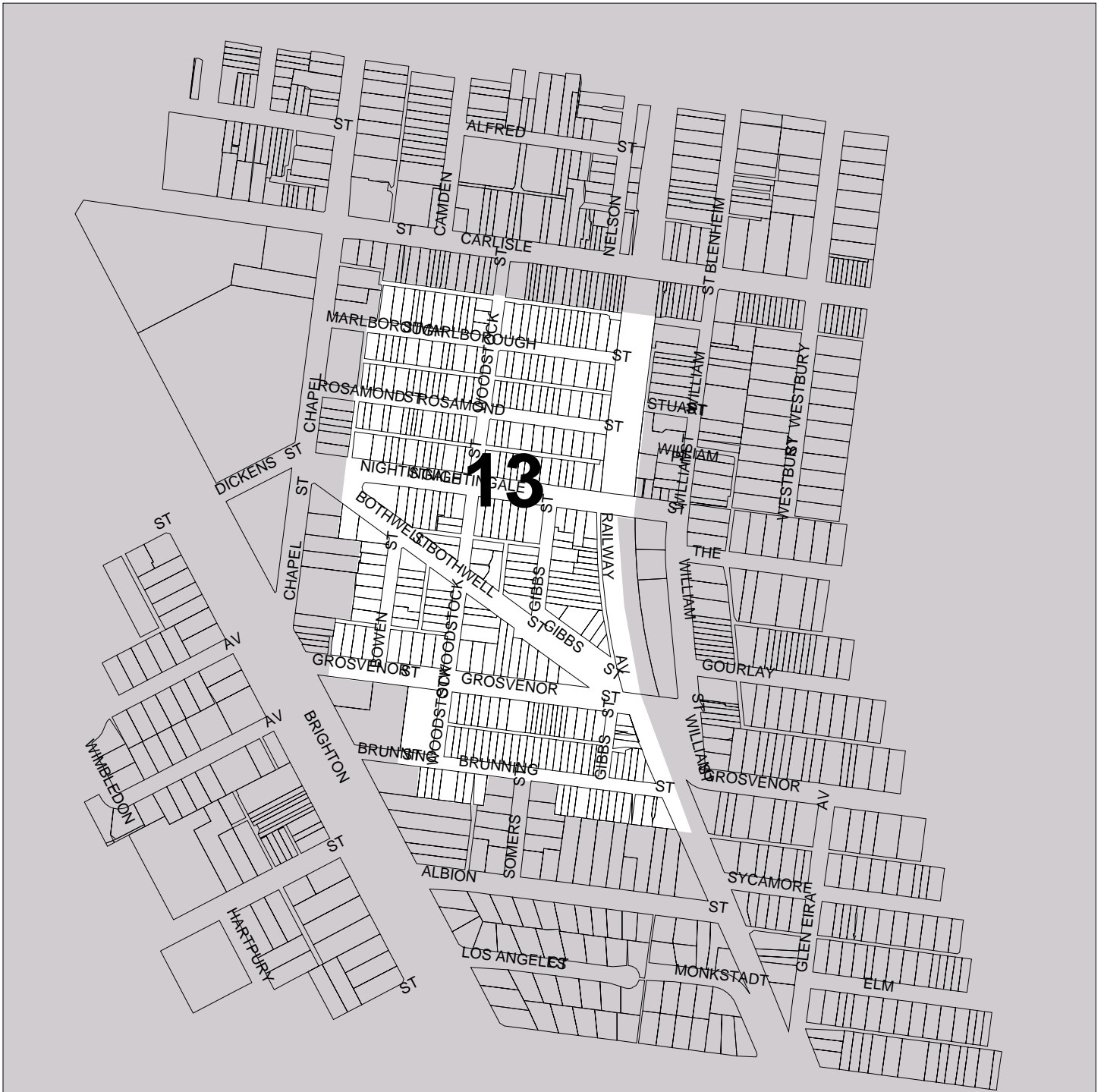
A comprehensive urban character assessment and policy statement for the area is available from the Council .



City of Port Phillip
URBAN CHARACTER DESCRIPTION

AREA 13: CHAPEL STREET, CARLISLE STREET, BRUNNING STREET, RAILWAY LINE

The following description should be used in carrying out the site analysis and design response to your site. Pay attention to the very important and important character elements listed here, as well as the actual conditions observed at the site analysis stage.



Boundary streets:

Chapel St, Carlisle St, Brunning St, Railway line.

Streets in the area:

BOTHWELL ST, BOWEN ST, BRUNNING ST, GIBBS ST, GROSVENOR ST, MARLBOROUGH ST, NIGHTINGALE ST, RAILWAY PL, ROSAMOND ST, SOMERS ST, WOODSTOCK ST,

Description of the character of the area:

The area was developed during the Victorian period, and is clearly distinguished from the adjoining inter-war housing to the west.

It has consistent small lots with small frontages, reduced setbacks and rear laneways. Most streets are quite narrow further reinforcing the intimate scale of the area.

Access to transport and services is excellent.

<i>Element</i>	<i>Description</i>
Dominant style	Pre-1900s
Dominant scale	one storey
Dominant material	timber or cladding/minor variation
Dominant roof	pitched/metal
Setback	less than 5 metres/consistent
Heritage	limited number of heritage places
Gardens	visible/small
Fences	low height/consistent materials

The existing urban character of the City of Port Phillip has been assessed using 24 character elements. The design should especially respond to the important and very important elements, which in this area are:

<i>Element</i>	<i>Importance</i>
Building style	Very important
Building scale	Very important
Lot size	Important
Pattern	Important
Street dimensions	Important
Parking	Important
Street detailing	Important
Overhead services	Important
Fences	Important
Setbacks	Important
Housing type	Important
Heritage	Important
Access	Important

A comprehensive urban character assessment and policy statement for the area is available from the Council .



City of Port Phillip
URBAN CHARACTER DESCRIPTION

AREA 14: LAND GENERALLY BOUNDED BY BRIGHTON ROAD, BRUNNING STREET, GLEN EIRA ROAD AND VICTORIA AVENUE, ST KILDA

The following description should be used in carrying out the site analysis and design response to your site.
Pay attention to the very important and important character elements listed here, as well as the actual conditions observed at the site analysis stage.



Boundary streets:

Brighton Rd, Glen Eira Rd, Brunning St.

Streets in the area:

Albion St, Glen Eira Rd, Los Angeles Ct, Maryville St, Monkstadt Av, Victoria Av,

Description of the character of the area:

This area is distinguished from adjoining areas to the north and south by its consistent larger lot sizes and the predominant inter-war building style.

Los Angeles Court, Monkstadt Avenue and part of Maryville Avenue represent an exceptional combination of interwar housing, gardens and streetscape. Other parts of this area were originally developed during the same era but have been modified by post-Second World War flat development.

The streets in the area vary in their dimensions, traffic flow and street trees. The area is close to a local shopping area and has good access to public transport.

<i>Element</i>	<i>Description</i>
Dominant style	1920s - 1940s
Dominant scale	one storey
Dominant material	stucco and render/minor variation
Dominant roof	pitched/terracotta
Setback	less than 5 metres/minor variation
Heritage	entire area identified as heritage place
Gardens	visible/medium
Fences	low height/consistent materials

The existing urban character of the City of Port Phillip has been assessed using 24 character elements. The design should especially respond to the important and very important elements, which in this area are:

<i>Element</i>	<i>Importance</i>
Building style	Very important
Heritage	Very important
Pattern	Important
Fences	Important
Front gardens	Important
Building scale	Important
Land use	Important
Access	Important

A comprehensive urban character assessment and policy statement for the area is available from the Council .



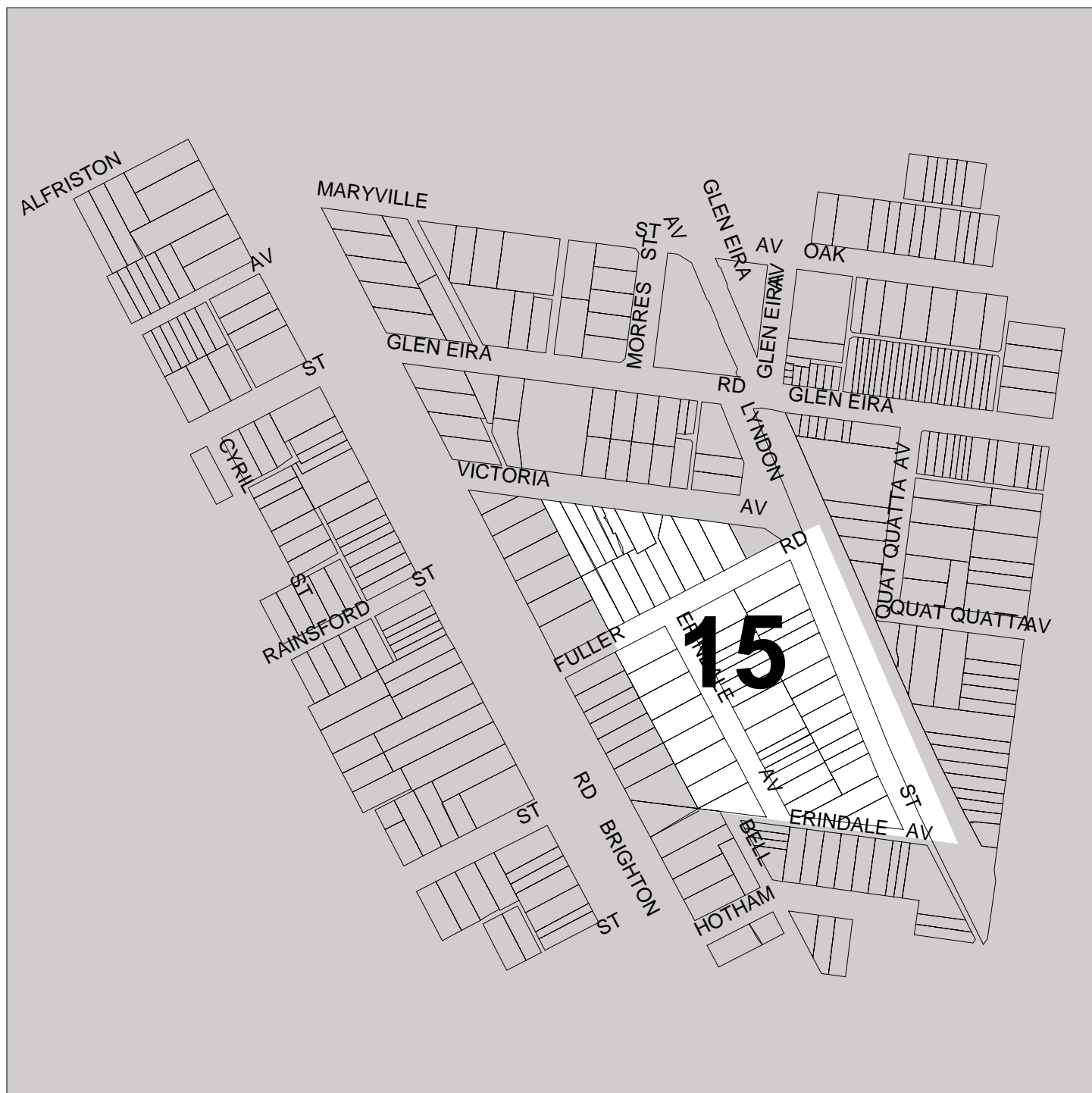
City of Port Phillip

URBAN CHARACTER DESCRIPTION

AREA 15: LAND GENERALLY BOUNDED BY VICTORIA AVENUE, LYNDON STREET, ERINDALE AVENUE AND BRIGHTON ROAD

The following description should be used in carrying out the site analysis and design response to your site.

Pay attention to the very important and important character elements listed here, as well as the actual conditions observed at the site analysis stage.



Boundary streets:

Victoria Ave, Erindale Ave, Railway line, Brighton Rd.

Streets in the area:

Erindale Av, Fuller Rd, Lyndon St,

Description of the character of the area:

Supporting a reasonably high population density, this small residential area is distinguished from adjoining areas to the north and south by its distorted subdivision pattern and relatively intact Edwardian and inter-war development.

Street detailing and street trees also contribute to the quality of the area.

The area is more distant from neighbourhood shopping centre, local community facilities and open space.

<i>Element</i>	<i>Description</i>
Dominant style	1900s - 1940s
Dominant scale	one storey
Dominant material	brick or masonry/consistent
Dominant roof	pitched/terracotta
Setback	5-9 metres/consistent
Heritage	entire area identified as heritage place
Gardens	visible/medium
Fences	medium height/various materials

The existing urban character of the City of Port Phillip has been assessed using 24 character elements. The design should especially respond to the important and very important elements, which in this area are:

<i>Element</i>	<i>Importance</i>
Building style	Very important
Building scale	Very important
Street trees	Important
Street detailing	Important
Fences	Important
Setbacks	Important
Front gardens	Important
Housing type	Important
Access	Important

A comprehensive urban character assessment and policy statement for the area is available from the Council .

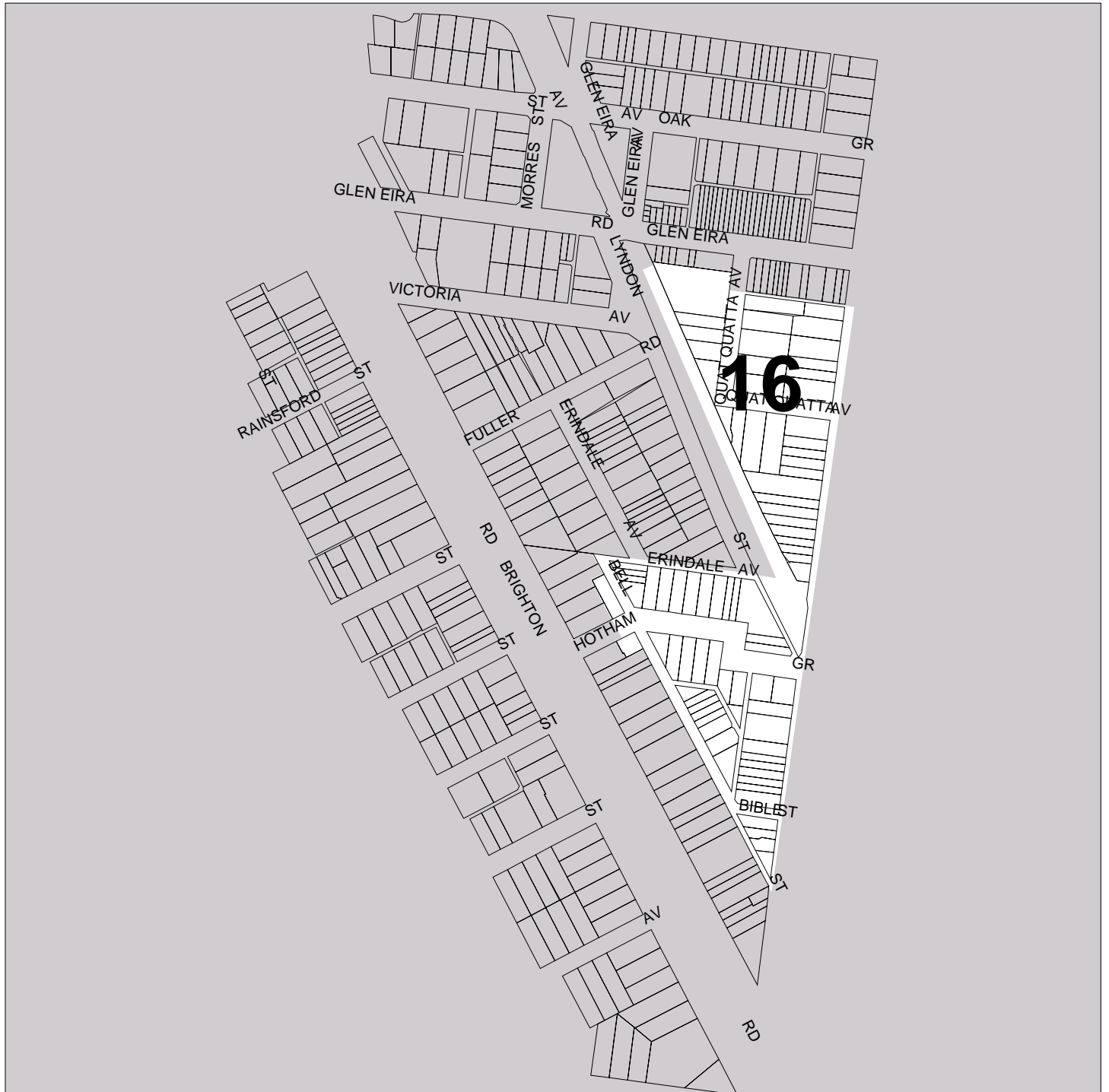


City of Port Phillip

URBAN CHARACTER DESCRIPTION

AREA 16: LAND GENERALLY BOUND BY GLEN EIRA ROAD, BELL STREET, ERINDALE AVENUE AND HOTHAM STREET

The following description should be used in carrying out the site analysis and design response to your site. Pay attention to the very important and important character elements listed here, as well as the actual conditions observed at the site analysis stage.



Bell St, Erindale Ave, Glen Eira Rd, Hotham St.

Streets in the area:

Bible St, Hotham Gv,

Description of the character of the area:

The entire area has undergone considerable redevelopment in the post war period with large walk-up flat blocks sitting side by side small timber cottages. This extremely varied scale is perhaps the area's most distinguishing characteristic.

The northern area includes the original Victorian homestead for the area with later development of suburban inter-war houses in the street. The southern area has been developed with more modest terraces or small timber cottages in the Victorian and Edwardian periods.

Streets and lanes are quite varied in scale, but most contain the original detailing. Street trees are also variable with excellent mature Plane trees in Quat Quatta Avenue and less successful planting of Ash and Paper barks in other streets.

The area has especially good access to shops and public transport.

<i>Element</i>	<i>Description</i>
Dominant style	1920s - 1940s
Dominant scale	one and two storey
Dominant material	brick or masonry/mixed
Dominant roof	pitched/terracotta
Setback	less than 5 metres/minor variation
Heritage	heritage places are located north of Quat Quatta Ave
Gardens	visible/medium/small
Fences	medium height/varied materials

The existing urban character of the City of Port Phillip has been assessed using 24 character elements. The design should especially respond to the important and very important elements, which in this area are:

<i>Element</i>	<i>Importance</i>
Building scale	Very important
Housing type	Important
Access	Important



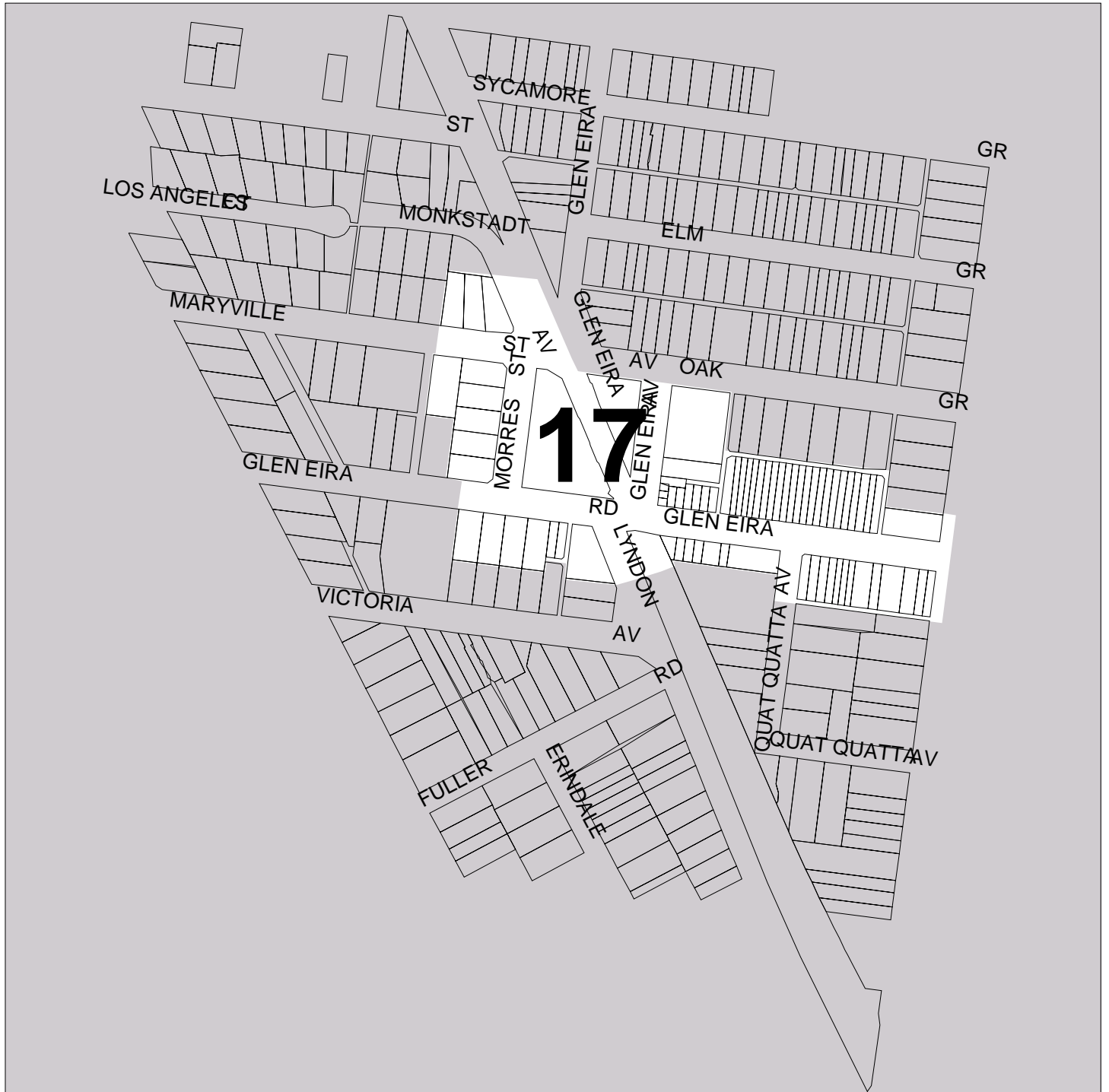
City of Port Phillip

URBAN CHARACTER DESCRIPTION

AREA 17: GLEN EIRA ROAD AND VICTORIA AVENUE

The following description should be used in carrying out the site analysis and design response to your site.

Pay attention to the very important and important character elements listed here, as well as the actual conditions observed at the site analysis stage.



Boundary streets:

Los Angeles Ct, Victoria Ave, Hotham Rd.

Streets in the area:

Glen Eira Av, Glen Eira Rd, Morres St, Quat Quatta Av,

Description of the character of the area:

This small area includes Ripponlea Station and its adjoining open space and streets with the Glen Eira shopping centre extending to the east of the railway station.

All development in this area was built in the decades immediately following the opening of the railway line in 1913. The shops are very consistent in their style, two storey scale and set back with little change since they were developed, apart from an unfortunate replacement building at the Hotham Road end.

The railway station buildings and small forecourt open spaces on both sides of the line create a strong sense of focus for the area. The park to the west of the line is edged with two storey flats on Glen Eira Road and single storey inter-war houses on Morres Street.

The area is quite distinct from adjoining residential areas, because of its shop use and consistent buildings and public focus.

<i>Element</i>	<i>Description</i>
Dominant style	1920s - 1940s
Dominant scale	two storey
Dominant material	brick or masonry/minor variation
Dominant roof	pitched/concealed by parapet
Setback	less than 5 metres/consistent
Heritage	entire area identified as heritage place
Gardens	visible/medium/small
Fences	low height/varied materials

The existing urban character of the City of Port Phillip has been assessed using 24 character elements. The design should especially respond to the important and very important elements, which in this area are:

<i>Element</i>	<i>Importance</i>
Building scale	Very important
Heritage	Very important
Views	Important
Parking	Important
Footpaths	Important
Overhead services	Important
Signage	Important
Fences	Important
Setbacks	Important
Land use	Important
Access	Important
Open space	Important

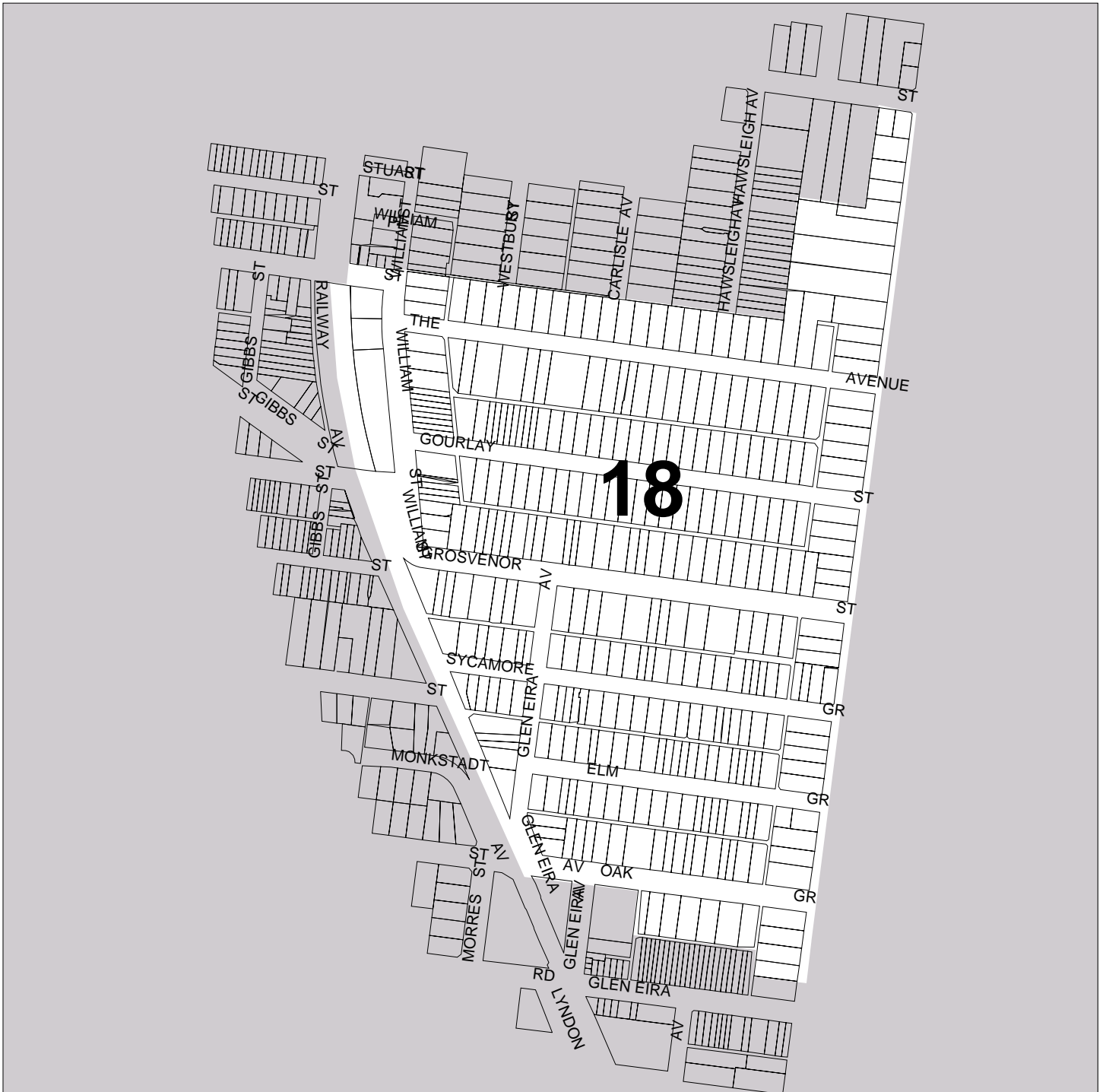
A comprehensive urban character assessment and policy statement for the area is available from the Council .



City of Port Phillip
URBAN CHARACTER DESCRIPTION

AREA 18: AREA GENERALLY BOUNDED BY HOTHAM STREET, OAK GROVE, RAILWAY LINE AND THE AVENUE

The following description should be used in carrying out the site analysis and design response to your site. Pay attention to the very important and important character elements listed here, as well as the actual conditions observed at the site analysis stage.



Boundary streets:

The Avenue, Hotham Rd, Oak Gve, Railway line.

Streets in the area:

Elm Gv, Gibbs St, Glen Eira Av, Gourlay St, Grosvenor St, Oak Gv, Sycamore Gv, The Avenue, William St,

Description of the character of the area:

The area was initially developed during Victorian, Edwardian and inter-war periods and pockets of each style remain. The area underwent substantial redevelopment with blocks of walk-up flats in the post war period and these now almost dominate its character.

The area also retains much of its original bluestone and bitumen street detailing, however traffic control devices and landscaping of questionable quality in some streets replace original details. Street trees are generally undersized mixed natives which do little to help unify these streets.

The area is somewhat distant from community facilities at its southern end and is lacking in open space.

<i>Element</i>	<i>Description</i>
Dominant style	1950s - 1960s
Dominant scale	one and two storey
Dominant material	brick or masonry/mixed
Dominant roof	pitched/terracotta
Setback	less than 5 metres/minor variation
Heritage	limited number of identified heritage places
Gardens	visible/medium/small
Fences	medium height/various materials

The existing urban character of the City of Port Phillip has been assessed using 24 character elements. The design should especially respond to the important and very important elements, which in this area are:

<i>Element</i>	<i>Importance</i>
Housing type	Very important
Pattern	Important
Parking	Important
Street detailing	Important
Building style	Important
Building scale	Important
Access	Important

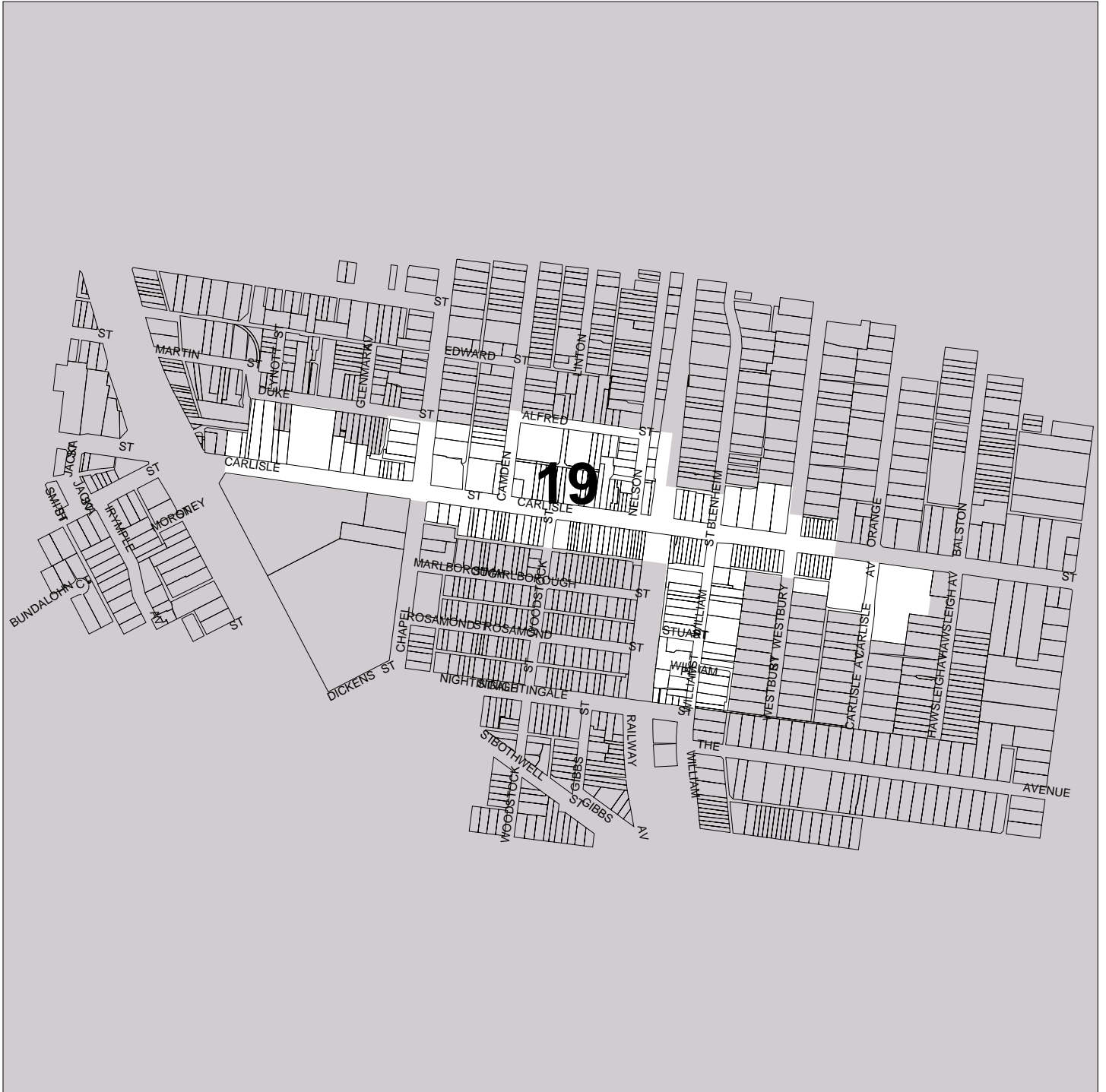
A comprehensive urban character assessment and policy statement for the area is available from the Council .



City of Port Phillip
URBAN CHARACTER DESCRIPTION

AREA 19: CARLISLE STREET, EAST OF ST KILDA ROAD

The following description should be used in carrying out the site analysis and design response to your site. Pay attention to the very important and important character elements listed here, as well as the actual conditions observed at the site analysis stage.



Boundary streets:

Alfred St, Duke St, St Kilda Rd, Nightingale St, Carslie St, Hawsleligh St.

Streets in the area:

Alfred St, Carlisle Av, Carlisle St, Stuart St, William Pl, William St,

Description of the character of the area:

The area is a classic urban village where a mix of residential, business and community uses exist in close proximity to public transport.

The frontage of Carlisle Street along most of this length is an active local shopping strip. The area also includes public car parks associated with the shopping and public facilities like the library, St Joseph's School and St Coleman's School, which is a landmark building at the high end of the street.

Whilst the residential areas surrounding this shopping centre are on the whole densely settled, opportunities for shop top housing and redevelopment of associated car parking lots exist.

There are also some industrial uses mostly on the south side of the shopping centre.

The centre appears to have been largely developed during the Edwardian and inter-war periods and whilst most of the building stock remains, it lacks a sense of cohesion due to the impact of signage and poor renovation.

Carlisle Street carries quite heavy traffic including trams and overhead services along both sides of the street have a strong visual presence.

<i>Element</i>	<i>Description</i>
Dominant style	1920s - 1940s
Dominant scale	one and two storey
Dominant material	stucco and render/minor variation
Dominant roof	pitched/metal
Setback	less than 5 metres/consistent
Heritage	includes considerable number of heritage places
Gardens	limited to schools and residential properties
Fences	none

The existing urban character of the City of Port Phillip has been assessed using 24 character elements. The design should especially respond to the important and very important elements, which in this area are:

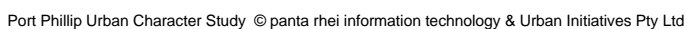
<i>Element</i>	<i>Importance</i>
Signage	Very important
Land use	Very important
Access	Very important
Lot size	Important
Street dimensions	Important
Traffic	Important
Parking	Important
Overhead services	Important
Fences	Important
Setbacks	Important
Building style	Important
Building scale	Important

A comprehensive urban character assessment and policy statement for the area is available from the Council .



AREA 20: LAND GENERALLY BOUNDED BY THE AVENUE, RAILWAY LINE, ALMA ROAD AND MARNE STREET

Pay attention to the very important and important character elements listed here, as well as the actual conditions observed at the site analysis stage.



The Avenue, Hotham Rd, Marne St East, Alma Rd, Railway line.

Streets in the area:

Alexandra St, Blenheim St, Cardigan St, Carlisle St, Chusan St, Hammerdale Av, Hawsleigh Av, Hotham St, Inkerman St, Lansdowne Rd, Mooltan Av, Orange Gv, Sunhill Ct, Wavenhoe Av, Westbury Gv, Westbury St,

Description of the character of the area:

Whilst initially developed during Victorian, Edwardian and inter-war periods with detached housing on large lots, substantial redevelopment in the post-war period with a high proportion of walk-up flat blocks has changed the residential density, scale and character of streetscapes.

Many of the streets in the area are wide and carry considerable traffic. Much of the original bluestone and bitumen street detailing remains, however, replacement concrete kerbs and driveway crossings are also common. Parking in the street is visually significant in this area.

Street trees are quite patchy and generally out of scale with the street spaces. Exceptions to this are Westbury Street (south of Carlisle Street), and Westbury and Carlisle Avenues.

<i>Element</i>	<i>Description</i>
Dominant style	1950s - 1960s
Dominant scale	one, two and three stories
Dominant material	brick or masonry/mixed
Dominant roof	pitched/terracotta
Setback	less than 5 metres/mixed
Heritage	limited number of heritage places identified
Gardens	visible/medium/small
Fences	medium/variable materials

The existing urban character of the City of Port Phillip has been assessed using 24 character elements. The design should especially respond to the important and very important elements, which in this area are:

<i>Element</i>	<i>Importance</i>
Building scale	Very important
Housing type	Very important
Lot size	Important
Street dimensions	Important
Traffic	Important
Parking	Important
Setbacks	Important
Building style	Important
Access	Important

A comprehensive urban character assessment and policy statement for the area is available from the Council .

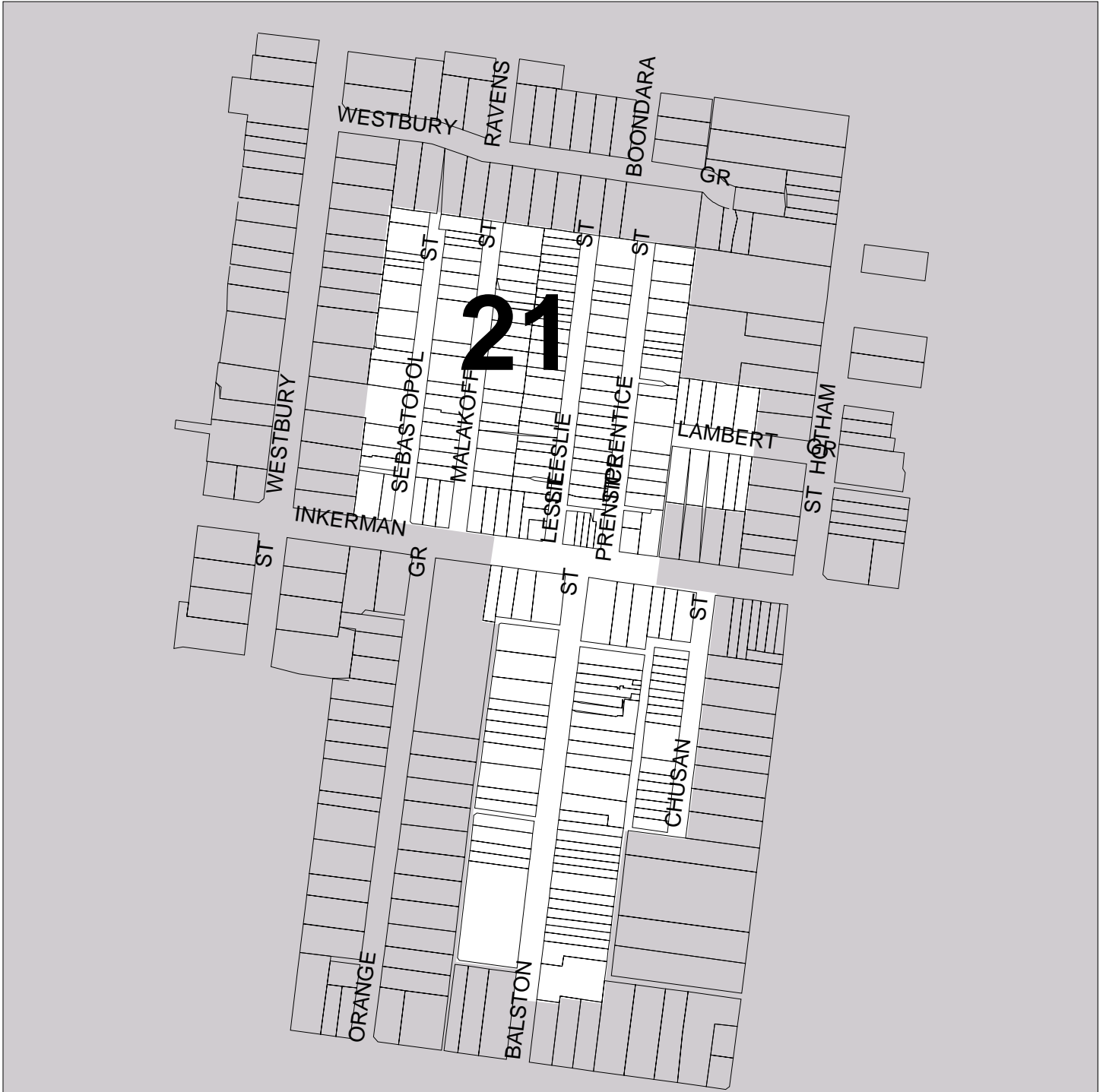


City of Port Phillip

URBAN CHARACTER DESCRIPTION

AREA 21: LAND GENERALLY BOUNDED BY WESTBURY GROVE, HOTHAM STREET, CARLISLE STREET, ORANGE GROVE AND WESTBURY STREET

The following description should be used in carrying out the site analysis and design response to your site. Pay attention to the very important and important character elements listed here, as well as the actual conditions observed at the site analysis stage.



Boundary streets:

Westbury Gve, Carlisle St, Sebastopol St, Chusan St.

Streets in the area:

Balston St, Inkerman St, Lambert Gv, Leslie St, Malakoff St, Prentice St, Sebastopol St,

Description of the character of the area:

This small area is primarily distinguished from surrounding area by its narrow streets, smaller lots and a limited number of flat blocks intermingled with modest housing.

It includes buildings from all periods since the Victorian era and more particularly, Victorian timber workers' cottages with heritage interest.

The area has commercial and industrial uses on Inkerman Street and most streets do not connect through to adjoining areas, effectively being cul-de-sacs off Inkerman Street.

The streets retain much of their original detailing and the recent introduction of street trees has limited influence on the character of the area.

Access to a neighbourhood shopping centre, community services and transport is limited in the area north of Inkerman Street.

<i>Element</i>	<i>Description</i>
Dominant style	pre-1900s
Dominant scale	one storey
Dominant material	timber or cladding/mixed
Dominant roof	pitched/metal
Setback	less than 5 metres/consistent
Heritage	virtually no heritage places identified
Gardens	visible/small
Fences	medium height/varied materials

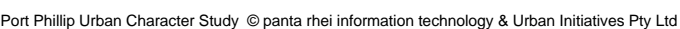
The existing urban character of the City of Port Phillip has been assessed using 24 character elements. The design should especially respond to the important and very important elements, which in this area are:

<i>Element</i>	<i>Importance</i>
Lot size	Very important
Setbacks	Very important
Building scale	Very important
Pattern	Important
Street dimensions	Important
Parking	Important
Fences	Important
Building style	Important
Housing type	Important
Land use	Important
Access	Important

A comprehensive urban character assessment and policy statement for the area is available from the Council .



The following description should be used in carrying out the site analysis and design response to your site. Pay attention to the very important and important character elements listed here, as well as the actual conditions observed at the site analysis stage.



Boundary streets:

Alma Rd, St Kilda Rd, Orrong Rd.

Streets in the area:

Alma Pl, Alma Rd, Boondara Gv, Holroyd Av, Hotham St, Raglan St, Raith Ct, Ravens Gv,

Description of the character of the area:

The area was initially developed with many large Victorian mansions built at the western end of Alma Road and more suburban housing from the inter-war period at the eastern end.

As many of the lots are large, extensive redevelopment with walk-up flat blocks, mostly in the post-war period, now dominates the character of many sections of this area.

This section of Alma Road has a wide reservation and wide carriageway and carries quite heavy volumes of through traffic.

To facilitate the heavy volumes of through traffic, the road has been widened with concrete kerbs and footpaths replacing original details, and nature strips narrowed or removed, leaving little space for the remaining few mature trees.

Overhead services also have a strong visual impact in this area due to the recent plantings of mixed natives being too small for the street.

Alma Park and the St Kilda cemetery on the north side of Alma Road makes a significant contribution to the area's character.

<i>Element</i>	<i>Description</i>
Dominant style	1950s - 1960s
Dominant scale	one and two storey
Dominant material	brick or masonry/mixed
Dominant roof	pitched/terracotta
Setback	less than 5 metres/mixed
Heritage	includes number of identified heritage places
Gardens	visible/large/medium/small
Fences	medium-high/various materials

The existing urban character of the City of Port Phillip has been assessed using 24 character elements. The design should especially respond to the important and very important elements, which in this area are:

<i>Element</i>	<i>Importance</i>
Housing type	Very important
Lot size	Important
Street dimensions	Important
Traffic	Important
Overhead services	Important
Front gardens	Important
Building scale	Important
Open space	Important

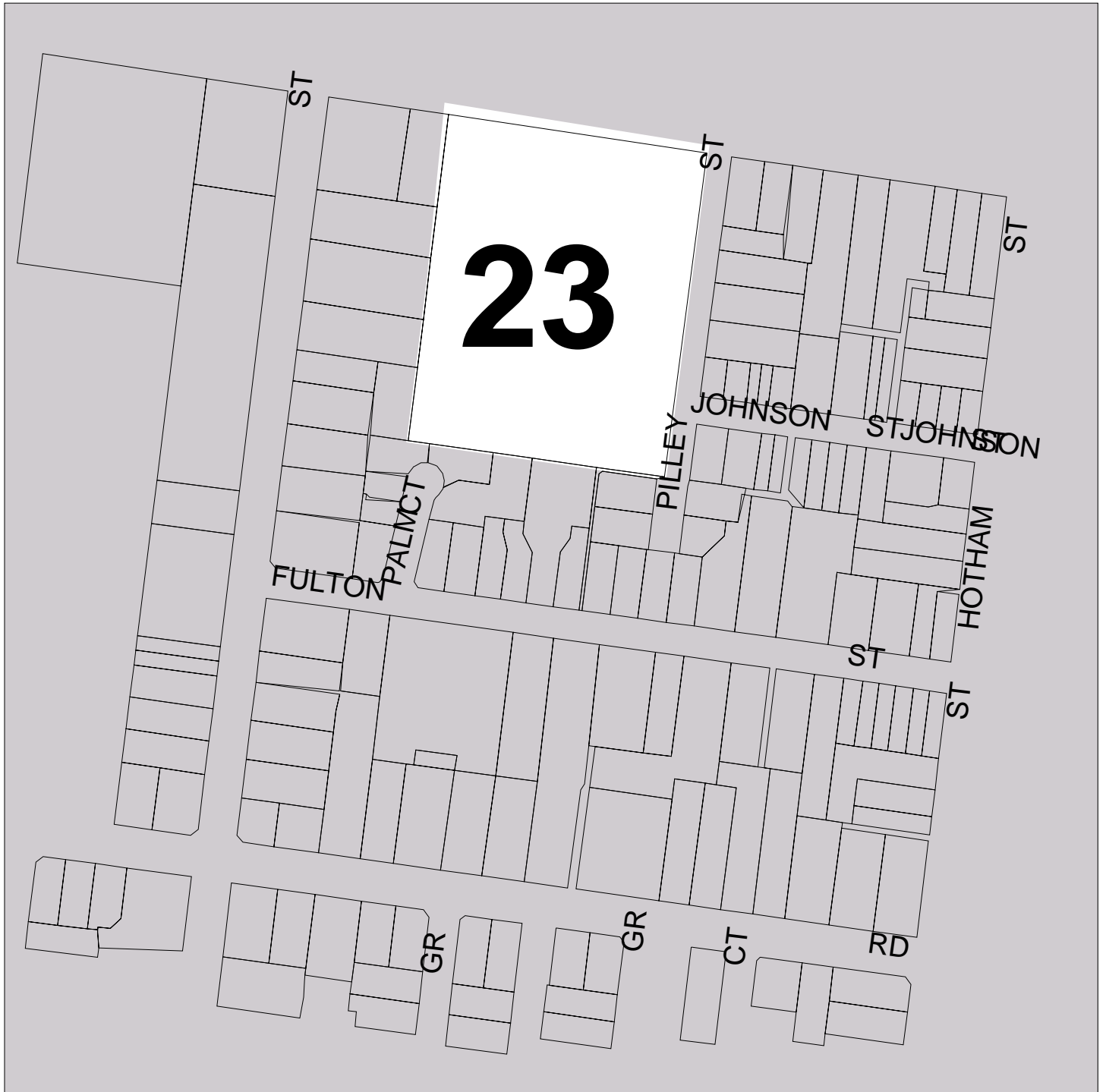
A comprehensive urban character assessment and policy statement for the area is available from the Council .



City of Port Phillip
URBAN CHARACTER DESCRIPTION

AREA 23: ARDOCH

The following description should be used in carrying out the site analysis and design response to your site.
Pay attention to the very important and important character elements listed here, as well as the actual conditions observed at the site analysis stage.



Boundary streets:

Ardoch Ave, Pilley St, Dandenong Rd.

Streets in the area:

Ardoch Av, Palm Ct,

Description of the character of the area:

The area is distinctive from adjoining areas because it is a comprehensive single development site.

It was initially developed in the Victorian period as large houses surrounded by gardens and later, during the inter-war period, was subdivided and redeveloped as an institutional complex. The most recent phase of redevelopment in the 1990's relates to the construction of new buildings along the Pilley Street frontage and the conversion of other buildings into apartments.

The internal landscape of the site has been totally redesigned to provide an integrated garden with limited narrow access roads to garages and parking spaces.

<i>Element</i>	<i>Description</i>
Dominant style	1920s - 1940s
Dominant scale	two storey
Dominant material	stucco and render/consistent
Dominant roof	pitched/terracotta
Setback	5-9 metres/varied
Heritage	identified as a heritage place
Gardens	visible/large
Fences	medium height/varied materials

The existing urban character of the City of Port Phillip has been assessed using 24 character elements. The design should especially respond to the important and very important elements, which in this area are:

<i>Element</i>	<i>Importance</i>
Front gardens	Very important
Building style	Very important
Building scale	Very important
Heritage	Very important
Open space	Very important
Views	Important
Lot size	Important
Street dimensions	Important
Street detailing	Important
Housing type	Important
Land use	Important
Access	Important

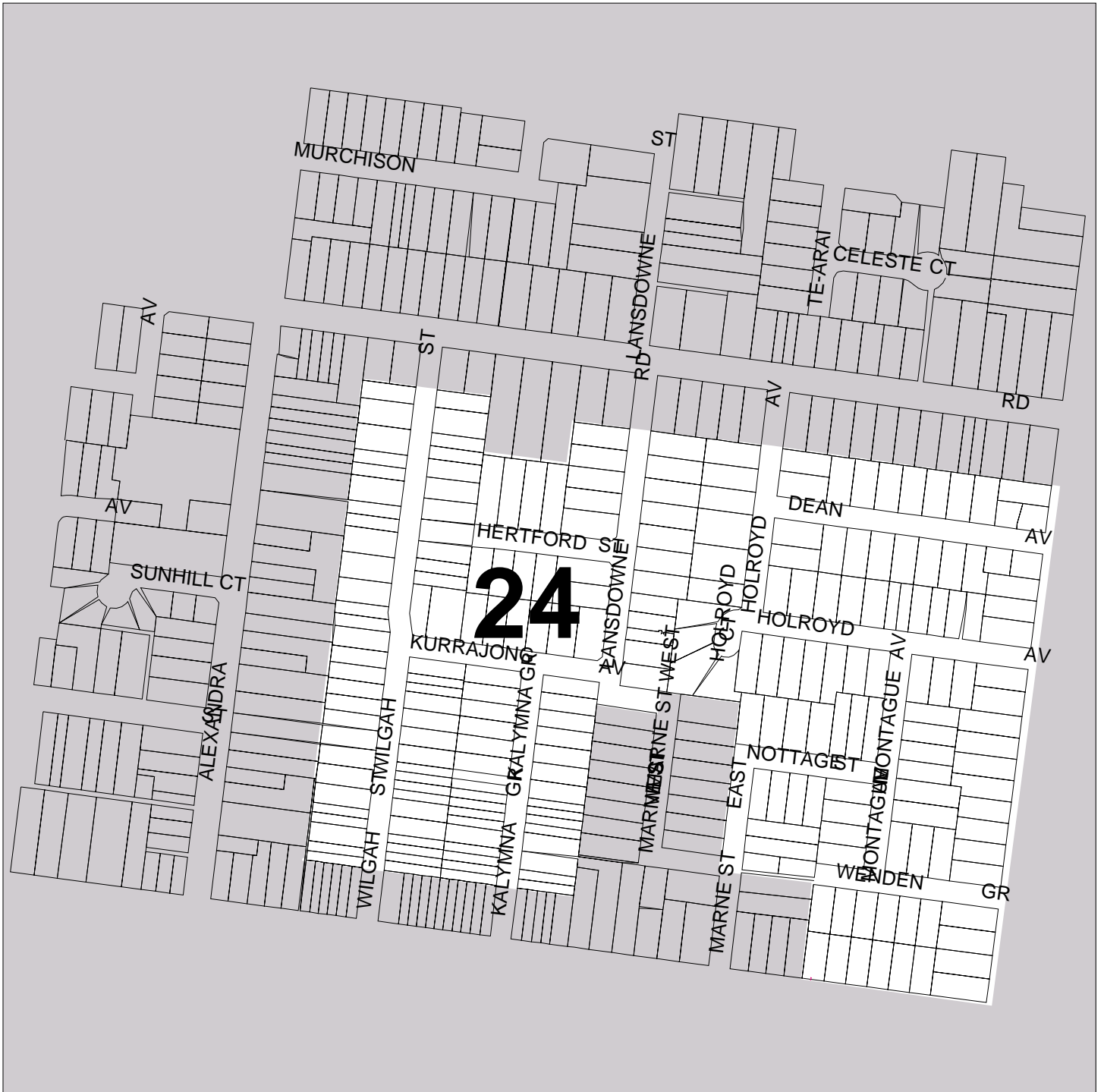
A comprehensive urban character assessment and policy statement for the area is available from the Council .



City of Port Phillip
URBAN CHARACTER DESCRIPTION

AREA 24: LAND GENERALLY BOUNDED BY ORRONG ROAD, ALMA ROAD, INKERMANS STREET AND WILGAH STREET, ST KILDA

The following description should be used in carrying out the site analysis and design response to your site. Pay attention to the very important and important character elements listed here, as well as the actual conditions observed at the site analysis stage.



Boundary streets:

Orrong Rd, Alma Rd, Wilgah St, Inkerman St.

Streets in the area:

Dean Av, Hertford St, Holroyd Av, Holroyd Ct, Kalymna Gv, Kurrajong Av, Lansdowne Rd, Marne St, Montague Av, Nottage St, Wenden Gv, Wilgah St,

Description of the character of the area:

This area was primarily developed in the inter-war period with a fairly consistent subdivision pattern. The area still retains most of its original housing stock of medium sized brick and stucco detached houses with attractive front gardens and consistent front fencing.

There is some redevelopment with flat blocks in Wilgah Street, however the balance of the area is relatively free from walk-up flat buildings and new replacement housing.

The western and southern parts of this area are quite steeply sloping adding to the interest of the streetscapes.

Streets are consistent in their scale and most retain their original bluestone kerbs and channels. Street trees include a mix of species that vary in size however the overall effect provides an established streetscape appearance. The area includes some exceptional flowering gum specimens.

<i>Element</i>	<i>Description</i>
Dominant style	1920s - 1940s
Dominant scale	one storey
Dominant material	stucco and render/minor variation
Dominant roof	pitched/terracotta
Setback	5-9 metres/minor variation
Heritage	no heritage place identified
Gardens	visible/medium
Fences	low-medium height/varied materials

The existing urban character of the City of Port Phillip has been assessed using 24 character elements. The design should especially respond to the important and very important elements, which in this area are:

<i>Element</i>	<i>Importance</i>
Landform	Very important
Front gardens	Very important
Building style	Very important
Building scale	Very important
Views	Important
Pattern	Important
Street trees	Important
Setbacks	Important
Housing type	Important

A comprehensive urban character assessment and policy statement for the area is available from the Council .



City of Port Phillip

URBAN CHARACTER DESCRIPTION

AREA 25: MURCHISON STREET, ST KILDA

The following description should be used in carrying out the site analysis and design response to your site.

Pay attention to the very important and important character elements listed here, as well as the actual conditions observed at the site analysis stage.



Boundary streets:

Alexandra St, Lansdowne St, Murchison St.

Streets in the area:

Murchison St,

Description of the character of the area:

Developed during the inter-war period, the area retains much of its original housing stock comprising detached houses and apartments with attractive front gardens and consistent front fences.

Most of this area slopes steeply to the west which adds to the interest of the streetscapes.

Streets flanked by large plane trees retain the original bluestone kerbs, channels, bitumen footpaths and some bitumen crossovers.

<i>Element</i>	<i>Description</i>
Dominant style	1920s - 1940s
Dominant scale	one storey
Dominant material	stucco and render/minor variation
Dominant roof	pitched/terracotta
Setback	5-9 metres/consistent
Heritage	no identified heritage places
Gardens	visible/medium
Fences	low height/consistent

The existing urban character of the City of Port Phillip has been assessed using 24 character elements. The design should especially respond to the important and very important elements, which in this area are:

<i>Element</i>	<i>Importance</i>
Landform	Very important
Street trees	Very important
Front gardens	Very important
Building style	Very important
Pattern	Important
Fences	Important
Setbacks	Important
Building scale	Important
Housing type	Important
Heritage	Important
Land use	Important

A comprehensive urban character assessment and policy statement for the area is available from the Council .



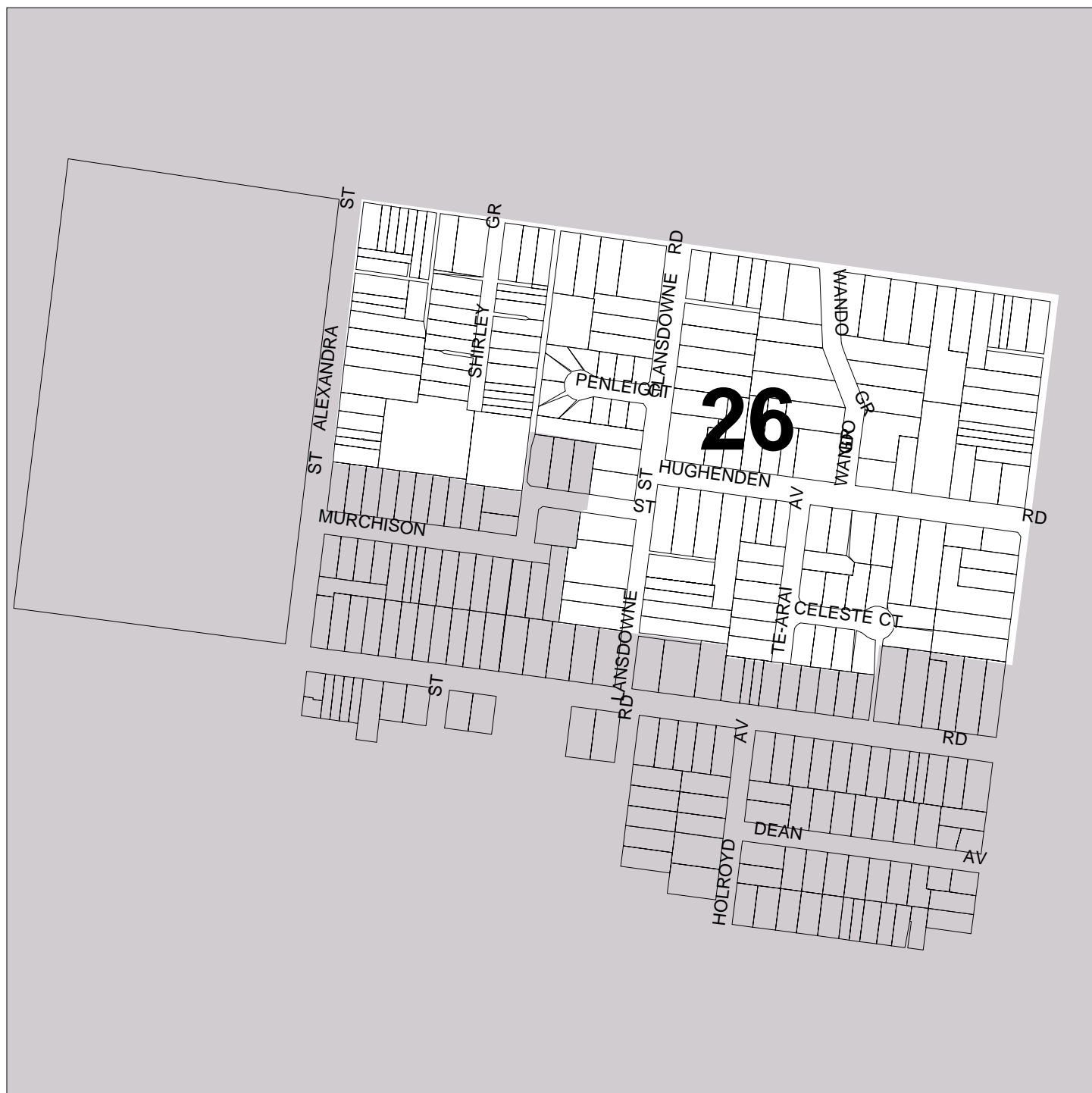
City of Port Phillip

URBAN CHARACTER DESCRIPTION

AREA 26: LAND GENERALLY BOUNDED BY ORRONG ROAD, ALMA ROAD, ALEXANDRA STREET AND DANDENONG ROAD, ST KILDA

The following description should be used in carrying out the site analysis and design response to your site.

Pay attention to the very important and important character elements listed here, as well as the actual conditions observed at the site analysis stage.



Dandenong Rd, Orrong Rd, Alma Rd, Alexandra St.

Streets in the area:

Alexandra St, Celeste Ct, Hughenden Rd, Lansdowne Rd, Penleigh Ct, Shirley Gv, Te-arai Av, Wando Gv,

Description of the character of the area:

The area appears to have been subdivided and developed in the Edwardian and inter-war period, with a high proportion of large allotments. These large lots attracted extensive redevelopment during the 1960's and 1970's with two and three storey walk-up flat blocks dominating the character of the area. The area now has a diverse mix of building types and styles at various set backs and height levels.

Streets were originally developed with bluestone kerbs, channels and nature strips, however, many now have concrete footpaths, crossings and kerbs .

The area has some mature street trees including flowering gum, liquid amber and others, although recent plantings with small scale mixed natives has changed the character to a certain degree.

<i>Element</i>	<i>Description</i>
Dominant style	1950s - 1960s
Dominant scale	one, two and three storey
Dominant material	brick or masonry/mixed
Dominant roof	flat/metal
Setback	less than 5 metres/mixed
Heritage	limited number of heritage places
Gardens	visible/medium/small
Fences	various heights/various materials

The existing urban character of the City of Port Phillip has been assessed using 24 character elements. The design should especially respond to the important and very important elements, which in this area are:

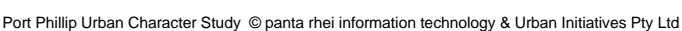
<i>Element</i>	<i>Importance</i>
Housing type	Very important
Lot size	Important
Street trees	Important
Building scale	Important
Access	Important

A comprehensive urban character assessment and policy statement for the area is available from the Council .



AREA 27: LAND GENERALLY BOUNDED BY DANDENONG ROAD, HOTHAM STREET, FULTON STREET AND WESTBURY STREET, ST KILDA

Pay attention to the very important and important character elements listed here, as well as the actual conditions observed at the site analysis stage.



Boundary streets:

Dandenong Rd, Fulton St, Westbury St, Hotham St.

Streets in the area:

Fulton St, Johnson St, Pilley St, Westbury St,

Description of the character of the area:

The area has been subdivided and developed in many stages from the time of its original development. This is reflected in the wide range of lot sizes although the lots are predominantly large.

Whilst the large lots have attracted extensive redevelopment in both the inter-war and post-war periods with large apartment blocks dominating the character of the area, small pockets of the area remain intact from the original phase of development. Johnson and Reilly Streets contain consistent housing from the Edwardian and inter-war periods and Palm Court is a fine example of inter-war houses and flats.

The streets are medium to wide in their scale and still retain original bluestone kerb and channel detailing. These are partially diminished by the numerous concrete crossovers and some sections of concrete footpaths.

Street trees in this area are generally inconsistent and out of scale with the streets.

<i>Element</i>	<i>Description</i>
Dominant style	1950s -1960s
Dominant scale	three storey
Dominant material	brick or masonry/mixed
Dominant roof	pitched/metal
Setback	less than 5 metres/varied
Heritage	some identified heritage places
Gardens	visible/medium/small
Fences	low height/various materials

The existing urban character of the City of Port Phillip has been assessed using 24 character elements. The design should especially respond to the important and very important elements, which in this area are:

<i>Element</i>	<i>Importance</i>
Building scale	Very important
Housing type	Very important
Lot size	Important
Parking	Important
Setbacks	Important
Building style	Important
Access	Important

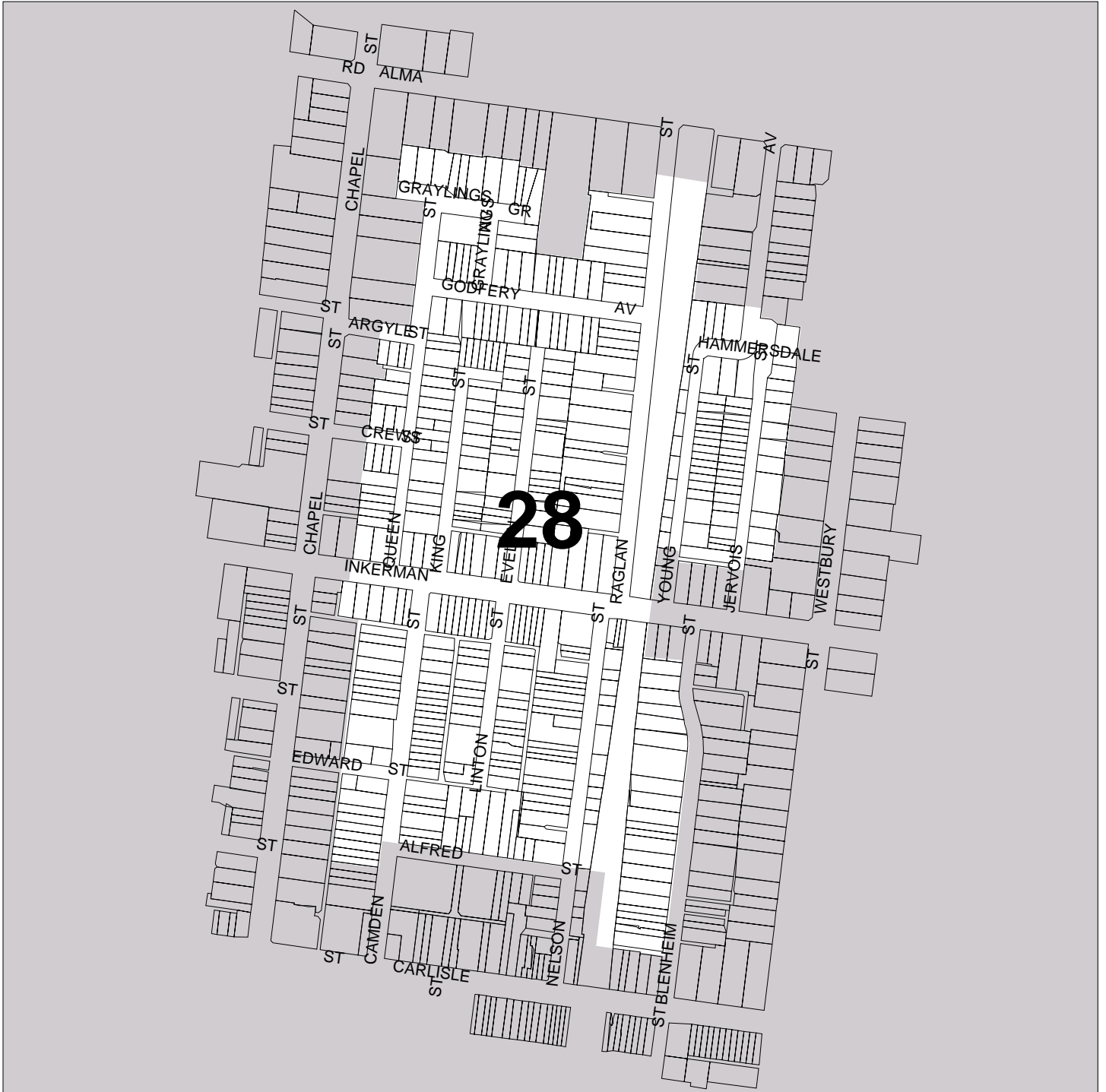
A comprehensive urban character assessment and policy statement for the area is available from the Council .



City of Port Phillip
URBAN CHARACTER DESCRIPTION

**AREA 28: LAND GENERALLY BOUND BY ALFRED STREET, CHAPEL STREET, ALMA ROAD
AND JERVOIS STREET, ST KILDA**

The following description should be used in carrying out the site analysis and design response to your site.
Pay attention to the very important and important character elements listed here, as well as the actual conditions
observed at the site analysis stage.



Boundary streets:

Alfred St, Chapel St, Alma Rd, Jervois St.

Streets in the area:

Camden St, Evelyn St, Godfrey Av, Graylings Av, Graylings Gv, Inkerman St, Jervois St, King St, Linton St, Nelson St, Phillips St, Queen St, Raglan St, Young St,

Description of the character of the area:

The area is distinguished from adjoining areas by its smaller scale streets and lots. This has discouraged development of flat buildings so much of the original housing stock from the Victorian, Edwardian, and inter-war periods remains relatively intact.

The streets retain much of their original bluestone detailing and some streets such as Raglan Street, Graylings Grove and Avenue and Godfrey Street, are flanked by mature Plane trees which enhance the character of these streets. Other streets with fewer trees derive their character from the diversity of architectural styles and the small scale of the streets and buildings.

The area is close to neighbourhood shopping centre, community facilities and transport and Alma Park provides substantial open space.

<i>Element</i>	<i>Description</i>
Dominant style	1900s - 1920s
Dominant scale	one storey
Dominant material	brick or masonry
Dominant roof	pitched/terracotta
Setback	less than 5 metres/minor variation
Heritage	@50% of area identified as heritage place
Gardens	visible/medium/small
Fences	medium height/various materials

The existing urban character of the City of Port Phillip has been assessed using 24 character elements. The design should especially respond to the important and very important elements, which in this area are:

<i>Element</i>	<i>Importance</i>
Building style	Very important
Building scale	Very important
Access	Very important
Lot size	Important
Pattern	Important
Setbacks	Important
Housing type	Important

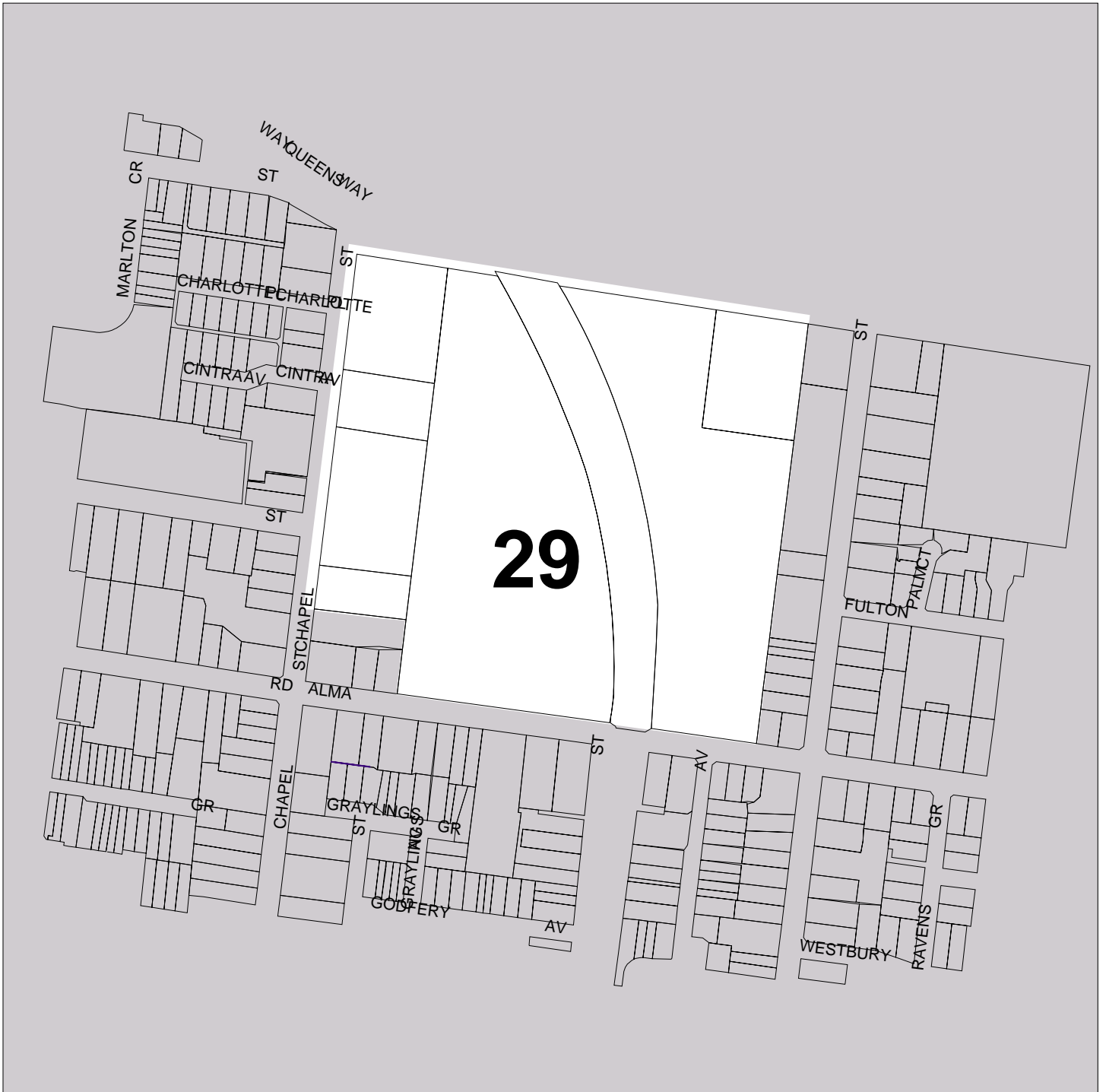
A comprehensive urban character assessment and policy statement for the area is available from the Council .



City of Port Phillip
URBAN CHARACTER DESCRIPTION

AREA 29: LAND GENERALLY BOUNDED BY DANDENONG ROAD, CHAPEL STREET, ALMA ROAD AND WESTBURY STREET

The following description should be used in carrying out the site analysis and design response to your site. Pay attention to the very important and important character elements listed here, as well as the actual conditions observed at the site analysis stage.



Dandenong Rd, Chapel St, Alma Rd, Westbury St.

Streets in the area:

Chapel St,

Description of the character of the area:

The area is distinguished from adjoining areas by the large size of the lot and the scale and set back of public buildings in a garden setting. Public buildings include churches, St Mary's school and the army depot

Whilst the Alma Road frontage is characterised by fine buildings, the gardens, front fences and streetscape are of poorer quality and tend to block views into the parkland.

Street trees in Dandenong Road contribute to the character of the area.

<i>Element</i>	<i>Description</i>
Dominant style	pre-1900s
Dominant scale	two storey
Dominant material	brick or masonry/mixed
Dominant roof	steeply pitched/slate
Setback	over 9 metres
Heritage	entire area identified as heritage place
Gardens	visible/large
Fences	none-medium/various materials

The existing urban character of the City of Port Phillip has been assessed using 24 character elements. The design should especially respond to the important and very important elements, which in this area are:

<i>Element</i>	<i>Importance</i>
Lot size	Very important
Heritage	Very important
Land use	Very important
Access	Very important
Open space	Very important
Front gardens	Important
Building style	Important

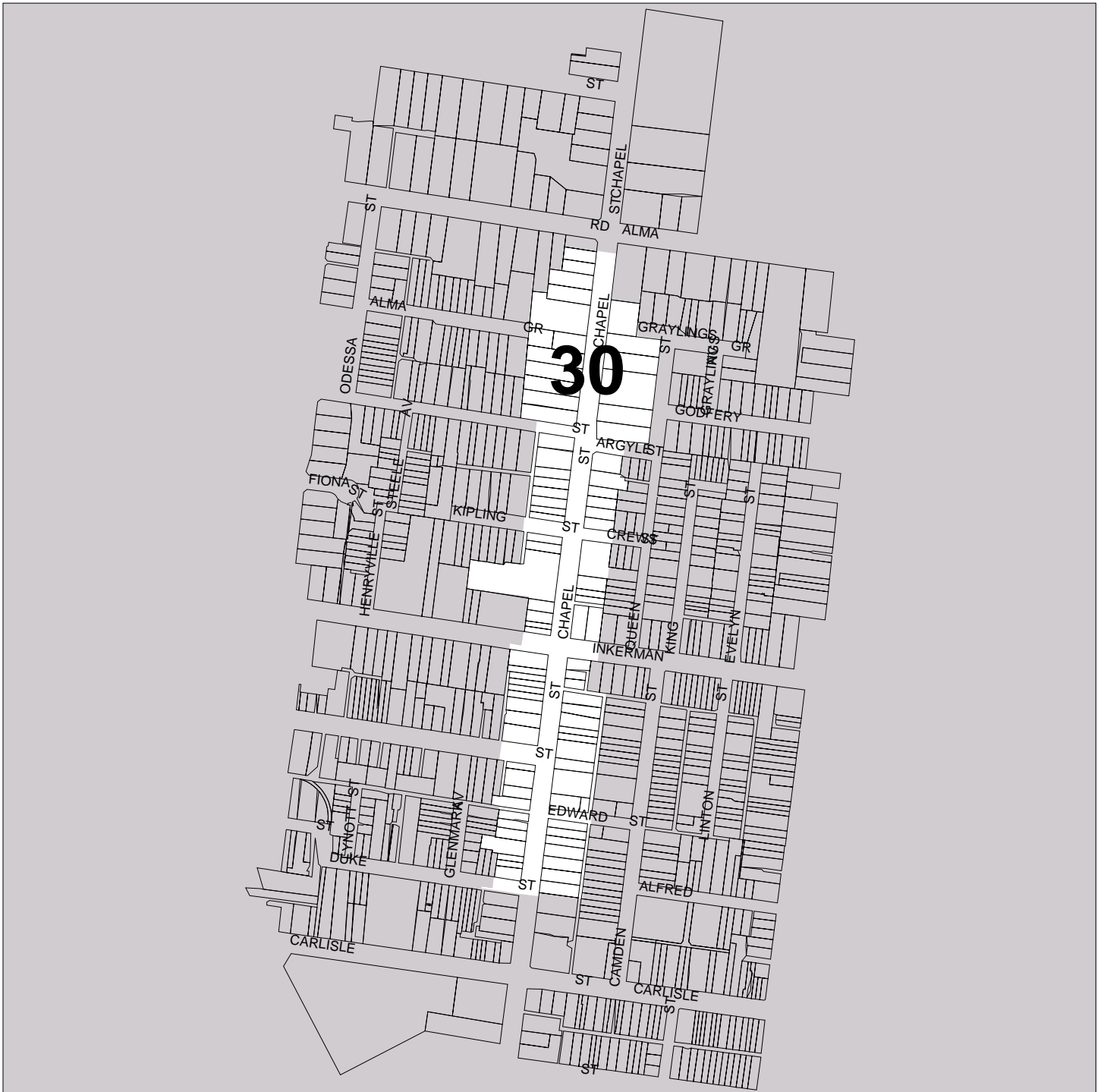
A comprehensive urban character assessment and policy statement for the area is available from the Council .



City of Port Phillip
URBAN CHARACTER DESCRIPTION

AREA 30: CHAPEL STREET BETWEEN ALMA ROAD AND CARLISLE STREET, ST KILDA

The following description should be used in carrying out the site analysis and design response to your site. Pay attention to the very important and important character elements listed here, as well as the actual conditions observed at the site analysis stage.



Chapel St, between Alma Rd and Carlisle St.

Streets in the area:

Argyle St, Chapel St, Crews St, Edward St,

Description of the character of the area:

The area has a wide variety of lot sizes and frontage widths which has precipitated substantial redevelopment since the Victorian period. This has created a streetscape of varied building styles and scales although flat blocks, particularly those built in the post-war period, are the most visually significant buildings. The diversity of building styles is also reflected in setbacks, front fences and front gardens.

Chapel Street has a wide reservation and wide pavement for carrying high volumes and traffic and also includes the central concreted tram strip. Footpaths and medians are quite narrow and the overhead services tend to be visually dominant. The street has recently been replanted with deciduous trees however some gum trees remain from an earlier planting.

The area has a good access especially to shops, public transport and public facilities.

<i>Element</i>	<i>Description</i>
Dominant style	1970s - 1990s
Dominant scale	one storey
Dominant material	brick or masonry/mixed
Dominant roof	pitched/terracotta
Setback	less than 5 metres/varied
Heritage	limited number of identified heritage places
Gardens	visible/small
Fences	various height and materials

The existing urban character of the City of Port Phillip has been assessed using 24 character elements. The design should especially respond to the important and very important elements, which in this area are:

<i>Element</i>	<i>Importance</i>
Traffic	Very important
Access	Very important
Street dimensions	Important
Overhead services	Important
Building scale	Important
Housing type	Important
Land use	Important

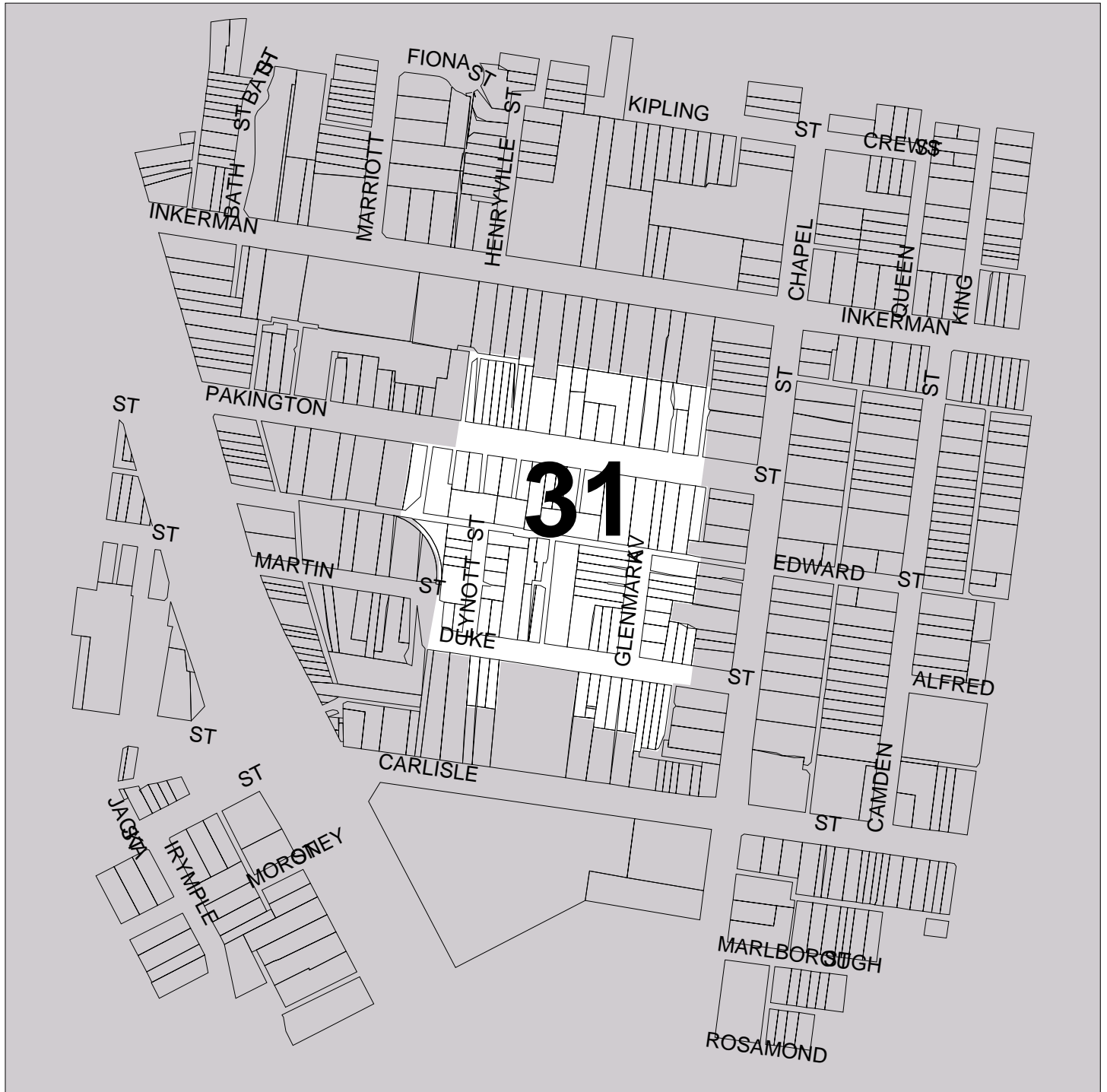
A comprehensive urban character assessment and policy statement for the area is available from the Council .



City of Port Phillip
URBAN CHARACTER DESCRIPTION

AREA 31: LAND GENERALLY BOUNDED BY DUKE STREET, PACKINGTON STREET, CHAPEL STREET AND LYNOTT STREET

The following description should be used in carrying out the site analysis and design response to your site. Pay attention to the very important and important character elements listed here, as well as the actual conditions observed at the site analysis stage.



Duke St, Packington St, Chapel St, Lynott St.

Streets in the area:

Duke St, Glenmark Av, Lynott St, Pakington St,

Description of the character of the area:

This is a small area which has been largely developed with intact timber and brick workers' cottages from the Victorian and Edwardian periods except in Packington Street. Glenmark Avenue is a street of Edwardian duplex houses with excellent mature plane trees.

Although Packington Street includes some original houses, sections of the street have larger blocks which have been redeveloped with flats and houses during the 1960's and 1970's.

A bluestone drain bisects the area and is also an important heritage asset.

Some streets retain original bluestone detailing and a significant feature is a bluestone drain that bisects the area. Other streets, including Packington Street, have been reconstructed with concrete kerbs and channels.

The area has good access to shops services and transport and includes a small pocket park.

<i>Element</i>	<i>Description</i>
Dominant style	pre-1900s
Dominant scale	one storey
Dominant material	timber or cladding/varied
Dominant roof	pitched/metal
Setback	less than 5 metres/minor variation
Heritage	@90% identified heritage places
Gardens	visible/small
Fences	medium height/various materials

The existing urban character of the City of Port Phillip has been assessed using 24 character elements. The design should especially respond to the important and very important elements, which in this area are:

<i>Element</i>	<i>Importance</i>
Building style	Very important
Heritage	Very important
Access	Very important
Lot size	Important
Street dimensions	Important
Parking	Important
Setbacks	Important
Building scale	Important

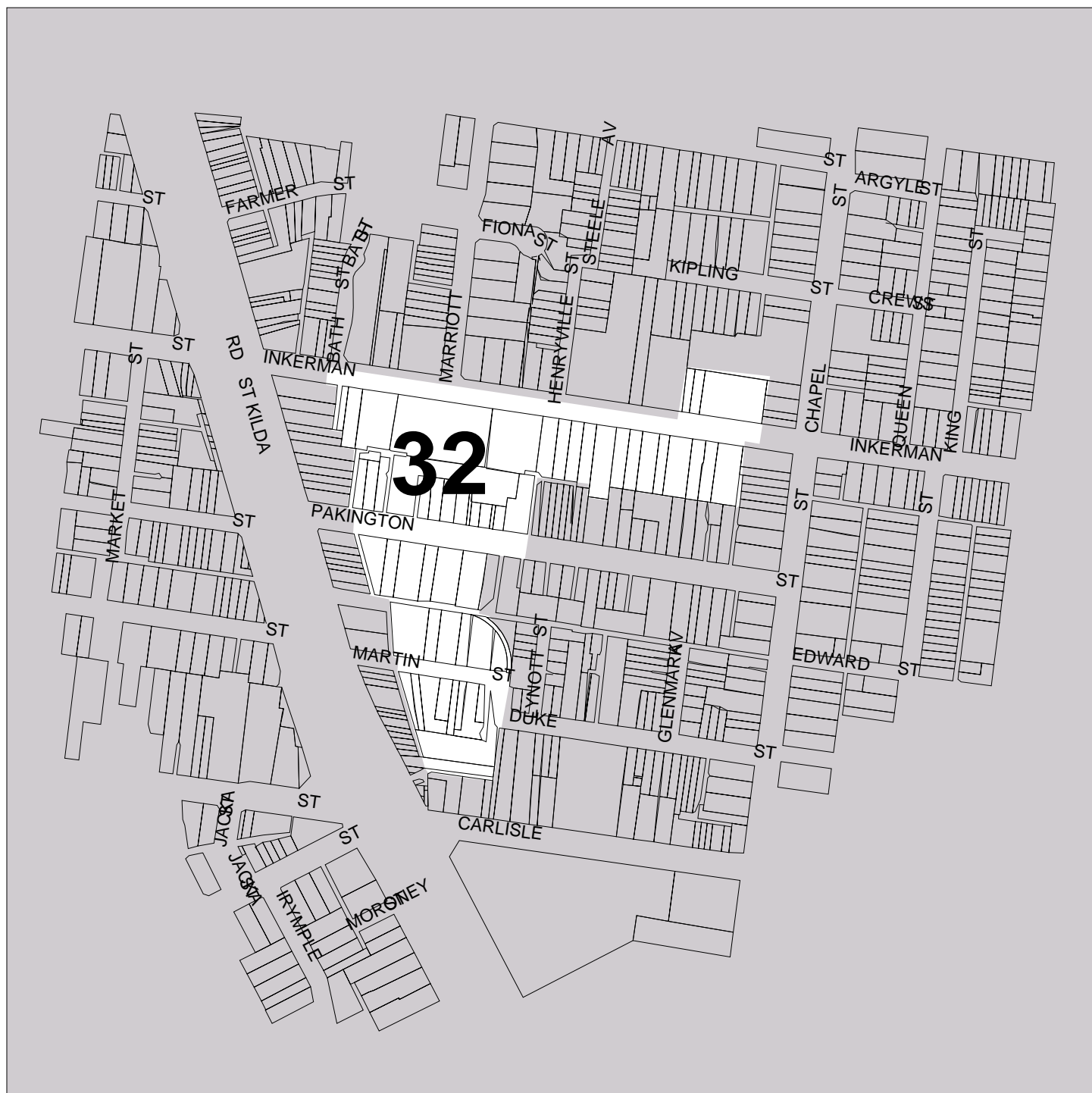
A comprehensive urban character assessment and policy statement for the area is available from the Council .

URBAN CHARACTER DESCRIPTION

AREA 32: LAND GENERALLY BOUNDED BY INKERMANN STREET, CHAPEL STREET, ST KILDA ROAD AND MARTIN STREET

The following description should be used in carrying out the site analysis and design response to your site.

Pay attention to the very important and important character elements listed here, as well as the actual conditions observed at the site analysis stage.



Inkerman St, Chapel St, St Kilda Rd, Martin St.

Streets in the area:

Inkerman St, Marlborough St,

Description of the character of the area:

This is a small area behind St Kilda Road distinguished from adjoining areas by its mixed land use and larger lot sizes. Industrial uses are concentrated in Martin and Packington Streets whilst properties fronting Inkerman Street have been developed as car outlets, offices and car parks together with original houses and flats.

Factory buildings in Packington and Martin Streets are generally poor design standard and on-site parking and crossings dominate the street frontage.

Streets lack significant street trees and signage of mixed quality has a significant presence.

The area is close to a neighbourhood shopping centre, community facilities and public transport.

<i>Element</i>	<i>Description</i>
Dominant style	1970s -1990s
Dominant scale	two storey
Dominant material	brick or masonry/mixed
Dominant roof	pitched/metal
Setback	less than 5 metres/minor variation
Heritage	virtually no identified heritage places
Gardens	visible/small
Fences	various height and materials

The existing urban character of the City of Port Phillip has been assessed using 24 character elements. The design should especially respond to the important and very important elements, which in this area are:

<i>Element</i>	<i>Importance</i>
Parking	Very important
Signage	Important
Setbacks	Important
Land use	Important
Access	Important

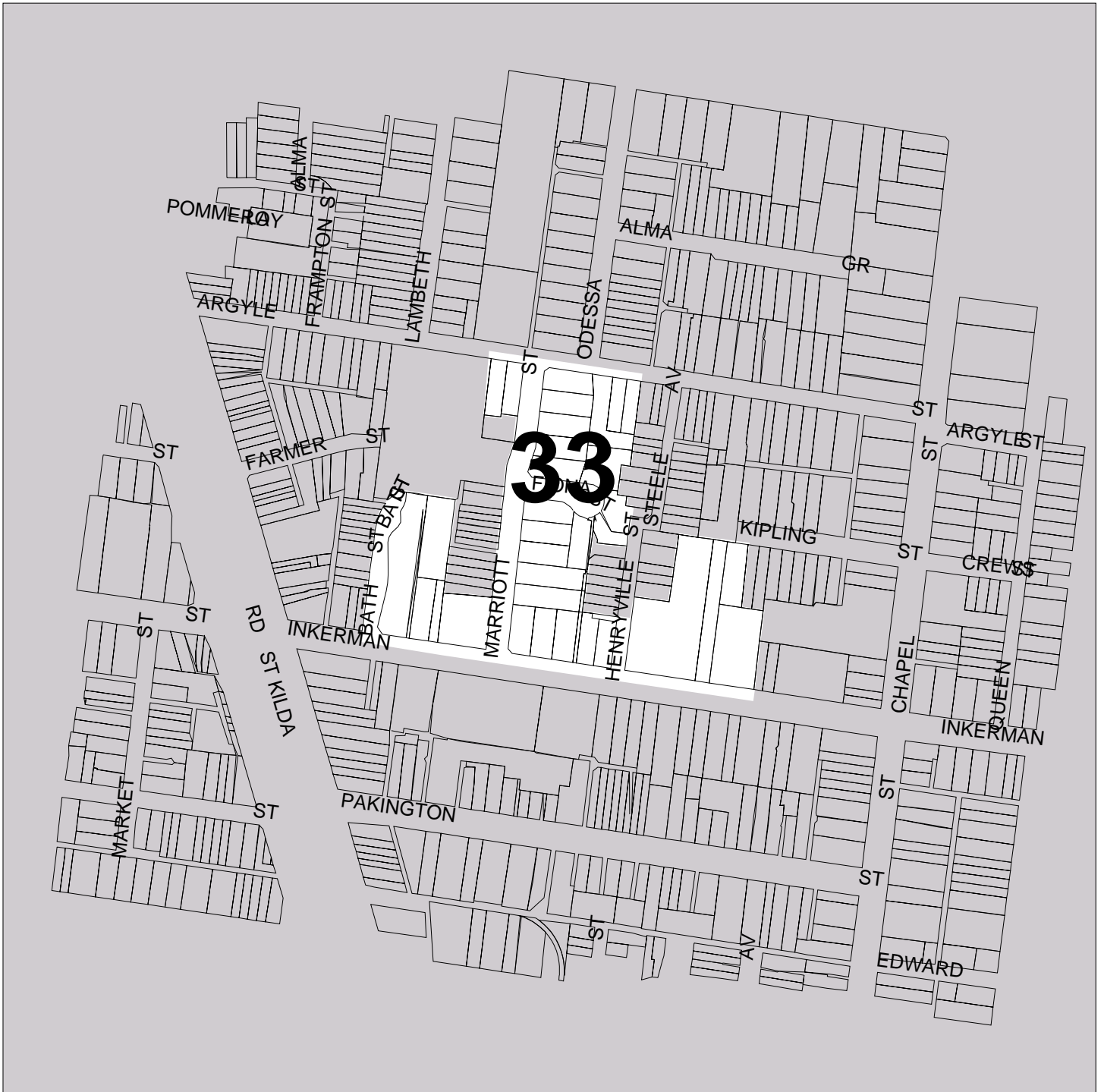
A comprehensive urban character assessment and policy statement for the area is available from the Council .



City of Port Phillip
URBAN CHARACTER DESCRIPTION

AREA 33: LAND GENERALLY BOUND BY INKERMAN STREET, BATH STREET, ARGYLE STREET AND KIPLING STREET

The following description should be used in carrying out the site analysis and design response to your site. Pay attention to the very important and important character elements listed here, as well as the actual conditions observed at the site analysis stage.



Inkerman St, Bath St, Argyle St, Kipling St.

Streets in the area:

Bath St, Fiona Ct, Inkerman St, Marriott St,

Description of the character of the area:

The area is predominantly medium and larger sized lots which have been redeveloped in the post-war period with a mixture of public and private flat developments. The public housing includes walk up blocks and a thirteen storey high rise block set in landscaped surrounds. The balance of the area consists of a number of mostly three storey cream brick flat blocks with small setbacks for off-street car parking.

The high rise building has extensive communal landscaped areas contrasting with private flat developments which have minimal landscaping.

Some original blue stone kerbs and channels remain from an earlier period. Street trees have little impact and parked cars are visually dominant.

<i>Element</i>	<i>Description</i>
Dominant style	1970s - 1990s
Dominant scale	three storey
Dominant material	brick or masonry
Dominant roof	pitched/terracotta
Setback	less than 5 metres/consistent
Heritage	virtually no identified heritage places
Gardens	visible/large/small
Fences	low-medium/various materials

The existing urban character of the City of Port Phillip has been assessed using 24 character elements. The design should especially respond to the important and very important elements, which in this area are:

<i>Element</i>	<i>Importance</i>
Building scale	Very important
Housing type	Very important
Lot size	Important
Parking	Important
Setbacks	Important
Building style	Important
Access	Important

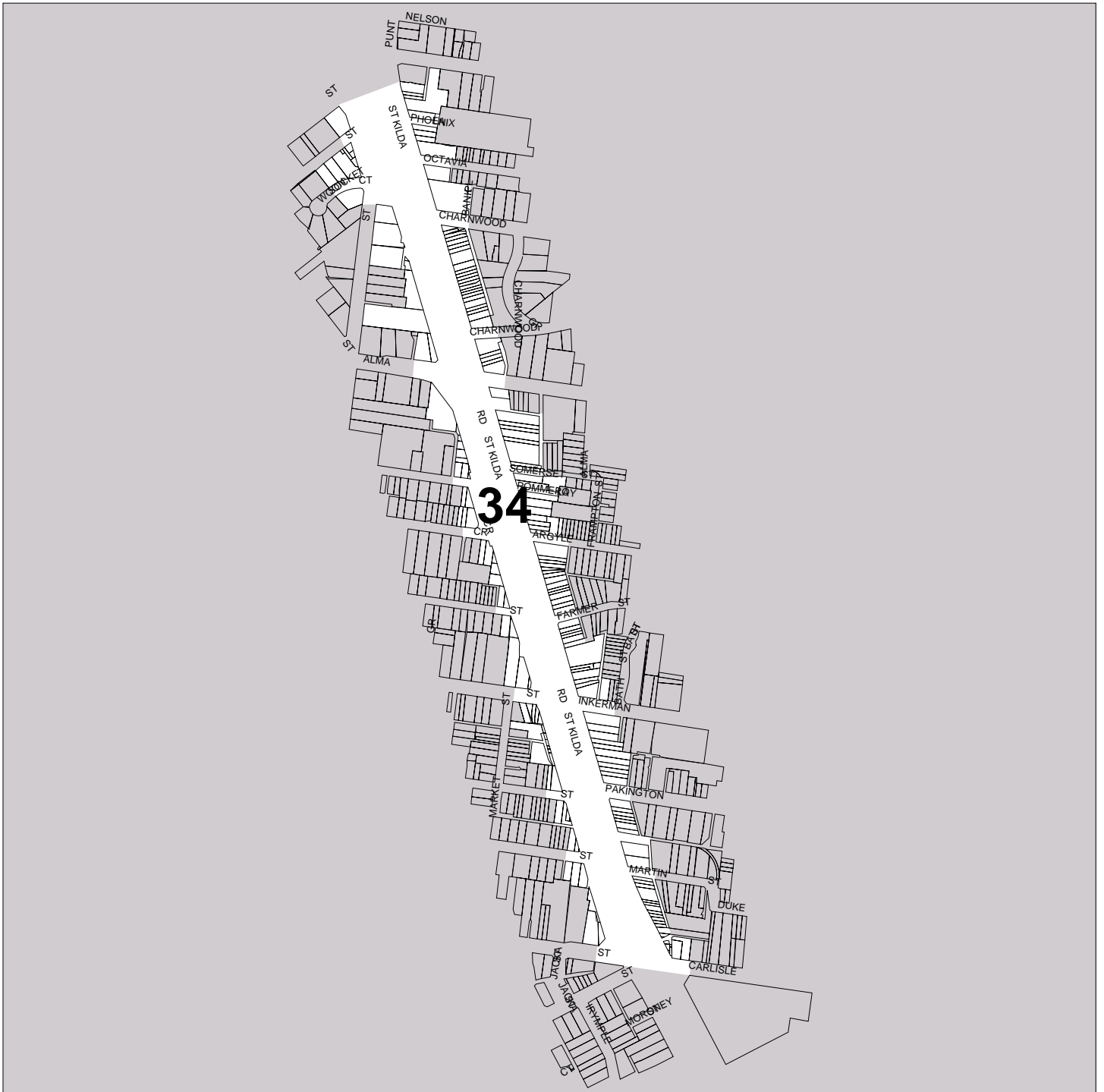
A comprehensive urban character assessment and policy statement for the area is available from the Council .



City of Port Phillip
URBAN CHARACTER DESCRIPTION

AREA 34: ST KILDA ROAD BETWEEN CARLISLE STREET AND FITZROY STREET, ST KILDA

The following description should be used in carrying out the site analysis and design response to your site.
Pay attention to the very important and important character elements listed here, as well as the actual conditions observed at the site analysis stage.



St Kilda Rd between Carlisle St and Fitzroy St.

Streets in the area:

Barkly St, Market St, Phoenix La, Pummeroy St, St Kilda Rd, St Kilda Rd, Woonsocket Ct,

Description of the character of the area:

The road was originally a typical strip shopping centre but Victorian commercial buildings were removed on the western side to create four traffic lanes in each direction, parking lanes and a wide median strip incorporating the tram line. The dominant characteristics of this new reservation are a wide street profile, two distinctly different edges, heavy traffic and the median with its semi-mature spotted gums.

The original development on the eastern side is mostly terraced shops and other commercial buildings, mostly dating from the Victorian period. Unfortunately, the heritage qualities of buildings along this edge have been undermined by poor renovation and uncontrolled signage.

The area sits between two neighbourhood shopping centres and has good tram access.

<i>Element</i>	<i>Description</i>
Dominant style	pre-1900s
Dominant scale	two storey
Dominant material	stucco and render/minor variation
Dominant roof	pitched/concealed by parapets
Setback	less than 5 metres/varied
Heritage	limited number of heritage places
Gardens	none
Fences	none

The existing urban character of the City of Port Phillip has been assessed using 24 character elements. The design should especially respond to the important and very important elements, which in this area are:

<i>Element</i>	<i>Importance</i>
Street dimensions	Very important
Traffic	Very important
Landform	Important
Street trees	Important
Signage	Important
Land use	Important
Access	Important

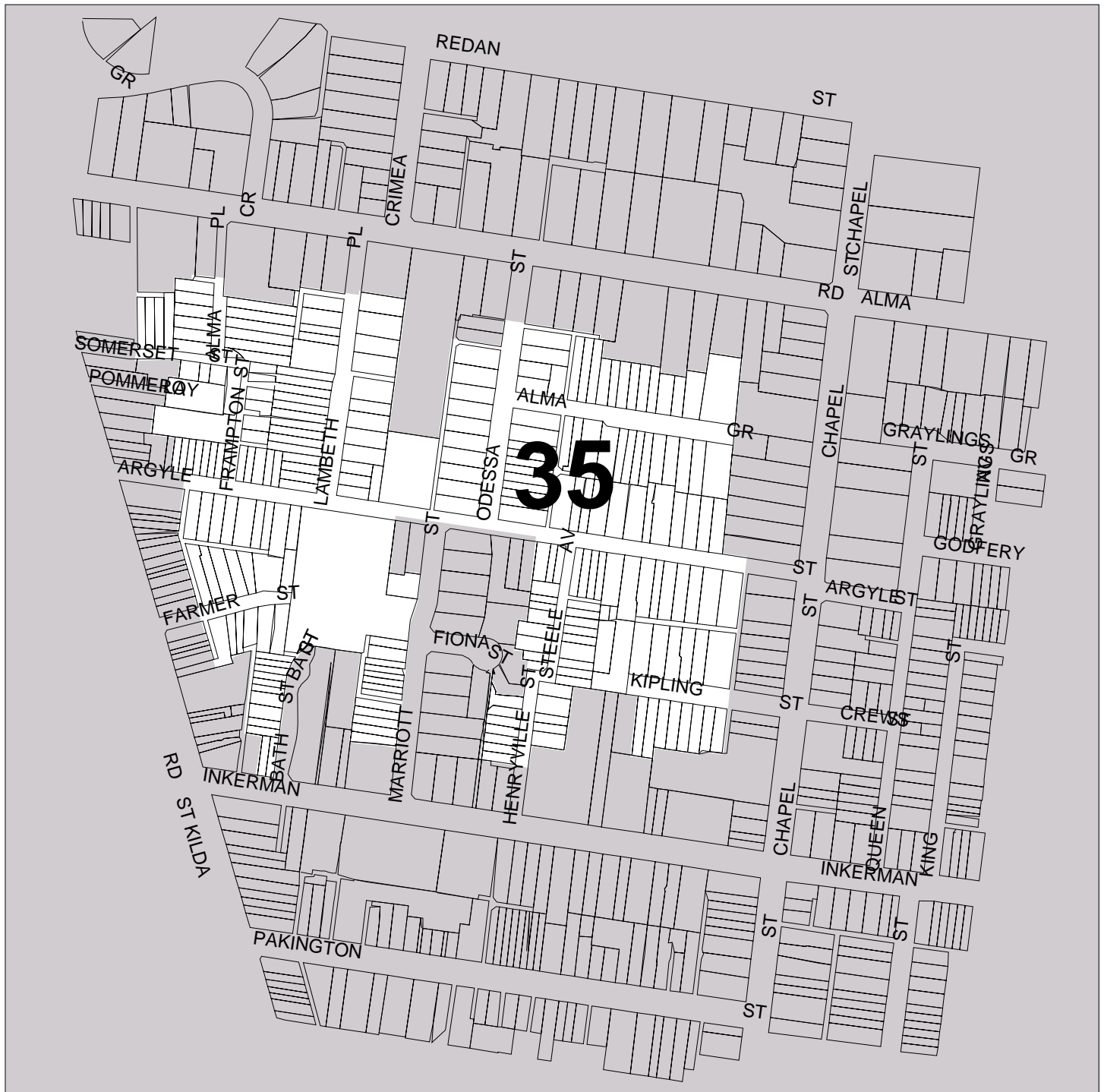
A comprehensive urban character assessment and policy statement for the area is available from the Council .



City of Port Phillip
URBAN CHARACTER DESCRIPTION

AREA 35: LAND GENERALLY BOUNDED BY ALMA ROAD, CHAPEL STREET, INKERMANS STREET AND ST KILDA ROAD

The following description should be used in carrying out the site analysis and design response to your site. Pay attention to the very important and important character elements listed here, as well as the actual conditions observed at the site analysis stage.



Alma Rd, St Kilda Rd, Chapel St, Inkerman St.

Streets in the area:

Alma Gv, Argyle St, Farmer St, Frampton St, Henryville St, Kipling St, Lambeth Pl, Odessa St, Somerset St, Steele Av,

Description of the character of the area:

The area is a complex, small scale subdivision with many medium and narrow streets and lanes. The lot size is typically small and duplex houses are common. Housing has been primarily developed during the Edwardian period although there are examples of Victorian and inter-war housing styles.

There has been limited redevelopment in the form of public housing and a limited number of private flat developments, so the area retains its small single storey scale.

Most streets retain original bluestone and bitumen detailing. Smaller native trees planted in the past few decades do not contribute greatly to the area.

The area is somewhat distant from a neighbourhood shopping centre and is densely settled.

<i>Element</i>	<i>Description</i>
Dominant style	1900s - 1920s
Dominant scale	one storey
Dominant material	brick or masonry/mixed
Dominant roof	pitched/terracotta
Setback	less than 5 metres/minor variation
Heritage	limited number of identified heritage places
Gardens	visible/medium/small
Fences	low-medium/various materials

The existing urban character of the City of Port Phillip has been assessed using 24 character elements. The design should especially respond to the important and very important elements, which in this area are:

<i>Element</i>	<i>Importance</i>
Building scale	Very important
Parking	Important
Street detailing	Important
Fences	Important
Setbacks	Important
Front gardens	Important
Building style	Important
Housing type	Important
Heritage	Important
Access	Important

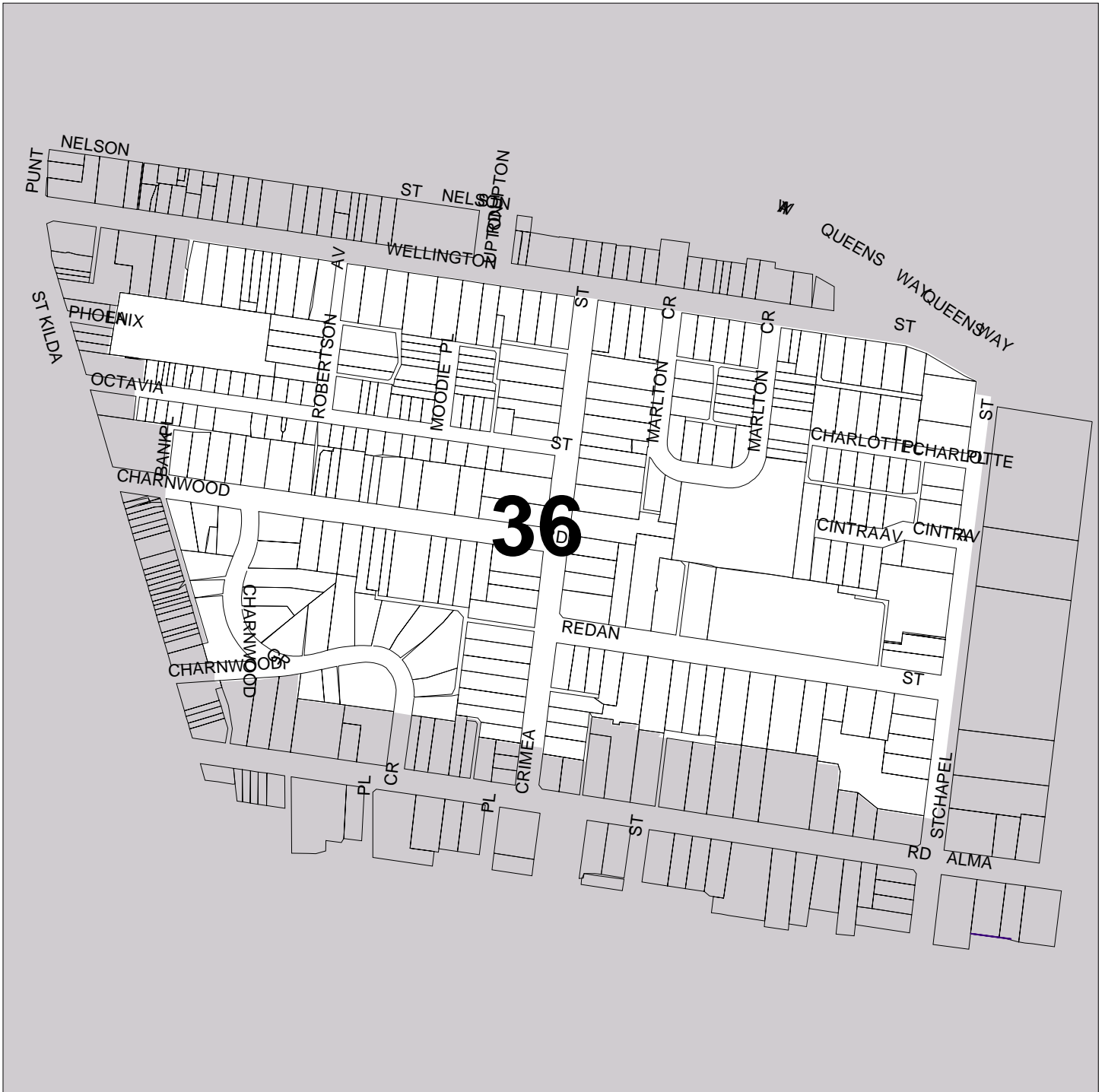
A comprehensive urban character assessment and policy statement for the area is available from the Council .



City of Port Phillip
URBAN CHARACTER DESCRIPTION

AREA 36: LAND GENERALLY BOUNDED BY WELLINGTON STREET, CHAPEL STREET, ST KILDA ROAD AND ALMA ROAD

The following description should be used in carrying out the site analysis and design response to your site.
Pay attention to the very important and important character elements listed here, as well as the actual conditions observed at the site analysis stage.



Wellington St, St Kilda Rd, Chapel St, Alma Rd.

Streets in the area:

Bank Pl, Charlotte Pl, Charnwood Cr, Charnwood Gv, Charnwood Rd, Cintra Av, Crimea St, Marlton Cr, Moodie Pl, Octavia St, Redan St, Robertson Av,

Description of the character of the area:

The area has a pattern of wide and narrow streets including some cul-de-sacs and curvilinear streets. Lot sizes vary but are predominantly quite large.

The area has been developed almost continuously from the Victorian period and many large houses from this period sit with large flat buildings from the inter and post-war period. St Michael's Grammar is a major non-residential land use in the area however it is reasonably well integrated in its urban design character

Most of the streets in this area are lined with mature Plane trees which are very important to the area's cohesiveness and character.

<i>Element</i>	<i>Description</i>
Dominant style	pre-1900s
Dominant scale	one, two and three stories
Dominant material	stucco and render/mixed
Dominant roof	pitched/metal
Setback	less than 5 metres/minor variation
Heritage	more than 75% identified as heritage place
Gardens	visible/medium
Fences	low-medium/various materials

The existing urban character of the City of Port Phillip has been assessed using 24 character elements. The design should especially respond to the important and very important elements, which in this area are:

<i>Element</i>	<i>Importance</i>
Street trees	Very important
Building style	Very important
Heritage	Very important
Landform	Important
Lot size	Important
Parking	Important
Building scale	Important
Housing type	Important
Access	Important

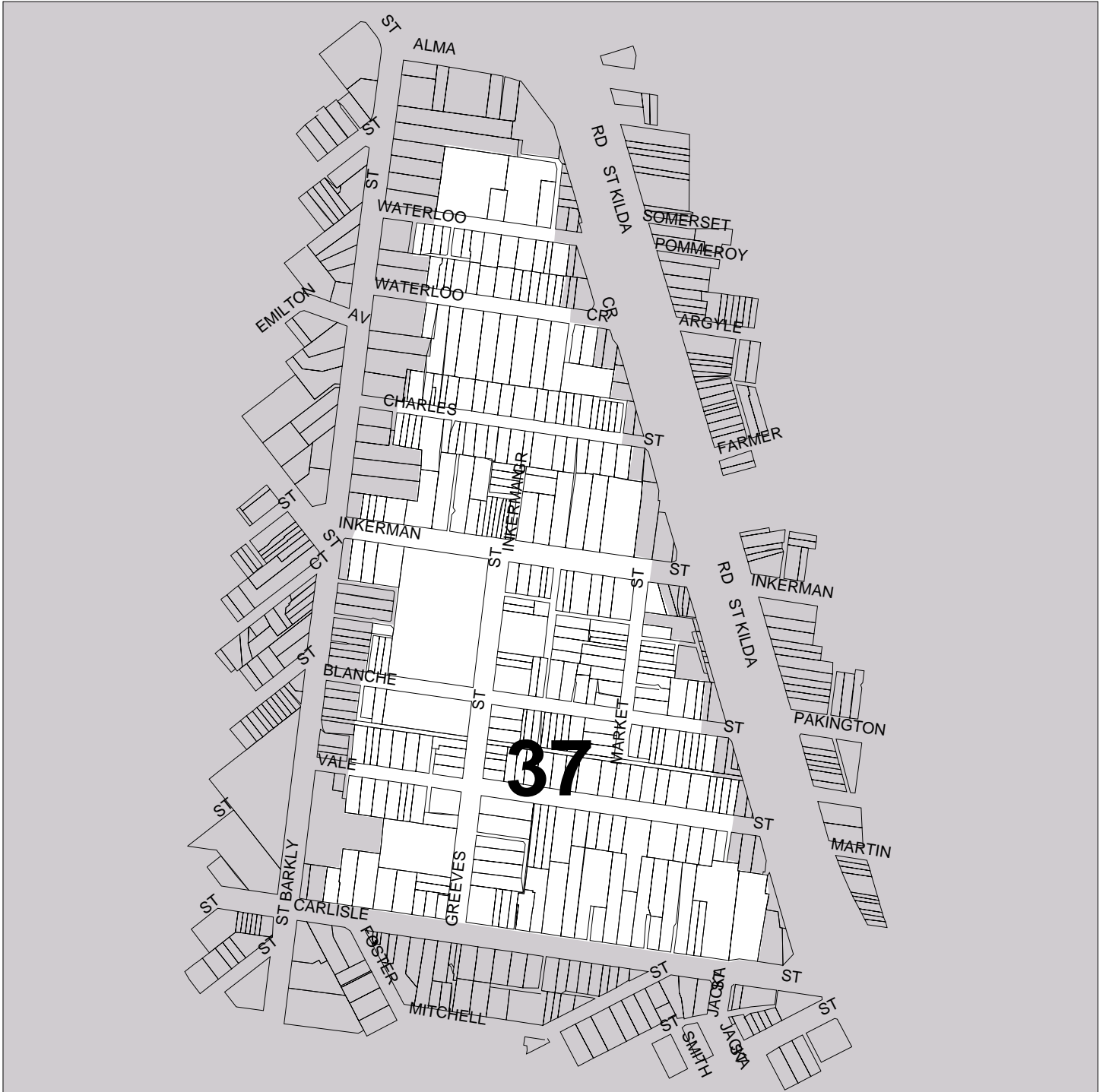
A comprehensive urban character assessment and policy statement for the area is available from the Council .



City of Port Phillip
URBAN CHARACTER DESCRIPTION

AREA 37: LAND GENERALLY BOUNDED BY ST KILDA ROAD, CARLISLE STREET AND BARKLY STREET

The following description should be used in carrying out the site analysis and design response to your site. Pay attention to the very important and important character elements listed here, as well as the actual conditions observed at the site analysis stage.



St Kilda Rd, Alma Rd, Barkley St, Inkerman St.

Streets in the area:

Barkly St, Blanche St, Charles St, Greeves St, Inkerman Gv, Inkerman St, Lyell St, St Kilda Rd, Vale St, Waterloo Cr,

Description of the character of the area:

Streets mostly run east-west and are quite varied in their dimensions. Lot sizes are also extremely varied.

The area has been developed with modest artisan housing from the Victorian and Edwardian periods although there has been considerable development of larger sites for multi unit development during the post-war periods. Housing close to St Kilda Road that is intermingled with light industry appears to be relatively run down.

The commercial and light industrial uses along Inkerman Street and further south together with residential use is perhaps the area's most outstanding characteristic.

Streets still retain most of their original details and street trees are generally inconsistent with smaller scale natives that do not contribute to the area's character.

<i>Element</i>	<i>Description</i>
Dominant style	pre-1900s
Dominant scale	one storey
Dominant material	brick or masonry/mixed
Dominant roof	pitched/metal
Setback	less than 5 metres/minor variation
Heritage	limited number of identified heritage places
Gardens	visible/medium
Fences	medium height/various materials

The existing urban character of the City of Port Phillip has been assessed using 24 character elements. The design should especially respond to the important and very important elements, which in this area are:

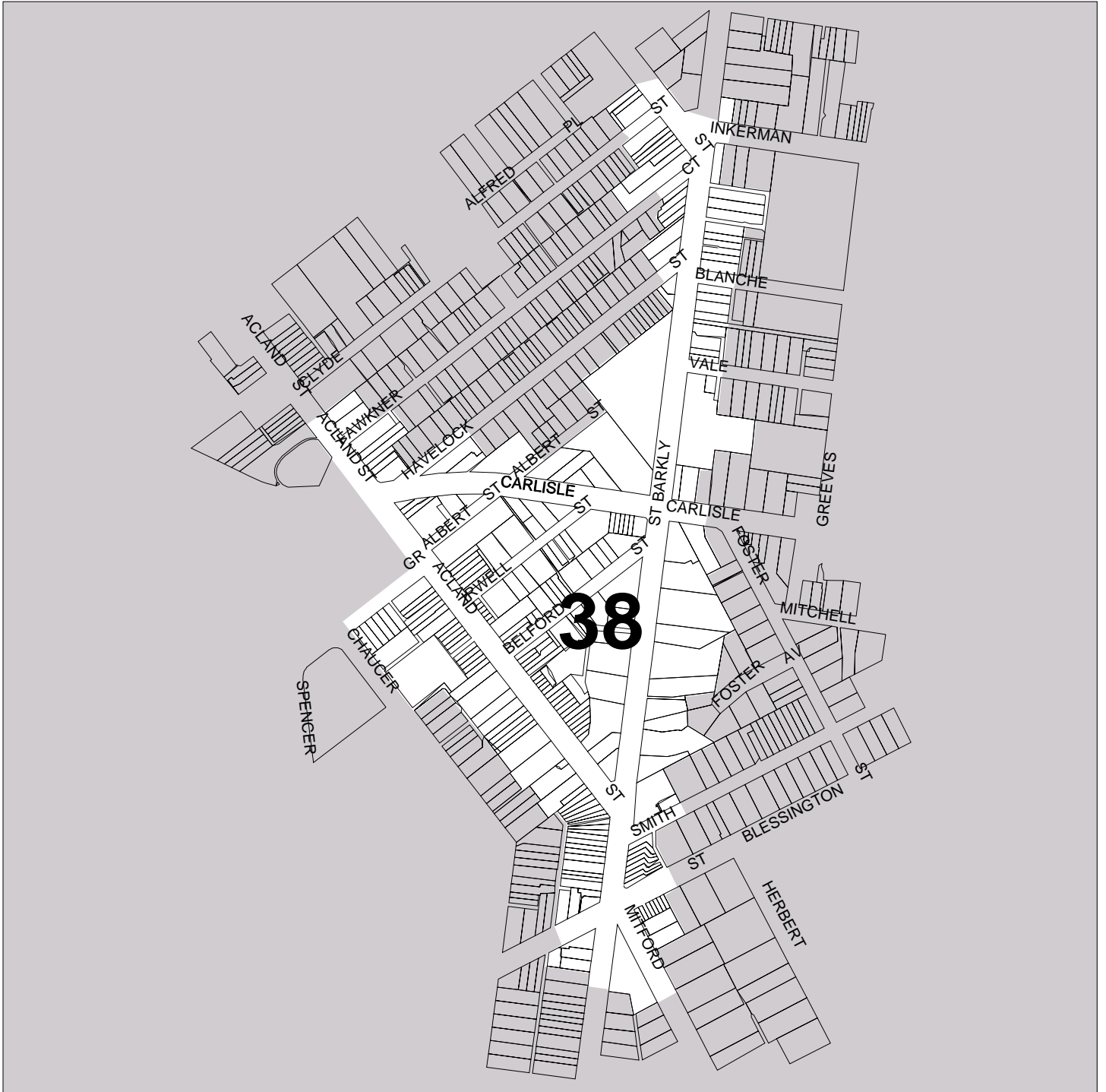
<i>Element</i>	<i>Importance</i>
Land use	Very important
Setbacks	Important
Building scale	Important
Access	Important



City of Port Phillip
URBAN CHARACTER DESCRIPTION

AREA 38: LAND IN VICINITY OF ACLAND STREET, CARLISLE STREET AND BARKLY STREET

The following description should be used in carrying out the site analysis and design response to your site.
Pay attention to the very important and important character elements listed here, as well as the actual conditions observed at the site analysis stage.



Barkly St between Blessington St and Inkerman St, Acland St, Carlisle St.

Streets in the area:

Acland St, Albert St, Barkly St, Belford St, Blanche St, Carlisle St, Foster St, Irwell St,

Description of the character of the area:

The area is distinguished from adjoining areas by its primary commercial land use.

The Acland Street shopping centre, or 'Village Belle' is both a local shopping centre and significant tourist attraction. In addition, the area contains a significant number of houses, flat blocks and some shop-top accommodation.

Barkly Street and Carlisle Street both carry heavy through traffic and support a mix of intermingled housing and commercial uses.

Acland Street is the intensive shopping and restaurant area with a new semi-pedestrian street design. Smaller streets between Acland and Barkly street accommodate mostly houses, flats and car parks serving the shopping centre.

The area is extremely accessible to shops and services and is served by trams in Acland and Carlisle Streets.

<i>Element</i>	<i>Description</i>
Dominant style	1920s - 1940s
Dominant scale	two storey
Dominant material	stucco and render/minor variation
Dominant roof	concealed by parapets/metal
Setback	less than 5 metres/consistent
Heritage	limited number of identified heritage places
Gardens	visible/small
Fences	buildings to property line

The existing urban character of the City of Port Phillip has been assessed using 24 character elements. The design should especially respond to the important and very important elements, which in this area are:

<i>Element</i>	<i>Importance</i>
Traffic	Very important
Parking	Very important
Setbacks	Very important
Building scale	Very important
Land use	Very important
Access	Very important
Overhead services	Important
Signage	Important
Open space	Important
Foreshore	Important

A comprehensive urban character assessment and policy statement for the area is available from the Council .



City of Port Phillip
URBAN CHARACTER DESCRIPTION

**AREA 39: BARKLEY AND GREY STREETS BETWEEN PATTISON STREET, FITZROY STREET
AND INKERMAN STREET**

The following description should be used in carrying out the site analysis and design response to your site.
Pay attention to the very important and important character elements listed here, as well as the actual conditions
observed at the site analysis stage.



Barkly St between Patterson St and Inkerman St, Grey St between Barkly St and Jackson St.

Streets in the area:

Alma Rd, Barkly St, Grey St, St Kilda Rd,

Description of the character of the area:

This area is distinct from adjoining areas because it follows large scale arterial roads and is lined with larger scale development. The hilltop at the junction of Barkly Street and Princess Street is a significant landmark in the area.

The combination of land form and busy wide streets also creates some significant long views to the north and the south.

The area was originally developed with large houses in the Victorian period and has subsequently undergone two phases of development with the addition of three storey walk-up flat blocks during the inter war period and more recently the post war period.

<i>Element</i>	<i>Description</i>
Dominant style	1920s - 1940s
Dominant scale	two and three storey
Dominant material	stucco and render/mixed
Dominant roof	pitched/terracotta
Setback	less than 5 metres/consistent
Heritage	@90% identified as heritage place
Gardens	visible/small
Fences	medium height/various materials

The existing urban character of the City of Port Phillip has been assessed using 24 character elements. The design should especially respond to the important and very important elements, which in this area are:

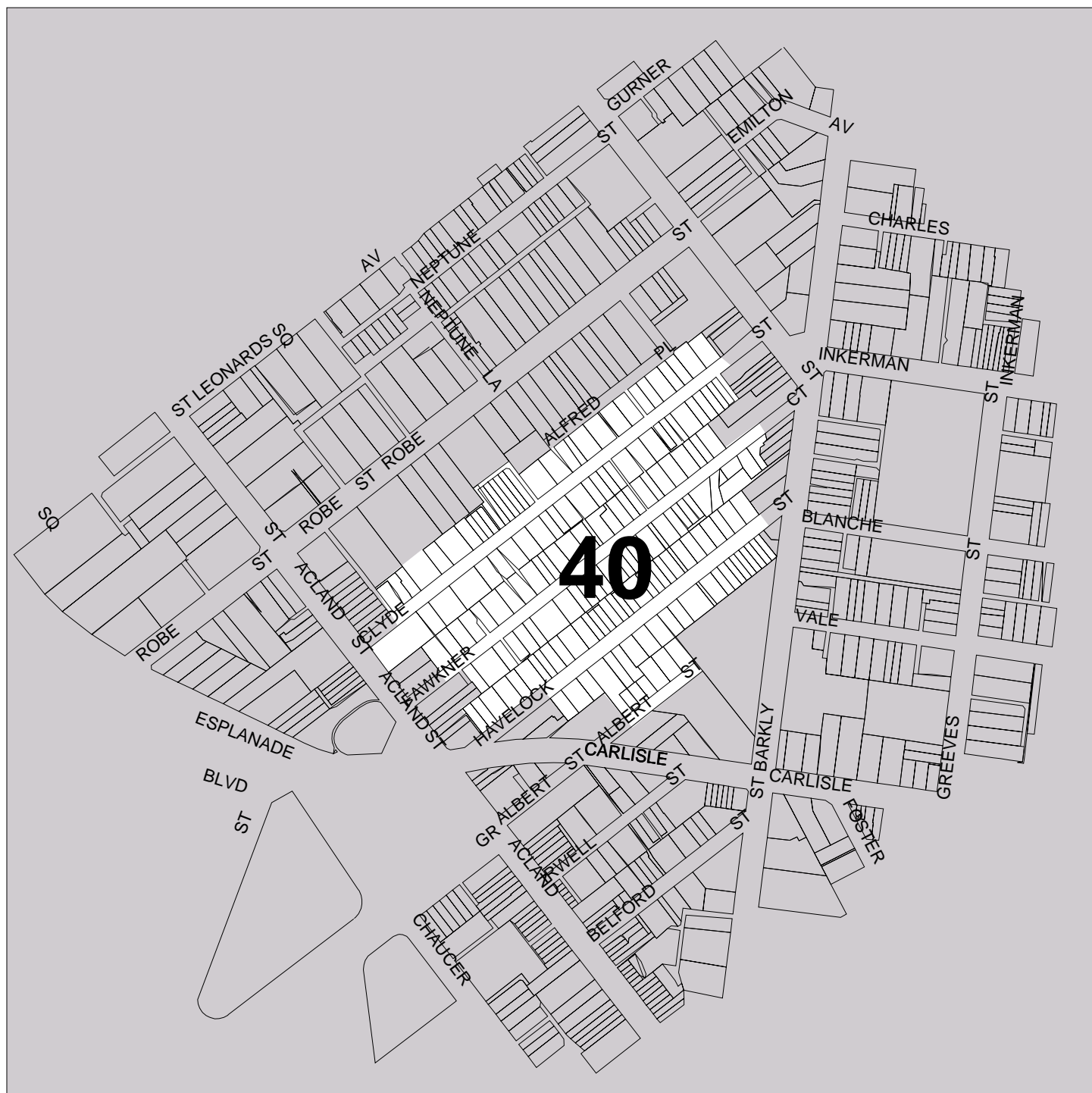
<i>Element</i>	<i>Importance</i>
Landform	Very important
Traffic	Very important
Building scale	Very important
Housing type	Very important
Heritage	Very important
Views	Important
Lot size	Important
Street dimensions	Important
Fences	Important
Setbacks	Important
Building style	Important
Access	Important

A comprehensive urban character assessment and policy statement for the area is available from the Council .

AREA 40: CLYDE STREET, FAWKNER CRESCENT AND HAVELOCK STREET

The following description should be used in carrying out the site analysis and design response to your site.

Pay attention to the very important and important character elements listed here, as well as the actual conditions observed at the site analysis stage.



Acland Street, Barkly Street, Albert Street, Clyde Street.

Streets in the area:

Description of the character of the area:

The area was initially developed as workers cottages during the Victorian period, although many buildings also date from the inter-war and Edwardian periods. Limited replacement of houses has also occurred in the post war period.

It derives its character from the small scale single storey housing which together with consistent small setbacks and narrow streets, provides an intimate urban scale.

The streets still retain their original detailing but some have been modified to become one way. Speed humps and a limited number of street trees are another recent addition.

Car parking appears to be at a premium in this area and visually dominates the streetscape. Front fences and front gardens are also important.

The area has excellent access to shops and public transport.

<i>Element</i>	<i>Description</i>
Dominant style	pre-1900s
Dominant scale	one storey
Dominant material	stucco and render/mixed
Dominant roof	pitched/metal
Setback	less than 5 metres/consistent
Heritage	entire area identified as heritage place
Gardens	visible/small
Fences	medium height/various materials

The existing urban character of the City of Port Phillip has been assessed using 24 character elements. The design should especially respond to the important and very important elements, which in this area are:

<i>Element</i>	<i>Importance</i>
Building scale	Very important
Access	Very important
Street dimensions	Important
Traffic	Important
Street detailing	Important
Fences	Important
Setbacks	Important
Building style	Important
Housing type	Important

A comprehensive urban character assessment and policy statement for the area is available from the Council .



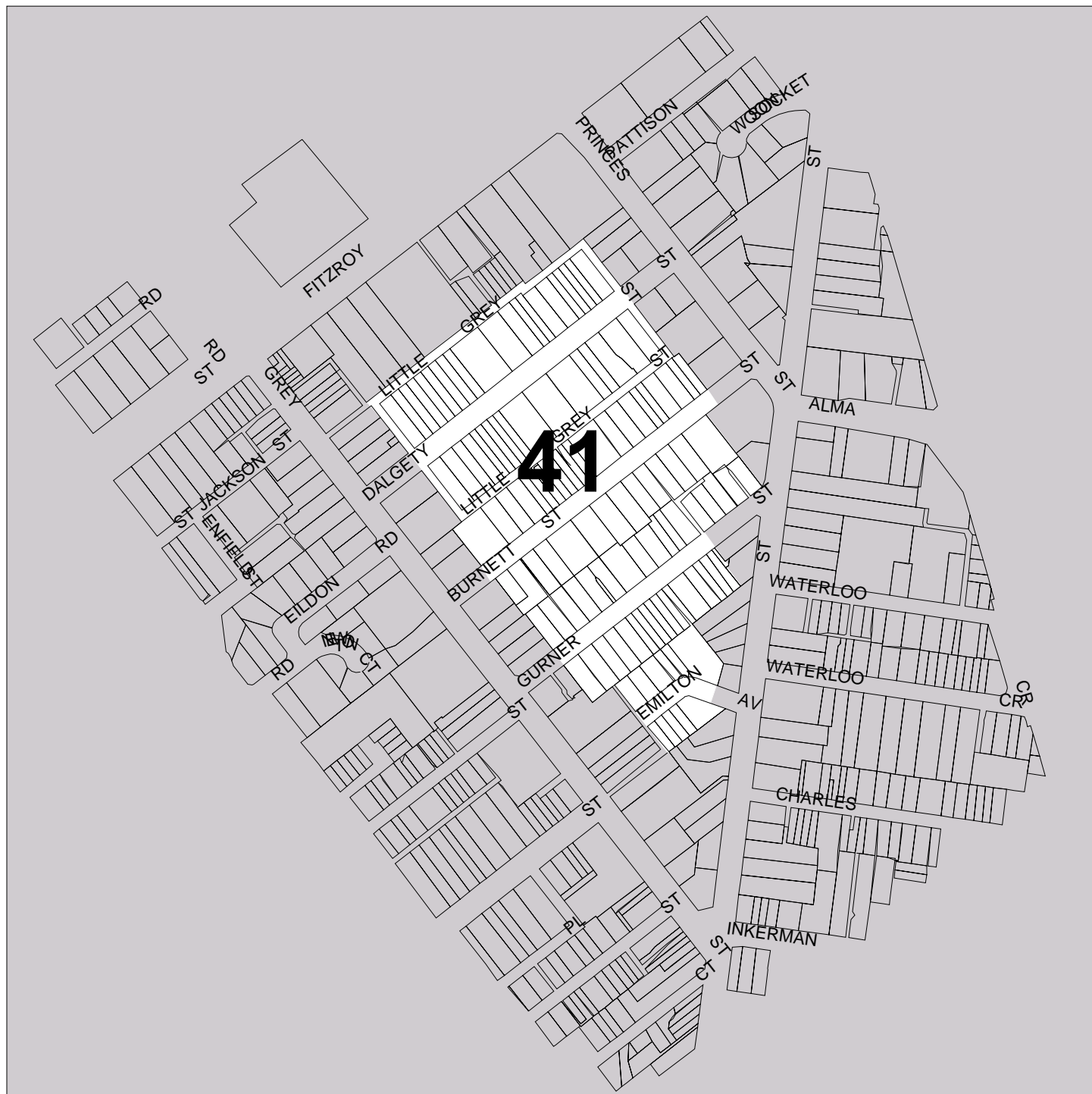
City of Port Phillip

URBAN CHARACTER DESCRIPTION

AREA 41: LAND GENERALLY IN THE VICINITY OF LITTLE GREY STREET, DALGETY STREET, BURNETT STREET AND GURNER STREET

The following description should be used in carrying out the site analysis and design response to your site.

Pay attention to the very important and important character elements listed here, as well as the actual conditions observed at the site analysis stage.



Boundary streets:

Emilton Street, Little Grey Street, Grey Street, Barkly Street.

Streets in the area:

Burnett St, Dalgety La, Dalgety St, Emilton Av, Gurner St, Little Grey St,

Description of the character of the area:

The area consists of a series of medium sized parallel streets running between Princess and Grey Streets. Little Grey Street and Dalgety Lane have an unusual formal lane layout with a wide range of lot sizes and frontages.

The original phase of development consisted of large Victorian homes and terraces, however the area has been further subdivided and developed during the Edwardian and inter-war period, especially in Dalgety Street.

Streets substantially maintain their original detailing, however street trees and other street landscaping is less than successful and tends to detract from the character of the area.

The area is close to Fitzroy Street shops, Albert Park and is well served by public transport.

<i>Element</i>	<i>Description</i>
Dominant style	pre-1900s
Dominant scale	one, two and three storey
Dominant material	brick or masonry/mixed
Dominant roof	pitched/metal
Setback	less than 5 metres/minor variation
Heritage	entire area identified as heritage place
Gardens	visible/large/medium/small
Fences	low-medium/various materials

The existing urban character of the City of Port Phillip has been assessed using 24 character elements. The design should especially respond to the important and very important elements, which in this area are:

<i>Element</i>	<i>Importance</i>
Heritage	Very important
Access	Very important
Front gardens	Important
Building style	Important

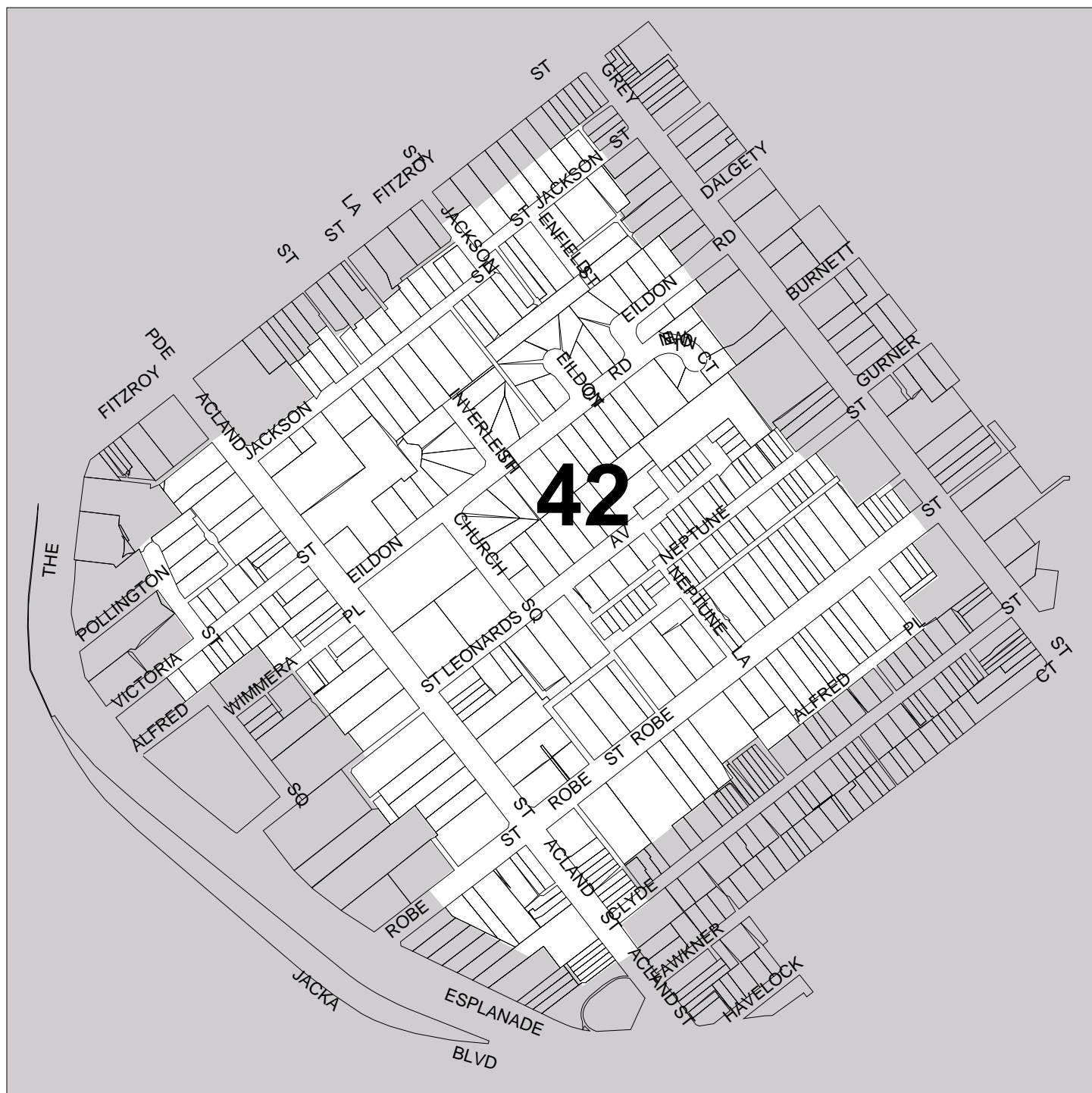
A comprehensive urban character assessment and policy statement for the area is available from the Council .

URBAN CHARACTER DESCRIPTION

AREA 42: LAND GENERALLY BOUNDED BY FITZROY STREET, GREY STREET, CLYDE STREET AND ACLAND STREET, ST KILDA

The following description should be used in carrying out the site analysis and design response to your site.

Pay attention to the very important and important character elements listed here, as well as the actual conditions observed at the site analysis stage.



Boundary streets:

Grey Street, Jackson Street, Alfred Street, Pollington Street.

Streets in the area:

Acland St, Alfred Pl, Church Sq, Eildon Ct, Eildon Rd, Enfield St, Grey St, Inverleith Ct, Jackson St, Neptune La, Neptune St, Newton Ct, Pollington St, Robe St, St Leonards Av, Victoria St, Wimmera Pl,

Description of the character of the area:

The area was originally subdivided Victorian mansions and has undergone subsequent periods of development in the inter-war and post-war periods with a wide variety of apartment buildings.

Buildings tend to be large in scale although there is a variety of lot sizes, frontages and housing types. High quality buildings from different eras making the area architecturally rich and diverse.

Streets in the area are also mixed in their dimensions and detailing, reflecting different periods of subdivision.

The area is no longer permeable by car due to the addition of numerous traffic control devices and system of one way streets.

Front gardens and front fences are important to the character of the area. Apart from Acland Street, street trees in the area are patchy and of limited value to the area.

The area is within walking distance of Acland Street and Fitzroy Street shopping areas and is also well located in relation to public transport and the foreshore.

<i>Element</i>	<i>Description</i>
Dominant style	pre-1900s
Dominant scale	one and two storey
Dominant material	brick or masonry/mixed
Dominant roof	pitched/terracotta
Setback	less than 5 metres/minor variation
Heritage	entire area identified as heritage place
Gardens	visible/medium/small
Fences	low-medium/various materials

The existing urban character of the City of Port Phillip has been assessed using 24 character elements. The design should especially respond to the important and very important elements, which in this area are:

<i>Element</i>	<i>Importance</i>
Building style	Very important
Building scale	Very important
Housing type	Very important
Heritage	Very important
Access	Very important
Setbacks	Important
Front gardens	Important
Land use	Important

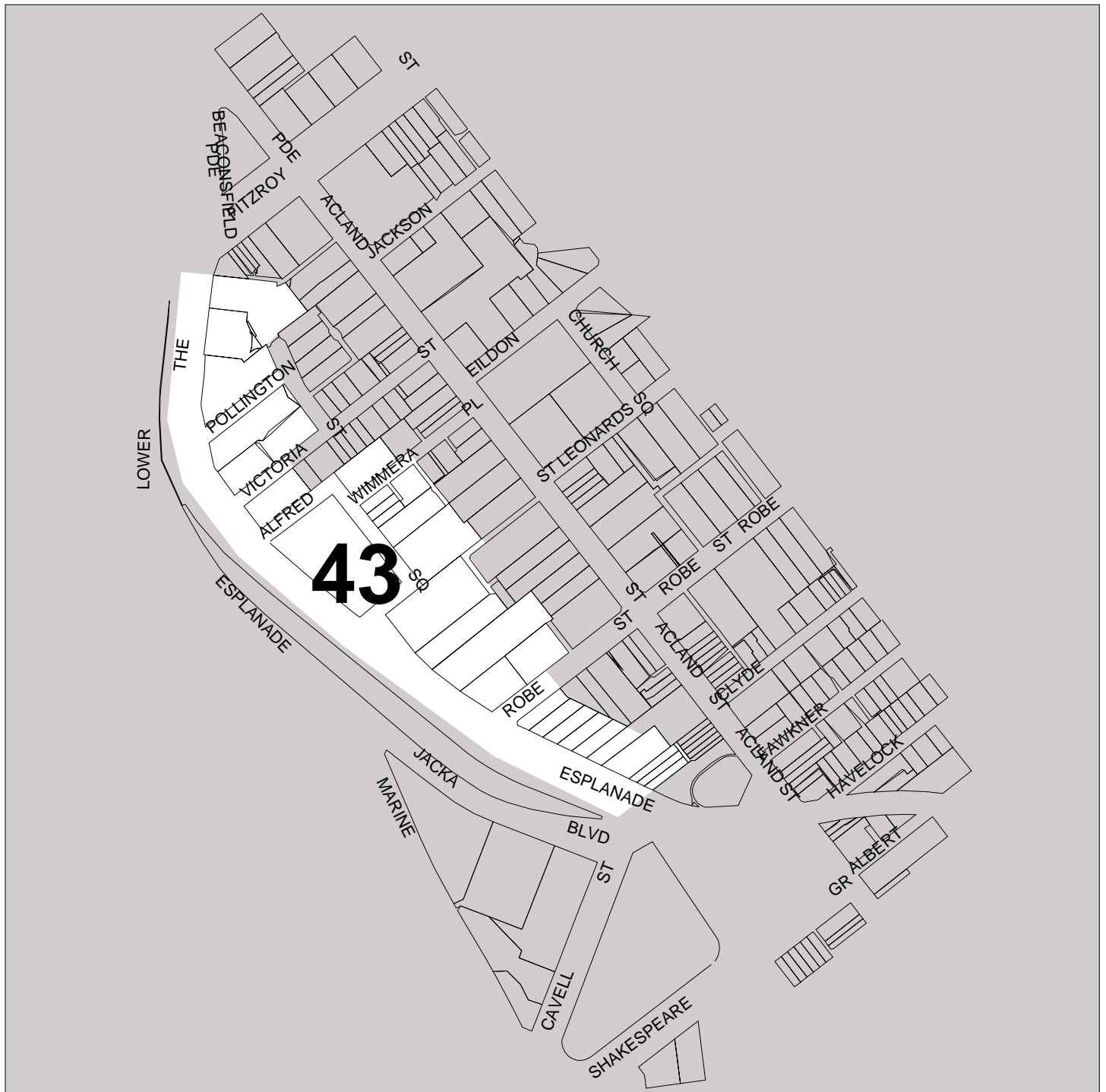
A comprehensive urban character assessment and policy statement for the area is available from the Council .



City of Port Phillip
URBAN CHARACTER DESCRIPTION

AREA 43: THE ESPLANADE BETWEEN FITZROY STREET AND ACLAND STREET

The following description should be used in carrying out the site analysis and design response to your site.
Pay attention to the very important and important character elements listed here, as well as the actual conditions observed at the site analysis stage.



Boundary streets:

Esplanade, Fitzroy Street, Acland Street.

Streets in the area:

Alfred Sq, Esplanade,

Description of the character of the area:

This area is distinctive from the area to the east because of its foreshore views, landform and extremely mixed building scale.

The Esplanade is a wide road lined by mature palm trees. It has wide promenade style footpaths with a sea outlook to the west and a tram route. Alfred Reserve is a central part of this grand open space and a focus for the area.

The area has mixed lot sizes which are predominantly large. It was initially developed with large houses, apartments and hotels with some smaller houses on side streets dating from the Victorian period.

Substantial redevelopment during the post war period has included several lift access hotel and apartment buildings, which vary from six to fifteen storeys in height, and sit awkwardly with the one, two and three storey buildings of earlier periods.

<i>Element</i>	<i>Description</i>
Dominant style	1970s - 1990s
Dominant scale	more than three stories
Dominant material	stucco and render/minor variation
Dominant roof	pitched/metal
Setback	less than 5 metres/minor variation
Heritage	@90% identified as heritage place
Gardens	visible/medium/small
Fences	various

The existing urban character of the City of Port Phillip has been assessed using 24 character elements. The design should especially respond to the important and very important elements, which in this area are:

<i>Element</i>	<i>Importance</i>
Views	Very important
Street trees	Very important
Building style	Very important
Building scale	Very important
Heritage	Very important
Foreshore	Very important
Landform	Important
Lot size	Important
Street dimensions	Important
Parking	Important
Footpaths	Important
Setbacks	Important
Housing type	Important
Access	Important
Open space	Important

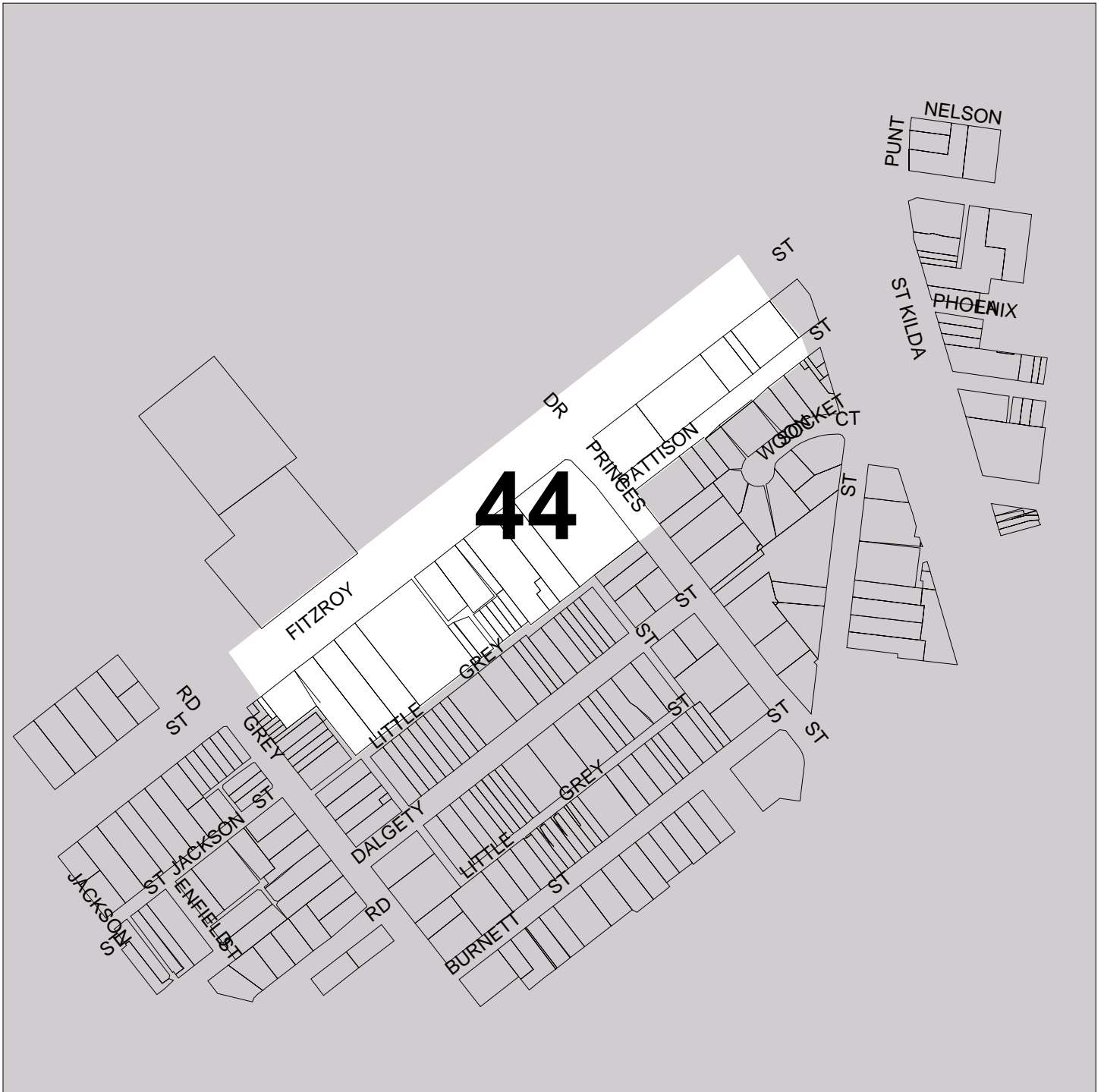
A comprehensive urban character assessment and policy statement for the area is available from the Council .



City of Port Phillip
URBAN CHARACTER DESCRIPTION

AREA 44: FITZROY STREET BETWEEN GREY STREET AND ST KILDA ROAD

The following description should be used in carrying out the site analysis and design response to your site.
Pay attention to the very important and important character elements listed here, as well as the actual conditions observed at the site analysis stage.



Fitzroy Street, Grey Street, Little Grey Street, St Kilda Road.

Streets in the area:

Fitzroy St, Pattison St, Princes St,

Description of the character of the area:

Fitzroy Street is a wide street with central tram reservation, nature strips, Plane trees along its south eastern side and generous footpaths.

This area faces Albert Park and the city beyond, creating a unique outlook and views.

It was originally developed with grand hotels and houses in the Victorian and Edwardian periods, and has since undergone substantial redevelopment in the post war period.

The scale of development is varied but many buildings are six or seven storeys high, giving the street a degree of continuity.

Buildings near St Kilda Road are occupied with office uses, but the majority of the buildings appear to be mixed use with café and entertainment uses on the ground floor and apartments above. Some new buildings incorporate basement or multi level car parks.

This is a high density area with exceptional access to shops, open space and public transport.

<i>Element</i>	<i>Description</i>
Dominant style	1970s - 1990s
Dominant scale	more than three stories
Dominant material	stucco and render/consistent
Dominant roof	flat/metal
Setback	less than 5 metres/consistent
Heritage	@75% identified as heritage place
Gardens	none
Fences	buildings to property line

The existing urban character of the City of Port Phillip has been assessed using 24 character elements. The design should especially respond to the important and very important elements, which in this area are:

<i>Element</i>	<i>Importance</i>
Street trees	Very important
Parking	Very important
Building scale	Very important
Heritage	Very important
Access	Very important
Open space	Very important
Views	Important
Lot size	Important
Street dimensions	Important
Traffic	Important
Setbacks	Important
Building style	Important
Housing type	Important
Land use	Important

A comprehensive urban character assessment and policy statement for the area is available from the Council .



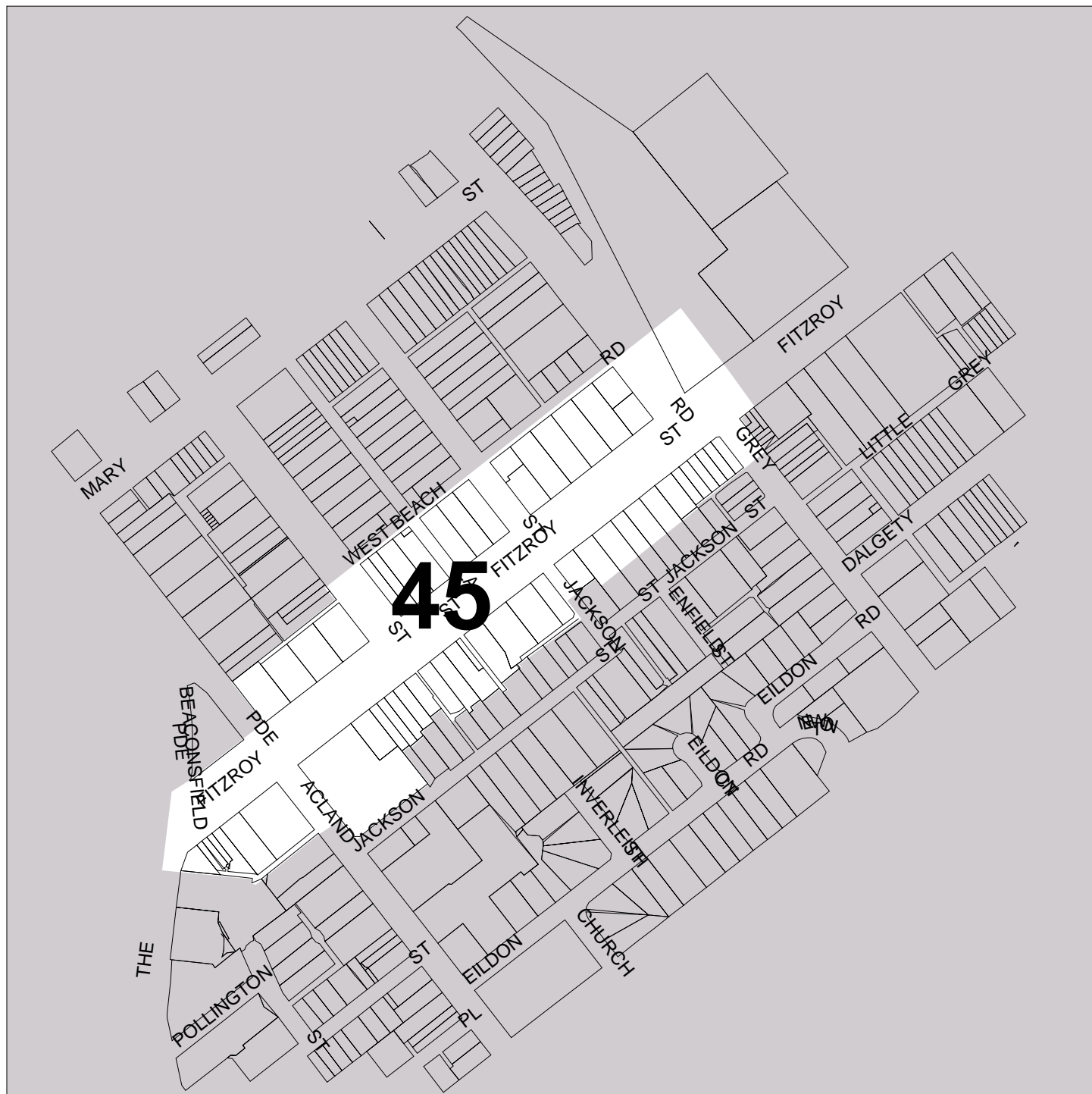
City of Port Phillip

URBAN CHARACTER DESCRIPTION

AREA 45: FITZROY STREET BETWEEN THE ESPLANADE AND GREY STREET

The following description should be used in carrying out the site analysis and design response to your site.

Pay attention to the very important and important character elements listed here, as well as the actual conditions observed at the site analysis stage.



Boundary streets:

Fitzroy St west bounded by: West Beach Rd, The Esplanade, Catabury Rd, Jackson St.

Streets in the area:

Fitzroy St, Grey St, Jackson St,

Description of the character of the area:

The area is primarily differentiated from adjoining residential areas to the north and south by its mixed land use, different building types without setback and the grand proportion of Fitzroy Street.

Fitzroy Street has wide pedestrian pavements, providing for parking, two traffic lanes each direction and a central tram strip with central poles and street lighting.

The area was originally developed with large Victorian houses, many detached and setback from the street. Since then, it has undergone continuous redevelopment including Edwardian apartment buildings and shops followed by similar developments and renovations in the inter-war and post-war periods. The area now contains an almost equal mix of buildings from all periods.

Building scale in this area is smaller than the adjoining area to the east. Buildings of one, two and three storeys are almost equally represented along the street. Fitzroy Street has consistent development along its northern side including apartment buildings, restaurants and former large houses.

The street landscape on the northern side contains mature elm trees, limited verandahs and some setback buildings with gardens. In contrast, the southern side has almost continuous verandahs and commercial frontage with a more recent landscape of smaller trees and fully paved footpaths.

<i>Element</i>	<i>Description</i>
Dominant style	pre-1900s
Dominant scale	"one, two and three stories"
Dominant material	stucco and render/minor variation
Dominant roof	concealed by parapets/terracotta
Setback	less than 5 metres/minor variation
Heritage	@75% identified as heritage place
Gardens	visible/small
Fences	various height and materials

The existing urban character of the City of Port Phillip has been assessed using 24 character elements. The design should especially respond to the important and very important elements, which in this area are:

<i>Element</i>	<i>Importance</i>
Parking	Very important
Access	Very important
Views	Important
Street dimensions	Important
Traffic	Important
Footpaths	Important
Overhead services	Important
Signage	Important
Setbacks	Important
Heritage	Important
Land use	Important
Foreshore	Important

A comprehensive urban character assessment and policy statement for the area is available from the Council .



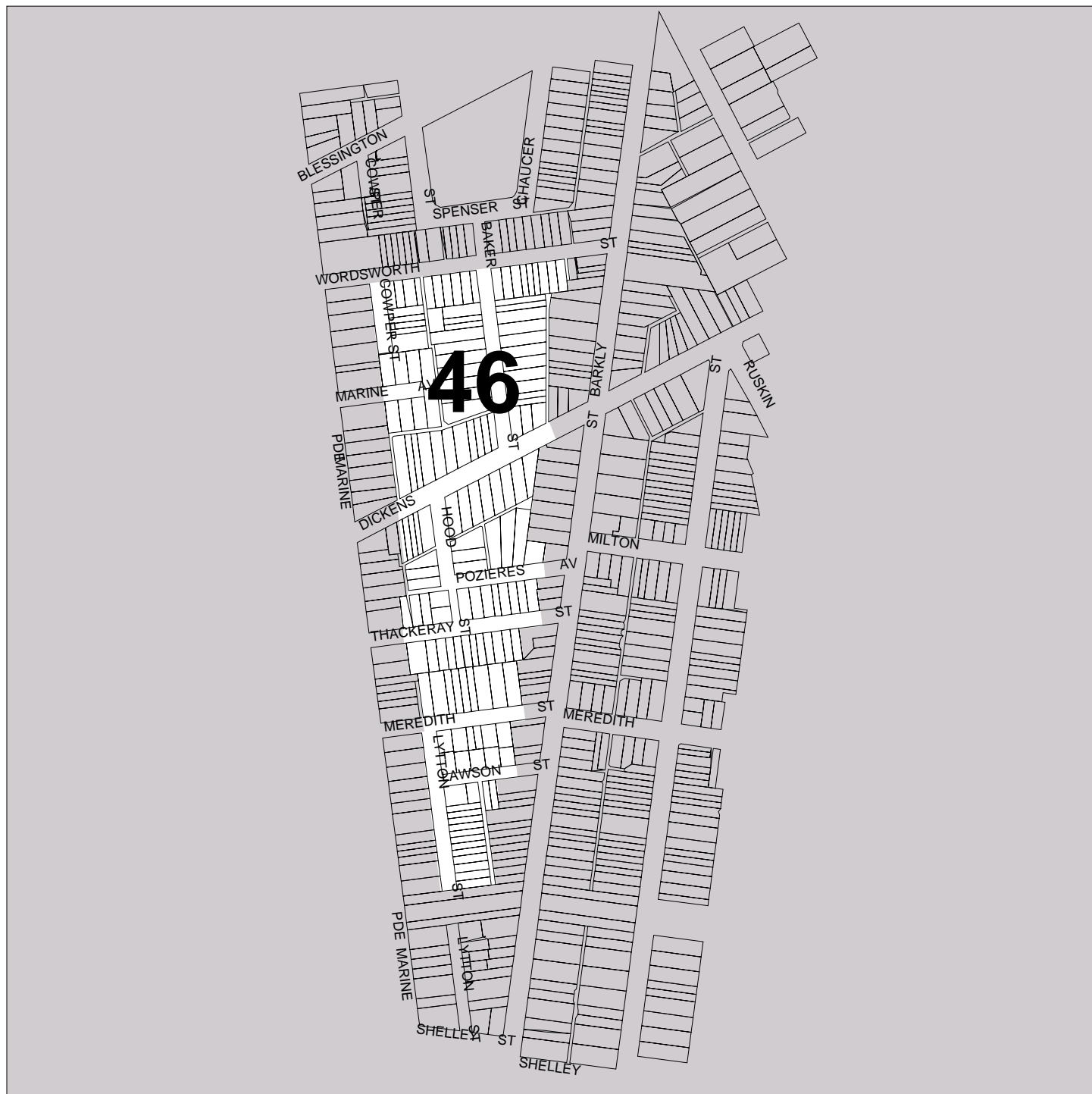
City of Port Phillip

URBAN CHARACTER DESCRIPTION

AREA 46: LAND GENERALLY BOUNDED BY MARINE PARADE, WORDSWORTH STREET AND BARKLEY STREET

The following description should be used in carrying out the site analysis and design response to your site.

Pay attention to the very important and important character elements listed here, as well as the actual conditions observed at the site analysis stage.



Marine Pde, Barkley St, Wordsworth St.

Streets in the area:

Baker St, Dickens St, Lawson St, Meredith St, Pozieres Av, Thackeray St,

Description of the character of the area:

Most of the area has been subdivided into smaller lots and developed primarily with single storey houses and consistent fencing through the Victorian, Edwardian and inter-war periods. It is relatively free from two and three storey post-war flat blocks.

Streets mostly retain their original bluestone and bitumen detailing and some streets have excellent mature plane trees.

The intimate scale of this area is created by its short, often narrow streets and small scale housing. This characteristic together with its relative freedom from post-war flat development are the outstanding characteristics of this area and the ones that differentiate it from adjoining areas.

<i>Element</i>	<i>Description</i>
Dominant style	1900s -1920s
Dominant scale	one storey
Dominant material	stucco and render/mixed
Dominant roof	pitched/terracotta
Setback	less than 5 metres/minor variation
Heritage	no identified heritage places
Gardens	visible/medium/small
Fences	low-medium/various materials

The existing urban character of the City of Port Phillip has been assessed using 24 character elements. The design should especially respond to the important and very important elements, which in this area are:

<i>Element</i>	<i>Importance</i>
Building scale	Very important
Street dimensions	Important
Street detailing	Important
Fences	Important
Building style	Important
Housing type	Important
Foreshore	Important

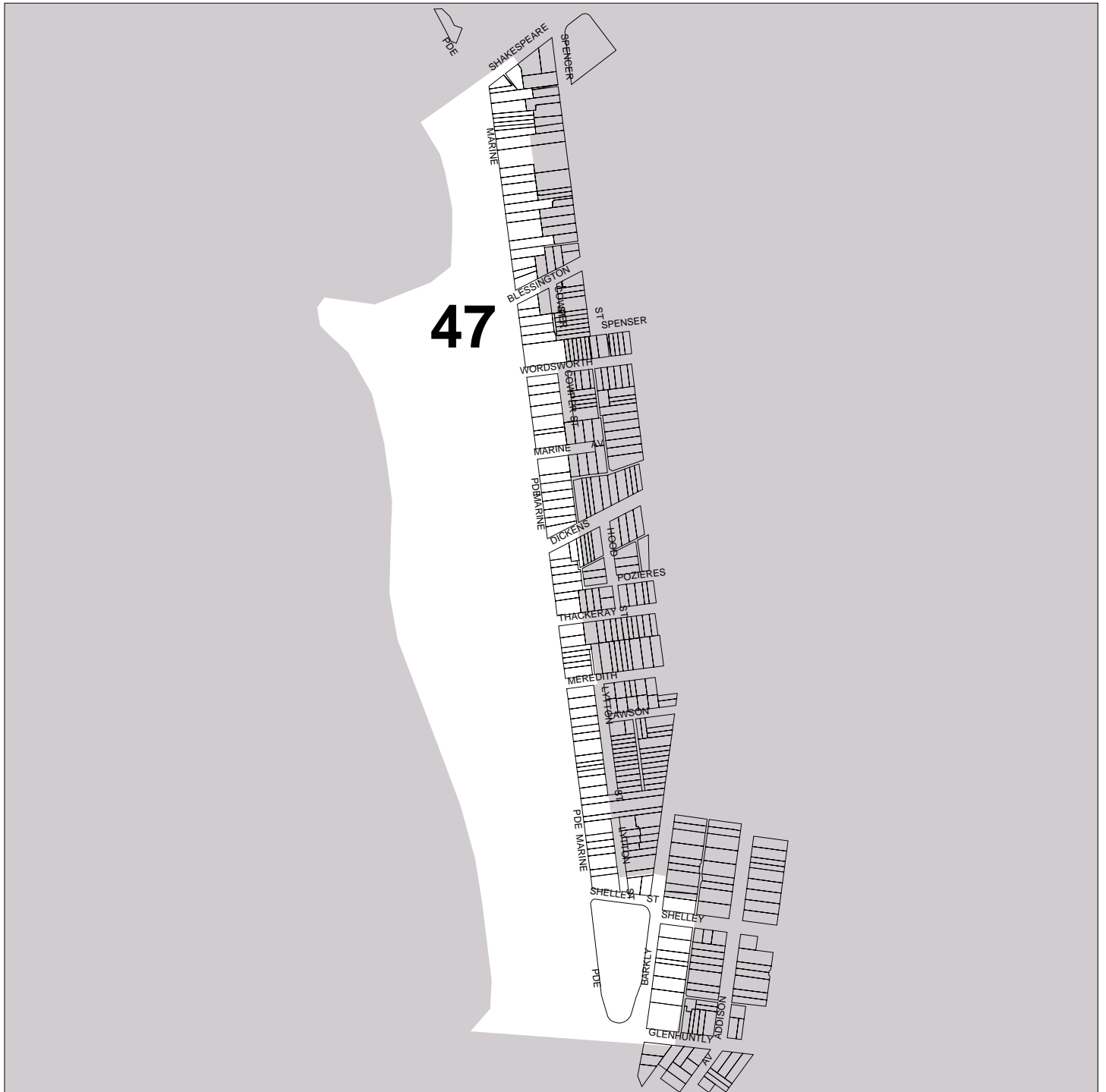
A comprehensive urban character assessment and policy statement for the area is available from the Council .



City of Port Phillip
URBAN CHARACTER DESCRIPTION

AREA 47: MARINE PARADE BETWEEN SHAKESPEARE GROVE AND GLENHUNTLY ROAD

The following description should be used in carrying out the site analysis and design response to your site.
Pay attention to the very important and important character elements listed here, as well as the actual conditions observed at the site analysis stage.



Marine Pde. between Glenhuntly Rd. and Shakespeare Gve.

Streets in the area:

Barkly St, Blessington Ct, Glenhuntly Rd, Hood St, Lytton St, Marine Av, Marine Pde, Shelley St, Spenser St,

Description of the character of the area:

This area includes two and three storey housing and apartment buildings bordering its eastern edge and extensive open space and foreshore recreation facilities to the west. It has undergone fairly continuous development since it was first established at the beginning of the century.

Marine Parade has been upgraded to a six lane arterial road carrying heavy through traffic which has a strong impact on the area. Landscaping along Marine Parade is immature and overhead services visually dominate much of the area.

Foreshore facilities include parking areas, passive parkland, beaches, shared pathways and restaurants.

The St Kilda Marina and the large service station are also prominent in the area.

Views are limited from Marine Parade but are excellent from the foreshore and upper levels of buildings in the area.

<i>Element</i>	<i>Description</i>
Dominant style	1920s - 1940s
Dominant scale	two storey
Dominant material	stucco and render/minor variation
Dominant roof	pitched/terracotta
Setback	less than 5 metres/minor variation
Heritage	no identified heritage places
Gardens	visible/medium/small
Fences	low-high/consistent materials

The existing urban character of the City of Port Phillip has been assessed using 24 character elements. The design should especially respond to the important and very important elements, which in this area are:

<i>Element</i>	<i>Importance</i>
Views	Very important
Traffic	Very important
Foreshore	Very important
Street dimensions	Important
Overhead services	Important
Fences	Important
Building style	Important
Building scale	Important
Housing type	Important
Open space	Important

A comprehensive urban character assessment and policy statement for the area is available from the Council .



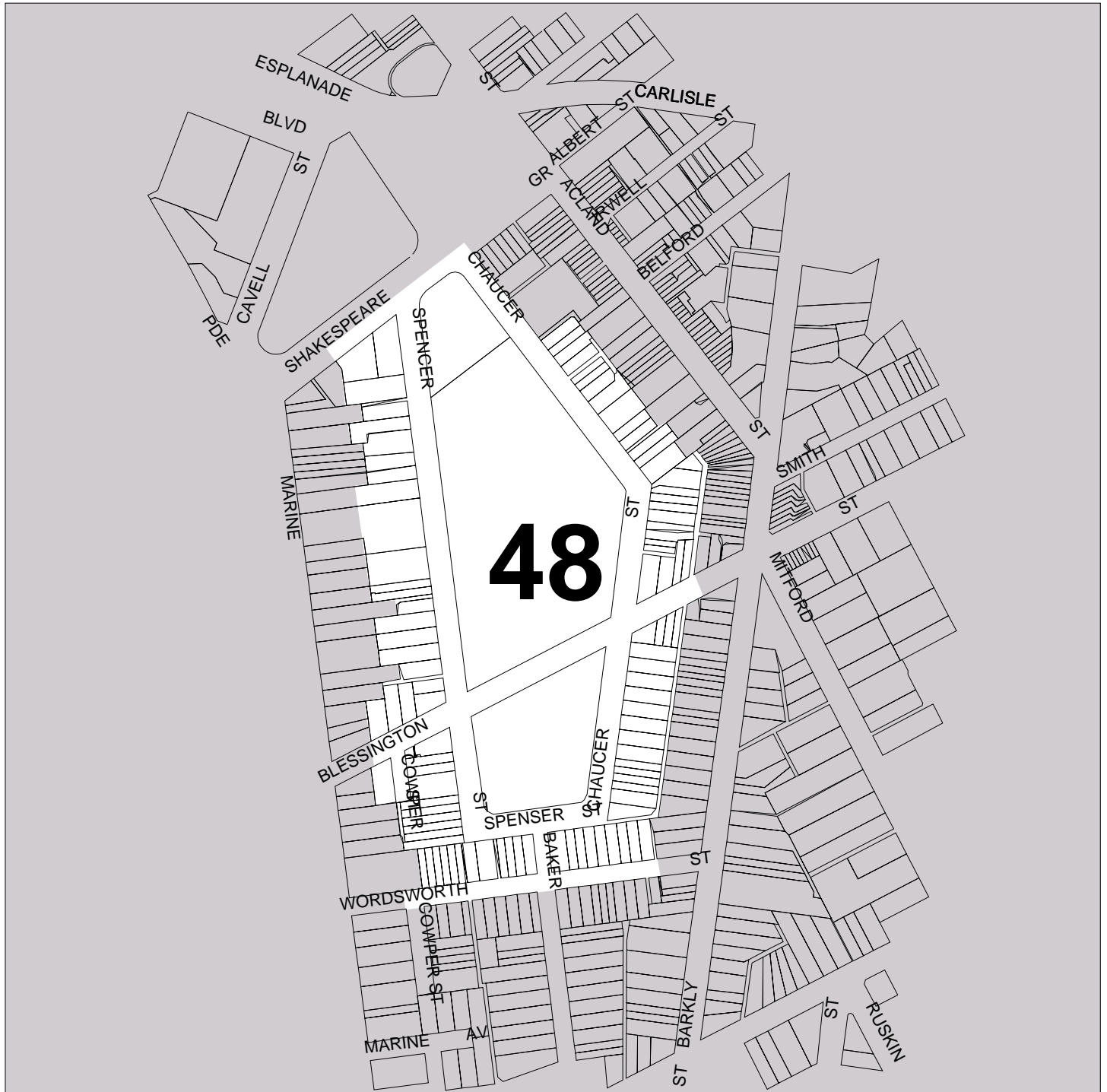
City of Port Phillip

URBAN CHARACTER DESCRIPTION

AREA 48: CHAUCER STREET AND SPENSER STREET

The following description should be used in carrying out the site analysis and design response to your site.

Pay attention to the very important and important character elements listed here, as well as the actual conditions observed at the site analysis stage.



Boundary streets:

Chaucer Street, Wordsworth St, Spencer St, Shakespeare Gve.

Streets in the area:

Baker St, Blessington St, Chaucer St, Spencer St, Wordsworth St,

Description of the character of the area:

Lots in the area vary widely in size and frontage, but all lots have frontages to Chaucer or Spencer Streets and the open space beyond.

The area was initially developed during the Victorian period but the majority of houses are Edwardian. The area has undergone redevelopment for flat buildings both in the inter-war and post-war periods. These two or three storey blocks are mainly concentrated in Spencer Street. One large flat building, Edgewater Tower, adjoins the area and is very visible on the skyline. Luna Park at the northern end of the area is another landmark which adds to the character of the area.

Streets retain their original detailing but lack consistent street trees. The open space retains many trees from its initial phase of development but most buildings and facilities are from the post war period.

The area appears to accommodate parking generated by adjoining Acland Street and tourist areas.

<i>Element</i>	<i>Description</i>
Dominant style	1900s - 1920s
Dominant scale	one and three storey
Dominant material	brick or masonry/mixed
Dominant roof	pitched/terracotta
Setback	less than 5 metres/minor variation
Heritage	no identified heritage places
Gardens	visible/small
Fences	medium height/various materials

The existing urban character of the City of Port Phillip has been assessed using 24 character elements. The design should especially respond to the important and very important elements, which in this area are:

<i>Element</i>	<i>Importance</i>
Open space	Very important
Landform	Important
Views	Important
Parking	Important
Setbacks	Important
Building style	Important
Building scale	Important
Housing type	Important
Access	Important
Foreshore	Important

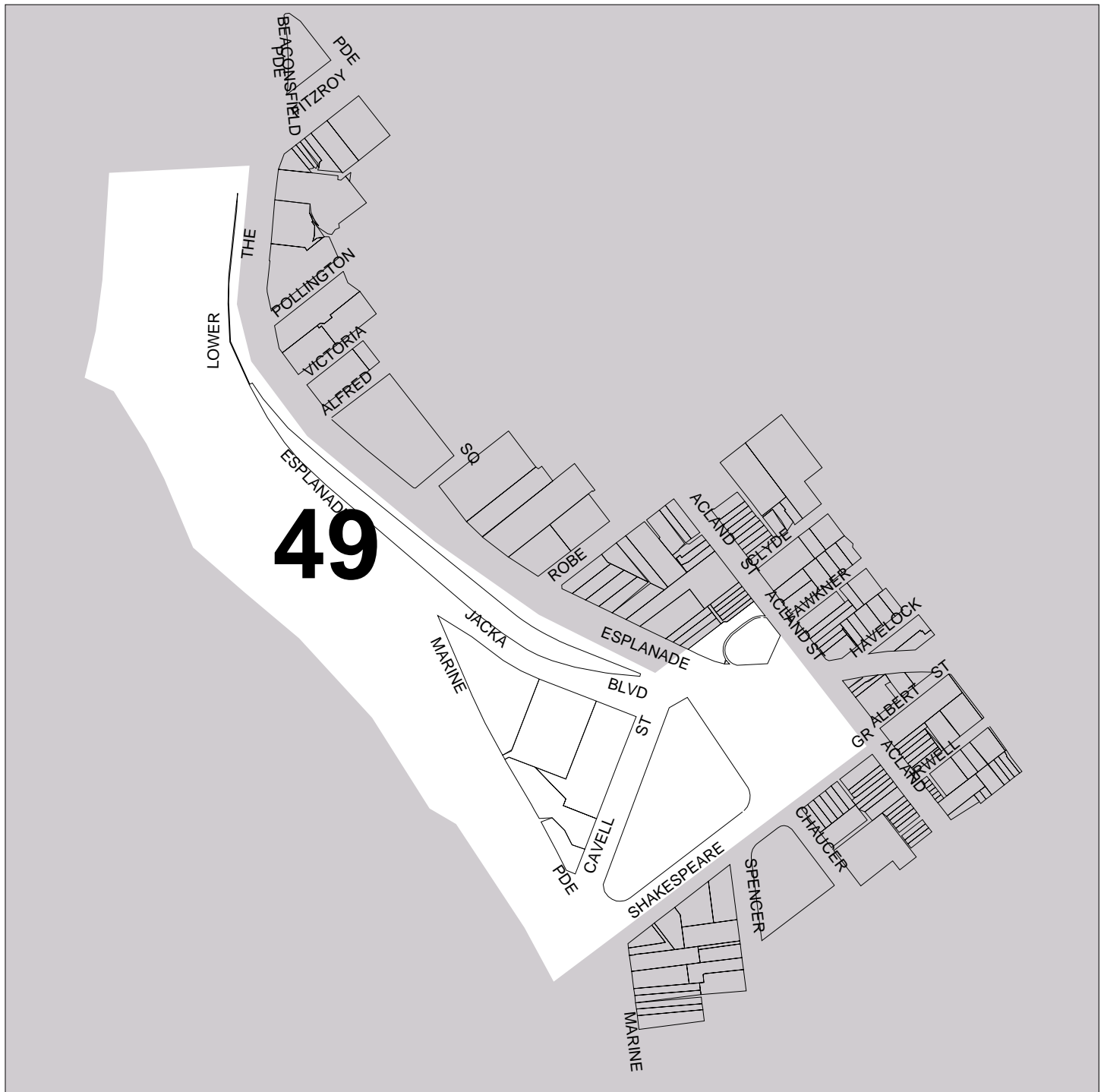
A comprehensive urban character assessment and policy statement for the area is available from the Council.



City of Port Phillip
URBAN CHARACTER DESCRIPTION

AREA 49: THE ESPLANADE BETWEEN FITZROY STREET AND SHAKESPEARE GROVE

The following description should be used in carrying out the site analysis and design response to your site.
Pay attention to the very important and important character elements listed here, as well as the actual conditions observed at the site analysis stage.



The Esplanade, Fitzroy St, Shakespeare Gve, Foreshore.

Streets in the area:

Acland St, Cavell St, Esplanade, Jacka Blvd, Lower Esplanade, Shakespeare Gv,

Description of the character of the area:

This section of the St Kilda Foreshore is distinguished from adjoining areas by its primary function as a recreation and entertainment area. The area has no housing and consists of a number of privately operated tourist venues including Luna Park, the Palais, the Palace, the Sea Baths and various restaurants on large lots, set amongst public parkland and beach reserves. Some sites within the area have been developed as unattractive temporary car parks

Roads in the area are large in dimension and Jacka Boulevard/Marine Parade carries heavy traffic through the area.

St Kilda beach, including the pier and restaurants, is an extremely popular tourist destination.

<i>Element</i>	<i>Description</i>
Dominant style	1920s - 1940s
Dominant scale	one and more than 3 stories
Dominant material	stucco and render/minor variation
Dominant roof	no dominance/metal
Setback	less than 5 metres/varied
Heritage	@50% identified as heritage place
Gardens	no boundary definition
Fences	buildings to property line

The existing urban character of the City of Port Phillip has been assessed using 24 character elements. The design should especially respond to the important and very important elements, which in this area are:

<i>Element</i>	<i>Importance</i>
Traffic	Very important
Parking	Very important
Heritage	Very important
Land use	Very important
Foreshore	Very important
Views	Important
Lot size	Important
Street dimensions	Important
Building style	Important
Building scale	Important
Access	Important
Open space	Important

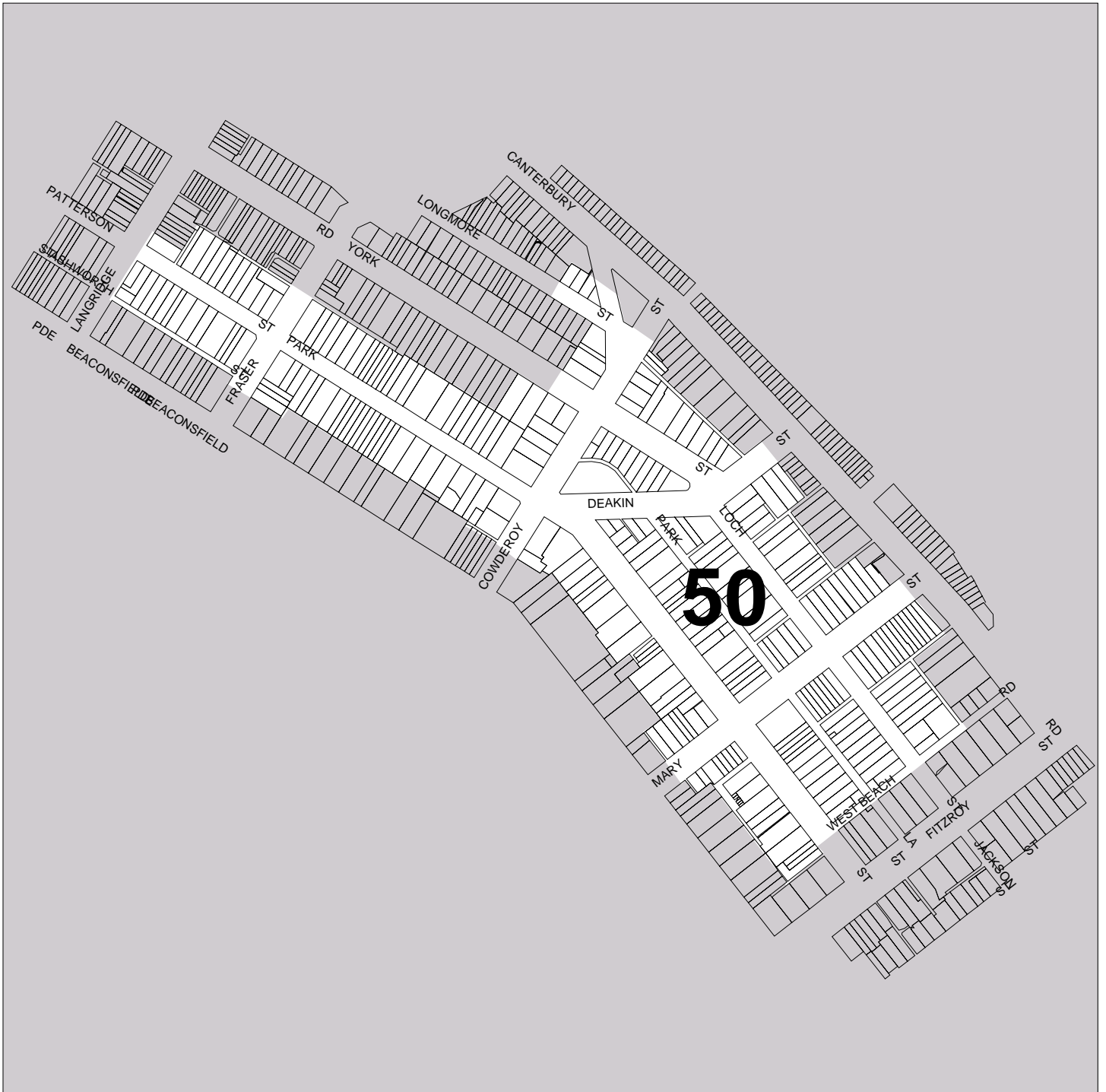
A comprehensive urban character assessment and policy statement for the area is available from the Council .



City of Port Phillip
URBAN CHARACTER DESCRIPTION

AREA 50: LAND IN THE VICINITY OF PARK STREET, DEAKIN STREET, LOCH STREET AND MARY STREET

The following description should be used in carrying out the site analysis and design response to your site.
Pay attention to the very important and important character elements listed here, as well as the actual conditions observed at the site analysis stage.



Canterbury Rd, Cowderoy St, Langridge St, Park St.

Streets in the area:

Cowderoy St, Deakin St, Fraser St, Loch St, Mary St, Park La, Park St, Patterson St, West Beach Rd, York St, Felix La, Bells La,

Description of the character of the area:

The area is significant for its street trees and collection of Edwardian houses and inter war apartment buildings. The northern edge is defined by a gradual reduction in building scale and a reduction in the presence of flat blocks.

Streets in the area are mostly wide with generous footpaths, medians and wide road pavements. There are also some smaller streets near Fitzroy Street and an extensive system of rear lanes.

Most views are internal although there are occasional glimpses to the foreshore skyline along the east/west streets. Some streets have exceptional street trees especially Park Street south, Mary and York Streets which are planted with a mix of mature plane and elm trees. Other trees are extremely mixed in their planting.

This area is extremely accessible to shops, services, beach, open space and public transport.

<i>Element</i>	<i>Description</i>
Dominant style	1900s - 1920s
Dominant scale	two and three storey
Dominant material	brick or masonry/minor variation
Dominant roof	pitched/terracotta
Setback	less than 5 metres/consistent
Heritage	entire area identified as heritage place
Gardens	visible/medium/small
Fences	medium height/various materials

The existing urban character of the City of Port Phillip has been assessed using 24 character elements. The design should especially respond to the important and very important elements, which in this area are:

<i>Element</i>	<i>Importance</i>
Street dimensions	Very important
Street trees	Very important
Heritage	Very important
Access	Very important
Building style	Important
Building scale	Important
Housing type	Important
Open space	Important
Foreshore	Important

A comprehensive urban character assessment and policy statement for the area is available from the Council .



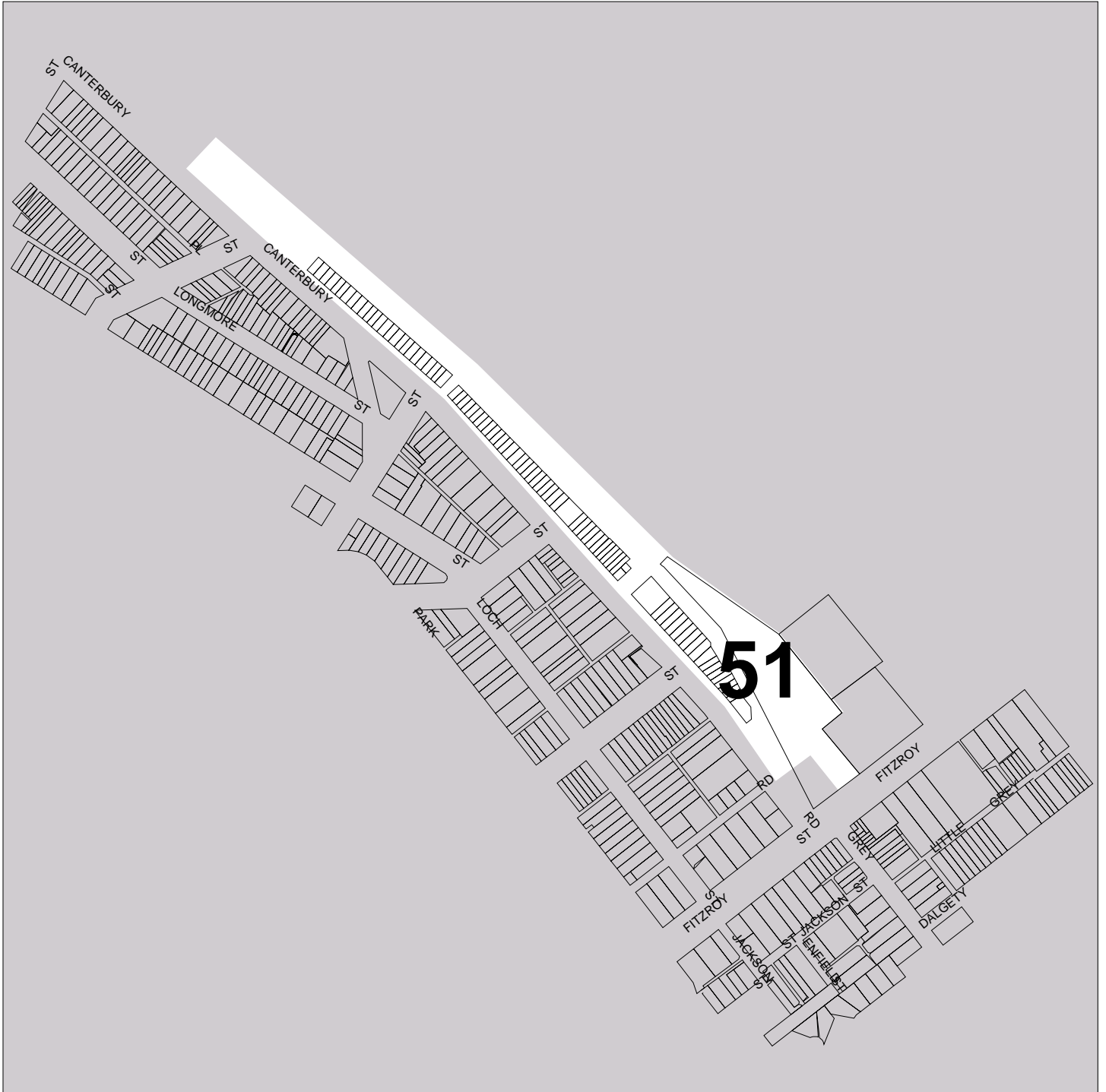
City of Port Phillip

URBAN CHARACTER DESCRIPTION

AREA 51: CANTERBURY ROAD, FITZROY STREET, ALBERT PARK

The following description should be used in carrying out the site analysis and design response to your site.

Pay attention to the very important and important character elements listed here, as well as the actual conditions observed at the site analysis stage.



Canterbury Rd, Fitzroy St, Albert Park, Fraser St.

Streets in the area:

Balluk Willam Ct, Balluk William Ct,

Description of the character of the area:

This area was developed in the 1990s following subdivision of land surplus to the requirements of Public Transport Corporation.

Development consists of contemporary terrace style dwellings with frontage to Canterbury Road and car access from the rear in most instances. The houses are generally three storey with balconies and deck space providing views across the light rail line to Albert Park.

The character of the area is quite distinct from the residential area on the opposite side of Canterbury Road where buildings date back to Victorian and Edwardian periods.

The area has a unique relationship with Albert Park and exceptional access to shops open space and public transport.

<i>Element</i>	<i>Description</i>
Dominant style	1970s-1990s
Dominant scale	three storey
Dominant material	stucco and render/minor variation
Dominant roof	concealed by parapet
Setback	less than 5 metres/minor variation
Heritage	entire area identified as heritage place
Gardens	visible/hidden/no boundary definition/small
Fences	various heights/mixed materials

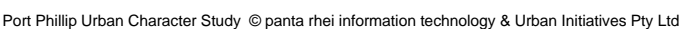
The existing urban character of the City of Port Phillip has been assessed using 24 character elements. The design should especially respond to the important and very important elements, which in this area are:

<i>Element</i>	<i>Importance</i>
Building style	Very important
Access	Very important
Open space	Very important
Views	Important
Pattern	Important
Traffic	Important
Setbacks	Important
Building scale	Important
Housing type	Important

A comprehensive urban character assessment and policy statement for the area is available from the Council .



The following description should be used in carrying out the site analysis and design response to your site. Pay attention to the very important and important character elements listed here, as well as the actual conditions observed at the site analysis stage.



Beaconsfield Pde, Fitzroy St, Kerford Rd, Ashworth St.

Streets in the area:

Ashworth St, Beaconsfield Pde,

Description of the character of the area:

The area is distinguished from adjoining residential areas by the larger scale of buildings facing Beaconsfield Parade and its foreshore location with views over the bay.

Housing in the area was initially developed in the Victorian period but has undergone incremental redevelopment with the result that a range of building styles are almost equally represented. Building types vary from single and double storey terraces through to larger detached Victorian and Edwardian houses. The area also includes many inter-war period apartments and post-war houses, multi-unit developments and high rise towers.

Beaconsfield Parade is a wide and busy arterial road with associated traffic noise and an evolving landscape character of palm trees framing views to the water. The new beach front promenade is also a significant part of this road reservation. The Middle Park foreshore and Catani Gardens are a major tourist destination generating significant on-street parking and tourist activity.

<i>Element</i>	<i>Description</i>
Dominant style	1920s - 1940s
Dominant scale	one storey - high rise
Dominant material	stucco and render/consistent
Dominant roof	concealed by parapet/terracotta
Setback	less than 5 metres/minor variation
Heritage	@90% identified heritage places
Gardens	visible/small
Fences	low-high/various materials

The existing urban character of the City of Port Phillip has been assessed using 24 character elements. The design should especially respond to the important and very important elements, which in this area are:

<i>Element</i>	<i>Importance</i>
Views	Very important
Street dimensions	Very important
Traffic	Very important
Street trees	Very important
Foreshore	Very important
Parking	Important
Footpaths	Important
Building style	Important
Building scale	Important
Housing type	Important
Heritage	Important
Land use	Important
Open space	Important

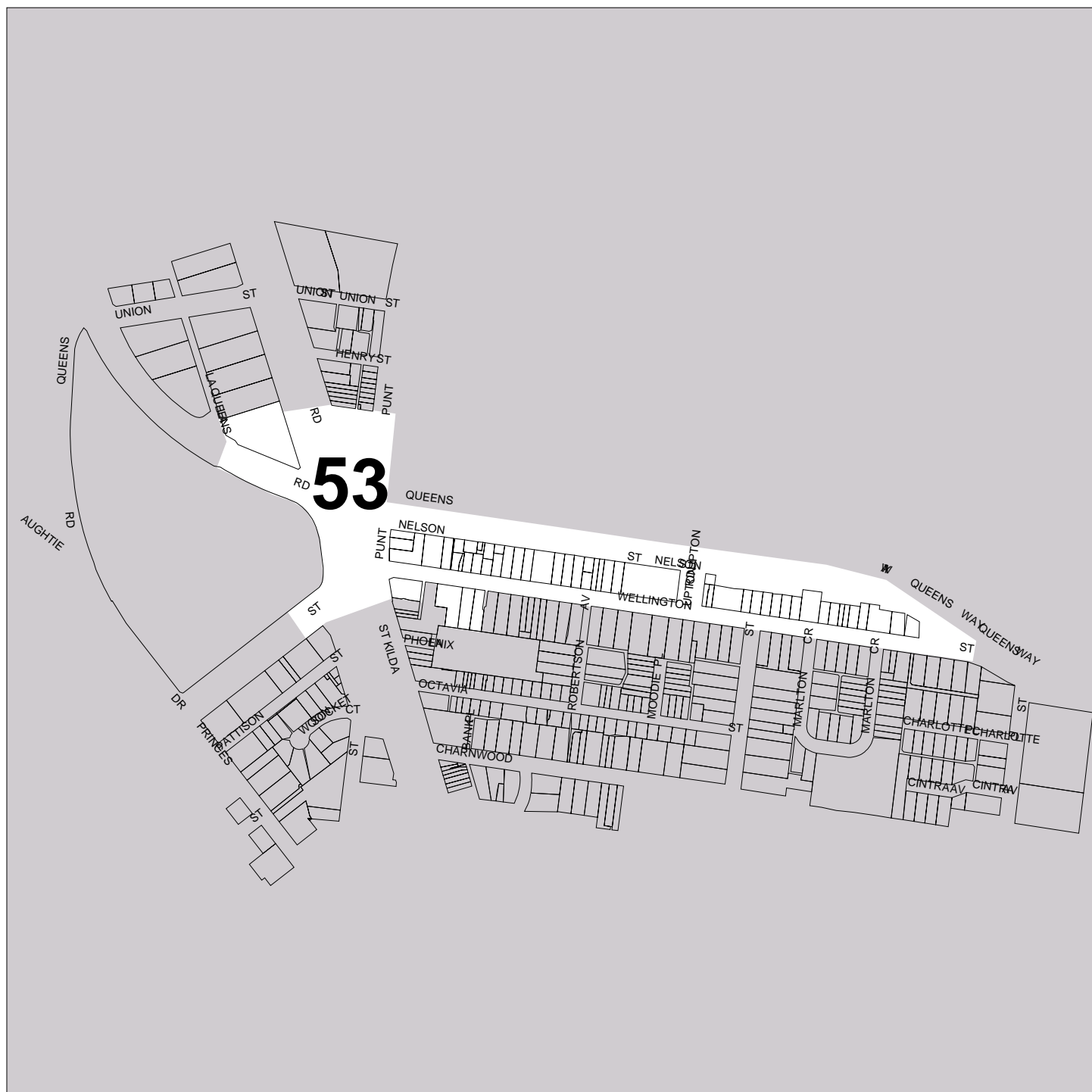
A comprehensive urban character assessment and policy statement for the area is available from the Council .



City of Port Phillip
URBAN CHARACTER DESCRIPTION

AREA 53: WELLINGTON STREET BETWEEN QUEENS WAY AND THE JUNCTION

The following description should be used in carrying out the site analysis and design response to your site.
Pay attention to the very important and important character elements listed here, as well as the actual conditions observed at the site analysis stage.



Boundary streets:

Wellington St, Queens Rd, St Kilda Rd.

Streets in the area:

St Kilda Rd, Wellington St,

Description of the character of the area:

This area is primarily influenced by St Kilda junction and includes commercial buildings which address the junction proper and a strip of development along the north side of Wellington Street which is also very visible from the junction.

Few buildings properly address this major intersection and those that do have been substantially devalued by large commercial advertising signs.

The area has been redeveloped by road authorities in the post-war period and is now a multi-level interchange and landmark location in the geography of Melbourne, being the intersection of Dandenong Road, Queens Road and St. Kilda Road. It incorporates tram routes and heavy traffic flows through the area make pedestrian movements difficult.

Landscape development of the intersection is a mixture of native trees, shrubs and planted embankments which are subservient to the functional design and scale of the intersection.

<i>Element</i>	<i>Description</i>
Dominant style	1970s - 1990s
Dominant scale	two or more stories
Dominant material	stucco and render/mixed
Dominant roof	concealed by parapets/metal
Setback	less than 5 metres/minor variation
Heritage	no identified heritage places
Gardens	no boundary definition
Fences	buildings to property line

The existing urban character of the City of Port Phillip has been assessed using 24 character elements. The design should especially respond to the important and very important elements, which in this area are:

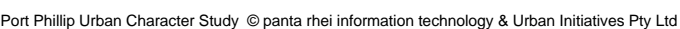
<i>Element</i>	<i>Importance</i>
Street dimensions	Very important
Traffic	Very important
Signage	Very important
Street detailing	Important
Access	Important

A comprehensive urban character assessment and policy statement for the area is available from the Council .



The following description should be used in carrying out the site analysis and design response to your site.

Pay attention to the very important and important character elements listed here, as well as the actual conditions observed at the site analysis stage.



St Kilda Rd, Queens Rd, Punt Rd, High St, Dorcas St, Palmerston Crescent.

Streets in the area:

Albert Rd, Arthur St, Beatrice St, Bowen La, Hanna St, Henry St, High St, Kings Way, Leopold St, Lorne St, Louise St, Park St, Queens La, Raleigh St, Roy St, St Kilda Rd, Union St, Queens Rd,

Description of the character of the area:

The area is primarily distinguished from adjoining areas by its land use, large lot size and the scale and nature of buildings in the area.

It has two distinct parts, properties which face St Kilda Road and those which face Queens Road and Albert Park.

The area was originally subdivided and developed in the Victorian period as a grand street of large mansions on consistent large lots. However, it has undergone substantial redevelopment with houses being replaced by both high density apartment and office buildings. This redevelopment has been substantial during the post-war period so that large modern buildings now dominate the character.

Garden setbacks have been retained along both Queens Road and especially St. Kilda Road contributing a sense of continuity. The grand dimensions and consistent street trees and street details in St Kilda Road also add substantially to the area's character.

Other streets in the area lack consistent details and street trees.

<i>Element</i>	<i>Description</i>
Dominant style	1970s - 1990s
Dominant scale	three or more stories
Dominant material	stucco and render/glass/mixed
Dominant roof	no dominance/metal
Setback	more than 9 metres/varied
Heritage	no heritage places identified
Gardens	visible/exotic
Fences	various heights/various materials

The existing urban character of the City of Port Phillip has been assessed using 24 character elements. The design should especially respond to the important and very important elements, which in this area are:

<i>Element</i>	<i>Importance</i>
Lot size	Very important
Street dimensions	Very important
Traffic	Very important
Building style	Very important
Building scale	Very important
Open space	Very important
Views	Important
Pattern	Important
Street trees	Important
Front gardens	Important
Housing type	Important
Heritage	Important
Land use	Important
Access	Important

A comprehensive urban character assessment and policy statement for the area is available from the Council .



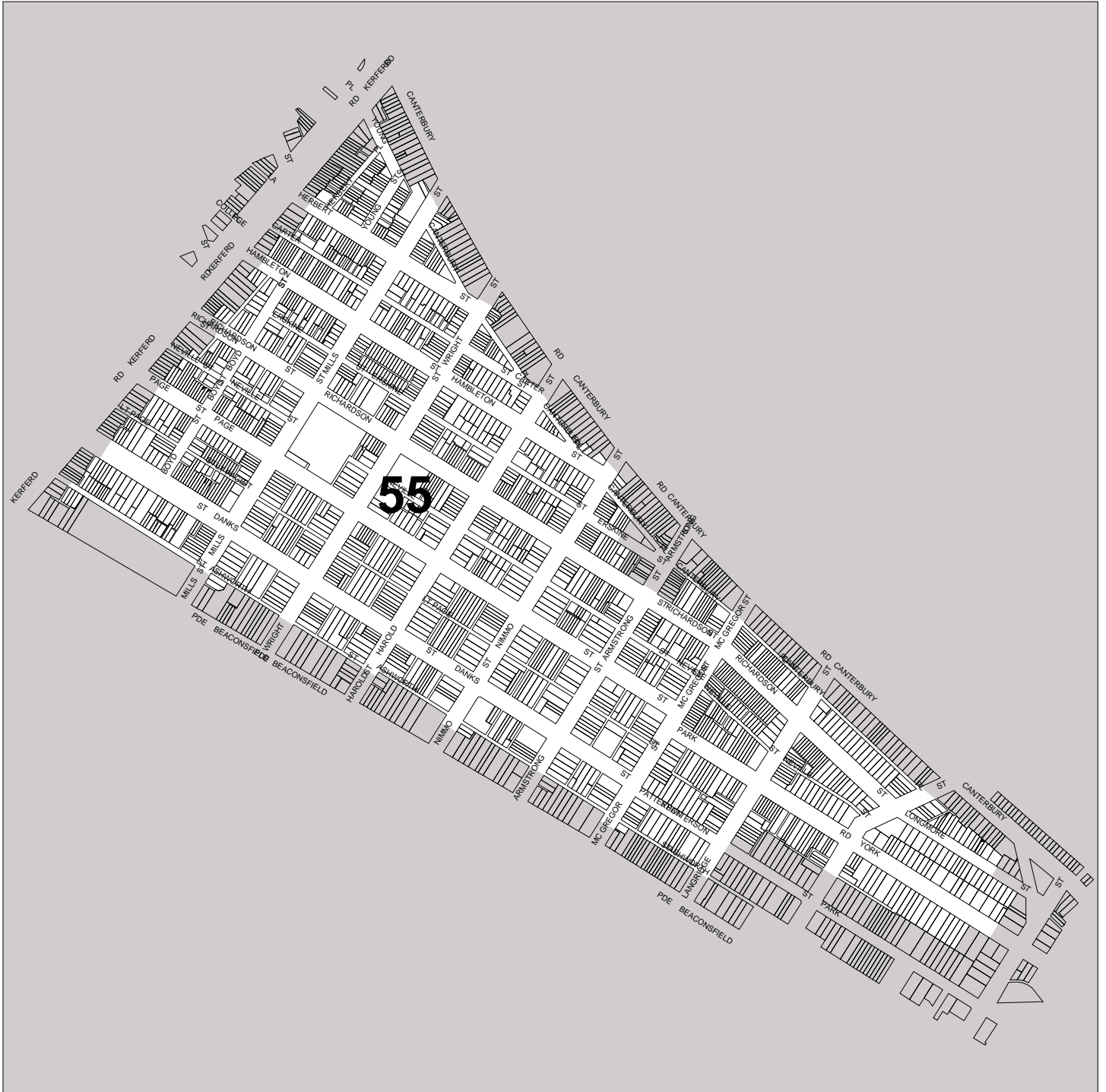
City of Port Phillip

URBAN CHARACTER DESCRIPTION

AREA 55: LAND GENERALLY BOUNDED BY CANTERBURY PLACE, KERFORD ROAD AND ASHWORTH STREET, MIDDLE PARK

The following description should be used in carrying out the site analysis and design response to your site.

Pay attention to the very important and important character elements listed here, as well as the actual conditions observed at the site analysis stage.



Canterbury Pl, Ashworth St, Cowderoy St, Kerford Rd.

Streets in the area:

Armstrong St, Boyd St, Canterbury Pl, Carter St, Cowderoy Pl, Danks St, Erskine St, Fraser St, Hambleton St, Harold St, Herbert Pl, Herbert St, Langridge St, Little Page St, Longmore St, McGregor St, Mills St, Neville St, Nimmo St, Page St, Park Rd, Patterson St, Richardson St, Wright St, York St,

Description of the character of the area:

The area has a consistent grid street pattern of wide, often tree-lined streets with a secondary system of smaller streets and lanes.

The vast majority of houses in this area are from either the Victorian or Edwardian periods in almost equal proportions. Smaller Victorian cottages are often timber creating a similar mix of brick, stucco and timber materials together with terracotta and metal roofs.

As there has been no significant redevelopment, the consistent Victorian and Edwardian housing dominates the character of the area.

The area has good access to public transport, Albert Park and the foreshore but limited convenience shopping and community facilities.

<i>Element</i>	<i>Description</i>
Dominant style	1900s - 1920s
Dominant scale	one storey
Dominant material	brick or masonry/mixed
Dominant roof	concealed by parapets/metal
Setback	less than 5 metres/consistent
Heritage	entire area identified as heritage place
Gardens	visible/small
Fences	medium height/consistent materials

The existing urban character of the City of Port Phillip has been assessed using 24 character elements. The design should especially respond to the important and very important elements, which in this area are:

<i>Element</i>	<i>Importance</i>
Pattern	Very important
Street dimensions	Very important
Building style	Very important
Building scale	Very important
Heritage	Very important
Street trees	Important
Nature strips	Important
Fences	Important
Setbacks	Important
Front gardens	Important
Housing type	Important
Land use	Important
Access	Important
Open space	Important
Foreshore	Important

A comprehensive urban character assessment and policy statement for the area is available from the Council .



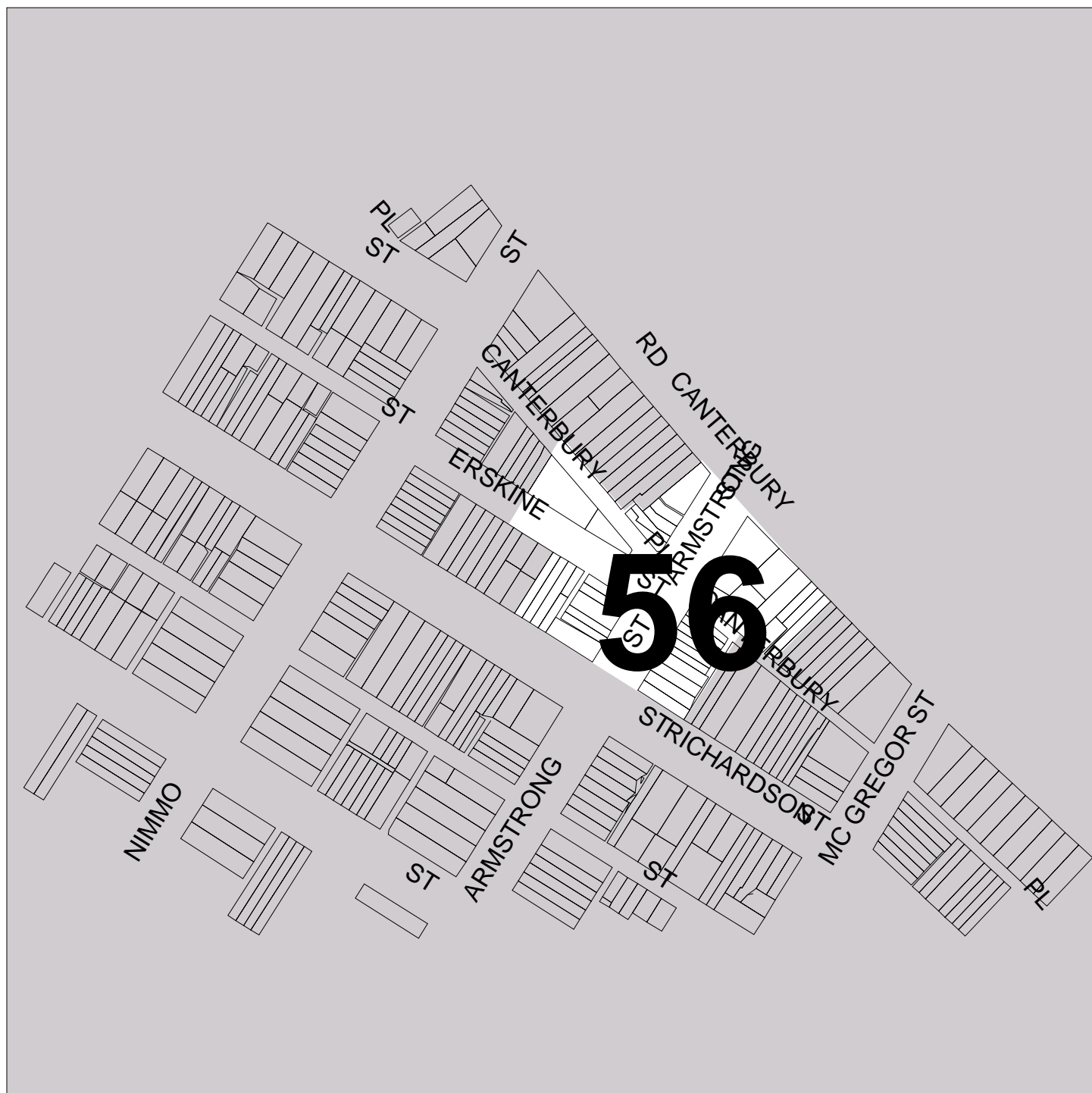
City of Port Phillip

URBAN CHARACTER DESCRIPTION

AREA 56: LAND IN THE VICINITY OF ARMSTRONG STREET, CANTERBURY ROAD AND RICHARDSON STREET

The following description should be used in carrying out the site analysis and design response to your site.

Pay attention to the very important and important character elements listed here, as well as the actual conditions observed at the site analysis stage.



Richardson St, Canterbury Rd, Armstrong St.

Streets in the area:

Armstrong St, Canterbury Pl, Canterbury Rd,

Description of the character of the area:

This small shopping centre serving Middle Park, includes most local shopping requirements, some offices, a grand hotel and a range of restaurants.

Armstrong and Richardson Streets are wide streets which allow parallel parking, wide footpaths and street trees. They carry limited through traffic, giving the area a relatively quiet feel despite its proximity to Canterbury Road.

Buildings closest to Middle Park Station and Canterbury Road derive from the Victorian period, with the balance of the centre being fairly consistent Edwardian style shops

<i>Element</i>	<i>Description</i>
Dominant style	1900s - 1920s
Dominant scale	two storey
Dominant material	brick or masonry/consistent
Dominant roof	concealed by parapets/metal
Setback	less than 5 metres/consistent
Heritage	entire area identified as heritage place
Gardens	none
Fences	buildings to property line

The existing urban character of the City of Port Phillip has been assessed using 24 character elements. The design should especially respond to the important and very important elements, which in this area are:

<i>Element</i>	<i>Importance</i>
Heritage	Very important
Lot size	Important
Parking	Important
Fences	Important
Building style	Important
Building scale	Important
Land use	Important
Access	Important

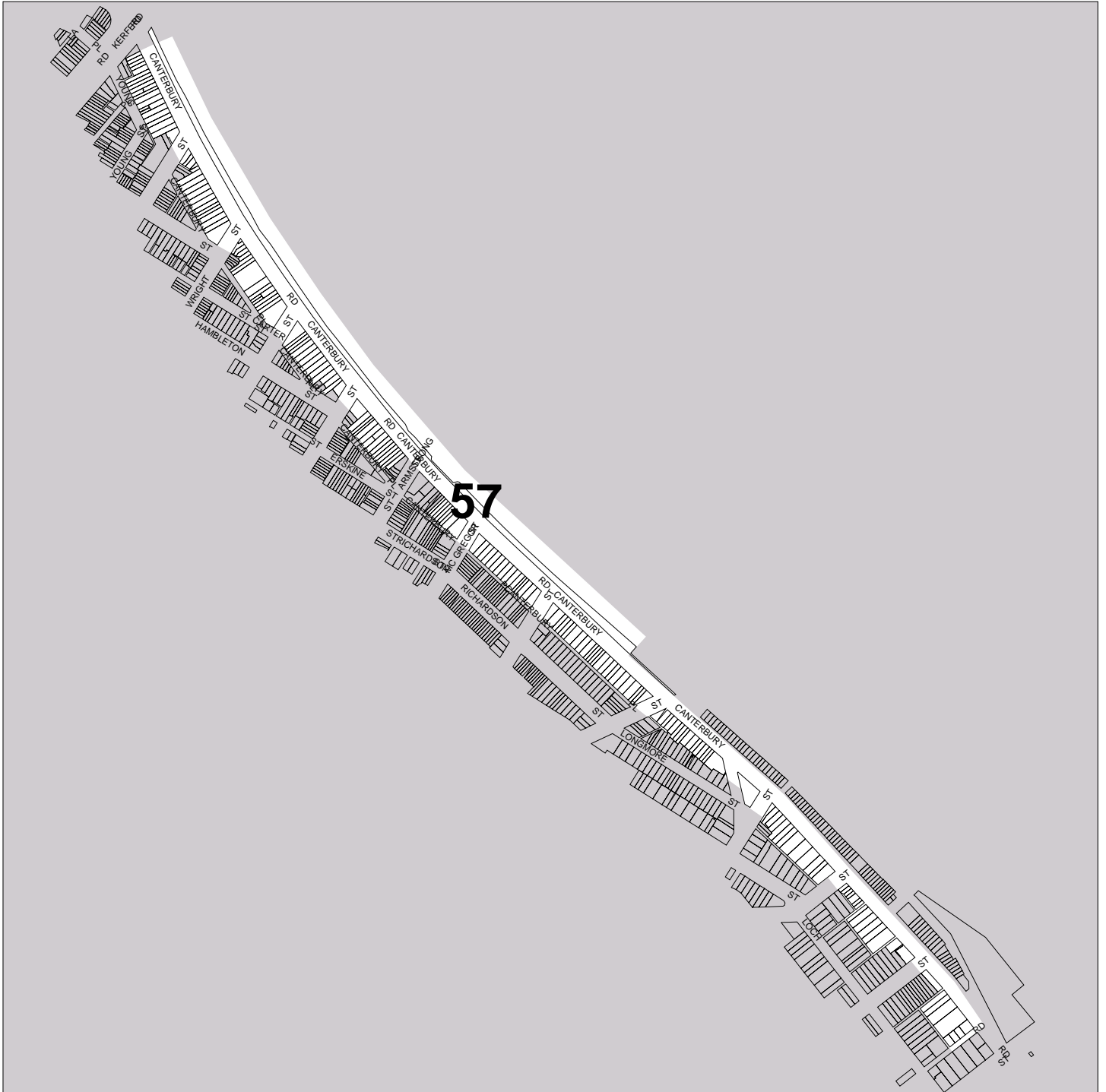
A comprehensive urban character assessment and policy statement for the area is available from the Council .



City of Port Phillip
URBAN CHARACTER DESCRIPTION

AREA 57: CANTERBURY ROAD BETWEEN KERFORD ROAD AND FITZROY STREET

The following description should be used in carrying out the site analysis and design response to your site.
Pay attention to the very important and important character elements listed here, as well as the actual conditions observed at the site analysis stage.



Canterbury Road between Fitzroy St and Kerford Rd.

Streets in the area:

Canterbury Rd, Cowderoy St, Deakin St, Harold St, Langridge St, McGregor St, West Beach Rd,

Description of the character of the area:

The area is distinguished from the adjoining residential areas by the slightly larger scale of buildings, their orientation towards Albert Park and the heavy traffic volumes along Canterbury Road.

It was predominantly developed in the Victorian and Edwardian periods with medium and large sized detached and terraced family houses. Most of these houses remain today and the area is an integral part of a large consistent heritage area covering all of Middle Park.

Canterbury Road has been redeveloped to cater for increasing traffic volumes and now has concrete kerbs and a median throughout its length.

Recent street trees planting has been incorporated within the parking lanes. Street trees are still quite mixed but an attempt has been made with recent planting to establish greater consistency.

<i>Element</i>	<i>Description</i>
Dominant style	pre-1900s
Dominant scale	one and two storey
Dominant material	stucco and render/minor variation
Dominant roof	pitched/metal
Setback	less than 5 metres/consistent
Heritage	entire area identified as heritage place
Gardens	visible/medium/small
Fences	medium height/consistent materials

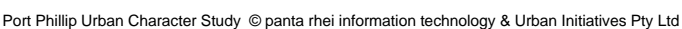
The existing urban character of the City of Port Phillip has been assessed using 24 character elements. The design should especially respond to the important and very important elements, which in this area are:

<i>Element</i>	<i>Importance</i>
Traffic	Very important
Open space	Very important
Setbacks	Important
Front gardens	Important
Building style	Important
Building scale	Important
Housing type	Important
Heritage	Important
Land use	Important
Access	Important

A comprehensive urban character assessment and policy statement for the area is available from the Council .



The following description should be used in carrying out the site analysis and design response to your site. Pay attention to the very important and important character elements listed here, as well as the actual conditions observed at the site analysis stage.



Kerferd Rd, Canterbury Rd, Beaconsfield Pde.

Streets in the area:

Kerferd Rd, Neville St, Richardson St, Young St,

Description of the character of the area:

This area is distinguished from adjoining residential areas by the extraordinary scale of the street reservation and its wide median providing internal views. Traffic is another factor that separates it from adjoining, quieter areas.

It has been developed with a range of housing styles and types from Victorian, Edwardian and inter-war periods. Although lots are mixed and relatively small, some of the houses in the street are quite grand. Houses and apartments are generally well maintained with consistent front fences and attractive gardens.

Street trees vary along the street with mature Plane trees east of Richardson Street and young flowering gums of Norfolk Pines to the west. Street trees planted in nature strips are extremely mixed and inconsistent and suffer from the influence of power lines along both sides of the street.

Much of the original detailing including bluestone kerbs and wide channel drains remain, however traffic management devices and signs have a significant negative presence.

<i>Element</i>	<i>Description</i>
Dominant style	pre-1900s
Dominant scale	one and two storey
Dominant material	brick or masonry
Dominant roof	pitched/metal
Setback	less than 5 metres/minor variation
Heritage	entire area identified as heritage place
Gardens	visible/medium/small
Fences	medium height/consistent materials

The existing urban character of the City of Port Phillip has been assessed using 24 character elements. The design should especially respond to the important and very important elements, which in this area are:

<i>Element</i>	<i>Importance</i>
Street dimensions	Very important
Building style	Very important
Heritage	Very important
Views	Important
Pattern	Important
Traffic	Important
Street trees	Important
Fences	Important
Setbacks	Important
Front gardens	Important
Building scale	Important
Housing type	Important
Land use	Important
Open space	Important
Foreshore	Important

A comprehensive urban character assessment and policy statement for the area is available from the Council .



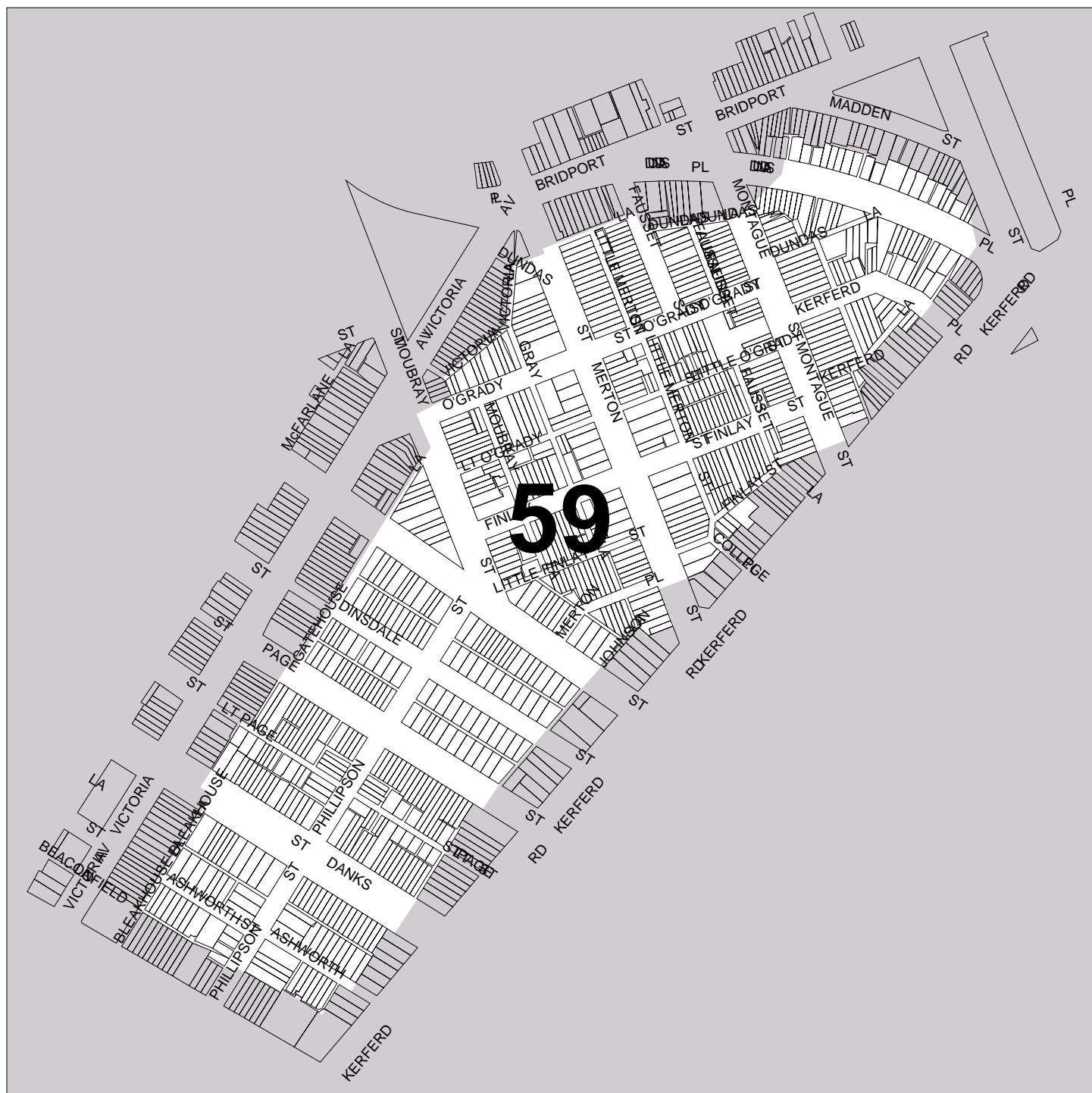
City of Port Phillip

URBAN CHARACTER DESCRIPTION

AREA 59: LAND GENERALLY BOUNDED BY VICTORIA AVENUE, BRIDPORT PLACE, KERFERD ROAD AND ASHWORTH STREET

The following description should be used in carrying out the site analysis and design response to your site.

Pay attention to the very important and important character elements listed here, as well as the actual conditions observed at the site analysis stage.



Dundas Pl, Kerford Rd, Victoria Ave, Beaconsfield Pde.

Streets in the area:

Ashworth St, Bleakhouse La, College Pl, Danks St, Dinsdale St, Dundas Pl, Faussett La, Finlay St, Gatehouse La, Gray La, Johnson La, Kerferd La, Kerferd Pl, Little Finlay St, Little Merton St, Little O'grady St, Little Page St, Merton Pl, Merton St, Montague La, Montague St, Moubray La, O'grady St, Page St, Philipson St, Richardson St,

Description of the character of the area:

The area is flat and streets vary from wide to narrow streets and laneways. The grid pattern shifts direction north of Richardson Street creating some angled intersections.

Lots are quite small throughout the area and the frontage width varies from narrow terraces to smaller double fronted houses. Setbacks also vary from buildings on property lines up to about five metres.

The area north of Richardson Street has a significant proportion of two storey houses and some three storey buildings, while the area south of Richardson Street is predominantly single storey.

The wider streets in the area tend to have fairly consistent larger scale Plane trees, with the exception of Danks Street which has a large median with Eucalypts. Small streets have very mixed plantings of a wide variety of species. Original kerbs and channels have been replaced with concrete through most of the area and many streets also have landscaped traffic management devices which detract from the consistency and heritage qualities of the area.

<i>Element</i>	<i>Description</i>
Dominant style	pre-1900s
Dominant scale	one and two storey
Dominant material	stucco and render/mixed
Dominant roof	pitched/metal
Setback	less than 5 metres/consistent
Heritage	entire area identified as heritage place
Gardens	visible/small
Fences	medium height/various materials

The existing urban character of the City of Port Phillip has been assessed using 24 character elements. The design should especially respond to the important and very important elements, which in this area are:

<i>Element</i>	<i>Importance</i>
Lot size	Important
Pattern	Important
Street dimensions	Important
Setbacks	Important
Building style	Important
Building scale	Important
Housing type	Important
Heritage	Important
Land use	Important
Access	Important
Foreshore	Important

A comprehensive urban character assessment and policy statement for the area is available from the Council .



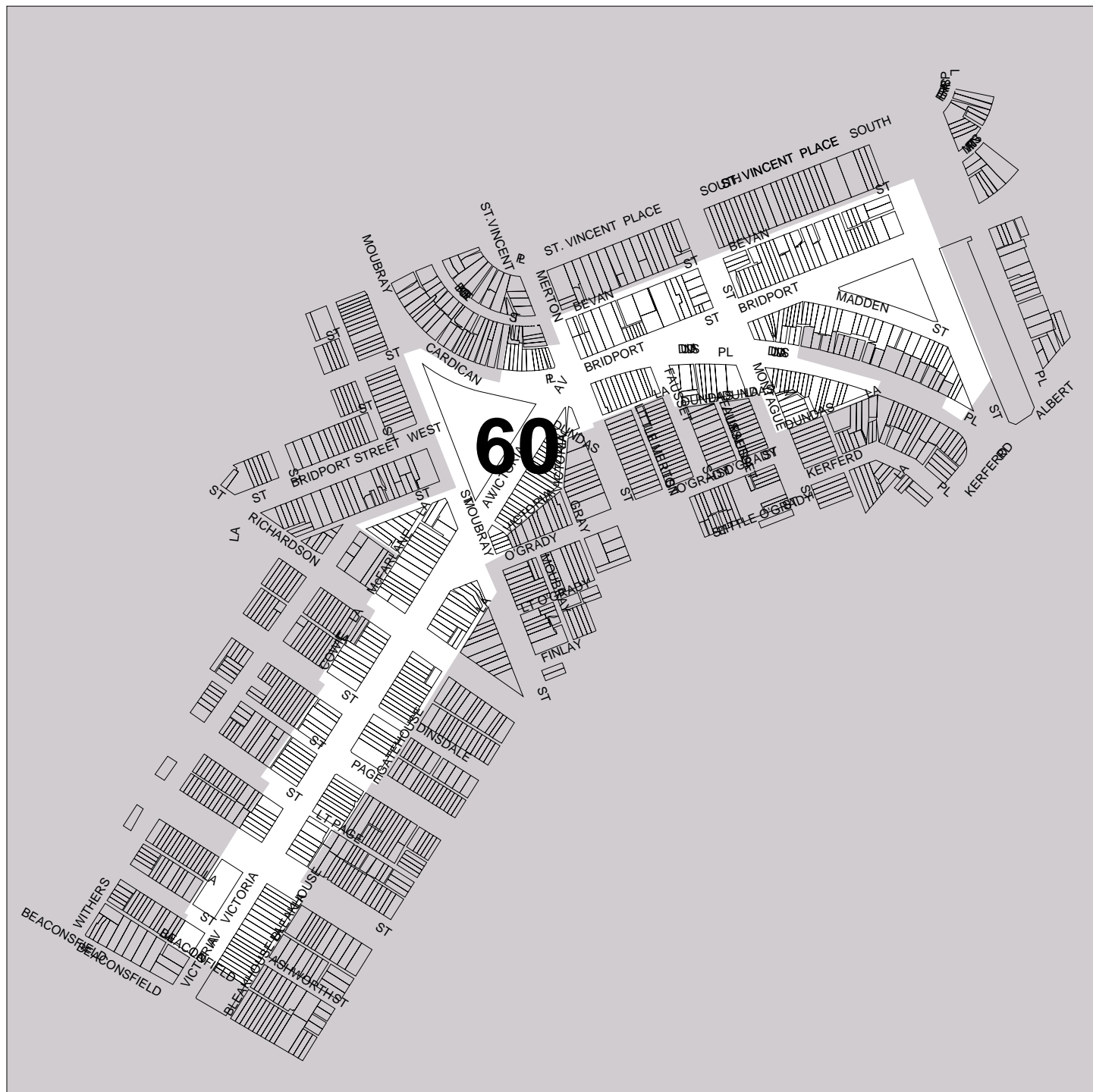
City of Port Phillip

URBAN CHARACTER DESCRIPTION

AREA 60: VICTORIA AVENUE AND BRIDPORT STREET BETWEEN BEACONSFIELD PARADE AND FERRARS STREET

The following description should be used in carrying out the site analysis and design response to your site.

Pay attention to the very important and important character elements listed here, as well as the actual conditions observed at the site analysis stage.



Boundary streets:

Victoria Ave, and Bridgeport St between Ferrars St, Beaconsfield Pde.

Streets in the area:

Bridport St, Danks St, Dinsdale St, Dundas La, Dundas Pl, Faussett St, Ferrars St, Gray La, Madden St, Mcfarlane La, Merton St, Montague La, Montague St, Moubray La, Moubray St, Richardson St, Victoria Av, Victoria La,

Description of the character of the area:

As the commercial and social focus for the Albert Park area, the area is distinguished from adjoining areas by its commercial land use and the presence of traffic and trams along its length.

It includes some residential properties mostly on the north side of Victoria Avenue and in Madden Street. These are usually Victorian and Edwardian terraces and detached houses with small setbacks and consistent front fences.

Bridport Street is the commercial heart of the area with shops, offices and restaurants on both frontages and a sense of place created by small triangular open spaces where angled streets intersect.

The area includes a range of street trees at various stages of development including some mature Elms near Moubray Street. Residential frontages also have median strips.

<i>Element</i>	<i>Description</i>
Dominant style	pre-1900s
Dominant scale	one and two storey
Dominant material	stucco and render/minor variation
Dominant roof	no dominance/metal
Setback	less than 5 metres/minor variation
Heritage	entire area identified as heritage place
Gardens	visible/small
Fences	buildings to property line

The existing urban character of the City of Port Phillip has been assessed using 24 character elements. The design should especially respond to the important and very important elements, which in this area are:

<i>Element</i>	<i>Importance</i>
Land use	Very important
Street dimensions	Important
Parking	Important
Footpaths	Important
Street detailing	Important
Fences	Important
Setbacks	Important
Building style	Important
Building scale	Important
Heritage	Important
Access	Important
Foreshore	Important

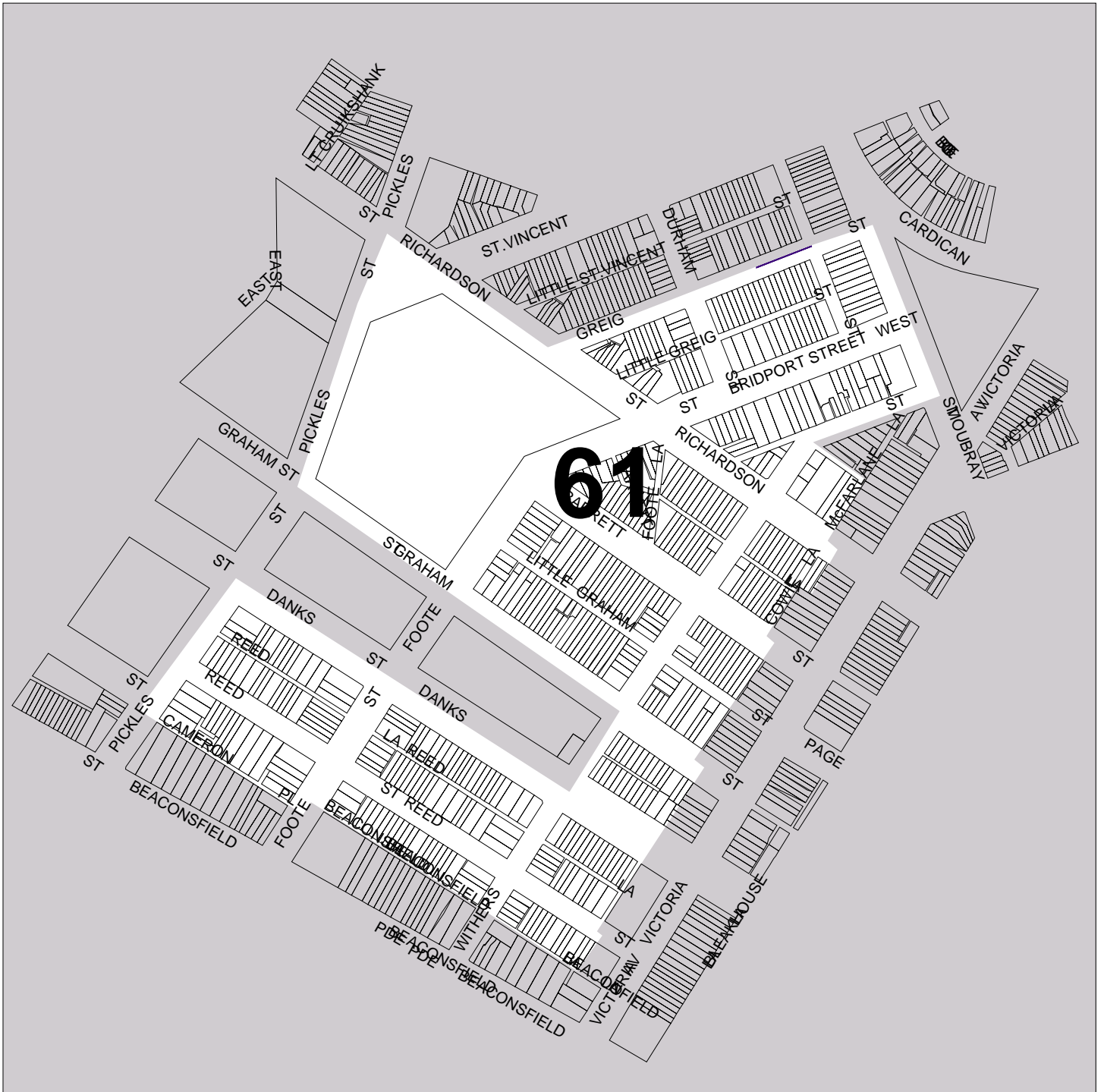
A comprehensive urban character assessment and policy statement for the area is available from the Council .



City of Port Phillip
URBAN CHARACTER DESCRIPTION

AREA 61: LAND GENERALLY BOUNDED BY PICKLES STREET, GREIG STREET, VICTORIA AVENUE AND BEACONSFIELD PARADE

The following description should be used in carrying out the site analysis and design response to your site. Pay attention to the very important and important character elements listed here, as well as the actual conditions observed at the site analysis stage.



Reed St, Victoria Pde, Pickles St, Greig St, Moray St.

Streets in the area:

Barrett La, Barrett St, Bridport St West, Cowie La, Danks St, Foote La, Foote St, Graham St, Little Graham St, Little Greig St, Little Withers St, Moubray St, Pickles St, Reed La, Reed St, Richardson St, Withers St,

Description of the character of the area:

This area is characterised by small lots and small scale Victorian housing with narrow frontages and reduced setbacks. Many houses in the area are timber, either terraces or detached cottages.

Streets are quite varied in their dimensions with some wide streets, and a secondary system of small streets and a network of rear laneways. Original detailing in these streets has been replaced with simple concrete kerb and channel and only a few laneways retain original bluestone pavements and detailing.

Some streets have mature trees especially gums in Danks Street and Planes elsewhere, however the area has been subject to many planting fashions resulting in inconsistent and mixed planting in many streets.

The area is close to the foreshore and the Victoria Avenue commercial area.

<i>Element</i>	<i>Description</i>
Dominant style	pre-1900s
Dominant scale	one storey
Dominant material	timber or cladding/minor variation
Dominant roof	pitched/metal
Setback	less than 5 metres/consistent
Heritage	entire area identified as heritage place
Gardens	visible/small
Fences	medium height/varied materials

The existing urban character of the City of Port Phillip has been assessed using 24 character elements. The design should especially respond to the important and very important elements, which in this area are:

<i>Element</i>	<i>Importance</i>
Building scale	Very important
Lot size	Important
Street dimensions	Important
Fences	Important
Setbacks	Important
Building style	Important
Housing type	Important
Heritage	Important
Land use	Important
Foreshore	Important

A comprehensive urban character assessment and policy statement for the area is available from the Council .



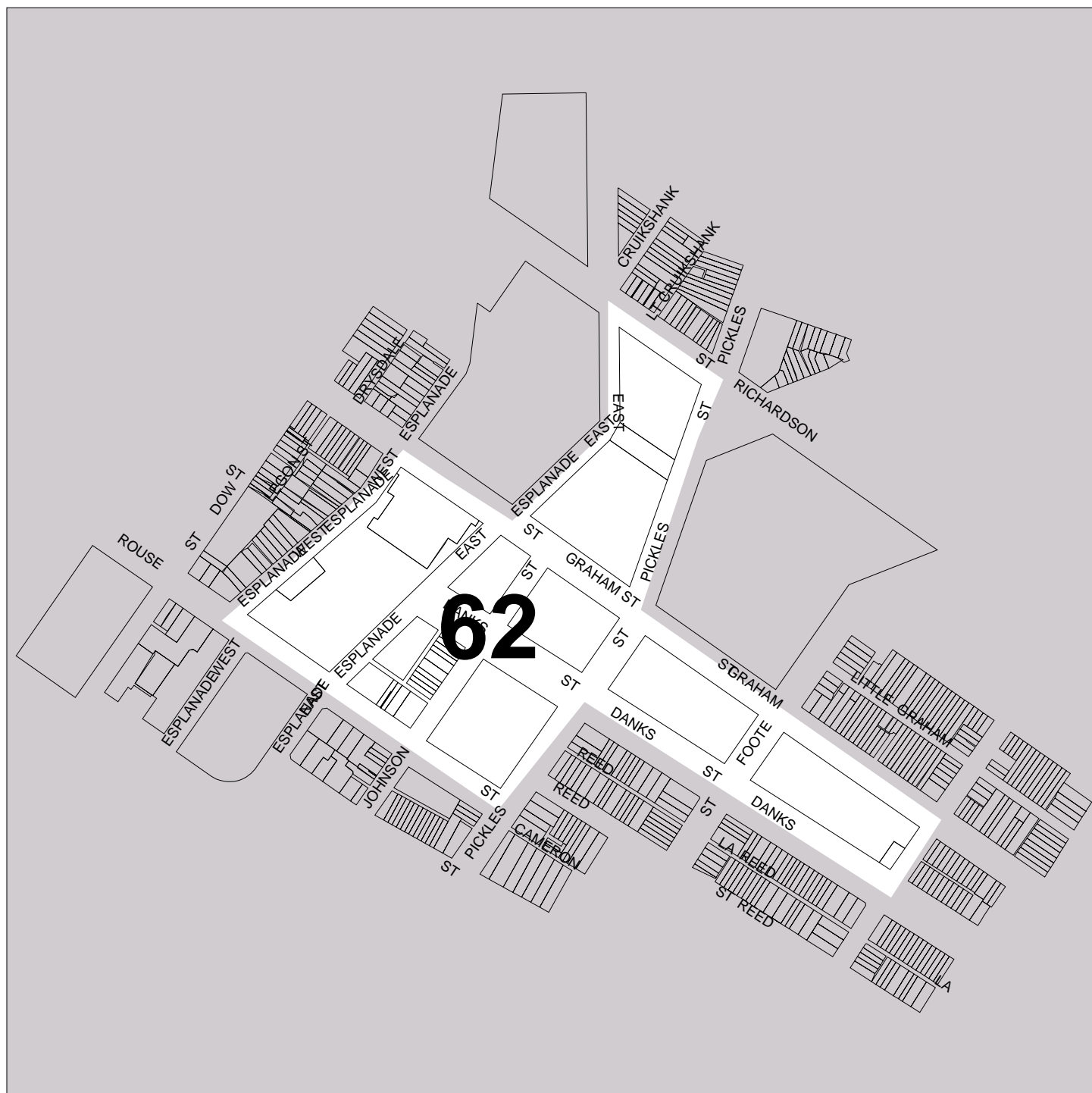
City of Port Phillip

URBAN CHARACTER DESCRIPTION

AREA 62: LAND GENERALLY BOUNDED BY ROUSE STREET, PICKLES STREET, ESPLANADE WEST AND FOOT STREET, PORT MELBOURNE

The following description should be used in carrying out the site analysis and design response to your site.

Pay attention to the very important and important character elements listed here, as well as the actual conditions observed at the site analysis stage.



Boundary streets:

Liardet St, Whithers St, Rouse St, Esplanade West.

Streets in the area:

Danks St, Esplanade East, Graham St, Johnson St, Pickles St,

Description of the character of the area:

This area is differentiated from adjoining areas by the existence of many large undeveloped lots on the verge of transition from industrial uses to high density housing.

The scale of buildings in the area is also quite different because industrial and educational buildings are generally equivalent to three or four storey housing developments. Given these circumstances, the area includes one high quality public housing estate consisting of four stories.

Original street detailing has been replaced with concrete kerb and channel whilst footpaths are a mix of bitumen and concrete. Overhead services have a strong visual presence, partly because street trees are relatively inconsistent and inadequate.

<i>Element</i>	<i>Description</i>
Dominant style	1970s - 1990s
Dominant scale	one, two and three storey
Dominant material	brick or masonry/consistent
Dominant roof	pitched/metal
Setback	less than 5 metres/minor variation
Heritage	limited number of heritage places
Gardens	visible/small
Fences	various heights and materials

The existing urban character of the City of Port Phillip has been assessed using 24 character elements. The design should especially respond to the important and very important elements, which in this area are:

<i>Element</i>	<i>Importance</i>
Lot size	Very important
Pattern	Important
Building scale	Important
Land use	Important
Access	Important
Foreshore	Important

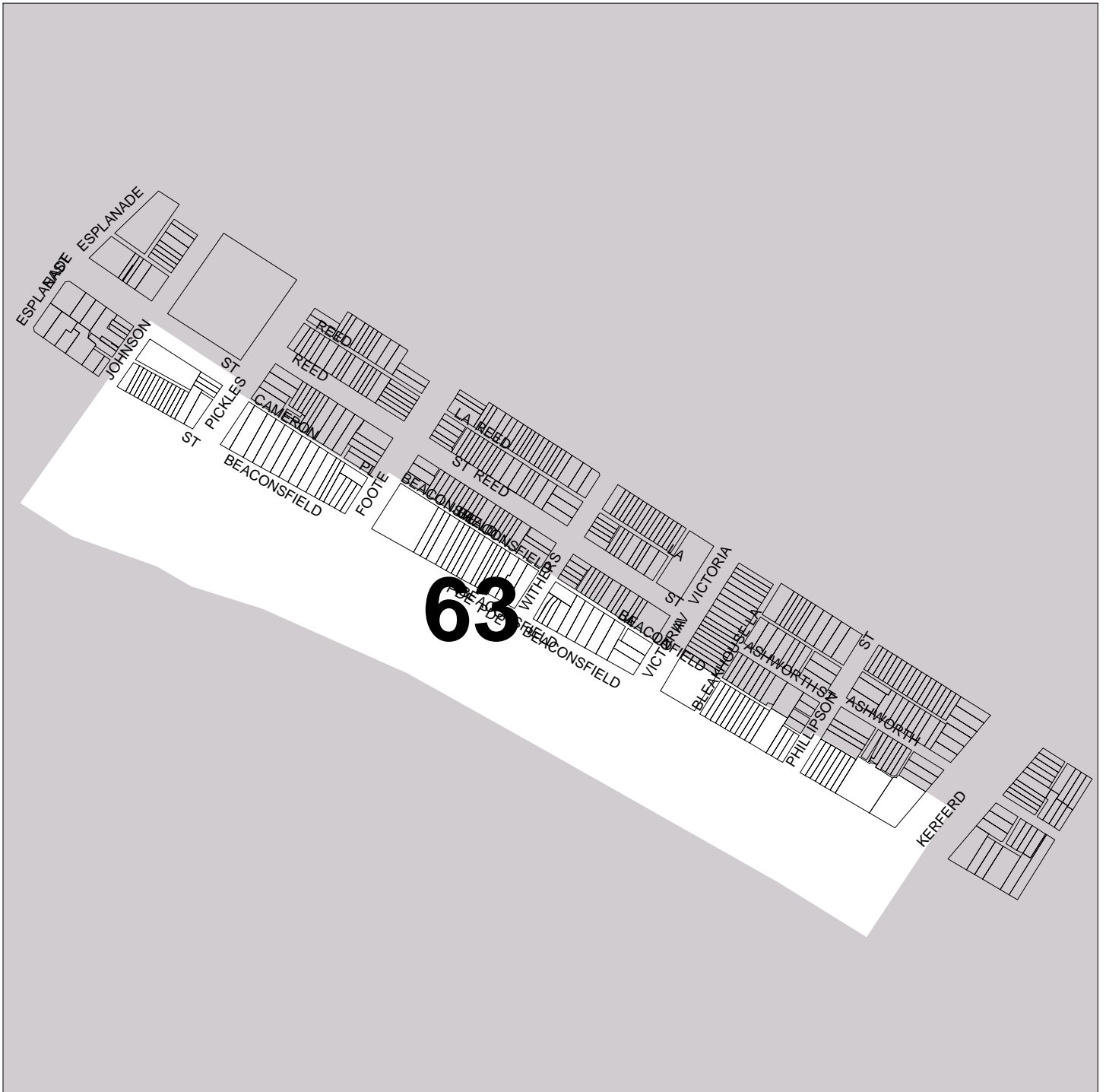
A comprehensive urban character assessment and policy statement for the area is available from the Council .



City of Port Phillip
URBAN CHARACTER DESCRIPTION

**AREA 63: BEACONSFIELD PARADE BETWEEN JOHNSON STREET AND KERFORD ROAD,
PORT MELBOURNE**

The following description should be used in carrying out the site analysis and design response to your site.
Pay attention to the very important and important character elements listed here, as well as the actual conditions observed at the site analysis stage.



Beaconsfield Pde, Kerford Rd, Johnson St.

Streets in the area:

Beach St, Beaconsfield La, Beaconsfield Pde, Cameron Pl, Johnson St,

Description of the character of the area:

This area is distinguished from adjoining residential areas by its foreshore location with foreshore views and large scale buildings facing Beaconsfield Parade.

Housing in the area was originally developed in the Victorian period and many of the terraces, detached houses, commercial and institutional buildings still remain. Despite significant renovation and replacement with apartment buildings, new houses and a service station, the area retains its predominantly two storey scale and fairly consistent setback except at the northern end which is single storey.

Whilst Beaconsfield Parade is a wide and busy arterial road with associated traffic noise, the landscape character of palm trees which frame views to the water and the beach front promenade add to the character of the area.

These attributes combine to make the Albert Park foreshore a major tourist destination that generates significant on-street parking and tourist activity.

<i>Element</i>	<i>Description</i>
Dominant style	pre-1900s
Dominant scale	one and two storey
Dominant material	stucco and render/minor variation
Dominant roof	concealed by parapets/metal
Setback	less than 5 metres/minor variation
Heritage	@75% of identified heritage places
Gardens	visible/hidden/small
Fences	medium height/various materials

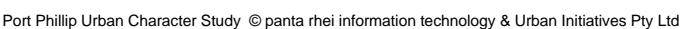
The existing urban character of the City of Port Phillip has been assessed using 24 character elements. The design should especially respond to the important and very important elements, which in this area are:

<i>Element</i>	<i>Importance</i>
Views	Very important
Street dimensions	Very important
Traffic	Very important
Street trees	Very important
Foreshore	Very important
Parking	Important
Footpaths	Important
Setbacks	Important
Building style	Important
Building scale	Important
Housing type	Important
Heritage	Important
Land use	Important
Access	Important

A comprehensive urban character assessment and policy statement for the area is available from the Council .



The following description should be used in carrying out the site analysis and design response to your site. Pay attention to the very important and important character elements listed here, as well as the actual conditions observed at the site analysis stage.



Boundary streets:

Bay St, Crockford St, Nelson Rd, St Vincents St, Esplanade West, Rouse St.

Streets in the area:

Bridge St, Bridge St, Coventry St, Cruikshank Pl, Cruikshank St, Davies St, Dorcas La, Dorcas St, Dow St, Drysdale St, Durham St, Esplanade East, Esplanade Pl, Esplanade West, Glover St, Graham St, Greig St, Henderson St, Iffla St, Ingles St, Kyme Pl, Liardet St, Little Boundary St, Little Cruikshank St, Little Glover St, Little Iffla St, Little Lyell St, Little Mountain St, Little St Vincent St, Little Tribe St, Lyell St, Lyons St, McCormack St, Mountain St, Normanby St, Pickles St, Raglan St, Seisman St, Smith St, Smith St, Spring St East, St Vincent St, Tribe St, Lagoon La, Mclarty La,

Description of the character of the area:

The area is characterised by consistently small, narrow lots with small scale housing mostly from the Victorian period behind reduced set backs. Also, many of the terraces and detached cottages are constructed in timber.

The area also includes a proportion of new infill development consisting of two storey brick dwellings and walk-up public housing blocks in Ingles Street.

Streets are quite varied in their dimensions with some wide streets, a secondary system of small streets and a network of rear laneways. Original detailing in these streets has been largely replaced with simple concrete kerb and channel and only a few laneways retain original bluestone pavements and detailing.

Some streets have mature trees, however, the area has been subject to many planting fashions resulting in inconsistent and mixed planting. Street trees in the old Port Melbourne part of the area are generally of poorer quality with more mixed species.

<i>Element</i>	<i>Description</i>
Dominant style	pre-1900s
Dominant scale	one storey
Dominant material	timber or cladding/minor variation
Dominant roof	pitched/metal
Setback	less than 5 metres/consistent
Heritage	@90% identified heritage places
Gardens	visible/small
Fences	medium height/various materials

The existing urban character of the City of Port Phillip has been assessed using 24 character elements. The design should especially respond to the important and very important elements, which in this area are:

<i>Element</i>	<i>Importance</i>
Lot size	Very important
Building scale	Very important
Pattern	Important
Fences	Important
Setbacks	Important
Building style	Important
Housing type	Important
Heritage	Important

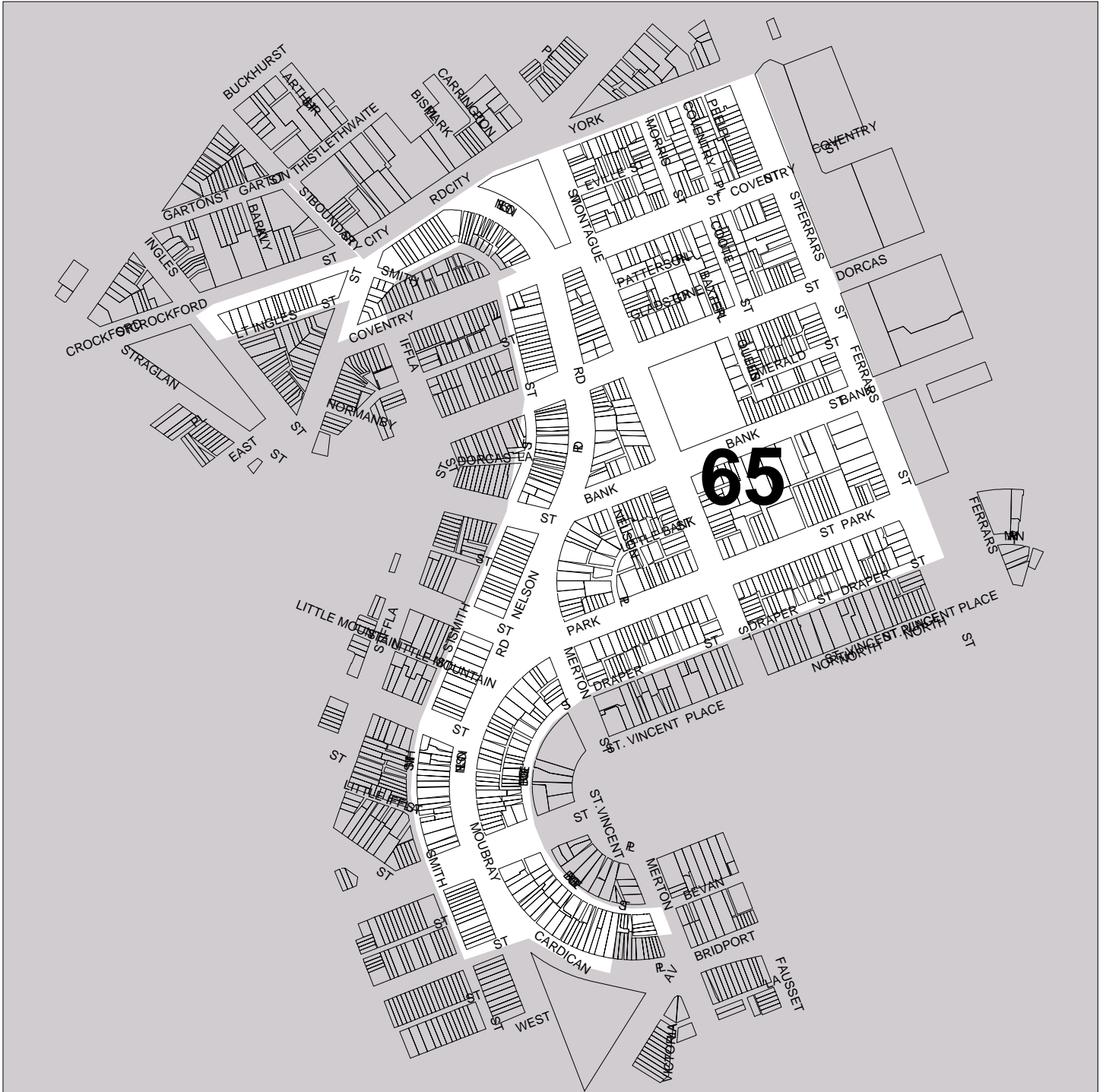
A comprehensive urban character assessment and policy statement for the area is available from the Council .



City of Port Phillip
URBAN CHARACTER DESCRIPTION

AREA 65: LAND GENERALLY BOUNDED BY YORK STREET, FERRARS STREET, DRAPER STREET AND NELSON ROAD, PORT MELBOURNE

The following description should be used in carrying out the site analysis and design response to your site. Pay attention to the very important and important character elements listed here, as well as the actual conditions observed at the site analysis stage.



Boundary streets:

Ferrars St, Draper St, York St, Smith St.

Streets in the area:

Bank St, Baxter Pl, Brooke St South, Cardigan Pl, Coote St, Coventry Pl, Coventry St, Dorcas St, Draper St, Emerald St, Eville St, Ferrars St, Gladstone Gv, Little Ingles St, Merton St, Montague St, Morris St, Moubray St, Nelson Pl, Nelson Rd, Park St, Patterson Pl, Pickles St, Queen St, Smith St,

Description of the character of the area:

This area is bounded by the light rail line to the east, industrial uses to the north, St Vincent Place to the south and similar residential areas to the west. It differs from the area to the west because of the steeper landform, sloping to the north and west, and more variety in land use and building scale. The area also retains more of its original street detailing.

It has a regular grid pattern of wide streets and an overlaying pattern of narrow small scale streets. The western edge of this grid is formed by the curvilinear Nelson Road creating unusual intersections and taped allotments.

The area was substantially developed during the Victorian period and includes some significant institutional buildings and a number of corner hotels. A limited amount of redevelopment in subsequent periods includes multi-unit housing in the post-war period.

Street trees are present in all but the narrowest of streets however the area has been subject to numerous planting fashions with remnants of each style remaining in many streets.

<i>Element</i>	<i>Description</i>
Dominant style	pre-1900s
Dominant scale	one and two storey
Dominant material	stucco and render/mixed
Dominant roof	pitched/metal
Setback	less than 5 metres/minor variation
Heritage	entire area identified as heritage place
Gardens	visible/small
Fences	medium height/various materials

The existing urban character of the City of Port Phillip has been assessed using 24 character elements. The design should especially respond to the important and very important elements, which in this area are:

<i>Element</i>	<i>Importance</i>
Building style	Very important
Lot size	Important
Street dimensions	Important
Street detailing	Important
Fences	Important
Setbacks	Important
Building scale	Important
Heritage	Important
Access	Important

A comprehensive urban character assessment and policy statement for the area is available from the Council .

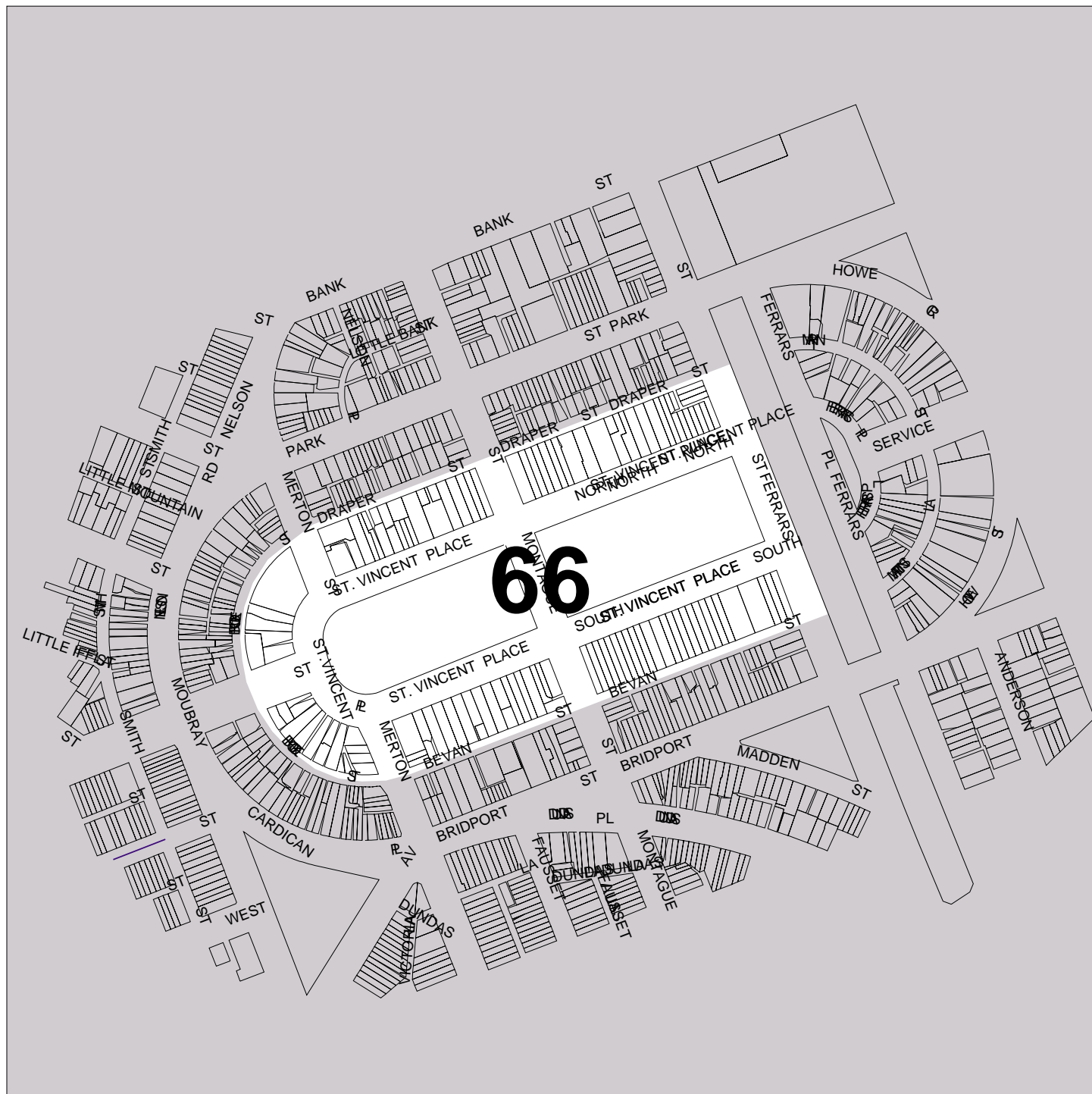
City of Port Phillip

URBAN CHARACTER DESCRIPTION

AREA 66: ST VINCENTS PLACE

The following description should be used in carrying out the site analysis and design response to your site.

Pay attention to the very important and important character elements listed here, as well as the actual conditions observed at the site analysis stage.



Ferrars St, Draper St, Bevan St, Brooke St.

Streets in the area:

Bevan St, Brooke St North, Ferrars St, Merton St, Montague St, St Vincent PI North, St Vincent PI South, St Vincent St,

Description of the character of the area:

This area includes housing which faces St Vincent Gardens and it is distinct from adjoining areas because of the subdivision design around the gardens and the larger scale of Victorian dwellings.

The area has especially high heritage value due to the presence of the gardens, the consistent residential style and the original street detailing.

Front fences, front gardens and generous street dimensions are also important contributors to the character of the area.

<i>Element</i>	<i>Description</i>
Dominant style	pre-1900s
Dominant scale	two storey
Dominant material	stucco and render/consistent
Dominant roof	concealed by parapet/slate
Setback	5-9 metres/minor variation
Heritage	entire area identified as heritage place
Gardens	visible/medium/small
Fences	medium height/consistent

The existing urban character of the City of Port Phillip has been assessed using 24 character elements. The design should especially respond to the important and very important elements, which in this area are:

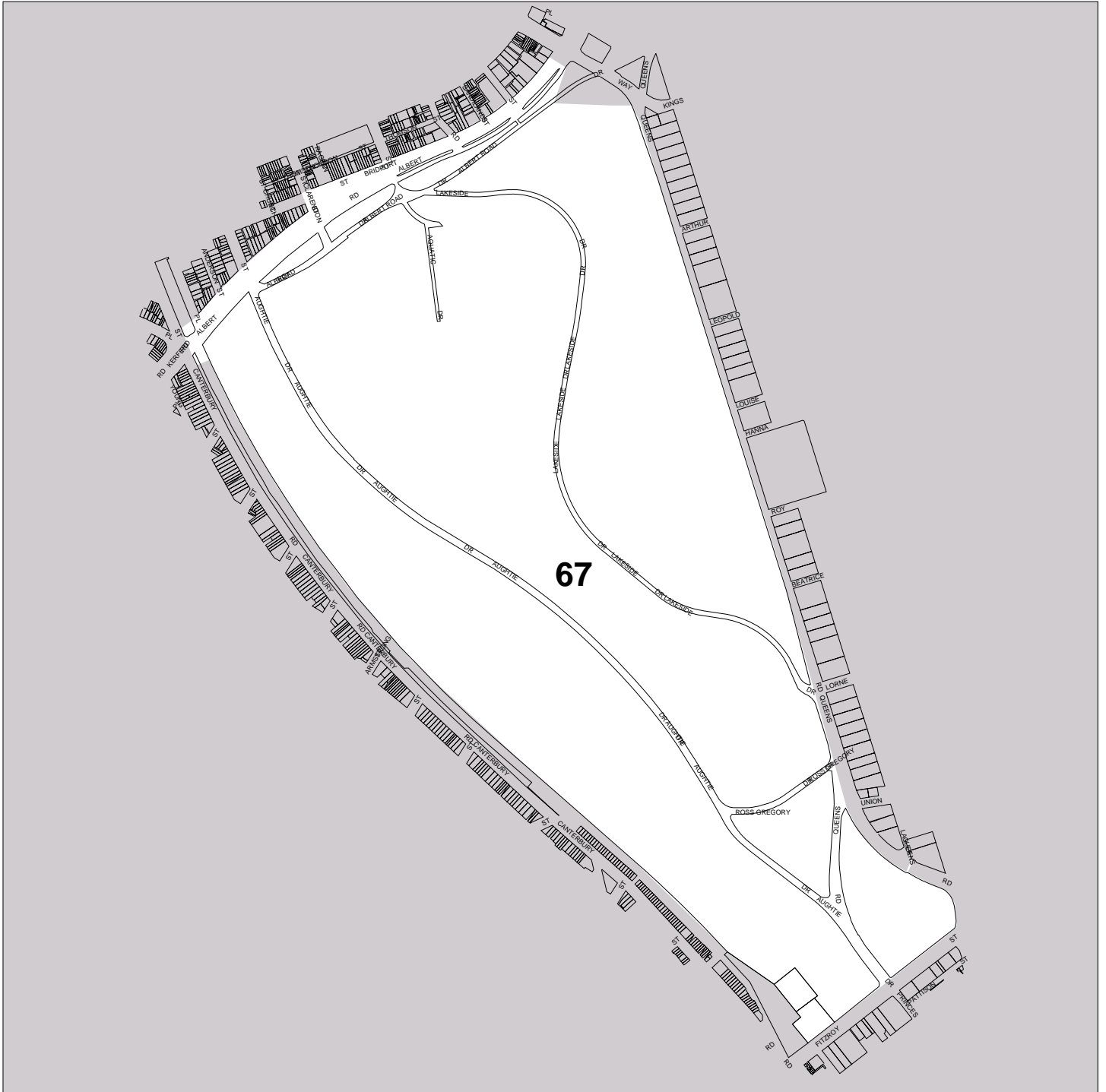
<i>Element</i>	<i>Importance</i>
Pattern	Very important
Building style	Very important
Heritage	Very important
Open space	Very important
Views	Important
Street dimensions	Important
Street trees	Important
Street detailing	Important
Fences	Important
Front gardens	Important
Building scale	Important
Land use	Important
Access	Important

A comprehensive urban character assessment and policy statement for the area is available from the Council .



City of Port Phillip
URBAN CHARACTER DESCRIPTION
AREA 67: ALBERT PARK

The following description should be used in carrying out the site analysis and design response to your site.
Pay attention to the very important and important character elements listed here, as well as the actual conditions observed at the site analysis stage.



Canterbury Rd, Fitzroy St, Queens Rd, Albert Rd.

Streets in the area:

Albert Rd, Queens Rd,

Description of the character of the area:

With the lake as the central focus, the park is an important recreation resource for surrounding residential areas and attracts more visitors than any other park in Melbourne.

It has an internal road system that provides access to numerous sporting facilities for both active and passive recreation and other institutional buildings used for education purposes.

The park has recently been redeveloped to accommodate the Melbourne Grand Prix with the park roads becoming a car racing circuit. During this period, approximately one month of each year, many temporary structures and facilities occupy the park.

The open nature of the park means that skyline views of city and St Kilda Road buildings are ever present. While the park has listed heritage status, modern buildings and landscape dominate the parks character over conventional heritage values.

<i>Element</i>	<i>Description</i>
Dominant style	pre-1900s
Dominant scale	two storey
Dominant material	timber or cladding/mixed
Dominant roof	flat/metal
Setback	Variable
Heritage	entire area identified as heritage place
Gardens	(no private domain)
Fences	(no private domain)

The existing urban character of the City of Port Phillip has been assessed using 24 character elements. The design should especially respond to the important and very important elements, which in this area are:

<i>Element</i>	<i>Importance</i>
Lot size	Very important
Signage	Very important
Open space	Very important
Views	Important
Traffic	Important
Building style	Important
Land use	Important

A comprehensive urban character assessment and policy statement for the area is available from the Council .



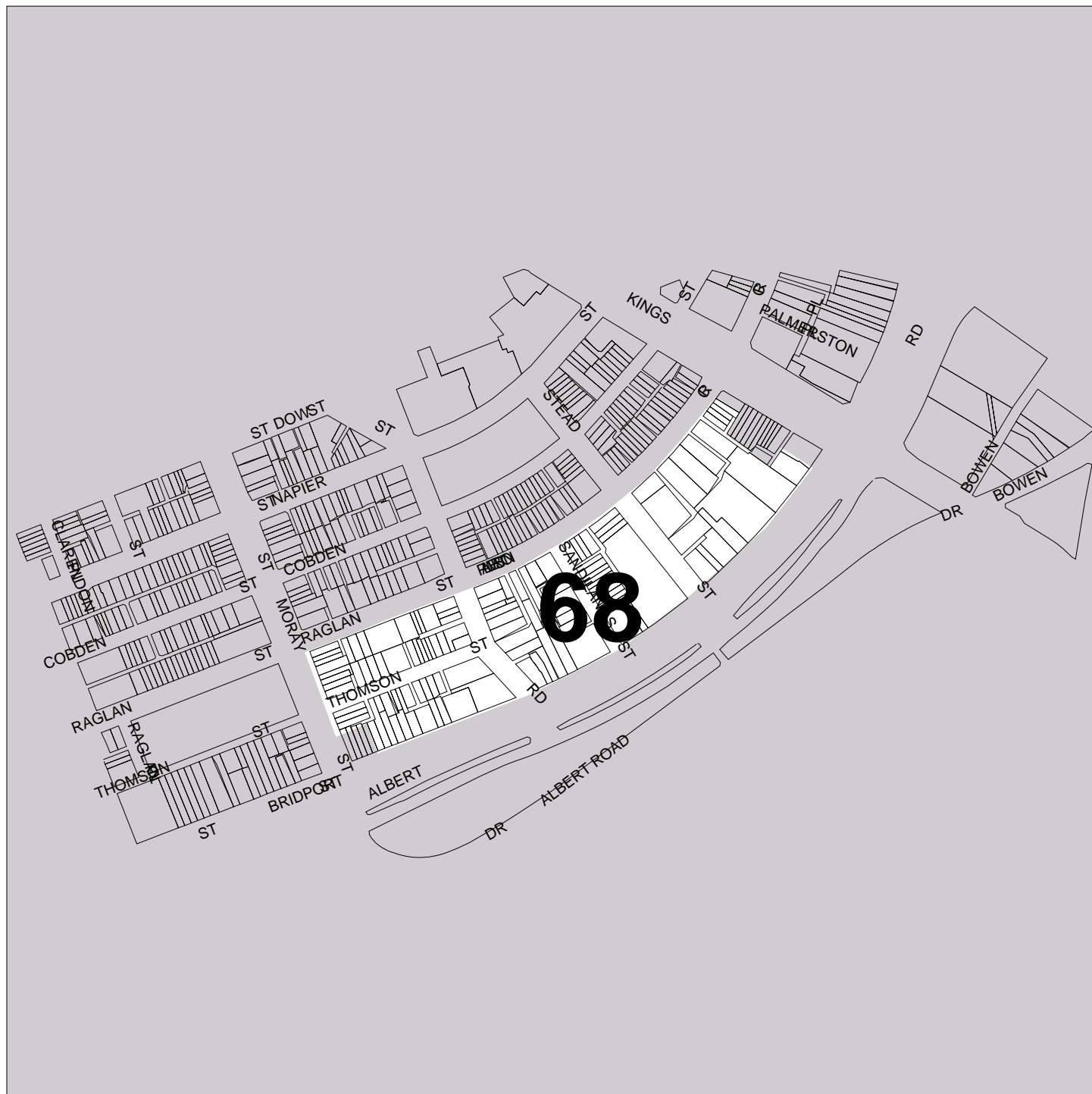
City of Port Phillip

URBAN CHARACTER DESCRIPTION

AREA 68: LAND GENERALLY BOUNDED BY ALBERT ROAD, MORAY STREET, RAGLAN STREET AND KINGSWAY, SOUTH MELBOURNE

The following description should be used in carrying out the site analysis and design response to your site.

Pay attention to the very important and important character elements listed here, as well as the actual conditions observed at the site analysis stage.



Albert Rd, Moray St, Palmerston St, Kings Way.

Streets in the area:

Sandilands St, Stead St, Thomson St,

Description of the character of the area:

This area is distinguished from adjoining areas to the north by its commercial land use and relatively large building scale. It is an extension of office development in St Kilda Road and Kings Way. The area has a mixed lot size with larger lots facing Albert Road, however, many of the original small lots have been amalgamated to create larger commercial sites.

The area retains little of its pre-war buildings with most being replaced during the 60's - 90's with office buildings of varied design, materials and scale. Many buildings are three storeys or more, with taller buildings closer to Kings Way overlooking Albert Road.

Most of the original street detailing has also been replaced in recent decades however the area retains consistent bitumen footpaths throughout.

The office use generates significant on-street parking despite the fact that most buildings have on-site car parks. Signage on buildings is generally quite discrete.

The area has good access to public transport and open space but is more distant from the core shopping and community area in Clarendon Street.

<i>Element</i>	<i>Description</i>
Dominant style	1970s -1990s
Dominant scale	more than 3 stories
Dominant material	mixed
Dominant roof	concealed by parapets/metal
Setback	less than 5 metres/minor variation
Heritage	no identified heritage places
Gardens	visible/small
Fences	no fences

The existing urban character of the City of Port Phillip has been assessed using 24 character elements. The design should especially respond to the important and very important elements, which in this area are:

<i>Element</i>	<i>Importance</i>
Building scale	Very important
Lot size	Important
Parking	Important
Building style	Important
Land use	Important
Access	Important
Open space	Important

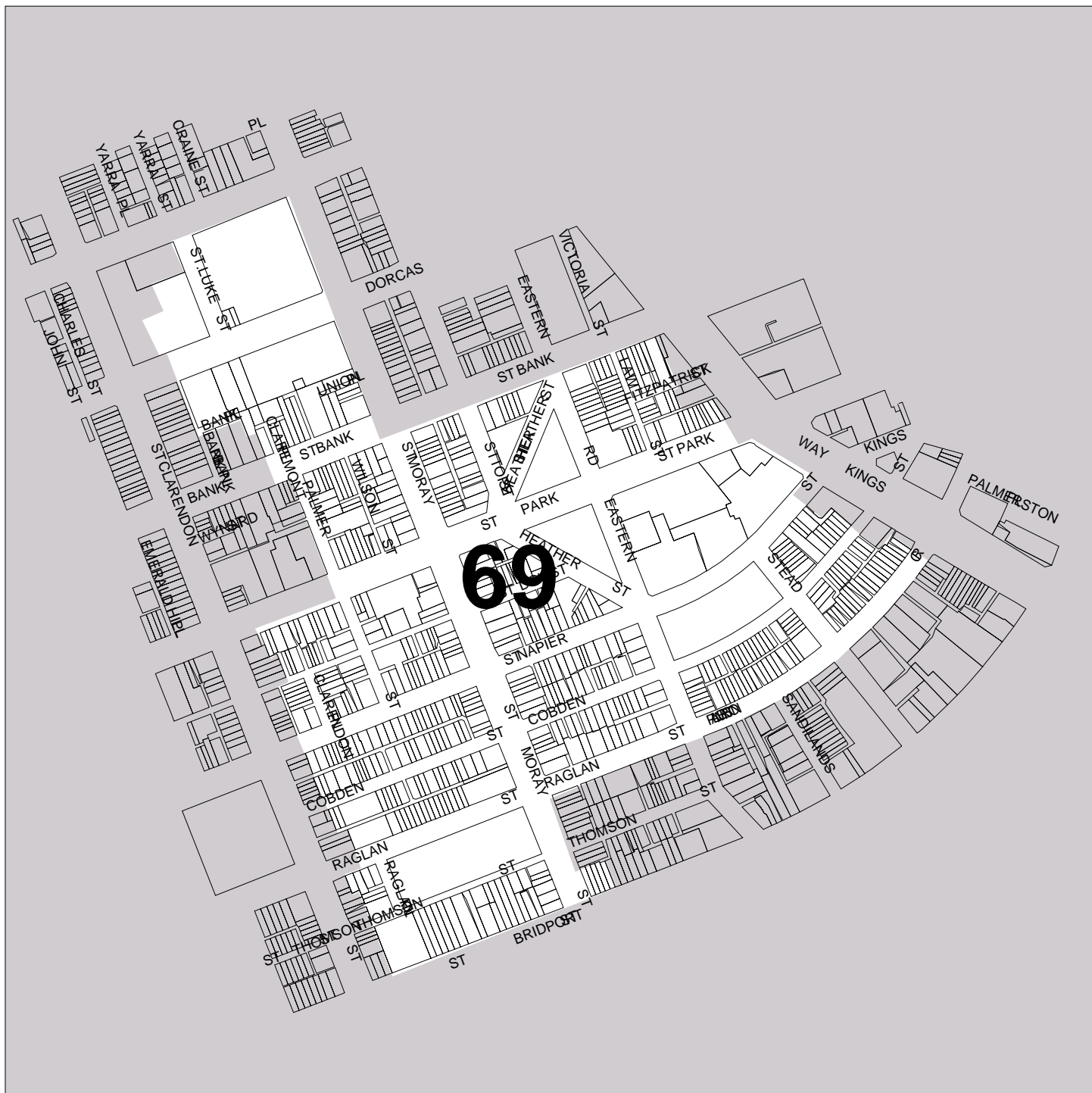
A comprehensive urban character assessment and policy statement for the area is available from the Council .

URBAN CHARACTER DESCRIPTION

AREA 69: AREA GENERALLY BOUNDED BY BRIDPORT STREET, KINGSWAY, CLARENDON STREET, COVENTRY STREET AND BANK STREET, SOUTH MELBOURNE

The following description should be used in carrying out the site analysis and design response to your site.

Pay attention to the very important and important character elements listed here, as well as the actual conditions observed at the site analysis stage.



Boundary streets:

Palmerston St, Kings Way, Banks St, Moray St, Coventry St, Clarendon St, Bridgeport St.

Streets in the area:

Bank Pl, Bank St, Clarendon Pl, Cobden St, Dorcas St, Dow St, Eastern Rd, Fitzpatrick St, Heather St, Law St, Moray St, Napier St, Palmer St, Palmerston Cr, Park St, Raglan St, Stead St, Thomson St, Union Pl, Wilson St,

Description of the character of the area:

The area has been subdivided into a grid of wide and narrow streets with a rich pattern of lane ways. The original pattern of streets has been modified with closures, one way streets and road narrowing to restrict through traffic to larger arterial roads.

Whilst most of the area has been developed with small lots and terraced housing constructed during the Victorian period, the area has undergone some redevelopment in the post-war period with several larger allotments being redeveloped with public and private medium and high density housing.

Much of the original bluestone kerb and channel and road geometry has been lost, however, the area retains consistent bitumen footpaths throughout. Car parking in the streets is visually dominant and overhead services also have some impact. Street trees have been introduced to most streets, but tend to be inconsistent in species and scale.

The area is ideally located in relation to shops open space and public transport. It also has a range of public facilities.

<i>Element</i>	<i>Description</i>
Dominant style	pre-1900s
Dominant scale	one and two storey
Dominant material	timber or cladding/mixed
Dominant roof	pitched/metal
Setback	less than 5 metres/minor variation
Heritage	@75% identified as heritage place
Gardens	visible/small
Fences	medium height/various materials

The existing urban character of the City of Port Phillip has been assessed using 24 character elements. The design should especially respond to the important and very important elements, which in this area are:

<i>Element</i>	<i>Importance</i>
Building style	Very important
Building scale	Very important
Parking	Important
Fences	Important
Setbacks	Important
Housing type	Important
Heritage	Important
Access	Important

A comprehensive urban character assessment and policy statement for the area is available from the Council .



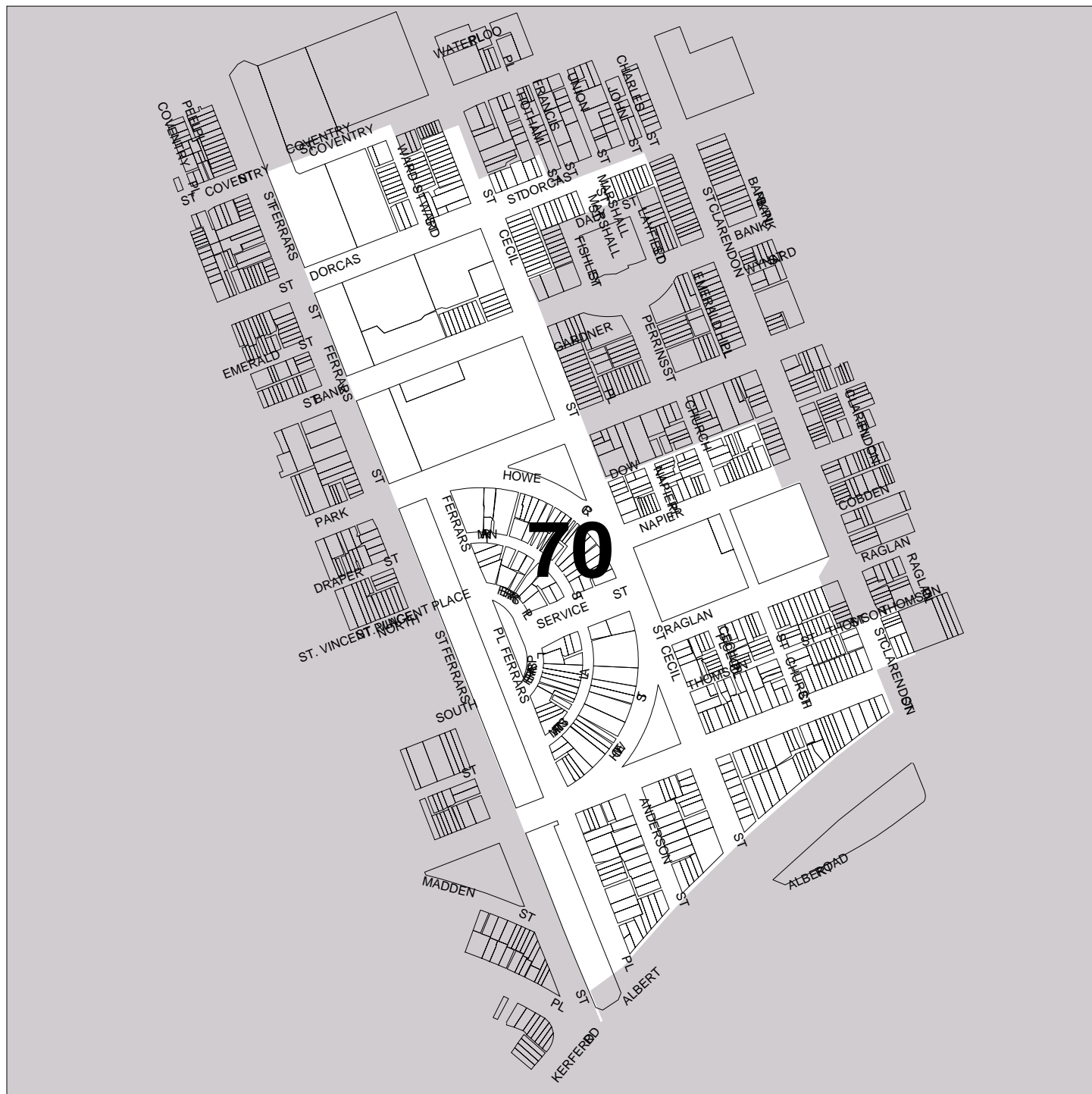
City of Port Phillip

URBAN CHARACTER DESCRIPTION

AREA 70: LAND GENERALLY BOUNDED BY ALBERT ROAD, FERRARS STREET, COVENTRY STREET AND CLARENDON STREET, SOUTH MELBOURNE

The following description should be used in carrying out the site analysis and design response to your site.

Pay attention to the very important and important character elements listed here, as well as the actual conditions observed at the site analysis stage.



Light rail, Albert Rd, Clarendon St, Cecil St, Coventry St.

Streets in the area:

Anderson St, Bank St, Bridport St, Cecil Pl, Cecil St, Church St, Clarendon St, Dorcas St, Emerald Hill Pl, Ferrars Pl, Fishley St, Howe Cr, James Service Pl, Layfield St, Marshall St, Martin St, Napier St, Park St, Raglan St, Thomson St, Ward St, Ferrars Pl,

Description of the character of the area:

The area has a formal subdivision pattern including the unusual curved streets of Howe Crescent and Ferrars Place. Many streets are wide, but there is an additional overlay of much smaller streets and lanes. Lot sizes and frontage width in the area are extremely varied although the greatest number of lots are less than 300 square metres.

The area was originally developed during the Victorian period and many houses and public buildings remain from this period. The area has also undergone redevelopment for private and public housing with the Park Towers estate on Park Street now a dominant building in the area. Larger institutional sites like the St Vincent Boys Home and Uniting Church site have been sub divided during the post-war period for multi-unit housing.

Streets in the area have been substantially modified with a range of bizarre traffic management devices and as a consequence, Victorian detailing is limited. Many roads have collector or arterial status and carry quite heavy volumes of through traffic whilst many others have been closed or made one way. Street trees are extremely mixed and do not reflect the grand proportions of many of the streets. The area has excellent accessibility.

<i>Element</i>	<i>Description</i>
Dominant style	pre-1900s
Dominant scale	one and two storey
Dominant material	stucco and render/minor variation
Dominant roof	pitched/metal
Setback	less than 5 metres
Heritage	entire area identified as heritage place
Gardens	visible/small
Fences	medium height/various materials

The existing urban character of the City of Port Phillip has been assessed using 24 character elements. The design should especially respond to the important and very important elements, which in this area are:

<i>Element</i>	<i>Importance</i>
Building style	Very important
Access	Very important
Lot size	Important
Pattern	Important
Street dimensions	Important
Traffic	Important
Parking	Important
Fences	Important
Building scale	Important
Housing type	Important
Heritage	Important
Open space	Important

A comprehensive urban character assessment and policy statement for the area is available from the Council .

URBAN CHARACTER DESCRIPTION

AREA 71: CLARENDON STREET BETWEEN GRANT STREET AND THOMPSON STREET, PARK STREET, CECIL STREET AND COVENTRY STREET, SOUTH MELBOURNE

The following description should be used in carrying out the site analysis and design response to your site.

Pay attention to the very important and important character elements listed here, as well as the actual conditions observed at the site analysis stage.



Clarendon St between Grant St and Thompson St, Park St to Cecil St and Coventry St to Ferrars St.

Streets in the area:

South Melbourne Mark, Alfred Pl, Bank Pl, Cecil St, Charles St, Church St, Clarendon St, Coventry St, Dorcas St, Dow St, Francis St, Hotham St, John St, Napier Pl, Palmer St, Park St, Raglan Pl, Ross Pl, Union St, Wynyard St, Yarra Pl,

Description of the character of the area:

The character of the area is primarily determined by its mixed land use dominated by shopping. The wide streets of Clarendon, Coventry and Park Streets are important contributing factors, although the area also includes many little streets and lane ways. The heart of the shopping area is in Clarendon Street between Park and Coventry Streets where there are extensive verandahs and limited street trees.

The area south of Coventry Street has fairly consistent streetscapes that retain a high percentage of Victorian and inter-war buildings although there has also been significant development in the post-war period. The Coles supermarket and car park is perhaps the best example of inappropriate post-war development with an entire block in the heart of the shopping centre dedicated to an ugly car park.

The area has good views to the north towards the city skyline and is very accessible by public transport with tram lines along Clarendon and Park Streets. Over head services and commercial signage are visually prominent in the area. The area retains much of its original bluestone and bitumen street detailing, however some sections have been lost and there are some awkward additions of new street widenings and planter beds at intersections.

<i>Element</i>	<i>Description</i>
Dominant style	pre-1900s
Dominant scale	two storey
Dominant material	stucco and render/minor variation
Dominant roof	concealed by parapet/metal
Setback	less than 5 metres/minor variation
Heritage	more than 50% identified as heritage place
Gardens	invisible
Fences	buildings to property line

The existing urban character of the City of Port Phillip has been assessed using 24 character elements. The design should especially respond to the important and very important elements, which in this area are:

<i>Element</i>	<i>Importance</i>
Land use	Very important
Views	Important
Street dimensions	Important
Traffic	Important
Parking	Important
Footpaths	Important
Overhead services	Important
Signage	Important
Fences	Important
Setbacks	Important
Building style	Important
Building scale	Important
Heritage	Important
Access	Important

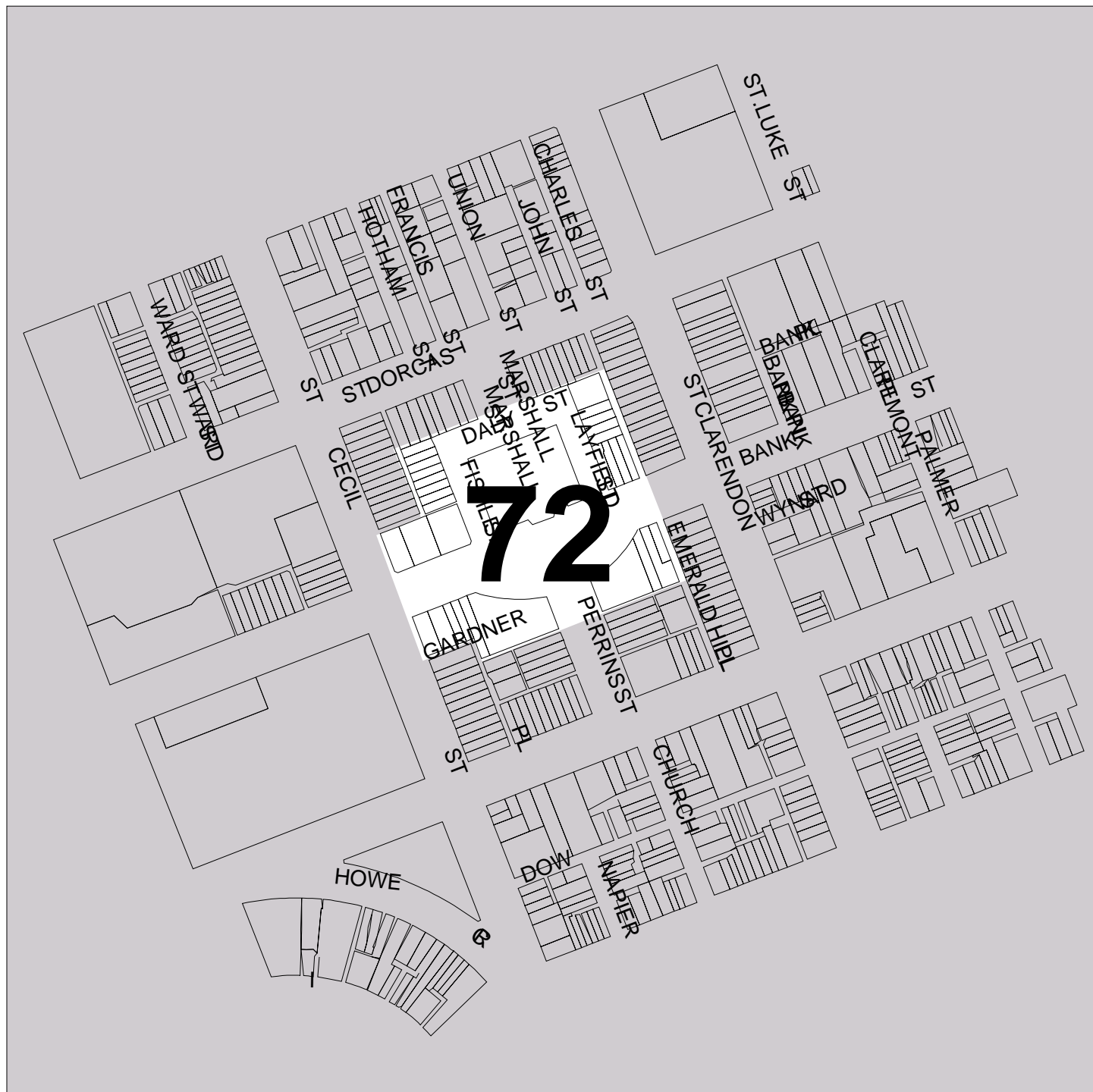
A comprehensive urban character assessment and policy statement for the area is available from the Council .

URBAN CHARACTER DESCRIPTION

AREA 72: DALY STREET, LAYFIELD STREET, FISHLEY STREET AND PERRINS STREET, SOUTH MELBOURNE

The following description should be used in carrying out the site analysis and design response to your site.

Pay attention to the very important and important character elements listed here, as well as the actual conditions observed at the site analysis stage.



Boundary streets:

Daly St, Layfield St, Fishley St, Perrins St.

Streets in the area:

Bank St, Daly St, Emerald Hill Pl, Perrins St,

Description of the character of the area:

This area is distinct from adjoining areas because of its formal plan layout and the presence of carefully grouped public buildings and rich mix of uses within a very small precinct.

Occupying the high point of Emerald Hill, it is dominated by the South Melbourne Town Hall as the central focus surrounded by the post office and court house as well as an interesting combination of shops, offices and housing.

Banks and Perrins Streets are wide with Bank Street curving outside the Town Hall to create a small green space. The area retains most of its original street detailing and is overlayed by more recent landscaping and street trees.

Building scale in the area is subservient to the Town Hall yet large enough to enclose street spaces. The careful modulation of building scale is very important to the character of the area.

The area has excellent access to public facilities, shops and public transport and includes some limited open space.

<i>Element</i>	<i>Description</i>
Dominant style	pre-1900s
Dominant scale	one and two storey
Dominant material	stucco and render/minor variation
Dominant roof	concealed by parapet/metal
Setback	less than 5 metres/minor variation
Heritage	entire area identified as heritage place
Gardens	visible/small
Fences	medium/ building to property line/various materials

The existing urban character of the City of Port Phillip has been assessed using 24 character elements. The design should especially respond to the important and very important elements, which in this area are:

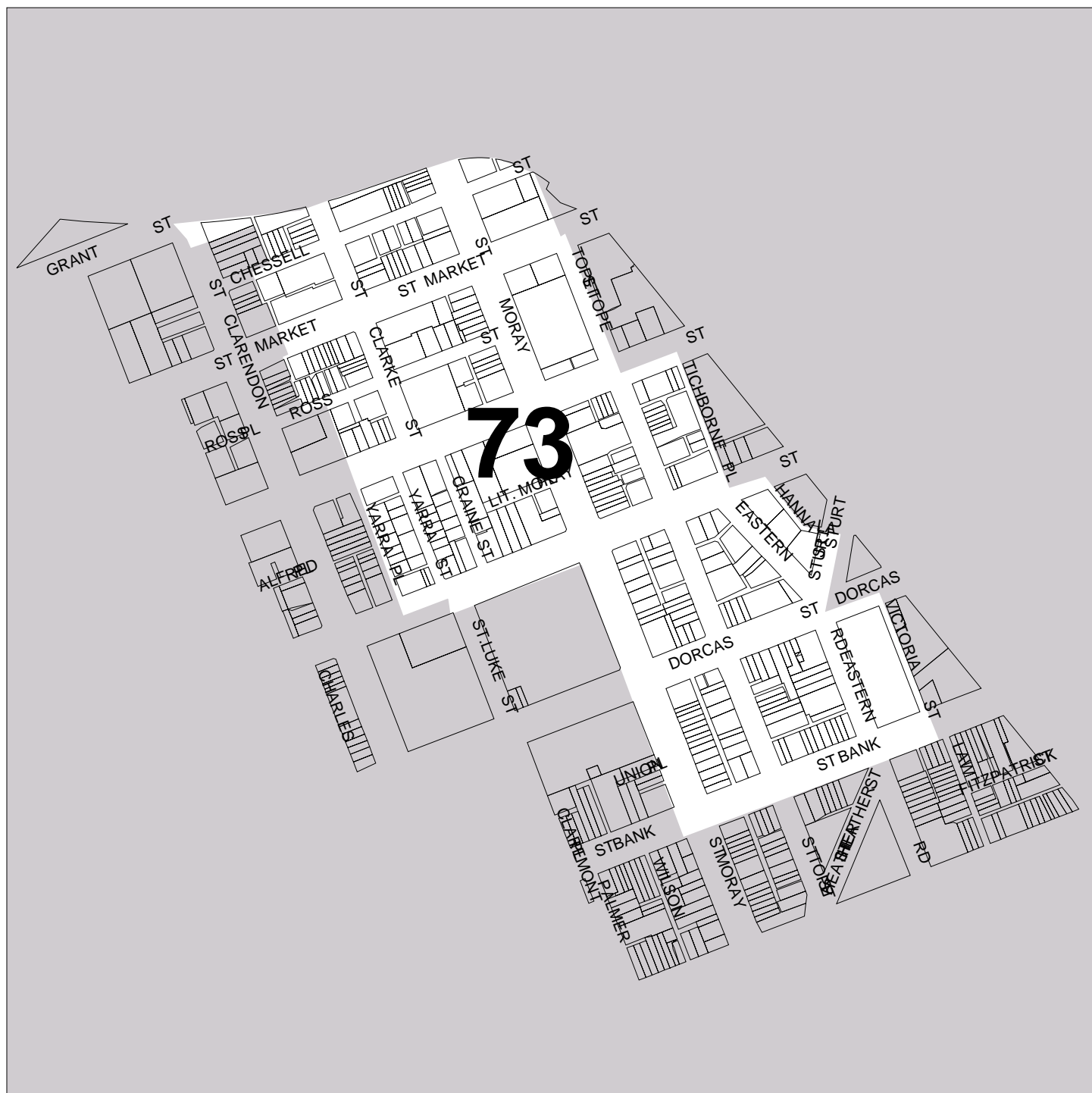
<i>Element</i>	<i>Importance</i>
Pattern	Very important
Building scale	Very important
Heritage	Very important
Access	Very important
Landform	Important
Views	Important
Lot size	Important
Street dimensions	Important
Parking	Important
Street detailing	Important
Setbacks	Important
Building style	Important
Land use	Important

A comprehensive urban character assessment and policy statement for the area is available from the Council .

AREA 73: MORAY STREET BETWEEN WESTGATE FREEWAY AND BANK STREET, SOUTH MELBOURNE

The following description should be used in carrying out the site analysis and design response to your site.

Pay attention to the very important and important character elements listed here, as well as the actual conditions observed at the site analysis stage.



West Gate Freeway, Clarendon St, Kings Way, Bank St, Moray St, Coventry St.

Streets in the area:

Bank St, Chessell St, Clarke St, Coventry St, Craine St, Dorcas St, Eastern Rd, Little Moray Pl, Market St, Moray St, Ross St, Tichborne Pl, Tope St, Victoria St, York St,

Description of the character of the area:

Providing services to the city, the area is distinguished from adjoining areas primarily by its industrial and commercial land uses including many printing, car repair, warehousing, building supplies and some office uses.

The area has an almost pure grid of streets and lanes of widely varying dimensions. It has been partially developed with terrace housing, however most of these smaller sites with narrow frontages appear to have been amalgamated and developed for factory and offices uses.

With few residences remaining in the area, buildings have been constructed during the post-war period with no setback and a height variation of one to three storeys. Most are crude commercial buildings and there is little consistency in materials, proportions or signage.

Streets in the area retain elements of original street detailing and there is fairly consistent bitumen footpaths throughout although they are often broken by concrete crossovers and loading bays. Street trees tend to be inconsistent, smaller scale and have limited influence on the character of the area with the exception of Moray Street which has reasonably consistent street trees. Power lines are also fairly dominant.

<i>Element</i>	<i>Description</i>
Dominant style	1970s - 1990s
Dominant scale	one and two storey
Dominant material	brick or masonry
Dominant roof	concealed by parapets/metal
Setback	less than 5 metres/consistent
Heritage	virtually no identified heritage places
Gardens	non-existent
Fences	buildings to property line

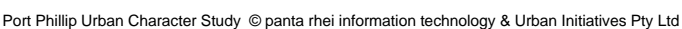
The existing urban character of the City of Port Phillip has been assessed using 24 character elements. The design should especially respond to the important and very important elements, which in this area are:

<i>Element</i>	<i>Importance</i>
Building scale	Very important
Land use	Very important
Lot size	Important
Traffic	Important
Parking	Important
Setbacks	Important
Building style	Important

A comprehensive urban character assessment and policy statement for the area is available from the Council .



The following description should be used in carrying out the site analysis and design response to your site. Pay attention to the very important and important character elements listed here, as well as the actual conditions observed at the site analysis stage.



West Gate Freeway, Wells Cr, Kings Way, Dorcas St.

Streets in the area:

Bank St, Bowen Cr, Cobden St, Dorcas St, Hanna La, Kings Pl, Kings Way, Little Bank St, Matthews La, Middleton La, Millers La, Napier St, Palmerston Cr, Palmerston Pl, Park St, Queens La, Wells St, York St,

Description of the character of the area:

The dominant influence in this area is Kingsway which is an extremely busy approach road to the city.

Kingsway was originally a suburban street serviced by a tram route in much the same way as Park or Sturt Street, however with the opening of Kings Bridge during the post-war period, it was widened and upgraded to incorporate six through traffic lanes. This widening also involved the loss of most of the original buildings along the street and these have been gradually replaced with a mixture of offices, show rooms and road service related uses.

The area also includes Dorcas, Bank and Park Streets between Kings Way and Wells Street which supports office and show room uses and the recently closed Tram Depot.

The incremental upgrading of Kingsway within a restricted road reservation has resulted in poor and inconsistent street detailing, an abundance of overhead services and a lack of significant street trees.

Kings Way is a real barrier to pedestrian circulation in the area and traffic noise and fumes are significant.

<i>Element</i>	<i>Description</i>
Dominant style	1970s - 1990s
Dominant scale	one or more than three stories
Dominant material	brick or masonry/mixed
Dominant roof	concealed by parapets/metal
Setback	less than 5 metres/various
Heritage	no identified heritage places
Gardens	no boundary definition/small
Fences	buildings to property line/various

The existing urban character of the City of Port Phillip has been assessed using 24 character elements. The design should especially respond to the important and very important elements, which in this area are:

<i>Element</i>	<i>Importance</i>
Street dimensions	Very important
Traffic	Very important
Signage	Very important
Views	Important
Overhead services	Important
Building style	Important
Building scale	Important
Land use	Important

A comprehensive urban character assessment and policy statement for the area is available from the Council .



The following description should be used in carrying out the site analysis and design response to your site. Pay attention to the very important and important character elements listed here, as well as the actual conditions observed at the site analysis stage.



Gladstone St, Boundary St, Crockford St, City Rd, Clarendon St, West Gate Freeway.

Streets in the area:

Albert Pl, Alfred St, Ann St, Arthur St, Browns La, Buckhurst La, Buckhurst St, Cecil St, City Rd, Cromwell Pl, Douglas St, Ferrars St, George St, Gladstone La, Gladstone Pl, Gladstone St, Grant St, Karrington Pl, Kent Pl, Kerr St, Little York St, Market Pl, Market St, Milsom Pl, Montague St, Meaden St, Northumberland St, Peel Pl, Railway Pl, Roseneath Pl, Rosherville Pl, Shamrock Pl, Stewart Pl, Tates Pl, Thistlethwaite St, Whiteman St, Williams Pl, Williams St, Wolseley St, York Pl, York St, Alfred St, Waterloo Pl,

Description of the character of the area:

Providing services to the city, the area is distinguished from adjoining areas primarily by its industrial and commercial land uses including many printing, car repair, warehousing, building supplies and some office uses.

The street pattern is distorted by the converging grids of South Melbourne and Port Melbourne with City Road and Crockford Street passing through the area creating many angular junctions. Streets and lanes in the area have widely varying dimensions.

It has been partially developed with terrace housing, however most of these smaller sites with narrow frontages appear to have been amalgamated and developed for factory and offices uses.

With few residences remaining in the area, buildings have been constructed during the post-war period buildings vary from one to three storeys giving the area a reasonably consistent building scale.

City Road and Crockford Street seems to be attracting a higher proportion of showrooms, shops and corporate offices because of a more prominent address. Streets in the area retain elements of original street detailing and the area has fairly consistent bitumen footpaths throughout although they are often broken by concrete crossovers and loading bays. Street trees tend to be inconsistent and have limited influence on the character of the area. Power lines are also fairly dominant visually.

<i>Element</i>	<i>Description</i>
Dominant style	1970s - 1990s
Dominant scale	one and two storey
Dominant material	brick or masonry/mixed
Dominant roof	concealed by parapets/metal
Setback	less than 5 metres/consistent
Heritage	limited number of heritage places
Gardens	non-existent
Fences	buildings to property line

The existing urban character of the City of Port Phillip has been assessed using 24 character elements. The design should especially respond to the important and very important elements, which in this area are:

<i>Element</i>	<i>Importance</i>
Building scale	Very important
Land use	Very important
Traffic	Important
Parking	Important
Signage	Important
Setbacks	Important
Building style	Important

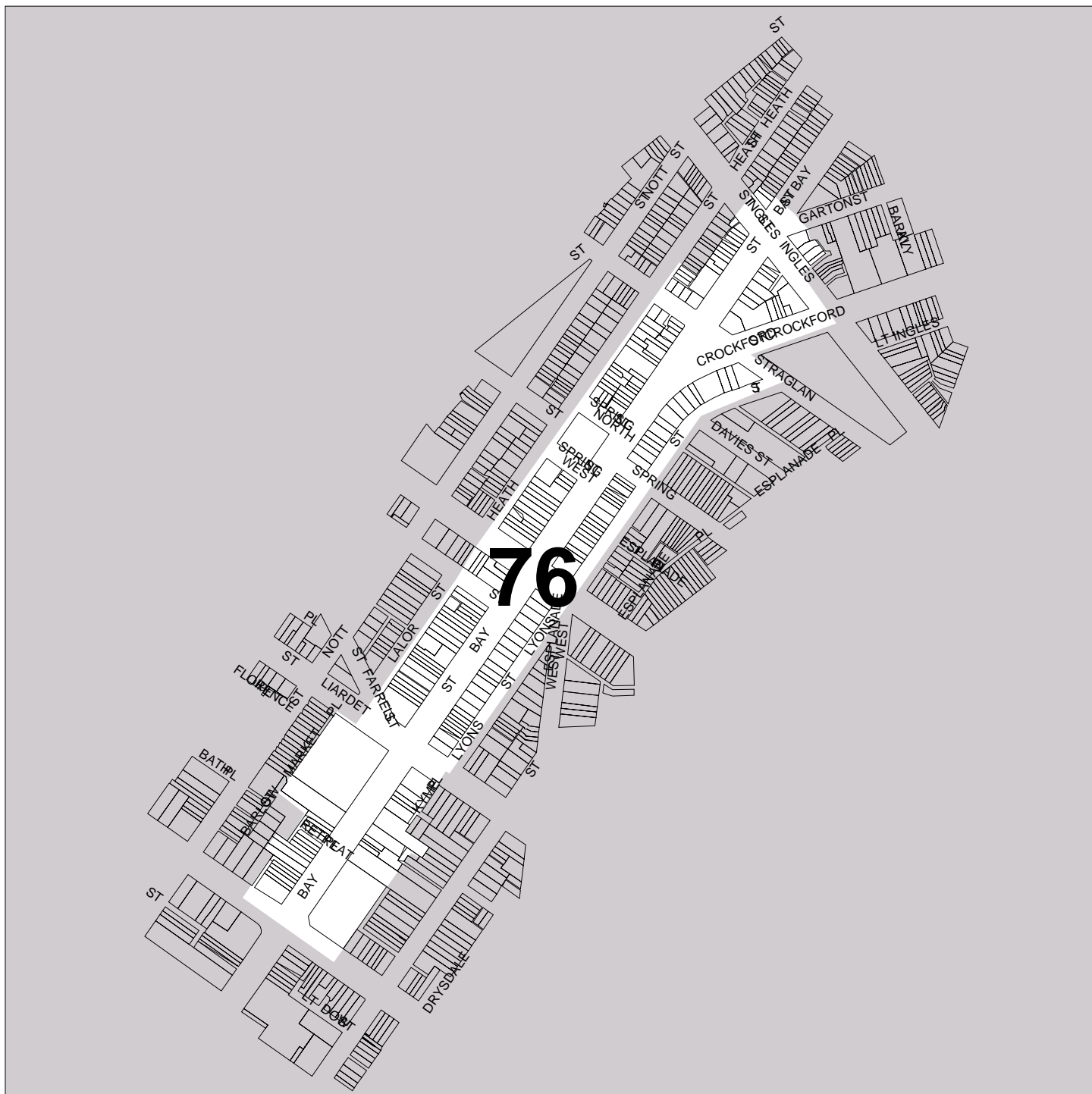
A comprehensive urban character assessment and policy statement for the area is available from the Council .

URBAN CHARACTER DESCRIPTION

AREA 76: BAY STREET BETWEEN GRAHAM STREET AND INGLES STREET, PORT MELBOURNE

The following description should be used in carrying out the site analysis and design response to your site.

Pay attention to the very important and important character elements listed here, as well as the actual conditions observed at the site analysis stage.



Bay St between Graham St and Ingles St.

Streets in the area:

Barlow St, Bay St, Crockford St, Ingles St, Lalor St, Liardet St, Market Pl,

Description of the character of the area:

Bay Street is a traditional strip shopping centre but also includes some residential, industrial and office uses. The boundary at the beach end of Bay Street has been drawn based on a reduced proportion of shops, higher traffic flows and a clear zoning intention to encourage new development.

The area was initially developed during the Victorian and Edwardian periods and most of the buildings from this period remain although there has been some redevelopment and renovation particularly on the southern side of the street. One and two storey buildings are generally built to the property line and many shops have verandahs.

Bay Street is a wide street with simple crossing intersections through most of its length except for the northern portion which forks to form Crockford Street. The street has a continuous grassed median planted with Brush Box trees through most of its length.

The area is significant for its consistent streetscape, building materials, colours and signage. Port Melbourne Town Hall is a focal building in the street.

<i>Element</i>	<i>Description</i>
Dominant style	pre-1900s
Dominant scale	one and two storey
Dominant material	stucco and render/consistent
Dominant roof	concealed behind parapets/metal
Setback	less than 5 metres/minor variation
Heritage	@90% identified heritage places
Gardens	visible/small
Fences	none

The existing urban character of the City of Port Phillip has been assessed using 24 character elements. The design should especially respond to the important and very important elements, which in this area are:

<i>Element</i>	<i>Importance</i>
Building scale	Very important
Views	Important
Pattern	Important
Street dimensions	Important
Parking	Important
Setbacks	Important
Building style	Important
Heritage	Important
Land use	Important
Access	Important

A comprehensive urban character assessment and policy statement for the area is available from the Council .



City of Port Phillip

URBAN CHARACTER DESCRIPTION

AREA 77: LAND GENERALLY BOUNDED BY BEACH STREET, ROUSE STREET, GRAHAM STREET AND PRINCES STREET, PORT MELBOURNE

The following description should be used in carrying out the site analysis and design response to your site.

Pay attention to the very important and important character elements listed here, as well as the actual conditions observed at the site analysis stage.



Johnson St, Beach St, Princes St, Graham St, Dow St, Rouse St.

Streets in the area:

Bay St, Beach St, Church St, Donaldson St, Dow St, Esplanade East, Esplanade West, Graham St, Little Bay St, Little Dow St, Nott St, Post Office Pl, Princes Pl, Princes St, Rouse St, Stokes St, Farrier Edwards La,

Description of the character of the area:

The area is essentially an industrial area in transition to a reasonably high density mixed use area. It includes traditional housing, new development, vacant land and the Bay Street shopping area.

Small pockets of the area, especially Stokes Street and Princes Street, have original terrace form housing on reasonably small lots, however most of the area is subdivided and developed into a wide range of lot sizes with many frontages well over twenty metres, and medium to large industrial buildings, some of which have been redeveloped for medium density housing. The area also has some office, entertainment and commercial uses.

Adjacent to Port Melbourne beach, it is effectively the last section of continuous beach and promenade extending beyond St Kilda.

Beach, Bay and Graham Streets are designated arterial roads and carry quite high volumes of private and commercial traffic. On-street parking generated by commercial, industrial and tourist uses also has a strong influence on the character of the area.

Streets are very mixed in their detailing and there are very few street trees in the area.

<i>Element</i>	<i>Description</i>
Dominant style	1970s -1990s
Dominant scale	three or more stories
Dominant material	stucco and render/mixed
Dominant roof	concealed by parapet/metal
Setback	less than 5 metres/minor variation
Heritage	limited number of identified heritage places
Gardens	visible
Fences	medium height/various materials

The existing urban character of the City of Port Phillip has been assessed using 24 character elements. The design should especially respond to the important and very important elements, which in this area are:

<i>Element</i>	<i>Importance</i>
Building scale	Very important
Foreshore	Very important
Views	Important
Lot size	Important
Traffic	Important
Parking	Important
Building style	Important
Housing type	Important
Land use	Important
Access	Important

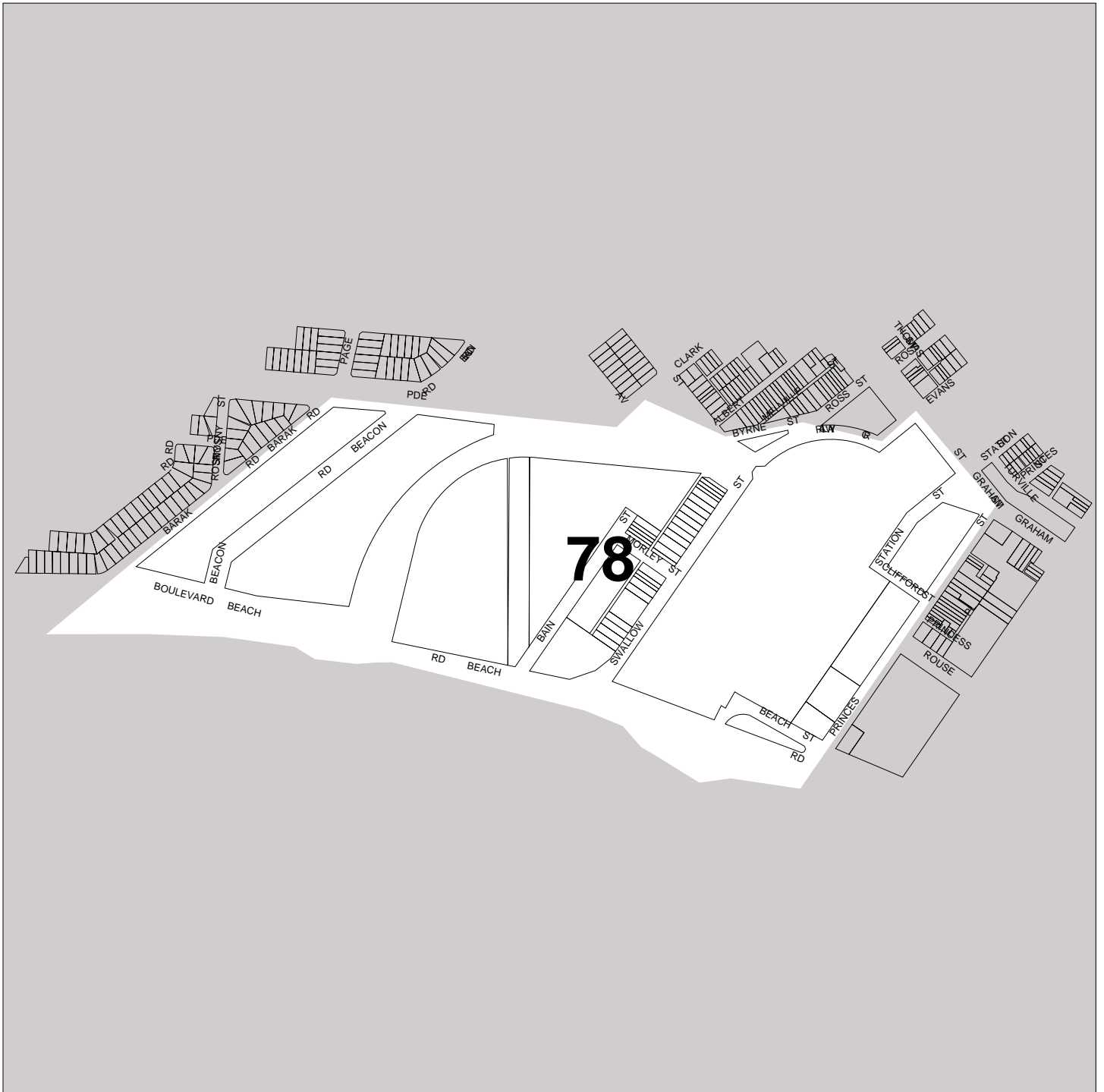
A comprehensive urban character assessment and policy statement for the area is available from the Council .



City of Port Phillip
URBAN CHARACTER DESCRIPTION

AREA 78: LAND GENERALLY BOUNDED BY THE FORESHORE, PRINCES STREET, GRAHAM STREET, HOWE PARADE AND BARAK ROAD, PORT MELBOURNE

The following description should be used in carrying out the site analysis and design response to your site.
Pay attention to the very important and important character elements listed here, as well as the actual conditions observed at the site analysis stage.



Foreshore, Princes St, Barak Rd, Howe Pde, Graham St.

Streets in the area:

Beach St, Beacon Rd, Canberra Pde, Centenary Dr, Clifford St South, Graham St, Harper Ct, Morley St, Railway Cr, Swallow St, Waterfront Pl, Webb Rd,

Description of the character of the area:

The area is characterised by a relatively undeveloped strip of foreshore, the light rail line passing through the area and terminating at Station Pier, and the strong blend of old and new architectural forms. These include a block of post-war public housing between Barak and Beacon Roads, a strip of Victorian houses along Swallow Street and a public housing estate off Princes Street. The balance of the area is currently being developed as a series of private housing estates.

The most recent private developments are typically two storey attached courtyard houses with lavish public landscape areas and no front boundary definition. They also include communal recreation and open space facilities developed and maintained to a high standard and subdivision layouts designed to deter through traffic.

There are limited shops and community services in the area with the result that trips to these facilities are generally by car.

<i>Element</i>	<i>Description</i>
Dominant style	1970s - 1990s
Dominant scale	two storey
Dominant material	stucco and render/consistent
Dominant roof	pitched/metal
Setback	less than 5 metres/minor variation
Heritage	
Gardens	visible/small
Fences	various height and materials

The existing urban character of the City of Port Phillip has been assessed using 24 character elements. The design should especially respond to the important and very important elements, which in this area are:

<i>Element</i>	<i>Importance</i>
Building style	Very important
Foreshore	Very important
Pattern	Important
Nature strips	Important
Street detailing	Important
Front gardens	Important
Building scale	Important
Housing type	Important

A comprehensive urban character assessment and policy statement for the area is available from the Council .



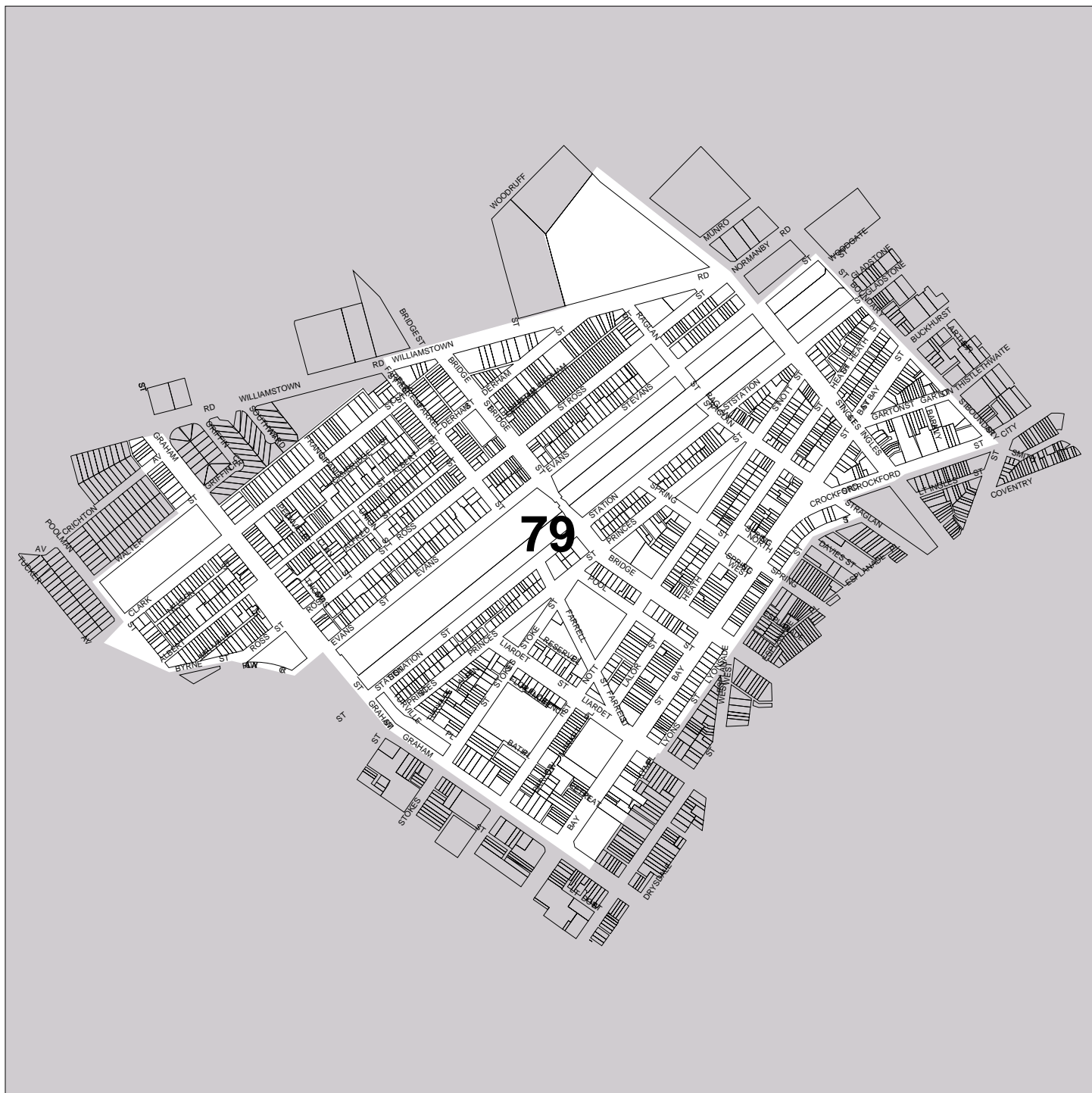
City of Port Phillip

URBAN CHARACTER DESCRIPTION

AREA 79: LAND GENERALLY BOUNDED BY WILLIAMSTOWN ROAD, BOUNDARY STREET, CROCKFORD STREET, BAY STREET AND GRAHAM STREET, SOUTH MELBOURNE

The following description should be used in carrying out the site analysis and design response to your site.

Pay attention to the very important and important character elements listed here, as well as the actual conditions observed at the site analysis stage.



Williamstown Rd, Boundary St, Heath St, Graham St, Byrne St, Walter St, Clark St.

Streets in the area:

Albert St, Alfred St, Allen Pl, Barkly Av, Bath Pl, Bay St, Boundary St, Bridge St, Byrne St, Cambridge St, Clark St, Clay St, Crockford St, Derham St, Evans St, Farrell St, Florence Pl, Garton St, Graham St, Heath St, Ingles St, Liardet St, Little Derham St, Melville St, Nelson St, Nott St, Peckville St, Pool St, Poolman St, Princes St, Raglan St, Ross St, Spring St West, Spring St North, Spring St South, Station St, Stokes St, Thomas St, Turville Pl, Union St, Williamstown Rd, Stokes St, Farrell St, Princes St, Rowan's Ln, May Pl,

Description of the character of the area:

The area is characterised by the light rail line and a wide strip of open space bisecting a consistent style and scale of residential development.

Except for the public housing blocks in Stokes Street and a few non-residential buildings in Graham Street and Williamstown Road, the area has been predominantly developed with small scale Victorian cottages constructed in timber on small, narrow lots with reduced setbacks.

Streets are quite varied in their dimensions with some wide streets, a secondary system of small streets and a network of rear lane ways. Original detailing in these streets has been largely replaced with simple concrete kerb and channel and only a few laneways retain original bluestone pavements and detailing.

Some streets have mature trees, however, the area has been subject to many planting fashions resulting in inconsistent and mixed planting.

<i>Element</i>	<i>Description</i>
Dominant style	pre-1900s
Dominant scale	one storey
Dominant material	timber of cladding/minor variation
Dominant roof	pitched/metal
Setback	less than 5 metres/consistent
Heritage	@50% identified heritage places
Gardens	visible/small
Fences	medium height/various materials

The existing urban character of the City of Port Phillip has been assessed using 24 character elements. The design should especially respond to the important and very important elements, which in this area are:

<i>Element</i>	<i>Importance</i>
Building scale	Very important
Lot size	Important
Overhead services	Important
Fences	Important
Setbacks	Important
Building style	Important
Housing type	Important
Heritage	Important
Access	Important
Open space	Important

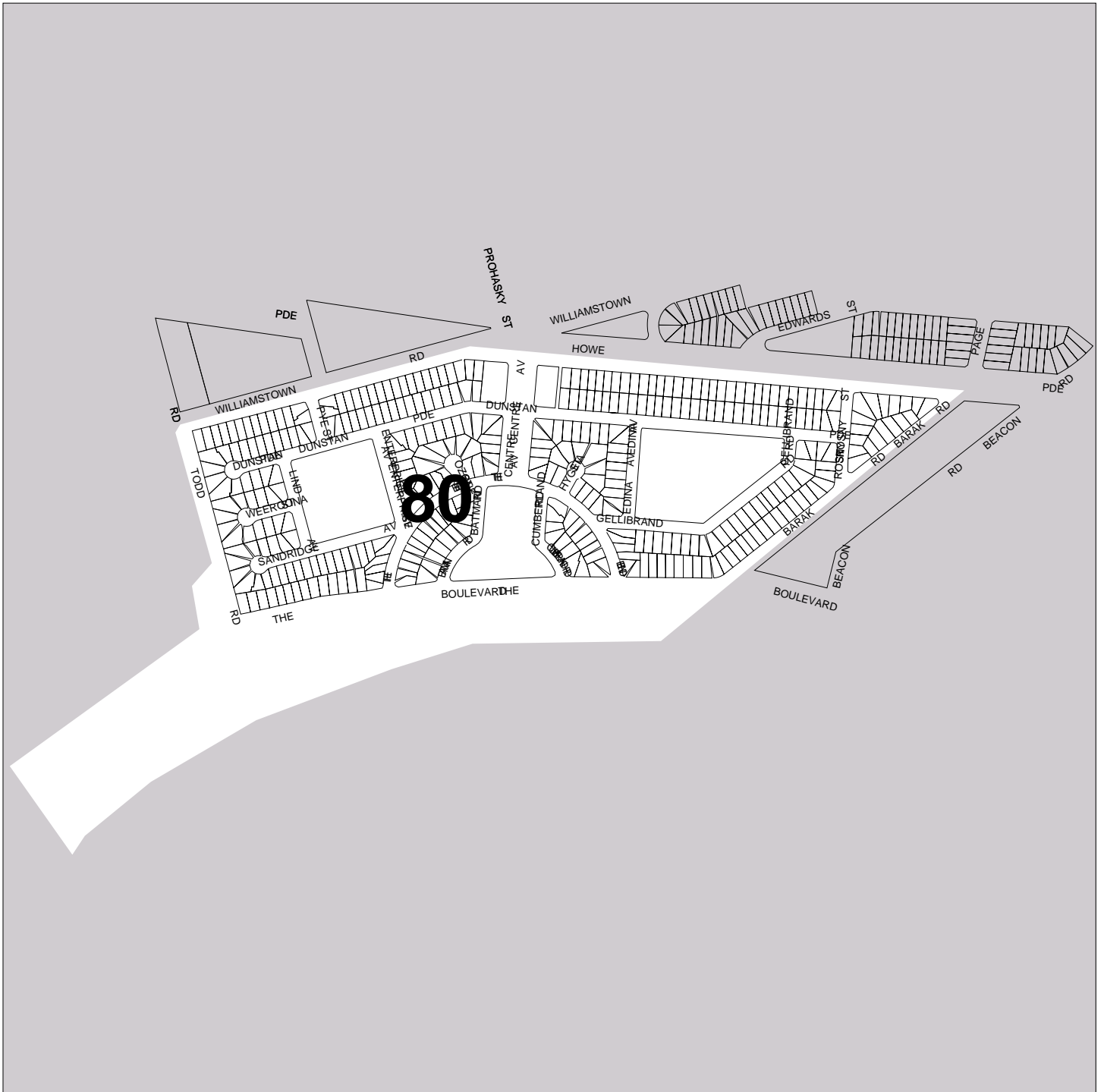
A comprehensive urban character assessment and policy statement for the area is available from the Council .



City of Port Phillip
URBAN CHARACTER DESCRIPTION

AREA 80: LAND BOUNDED BY THE BOULEVARD, TODD ROAD, WILLIAMSTOWN ROAD, HOWE PARADE AND BARAK PARADE, PORT MELBOURNE

The following description should be used in carrying out the site analysis and design response to your site. Pay attention to the very important and important character elements listed here, as well as the actual conditions observed at the site analysis stage.



The Boulevard, Todd Rd, Williamstown Rd, Howe Pde, Barak Pde.

Streets in the area:

Barak Rd, Batman Rd, Centre Av, Cumberland Rd, Dunstan Pde, Edina Av, Enterprise Av, Gellibrand Rd, Hygeia St, Lind Av, Ozone St, Rosny St, Sandridge Av, The Bend, The Boulevard, Weeroona St, The Boulevard,

Description of the character of the area:

This area is a planned public housing estate from the inter-war period, similar in many respects to the Garden City estate except for clear differences in housing scale, design, open space and relationship to the foreshore.

It has been designed with axial and curvilinear roads permeating uniform lot sizes of approximately 300 square metres.

All houses are duplex and built in accordance with a set of standard designs. They are usually setback about 5 metres and either face wide boulevards or one of three open spaces. Houses on corner sites have a corner orientation and the limited number of two storey houses are usually located on corner sites.

Properties generally have open front gardens enclosed by a wide range of fence types that have replaced the original low wire mesh fences. Similarly, some houses have undergone inappropriate renovations and car ports have been constructed within the front setback areas.

Streets retain their consistent detailing throughout the area, however street trees fail to enhance the area due to replacement planting of deciduous trees with mixed natives.

<i>Element</i>	<i>Description</i>
Dominant style	1920s -1940s
Dominant scale	one and two storey
Dominant material	brick or masonry/minor variation
Dominant roof	pitched/terracotta
Setback	5-9 metres/consistent
Heritage	no identified heritage places
Gardens	visible/medium
Fences	low-medium height/various materials

The existing urban character of the City of Port Phillip has been assessed using 24 character elements. The design should especially respond to the important and very important elements, which in this area are:

<i>Element</i>	<i>Importance</i>
Lot size	Very important
Pattern	Very important
Housing type	Very important
Open space	Very important
Foreshore	Very important
Views	Important
Nature strips	Important
Street detailing	Important
Fences	Important
Setbacks	Important
Building style	Important
Building scale	Important
Heritage	Important
Land use	Important

A comprehensive urban character assessment and policy statement for the area is available from the Council .



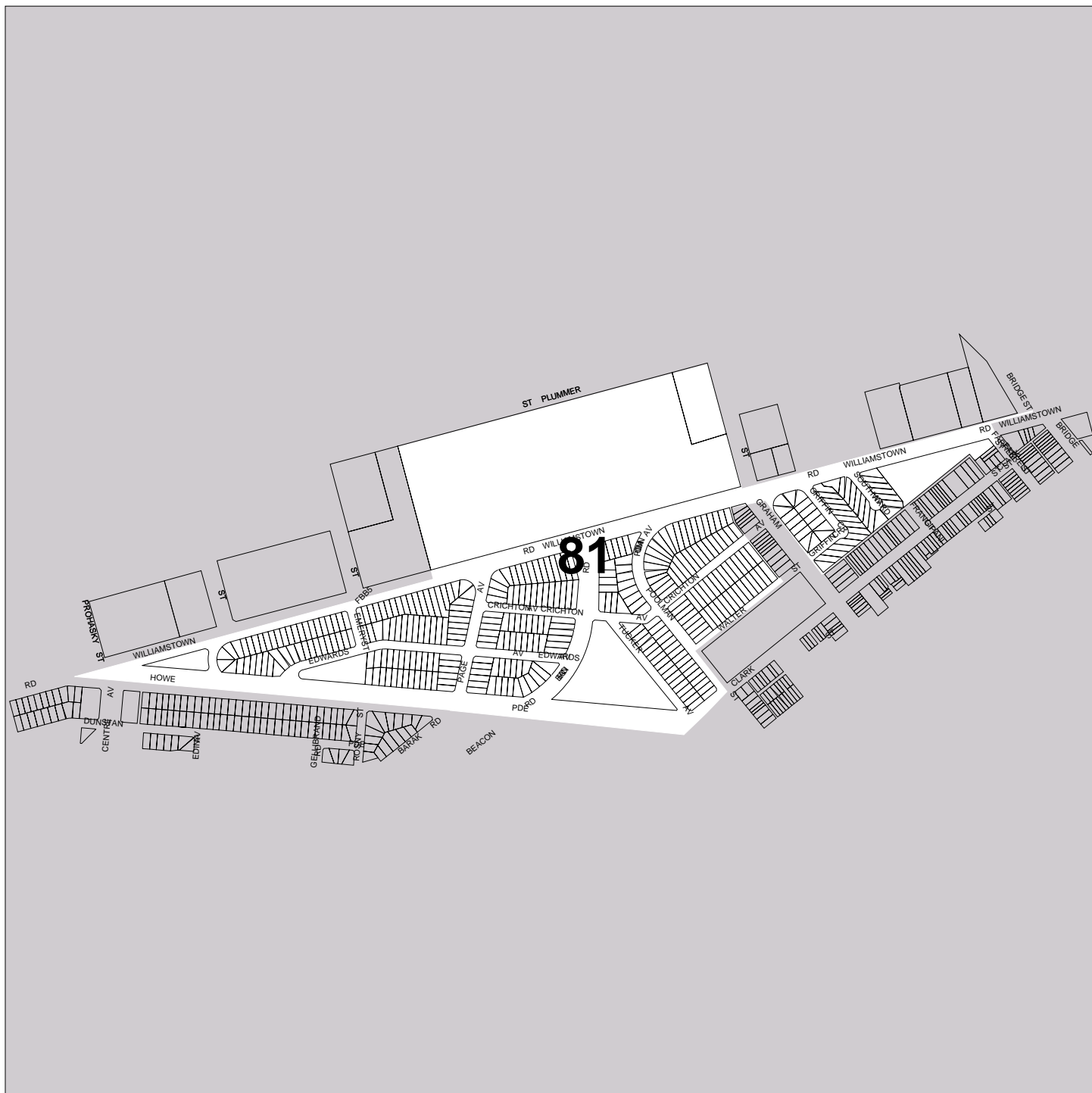
City of Port Phillip

URBAN CHARACTER DESCRIPTION

AREA 81: LAND GENERALLY BOUNDED BY WILLIAMSTOWN ROAD, HOWE PARADE, WALTER STREET AND CLARK STREET, PORT MELBOURNE

The following description should be used in carrying out the site analysis and design response to your site.

Pay attention to the very important and important character elements listed here, as well as the actual conditions observed at the site analysis stage.



Boundary streets:

Williamstown Rd, Howe Pde, Walter St, Clark St.

Streets in the area:

Beacon Rd, Centre Av, Crichton Av, Edwards Av, Frangipani Ct, Griffin Cr, Howe Pde, Page Av, Poolman St, Southward Av, Tucker Av, Walter St, Williamstown Rd,

Description of the character of the area:

This area includes the Garden City housing estate built between 1926-48 and the Griffith Crescent/Southward Avenue estate built by the State Government for rental in 1936. Both estates have similar road layouts, lot sizes and housing scale but building styles differ.

The Garden City estate is the larger estate distinguished by its street detailing of crisp precast concrete kerb and channel and concrete roads, crossings and footpaths, giving a distinctive appearance to the public realm. It also includes areas of open space together with a major sporting area at J.L. Murphy Reserve on the other side of Williamstown Road.

Factors which detract from the consistency of these areas are the occasional inappropriate colour scheme, modified windows or carports, the variation in fencing and the quality of street trees.

<i>Element</i>	<i>Description</i>
Dominant style	1920s - 1940s
Dominant scale	two storey
Dominant material	stucco and render
Dominant roof	pitched/terracotta
Setback	5-9 metres/consistent
Heritage	@75% identified as heritage place
Gardens	visible/medium
Fences	medium-high/various materials

The existing urban character of the City of Port Phillip has been assessed using 24 character elements. The design should especially respond to the important and very important elements, which in this area are:

<i>Element</i>	<i>Importance</i>
Lot size	Very important
Street detailing	Very important
Building style	Very important
Building scale	Very important
Heritage	Very important
Pattern	Important
Street dimensions	Important
Nature strips	Important
Parking	Important
Fences	Important
Setbacks	Important
Front gardens	Important
Housing type	Important
Land use	Important
Open space	Important

A comprehensive urban character assessment and policy statement for the area is available from the Council .



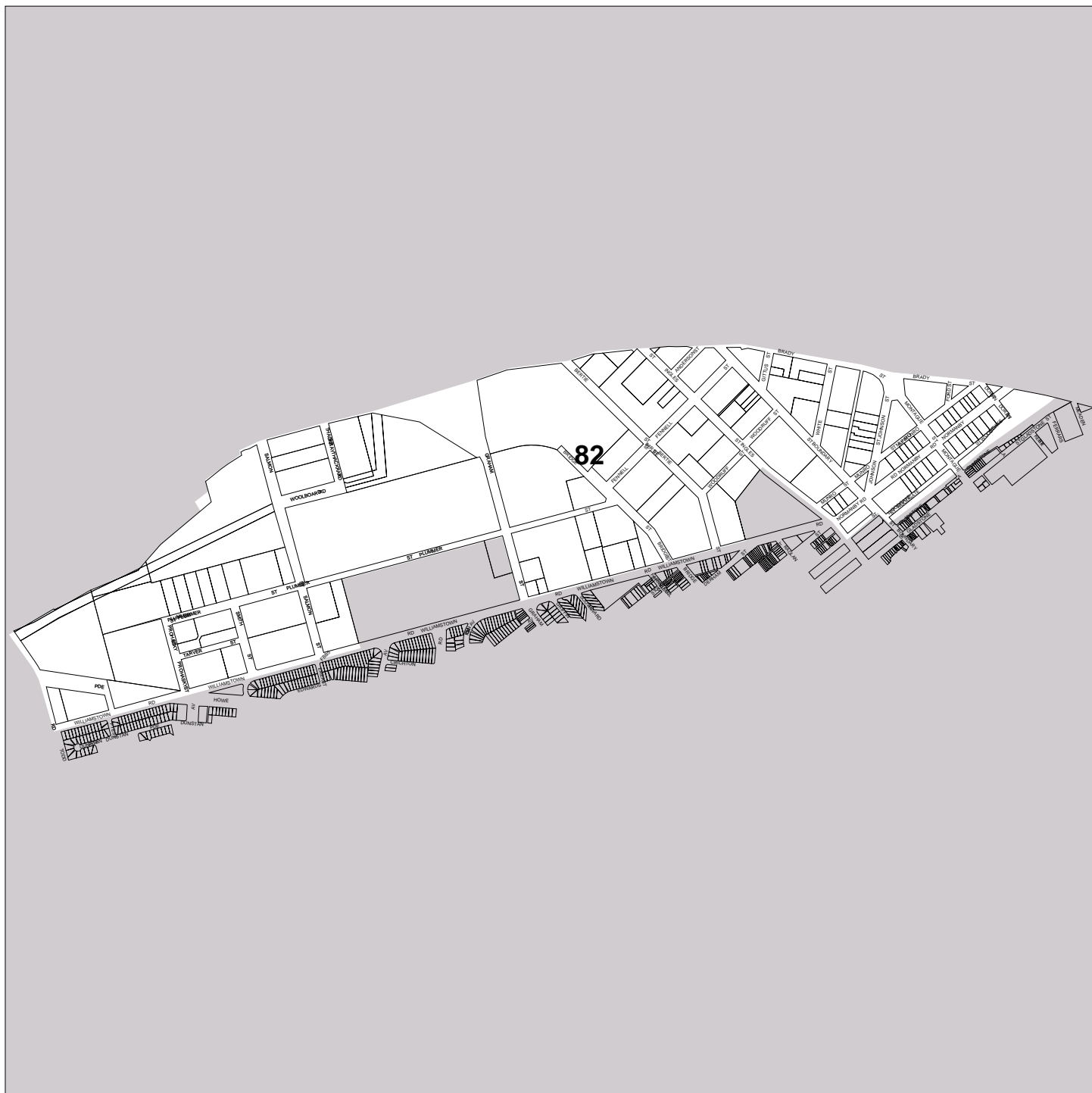
City of Port Phillip

URBAN CHARACTER DESCRIPTION

AREA 82: LAND GENERALLY BOUNDED BY TODD ROAD, WESTGATE FREEWAY, WILLIAMSTOWN ROAD AND WOODGATE STREET, PORT MELBOURNE

The following description should be used in carrying out the site analysis and design response to your site.

Pay attention to the very important and important character elements listed here, as well as the actual conditions observed at the site analysis stage.



Boundary streets:

Todd Road, Wesgate Freeway, Williamstown Road, Woodgate Street.

Streets in the area:

Anderson St, Bertie St, Boundary St, Brady St, Bridge St, Doran St, Emery St, Evans St, Fennell St, Gittus St, Graham St, Ingles St, Johnson St, Leith Cr, Montague St, Munro St, Normanby Rd, Plummer St, Prohasky St, Pye St, Salmon St, Smith St, Tarver St, Thackray Rd, White St, Williamstown Rd, Woodgate St, Woodruff St, Wool Board Rd, Boundary St, Howe Pde,

Description of the character of the area:

As an extension of Fishermens Bend on the other side of the Westgate Freeway, this area has extremely large lots which have been developed with large factories or other commercial buildings during the inter-war period.

Streets are generally quite wide although some are just local streets free of through traffic. Nevertheless, the majority are collector and arterial roads carrying heavy traffic.

Although there are some quite good plantations in the area, the streets generally look less cared for than those of residential areas due to the lack of continuity of design and inadequate maintenance. Overhead services are visually intrusive and there are no footpaths in many places.

<i>Element</i>	<i>Description</i>
Dominant style	1920s - 1940s
Dominant scale	one and two storey
Dominant material	timber or cladding/mixed
Dominant roof	flat/metal
Setback	less than 5 metres/varied
Heritage	no identified heritage places
Gardens	visible/large
Fences	high/various materials

The existing urban character of the City of Port Phillip has been assessed using 24 character elements. The design should especially respond to the important and very important elements, which in this area are:

<i>Element</i>	<i>Importance</i>
Lot size	Very important
Setbacks	Very important
Building scale	Very important
Street dimensions	Important
Traffic	Important
Overhead services	Important
Land use	Important

A comprehensive urban character assessment and policy statement for the area is available from the Council .

FISHERMANS BEND ESTATE GUIDELINES

**THESE ARE ADVISORY GUIDELINES TO ASSIST RESIDENTS IN
RENOVATING AND MAINTAINING THEIR HOUSES**



*Source: City of Port Phillip Geographic Information System and Land Victoria data.
Aerial photo taken May 2000.*

Adopted October 2001 (Updated 2010)

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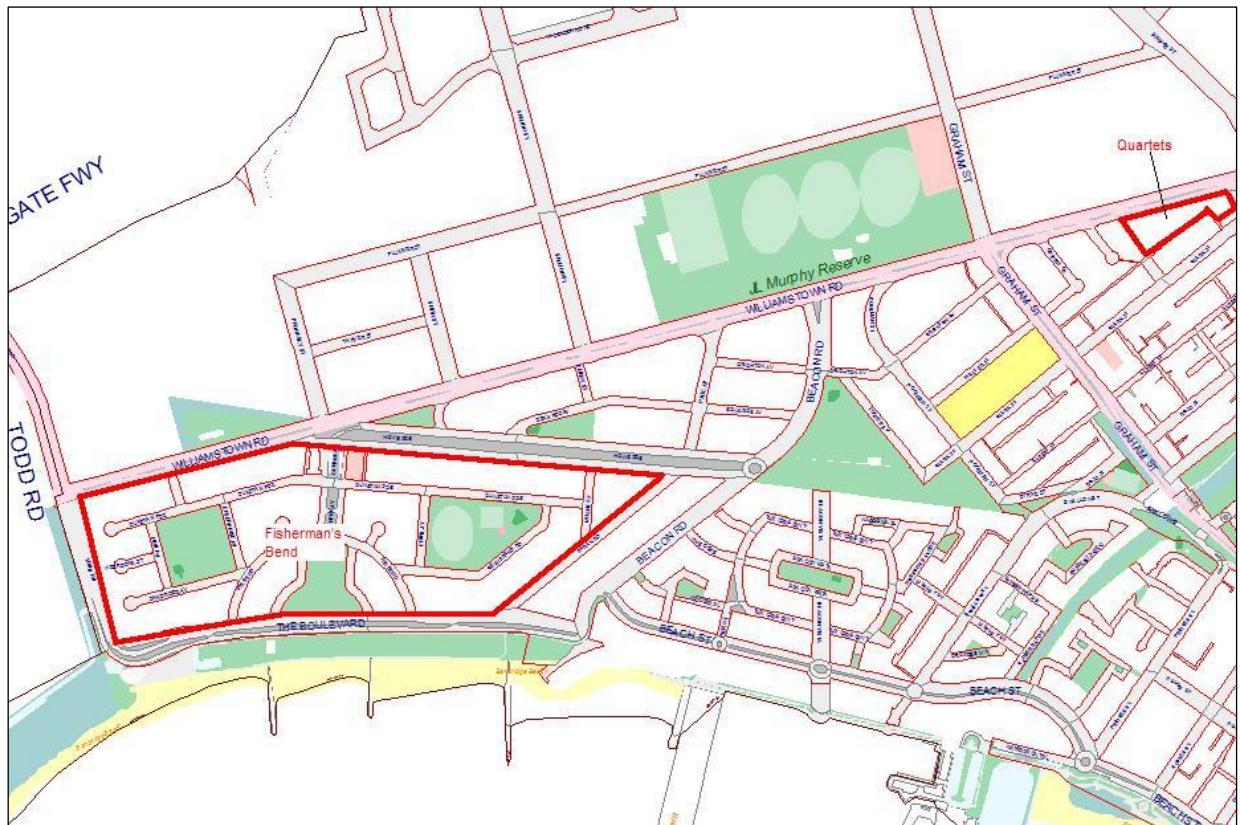
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Prepared by the City of Port Phillip, in association with heritage advisers
Trevor Westmore Robert Sands and Bryce Raworth
Update by the City of Port Phillip

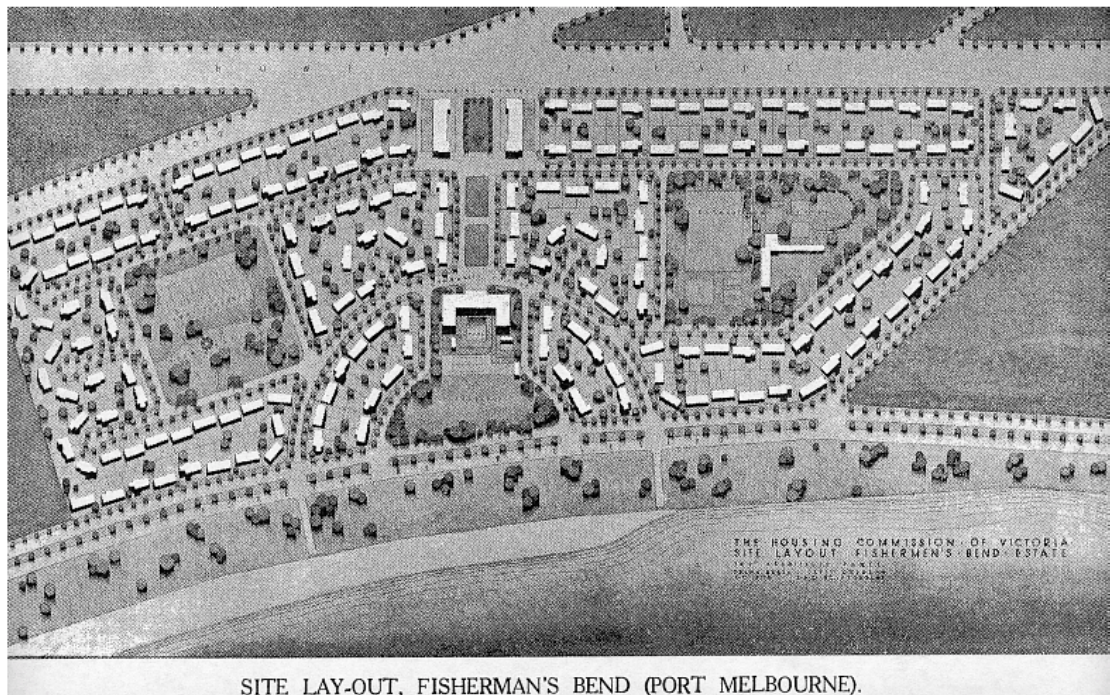
PART ONE:
BACKGROUND TO THE GUIDELINES

1. INTRODUCTION

The Fishermans Bend area in Port Melbourne has two parts. The main part of fifty five acres containing 376 dwellings is the area bounded by The Boulevard to the south, Todd Road to the west, Howe Parade and Williamstown Road to the north and Barak Road to the east. The smaller section of two acres is at 444-478 Williamstown Road, just east of Southward Avenue and contains 36 flats in nine two storey blocks, known as "Quartets".



The estates were built between 1939 and 1942, by the newly formed Housing Commission of Victoria (HCV), as their first project in the series of estates which dominated public housing programs and considerable tracts of suburbia in Victoria until the 1970s, climaxing in the 30 storey Park Towers in South Melbourne.



SITE LAY-OUT, FISHERMAN'S BEND (PORT MELBOURNE).

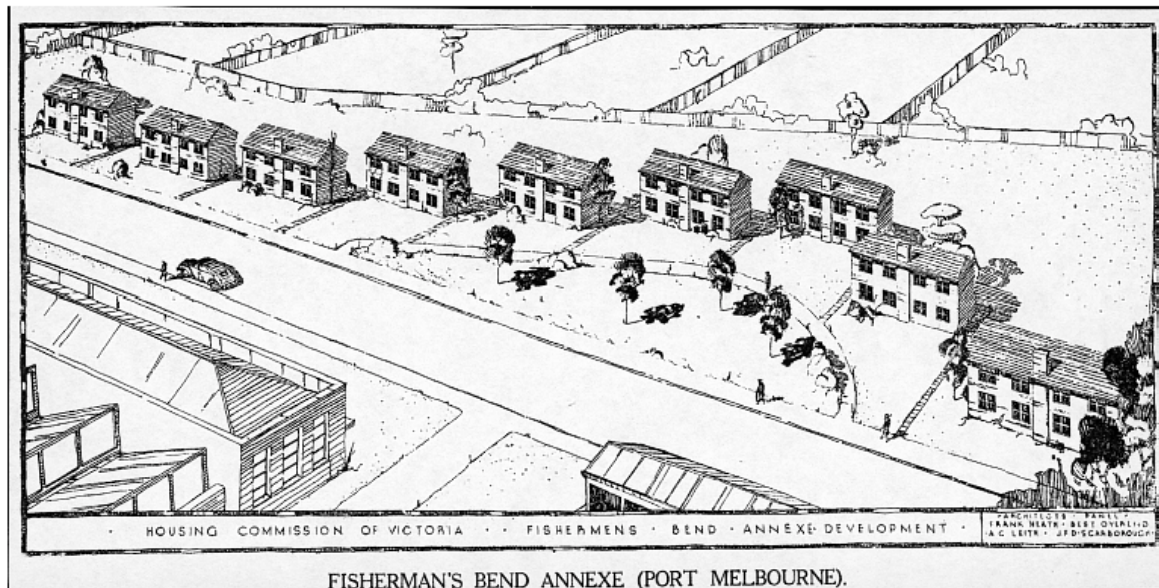
The original layout plan of Fishermans Bend.

Source: Second Report of the Housing Commission of Victoria, 1940.

Note the central spine with the shopping centre at the north and the proposed community centre at the south, the organised layout of the reserves and the proposed regular street tree planting.

Fishermans Bend followed the only two substantial initiatives in the field of public housing to that time, both also in Port Melbourne. The first was the State Bank Estate (known as Garden City), started in 1926 and the second, the small Dunstan Estate at the corner of Williamstown Road and Graham Street, built by the Public Works Department for then Country Party Premier Albert Dunstan's Department in 1936. Thus the area south of Williamstown Road contains the physical expression of the genesis of public housing in the State of Victoria. Consequently, the area has historic and social significance to the municipality.

Fishermans Bend represents the State's answer to the inter war and post war housing crises for the lower working class and under-privileged of our society during the 1920-40s. It represents the power of the social reformers from this era, most particularly the indefatigable Oswald Barnett, to influence government policy. Its physical expression, incorporating Garden City concepts from England and the latest American ideas in estate layout, is an altruistic attempt to provide for a neglected section of society. It is considered to be the finest example of a housing estate ever built by the HCV. It was designed to incorporate all of the facilities a neighbourhood community could require - shops, community centre, recreation space and a kindergarten. Its original intent, with fully integrated shopping, sporting and recreational facilities and street tree planting was never fully realised however, its existing structure remains in accordance with the original plan.



The "Quartets" on the two acre site at 444-478 Wiliamstown Road.

It is also important as the cradle of the HCV's concrete house project, employing the techniques of TW Fowler, which formed the basis of the massive housing program to follow after the war. The first experimental concrete dwellings (at 324-326 Howe Parade) still remain and are included on the Victorian Heritage Register.

This estate is the only one conceived as a result of a design competition, and the architect's panel appointed by the HCV were able to produce a quality of architectural and urban design never repeated in later estates.

For these reasons, Fishermans Bend is significant and the City of Port Phillip and its residents, as its custodians, should maintain and enhance its unique character. It is also important that the needs of individual property owners are recognised. These guidelines aim to balance these objectives. They describe the features that make the HCV Fishermans Bend Estate significant and offer suggestions to help owners maintain its significance, whilst allowing for renovations and development to occur.



The first experimental dwellings using the TW Fowler system of concrete prefabrication, 324-326 Howe Parade, Fishermans Bend.



One of the most intact early concrete dwellings, 350 Howe Parade, Fishermans Bend. Note the splayed roof verge and gutter, the small concrete entry canopy carried on slender steel posts, the timber sliding sashes with horizontal glazing bars and the pipe and wire mesh fence

In 2001, 82 properties in Fishermans Bend were owned by the Office of Housing. These are shown on Figure 1, including the locations of the 30 pairs of concrete dwellings.

Figure 2 shows the distribution of the original two storey pairs of dwellings and new two storey additions.

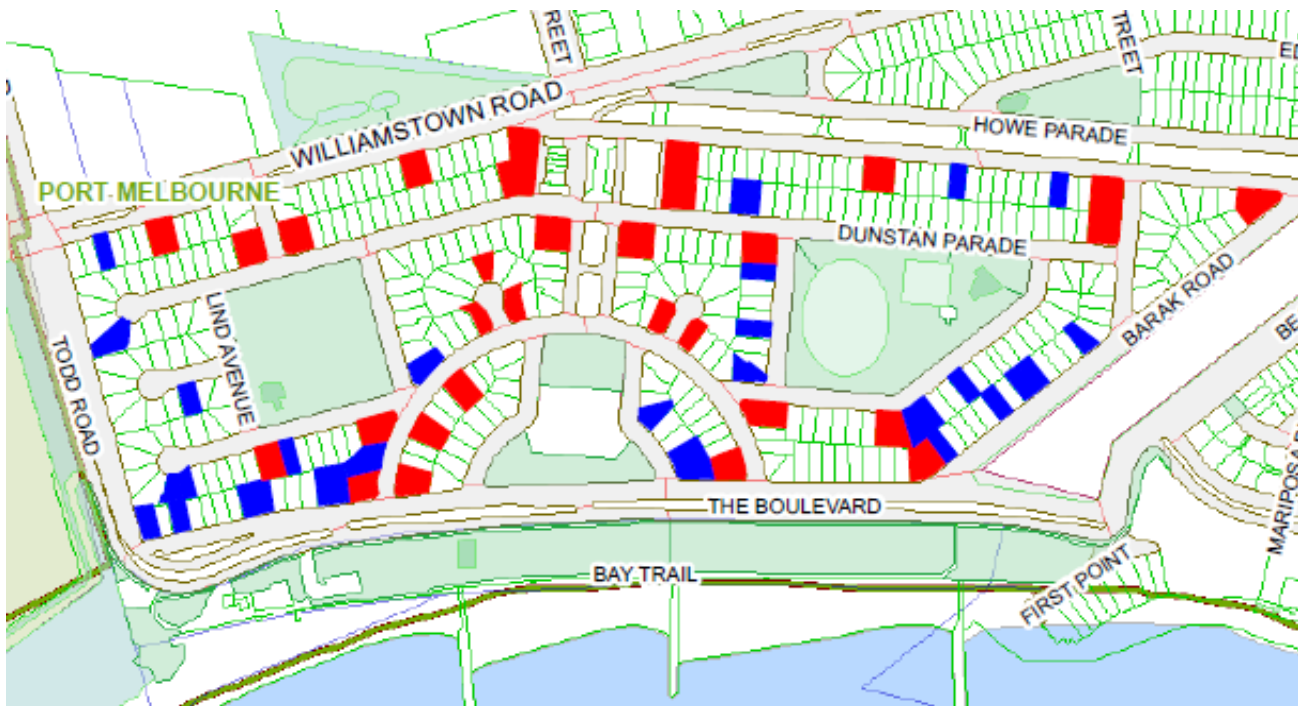


Figure 2: Map of Fishermans Bend (June 2010) showing the distribution of original two storey dwellings and recent two storey additions.

The original distribution of the two storey dwellings is clearly for deliberate effect, varied in its symmetry because of the asymmetrical form of the estate as a whole.

- Original two storey dwellings
- Recent two storey additions

2. THE SIGNIFICANCE OF FISHERMANS BEND

The Fishermans Bend area is recognised as an area of heritage significance. This significance is documented in the Port Phillip Heritage Review, the recommendations of which led to the inclusion of the area in a heritage overlay area (HO2) in the Port Phillip Planning Scheme (in December 1998). The HO2 area covers the whole garden city neighbourhood, including Fishermans Bend (apart from the first experimental concrete dwellings at 324-326 Howe Parade which are on the Victorian Heritage Register and have an individual heritage overlay, HO158), the bank house (garden city) estate and the Dunstan estate.

The main bases of the Fishermans Bend area's significance are as follows:

- It is the first experiment in public housing for rental by a State Government instrumentality in Australia, the Housing Commission of Victoria, and formed the baseline of their massive housing program to follow. It was the first step in the dominant pattern of social engineering employed by the HCV until the 1970's.
- It is the first and only estate by a government instrumentality derived from a design competition.
- It contains the first experimental concrete dwellings and the first group of concrete dwellings built in Victoria, using the TW Fowler system of prefabrication, that was to form the basis of much of the HCV's later program and which led to the mass production facilities at Holmesglen and ultimately to the HCV high rise construction program.
- It employs a combination of the most up to date ideas in planning and urban design from England and America, such as its geometry, curved streets, the use of residential courts and creation of neighbourhoods.
- Its proposed social and physical integration of community facilities is to a degree not seen again in estate planning in Australia.
- It is the finest example of a HCV estate in both architectural and urban design terms.
- In spite of some change, it is substantially intact and still clearly expresses its original intent.

Fishermans Bend owes its special character to the vision of its social and physical planners, expressed in the clarity and ordered variety of its design, with strategically placed two storey elements in the single storey context, its unique layout and infrastructure, with open space defining its two precincts. The consistent use of attached dwellings, built form and materials enhance its individuality.

It is a unique demonstration of the concepts of planners of the time, not so comprehensively expressed anywhere else in Australia.

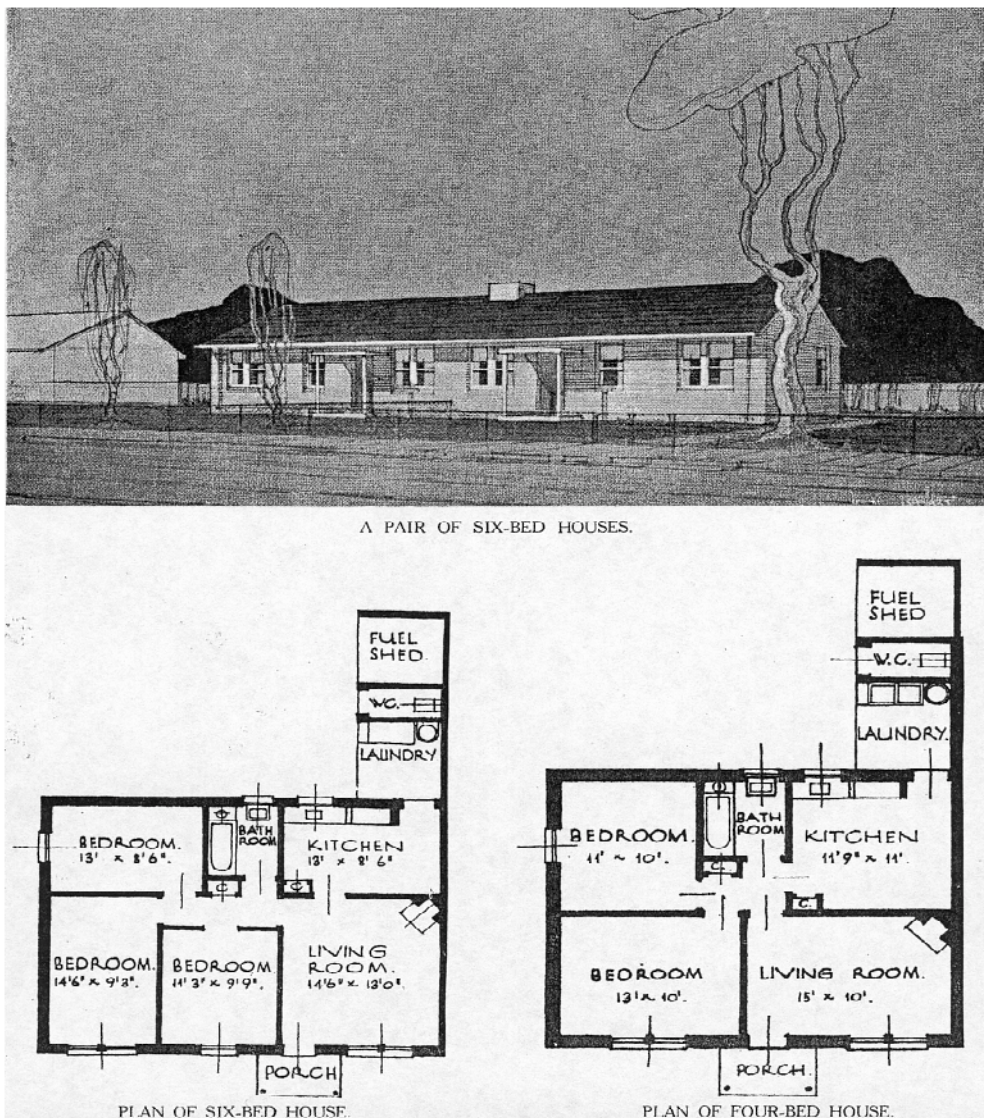
3. IMPORTANT FEATURES

The most important features of Fishermans Bend are its layout, the original buildings (including the dwellings), the communal open spaces and the shopping centre.

These elements contribute to the special character of the place as a whole; for it is the whole place that is important. The individual elements each help establish and reinforce the consistency of the overall character in their variations of repeating forms, materials and construction details.

The main features are:

- The scale of the buildings. The area was designed as a lower “working class” estate and thus its origins are expressed in its housing stock of small dwellings on small sites, devised in accordance with the social philosophies and economic constraints of the time. This is the core of its cultural significance and is fundamental to any consideration of the fabric of the place.



*Typical plans of the medium size dwellings.
Source: Second Report of the HCV, 1940.*

- The overall street layout and the specifics of its design, including the curvilinear streets, which produced ordered variety in the orientations of the dwellings.
- The symmetrical placement of facilities in the central spine between the seafront and Williamstown Road.
- The open space areas, which balance the composition, define two distinct precincts, and which allow the long views that add to and reveal the special character of the area.
- The designs of the individual dwellings as pairs and their consistent repetition of elements and massing, which add to the strong identity of the area.
- The consistent use of similar materials, relieved by sections with concrete, rather than brick dwellings, and the subtle variations in the bricks used, ranging from over-burnt (clinker) to pink.
- The siting of the two storey dwellings at strategic locations, generally on street corners, but also breaking up long sweeps of single storey buildings.



Typical strategic corner location of a two storey pair of dwellings.

- The varied setbacks of the pairs, which relieve the potential monotony of their similar designs and the diagonal siting of corner dwellings.
- The consistent detailing of the individual pairs including their roofs, roof drainage, gable ends, walls, windows and doors, chimneys and porches.
- The consistent design of front fences of low height, particularly the early galvanised pipe and chain wire mesh examples with high paling fences used only behind the fronts of the dwellings.
- The alterations to the porches and windows which have been made by the HCV since the original construction of the estate.
- The stepped frontage of shops to Central Avenue.
- The form and function of the Community Centre on Central Avenue.



A streetscape showing the contrasting two storey pair, the varied setbacks and the consistently low pipe and wire mesh fences.

The “Quartets”, nine two storey blocks, each of four small flats, at 444-478 Williamstown Road, are designed with similar principles in mind. Although repetitive in form, their staggered layout responds to the triangular site and creates a forecourt area, giving a strong sense of identity.

These features make Fishermans Bend a unique environment far removed from the popular idea of a characterless housing commission estate.



The two, two storey pairs of dwellings at the corners of Dunstan Parade and Centre Avenue frame the vista of the street. The vista was originally intended to be closed by the community centre.

4. CURRENT CONDITION OF FISHERMANS BEND

The population of Fishermans Bend remained reasonably stable for many years and this, along with the control exerted by the HCV, saw little change, except for that regulated by the HCV. More recently, the demographic profile of the area has changed, with the passing of many of the early inhabitants and the sale of dwellings by the HCV.

There is an increasing demand for facilities and space superior to the basic forms provided by the HCV for the lowest income groups. The concern of Council is to balance this demand with protecting the heritage significance of the area, so that the consistency of the streetscape character and the themes of its working class origins are maintained.

The layout and architectural form of Fishermans Bend remains relatively intact. There are many dwellings and fences in their original form. Also, many dwellings have new aluminium windows and projecting porches to replace the original small canopies. These elements were altered by the HCV in consistent designs and are considered to be part of the historic development of the area. Also, the HCV added carports to some properties.

A number of other changes have been made to individual dwellings which, if repeated further, have the potential to irrevocably alter its character. Examples of these changes include the demolition of dwellings, visually dominant second storey additions to single storey dwellings, alterations to facades and fences, and the addition of verandahs and other elements in styles foreign to the area. Most of these changes took place prior to the introduction of heritage provisions for the Fishermans Bend area, in December 1998. This earlier development occurred predominately along The Boulevard and Barak Road, some of which does not reflect the character of the estate sought to be protected and reinforced through these guidelines

The layout of the streets and their infrastructure of concrete kerb and channel and concrete footpaths is essentially unchanged, but the original vision of the estate's planners was never fully realised. Only one side of the shopping centre was built, the eastern side being replaced by the "temporary" community centre that exists today. The proposed community centre at the end of Centre Avenue was never built. The extensive recreational facilities to be located in Buckingham and Julier Reserves were never built and the regular street tree planting was never undertaken. Thus Fishermans Bend has an unfinished look.

The provision of community facilities, street trees and the treatment of the reserves in Fishermans Bend need attention, and will be considered as part of the ongoing management of the garden city neighbourhood.



The dwelling to the left of the pair has an HCV porch and aluminium windows. That to the right has its original timber double hung windows and porch (now built in).

5. APPLICATION OF THE GUIDELINES

These Guidelines aim to enable residents to renovate and improve their dwellings to modern standards, in the context of maintaining the heritage significance of the area.

Since December 1998, the Fishermans Bend area has been subject to new planning provisions in the Port Phillip Planning Scheme, including policy, heritage, amenity and design and development provisions. The heritage provisions are as follows:

Clauses 21 and 22 – Local Planning Policy Framework

The Municipal Strategic Statement (Clause 21.05-5) and the Port Phillip heritage policy (Clause 22.04) outline objectives, strategies and policies that apply to all land within a heritage overlay. These aim to conserve identified significant heritage places and manage new development so that it respects the heritage significance of an area.

Clause 43.01 - Heritage Overlay

In a heritage overlay, a planning permit is required to:

- Subdivide or consolidate land.
- Demolish or remove a building.
- Construct a building.
- Externally alter a building.
- Construct or carry out works.
- Externally paint a building.

Note: No planning permit is required for internal alterations to a dwelling in Fishermans Bend.

Before deciding on an application in a heritage overlay, the responsible authority must consider various specified matters, including the Port Phillip heritage policy and the Port Phillip heritage review.

Clause 81 - Port Phillip Heritage Review

The heritage review contains a statement of significance for the garden city neighbourhood and its various estates.

Clause 81 - City of Port Phillip Heritage Policy Map

The heritage policy map identifies all of the buildings and open spaces in Fishermans Bend as significant heritage places.

The heritage overlay is a standard Victorian Planning Provision. While it offers general protection of heritage places, it does not provide specific guidance on the implementation of its objectives. In particular, highly consistent areas such as Fishermans Bend require definition and interpretation of the heritage overlay objectives in order to respond to their particular significance and built form. Similarly, the Port Phillip heritage policy offers general policies and performance measures for all heritage overlay areas and does not provide specific guidance for highly consistent heritage overlay areas.

These guidelines will be used by Council to assist with the assessment of planning permit applications in the Fishermans Bend area, including the “Quartets” at 444-478 Williamstown Road. An applicant is encouraged to use these guidelines when preparing a development application for a property in the area.

A development proposal in Fishermans Bend:

- should satisfy the heritage objectives in the Port Phillip planning scheme as well as the objectives of the guidelines; and
- should respond to the guidelines where appropriate.

Where an application is lodged for a development that does not satisfy the guidelines, the onus will be on the applicant to demonstrate that their proposal does not detract from the heritage significance of the area.

PART TWO:

THE GUIDELINES

The following Guidelines apply to the Fishermans Bend area, bounded by The Boulevard to the south, Todd Road to the west, Howe Parade and Williamstown Road to the north and Barak Road to the east, and 444-478 Williamstown Road.

6. OBJECTIVES OF THE GUIDELINES

- To protect the heritage significance and architectural character of the Housing Commission of Victoria Fishermans Bend Estate, which is of identified local heritage importance.
- To recognise the changing needs of people living in Fishermans Bend and the potential to make alterations and additions to dwellings for amenity purposes which do not affect the significance of the area.
- To increase community awareness about the special qualities and significance of the area.
- To provide a clear and consistent basis for the assessment of planning permit applications having regard to the heritage overlay and heritage policy provisions in the Port Phillip Planning Scheme that apply to the area.

7. WORKS IN PUBLIC AREAS

Principle:

That Council, as the responsible authority for the infrastructure of Fishermans Bend, plays its part in the conservation and enhancement of the areas' overall character.

Guidelines:

- Works within the public areas of Fishermans Bend, including to roads and the public reserves, should be consistent with the forms and materials of the original works, with the minimum modification required to the original layout in order to meet current requirements and standards. This applies to roads and road surfaces, kerbs and channels, footpaths, crossovers, nature strips and public open spaces.
- Any new works that are not part of the original design (such as kerb extensions and parking bays) and repairs should match the form and materials of the original parts.
- Speed humps, if necessary, should be of asphalt.
- Roundabouts, if necessary, should be of minimal size and not significantly alter the original kerb lines.
- Tree planting informed by the original layout should be introduced. Consideration could be given to using species that might have been planted at the time of construction of the estate and that are suitable to the local environment.
- Development of the three major open spaces, Buckingham Reserve, Julier Reserve and the reserve on the Boulevard, should be undertaken to satisfy current and future resident needs and using the original plan as a basis.
- Any new landscaping should achieve environmental outcomes, including water conservation, the inclusion of water sensitive urban design and the provision of fauna habitat.



The existing playground area of Buckingham Reserve (2010).

8. ADDITIONS AND ALTERATIONS TO DWELLINGS

Preamble:

The general approach for additions and alterations to dwellings is that the issues of concern should be limited only to those elements obviously visible from the public domain. For the purposes of these guidelines, the public domain is defined as any place within the public areas of Fishermans Bend or the areas around its boundary. The principal views of and within Fishermans Bend are not generally confined to street widths (as in a typical Port Phillip streetscape setting), but are long views, for example, those across the reserves.

Principle:

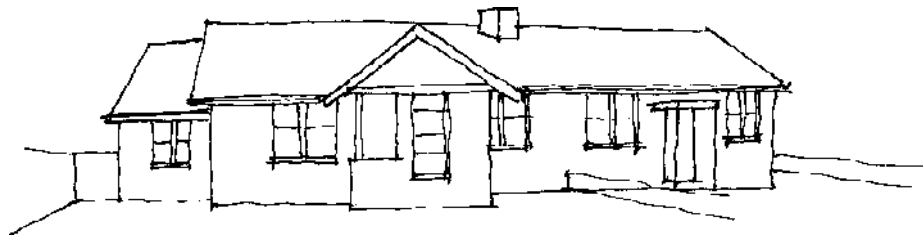
Visible additions and alterations to a dwelling should maintain the character of the area and be harmonious with the existing dominant character of the area. Such additions should generally employ similar design forms, materials and detail, but should be recognisable as a new element.

8.1 SINGLE STOREY ADDITIONS AND ALTERATIONS

Guidelines:

Single storey additions and alterations to a dwelling should address the following guidelines:

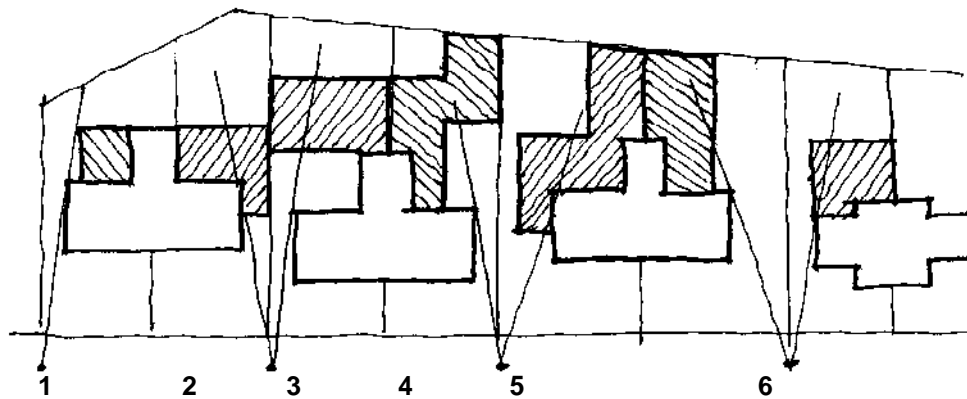
- They should not be in front of or part of the front façade of the dwelling, except for new porches.
- They should preferably be at the rear of the dwelling and should not be visible from the street.
- If at the sides of a dwelling, they should be set back at least 1.5 metres from the front wall of the dwelling and the roof should be of noticeably lesser height than that of the dwelling. In the case of a two storey dwelling, a single storey addition at the side should be noticeably lower than the main roof of the adjacent single storey dwelling.



A simple gable side addition, possible where there can be a boundary setback. Note that it is set back from the front and is lower than the main roof.



A side hip roof addition, required when building to the boundary.



Siting and forms of single storey additions that are concealed or largely concealed and therefore preferable.

1. A small addition which is not visible
2. A boundary to boundary rear addition, obscured behind a side addition.
3. A boundary to boundary rear addition, forming an internal court and set well back.
4. A multiple courtyard solution.
5. Party wall addition with rear addition.
6. Simple party wall additions.

- Roof forms, pitches and materials should generally be the same as the main structure, although other options may be considered (eg a small addition may incorporate parapet walls with a flat roof).
- Materials sympathetic to the existing dwelling should be used.
- The original chimney, if in front of the roof ridge line, should preferably be retained.
- Window openings, if visible from the street, should be of similar proportions to those of the existing dwelling.
- New window openings in the existing dwelling should be avoided. Where new openings are required, timber frames should be used to match the proportions of the original front windows. The extent of any new openings should be minimised.

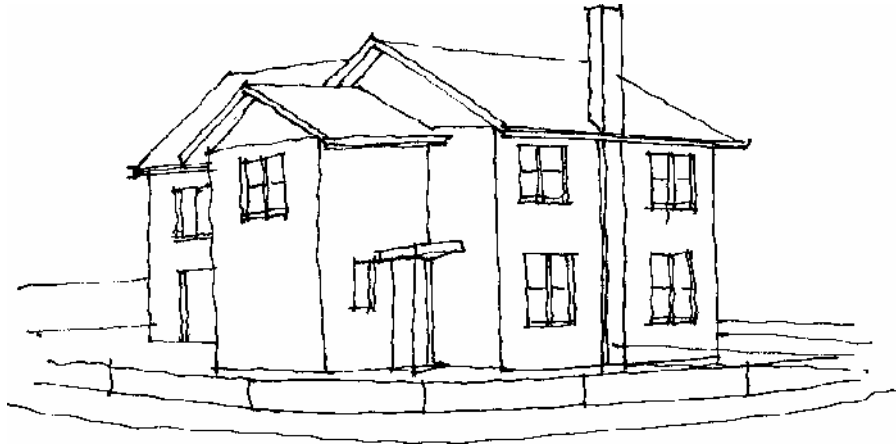
8.2 Two storey additions to two storey dwellings

Guidelines:

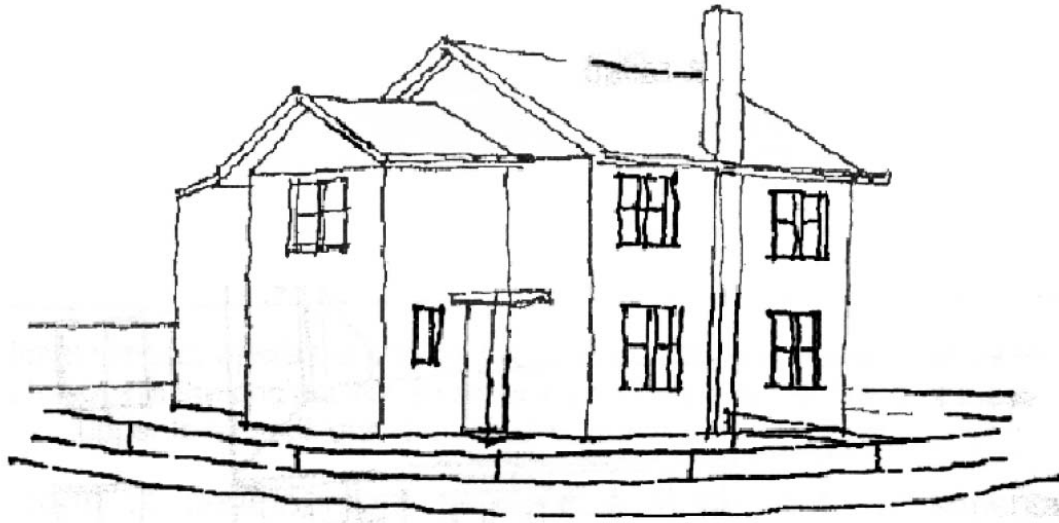
Two storey additions to two storey dwellings should address the following guidelines:

- They should be located at the rear of the existing dwelling.
- The roof should be of noticeably lesser height than that of the existing dwelling.
- The walls of the addition should not exceed the height of those of the existing dwelling.
- Where visible from the public domain (eg corner sites), the roof form, pitch and materials of an addition should be similar to the main structure.
- The original chimney should preferably be retained.
- Window openings, if visible from the street, should be of similar proportions to those of the existing dwelling.
- New window openings in the existing dwelling should be avoided. Where new openings are required, timber frames should be used to match the proportions of the original front windows. The extent of any new openings should be minimised.

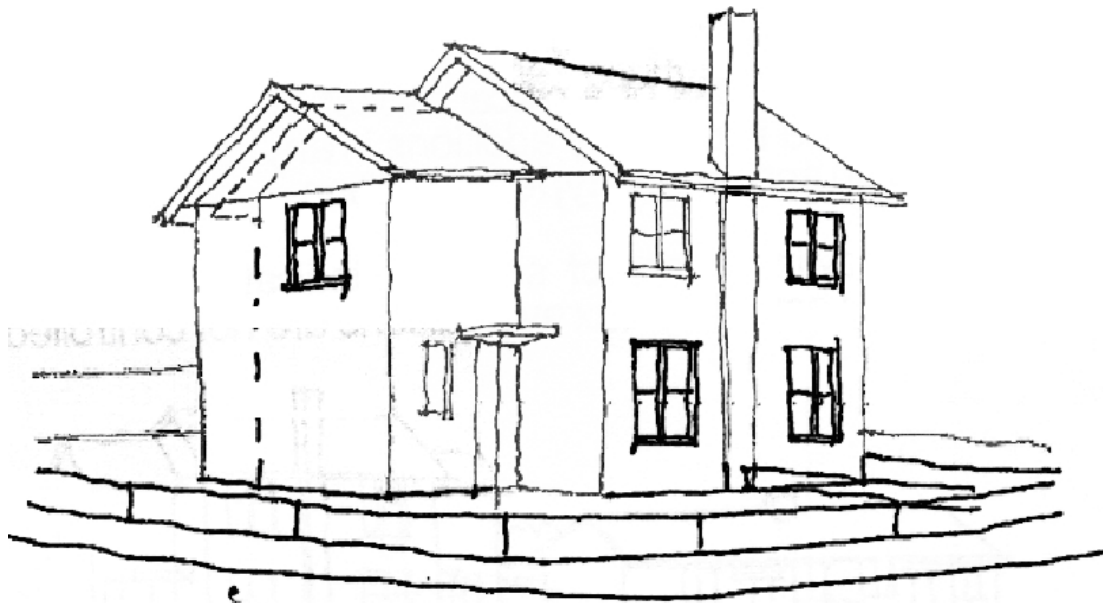
The following are three examples of appropriate types of addition, which can be used in combination.



A rear gable addition. This could incorporate a hip roof, if height reduction at the rear was necessary.



The addition of a skillion at the rear, stepped back from the side gable to ensure clarity of built form.



Enlargement of the side gable so that the back roof plane is aligned with the main gable.

8.3 TWO STOREY ADDITIONS TO SINGLE STOREY DWELLINGS

Preamble:

Two storey additions to single storey dwellings is the most sensitive matter addressed by these guidelines.

The original mix of one and two storey dwellings is considered a key aspect of the significance of the Fishermans Bend area. The number and variety of single storey dwellings are also of significance, as is the technology of single storey concrete dwellings. It is desirable that these aspects of significance are protected.

It is of note that whilst Fishermans Bend seems of reasonably low density, it is as much as four times as dense in composition as typical inter war estates, in that dwellings are roughly only half as wide and allotments only half as deep as in other inter war subdivisions.

In this context, and having regard to the desire of many owners to improve the amenity of the dwellings, it is recognised by Council that second storey additions to single storey dwellings may be appropriate.

Principles:

Single storey additions to single storey dwellings are encouraged ahead of two storey additions.

Second storey additions to single storey dwellings should be designed in a manner which allows the character of the original single storey dwelling to remain dominant and legible.

Dwellings were designed as mirrored pairs. A second storey addition to one of a pair of dwellings results in a visual mismatch. Retaining the rhythm of pairs, with both dwellings added to in the same form, is unlikely but should nevertheless be attempted.

Guidelines:

Two storey additions to single storey dwellings should address the following guidelines:

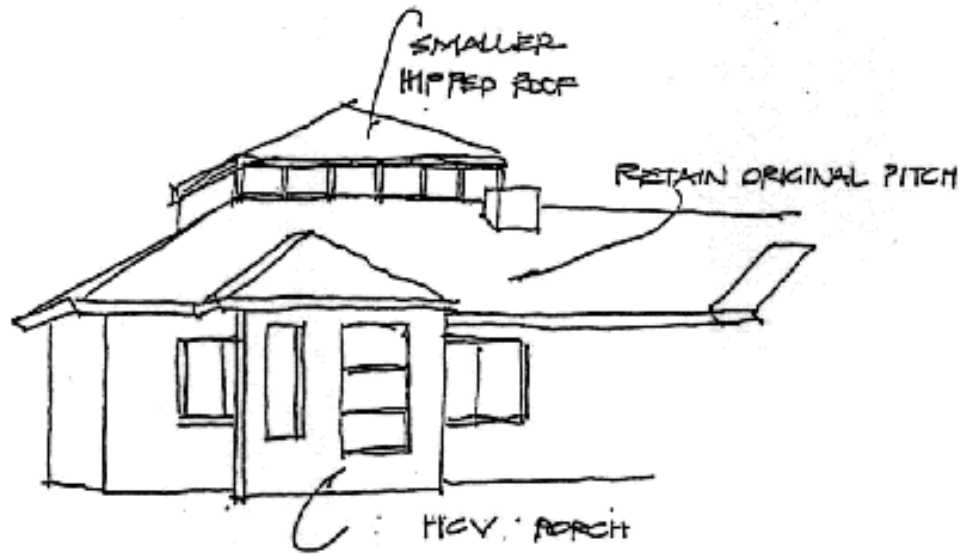
- Additions into the front setback of the site are discouraged.
- Courtyard developments at single storey height are the most suitable form for additions.
- Small second storey additions, visible from the street, are possible. The front roof plane shared by the pair should be retained, ie the addition should be sited on or behind the existing roof ridge line.
- Sheer, two storey high walls on street elevations, or in front of a vertical line taken from the original ridge point on the side wall, are inappropriate.
- The height of the visible roof of an addition, measured vertically from the gutter line to the ridge line, should be less than that of the existing roof. Refer illustrations (a) and (b).
- The sense of the original single storey end gable on any side wall should be retained, eg retain existing eaves or eaves line. Refer illustrations (a) and (b).
- Two storey additions should not be higher than any original two storey dwelling in the Fishermans Bend area.
- Symmetry should be retained around the pair of dwellings and not the half being altered.

Some suggested second storey additions to single storey dwellings are shown in the following illustrations (a) to (l).

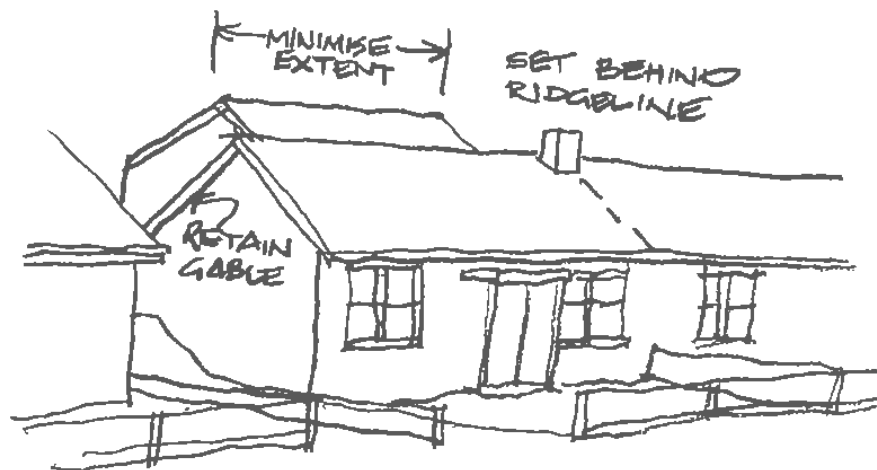
These suggestions include alternatives where the front door is relocated on the facade of the original dwelling to allow for an improved internal layout. As change to the front facade of a dwelling that is visible from the public domain should be minimised, a sympathetically extended porch roof or the addition of a similar style enclosed porch may assist in diminishing the change associated with relocating a front door. Refer illustrations (f) and (h).

Whilst the least preferred suggestion, a possible exception to the second storey addition guidelines would be an addition in the form of a skillion dormer inserted into an existing or extended visible roof plane. Such an addition should be modest in scale (for example, the length of the dormer should be no more than one half of the width of the dwelling being altered) and should be kept well back from the front wall plane. This should ensure that the dormer does not dominate the existing dwelling.

SUGGESTED TWO STOREY ADDITIONS TO SINGLE STOREY DWELLINGS

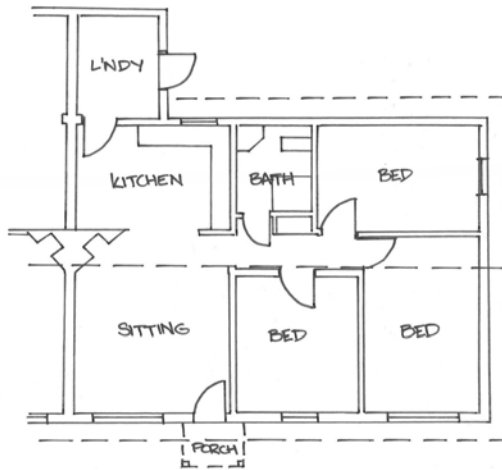


A. Gable roof, brick houses



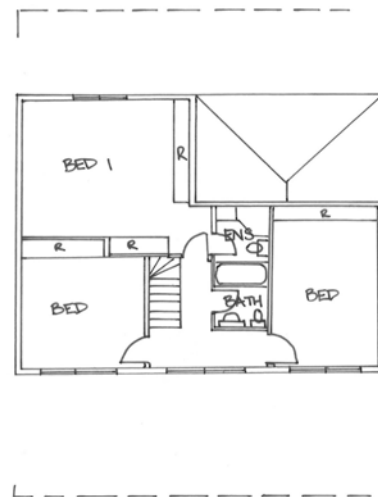
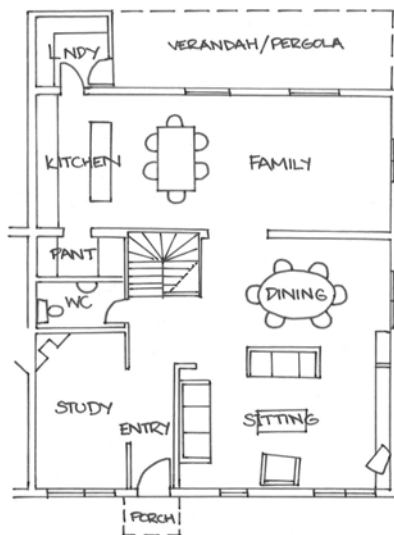
B. Hip roof, concrete houses

SUGGESTED TWO STOREY ADDITIONS TO SINGLE STOREY DWELLINGS



C. 0 1 2 3m Approx.

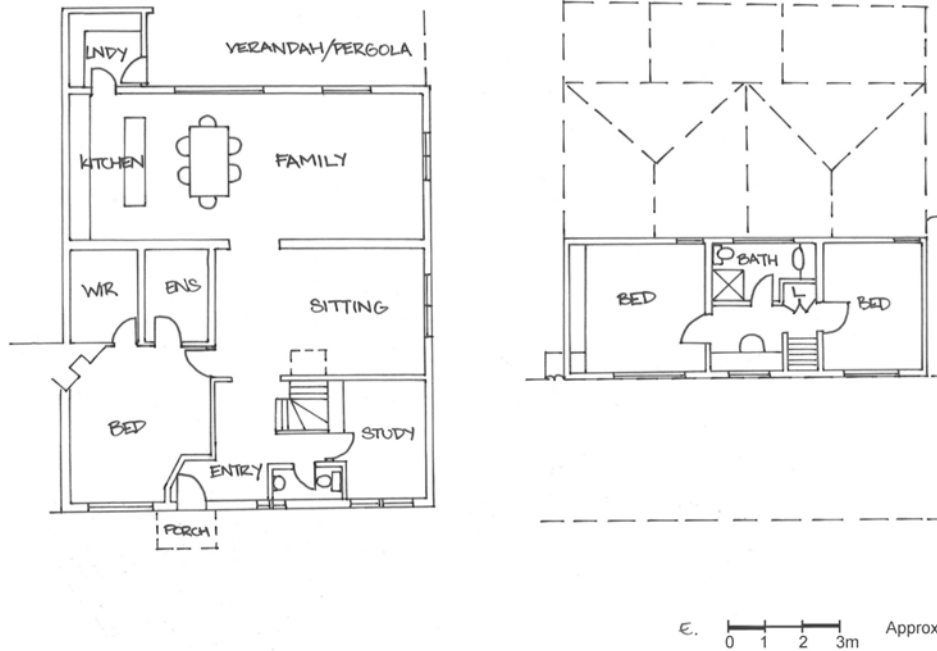
C. Three bed house, gable roof, original plan



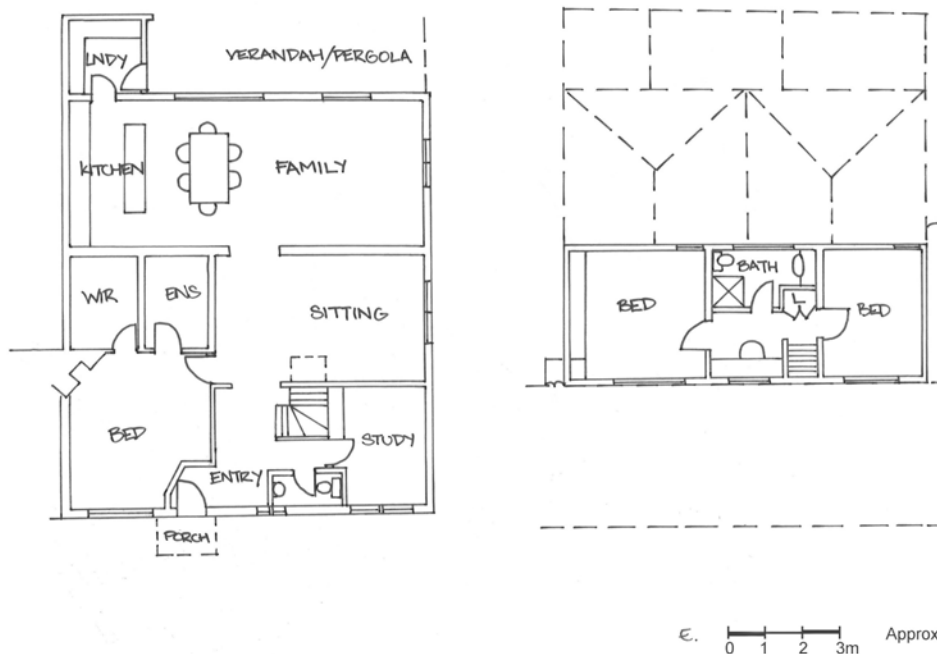
D. 0 1 2 3m Approx.

D. Three bed house, gable roof, indicative plan

SUGGESTED TWO STOREY ADDITIONS TO SINGLE STOREY DWELLINGS

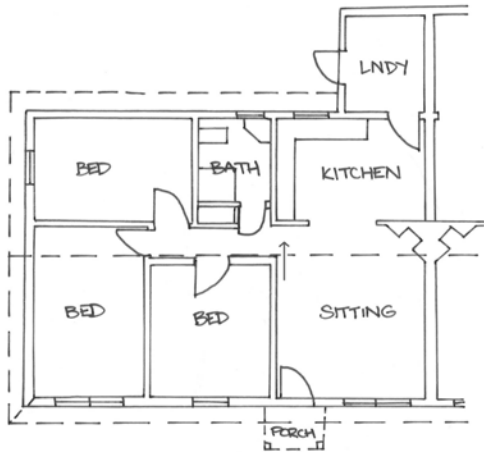


E. Three bed house, gable roof, indicative plan



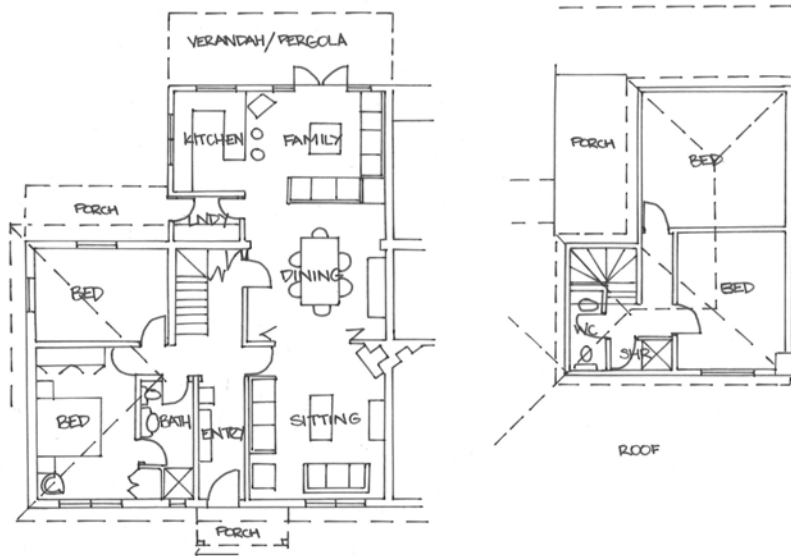
F. Three bed house, gable roof, indicative plan

SUGGESTED TWO STOREY ADDITIONS TO SINGLE STOREY DWELLINGS



G. 0 1 2 3m Approx.

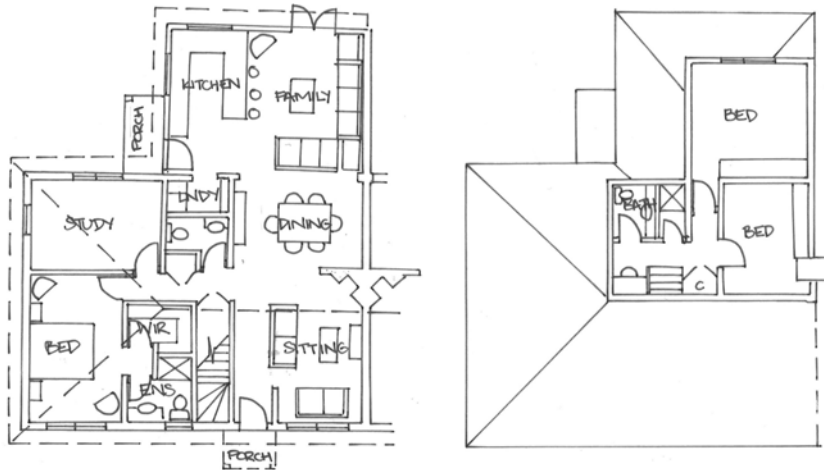
G. Three bed house, hip roof, original plan



H. 0 1 2 3m Approx.

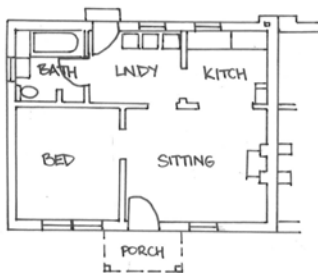
H. Three bed house, hip roof, indicative plan

SUGGESTED TWO STOREY ADDITIONS TO SINGLE STOREY DWELLINGS



I. 0 1 2 3m Approx.

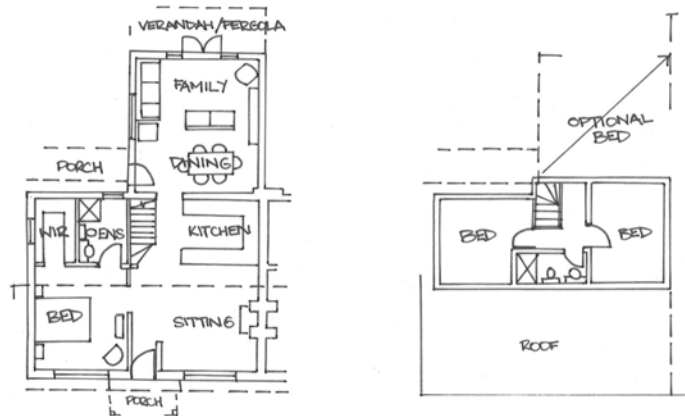
I. Three bed house, hip roof, indicative plan



J. 0 1 2 3m Approx.

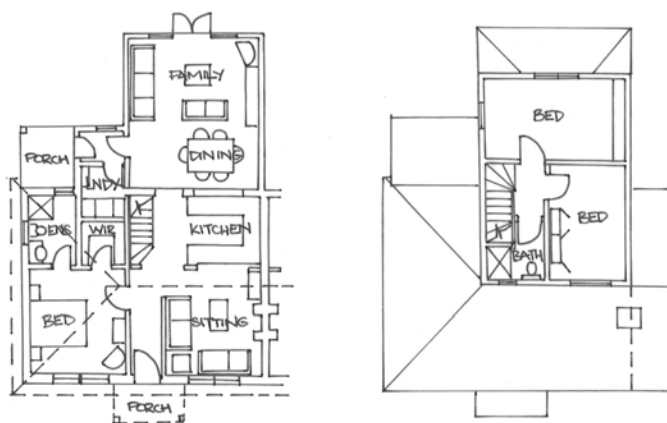
J. Small house, gable roof, original plan

SUGGESTED TWO STOREY ADDITIONS TO SINGLE STOREY DWELLINGS



K. 0 1 2 3m Approx.

K. Small house, gable roof, indicative plan



L. 0 1 2 3m Approx.

L. Small house, hip roof, indicative plan

9. PARKING, GARAGES AND CARPORTS

Preamble:

The layout of Fishermans Bend was not designed to accommodate a car on every allotment. Generally there are few garages or carports in the front setbacks of the dwellings in Fishermans Bend.

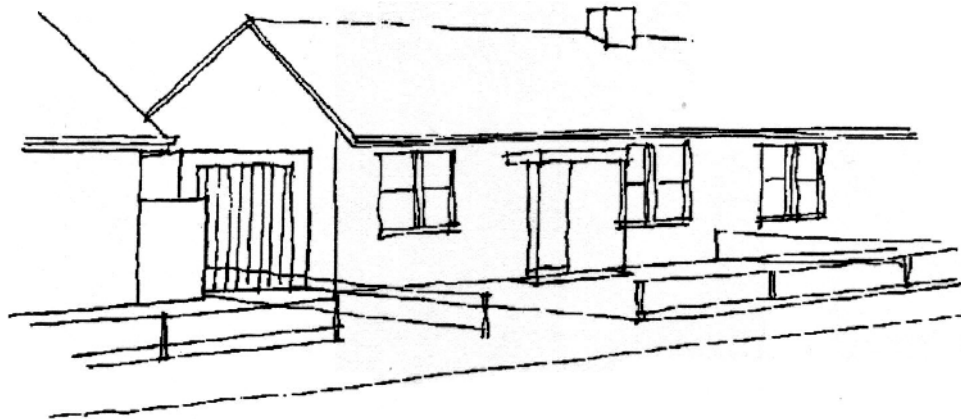
Principle:

A garage or carport should be sited and designed so that the dominance of the fronts of dwellings in the streetscape and the dwelling in a garden setting is maintained.

Guidelines:

Parking, garages and carports should address the following guidelines:

- Parking in driveways, carports or garages, is encouraged on the proviso that any crossover does not coincide with the location of an existing street tree.
- The space in front a dwelling should be retained as lawn or garden and not used as a space to park vehicles.
- Garages or carports should be sited behind the adjacent front wall of the dwelling.
- Carports should be recessed 500mm from the front wall of the existing dwelling and should be constructed of the visually lightest framing possible, either in timber or steel.
- Garages, because of their opaque form, should be set back from the front wall of the dwelling at least 500mm. Eaves, if included, may reach the adjacent front wall of the dwelling. Roller shutter doors should be avoided as they add extra height.



This garage has a flat roof and a vertically sheeted tilt door. Note the 500mm setback from the front wall of the dwelling.



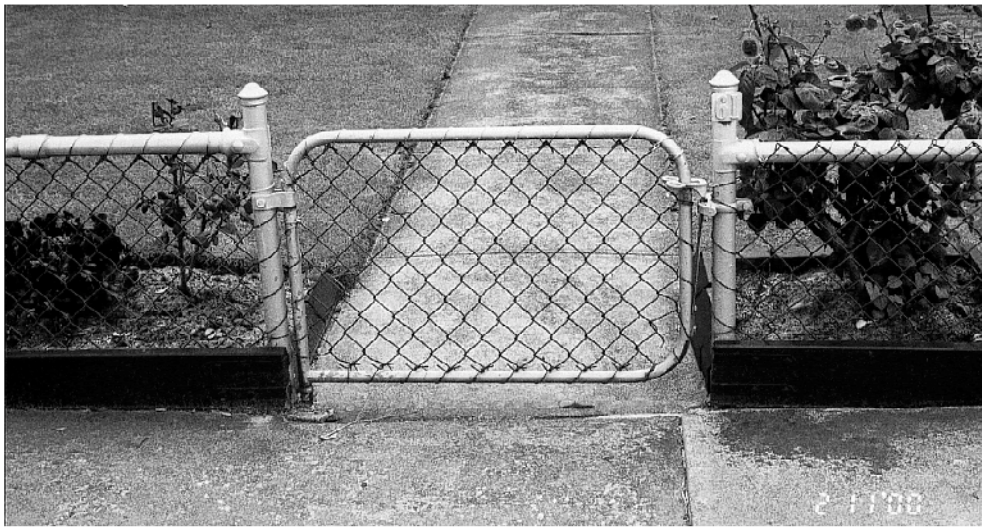
This carport, although setback by 500mm, has uncharacteristic decoration making it a prominent element of the site in comparison with the dwelling, thus attracting attention not warranted by a secondary element

10. FENCES

Preamble:

The Fishermans Bend area originally had common fence types for all properties. Those at the front of the dwellings were the low galvanised pipe and chain wire mesh fences that are still quite common.

Most replacement fences are relatively low and are of various combinations of materials, with brick and wrought iron predominant.



Portion of an original front fence. All fences were constructed like this, although some were higher (eg Howe Parade where the first experimental dwellings were built).

Principles:

Retention of original fences is encouraged.

To maintain the character of the area, front fences should remain low.



The existing variety of fences, some original, is mostly low.



High front fences introduce a new and inappropriate element into the streetscape; they alter the open character of the area and prevent surveillance and communication between street and dwellings.

Guidelines:

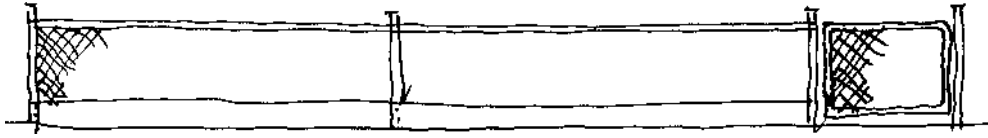
New fences should address the following guidelines:

- Fences at the front of properties, back to the line of the adjacent front wall, or the front wall of the adjacent property if it is closer to the street, should generally be no higher than 1 metre to the top of the fence panels. Posts could be higher.
- Should be of a style that is reasonably appropriate to the period of the estate, ie. Inter-war and immediate post war styles.
- Apart from posts or masonry piers, fences should have a straight top.
- For side and rear fences behind the front wall of the dwelling, and from the side boundary to the building, should not exceed 2 metres in height and preferably be of palings.

The materials and design of fences within the above guidelines should match the materials of the dwelling. Some appropriate fence examples are shown in the illustrations below.

SUGGESTED FENCE TYPES

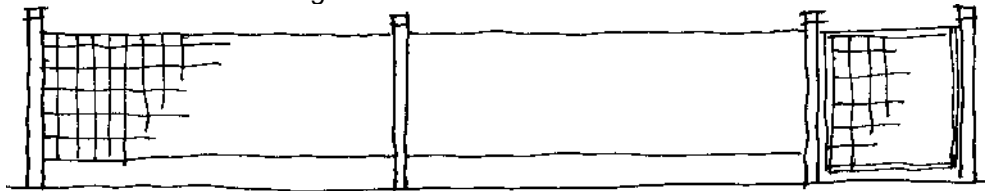
These are only general suggestions for fences. Many other designs may be acceptable.



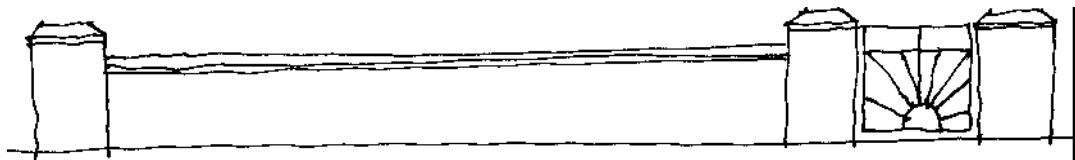
*The low pipe and chain wire mesh fence used originally on the estate.
From 600mm to 1000mm high.*



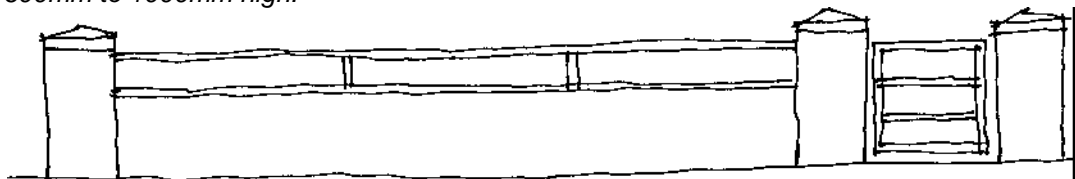
*Chain wire mesh fixed to a wooden frame.
From 800mm to 1000mm high.*



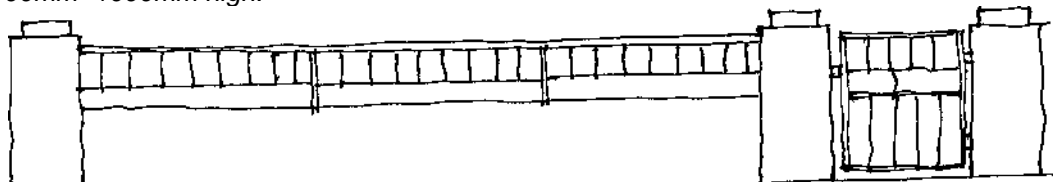
*Modern weld-mesh fixed to either a galvanised steel or timber frame.
1000mm high.*



*A simple brick fence with a chamfered capping brick and wrought iron gate.
600mm to 1000mm high.*



*A brick fence with a steel pipe rail.
700mm- 1000mm high.*



*A brick fence with a wrought iron top section. Proportion of plinth to height of iron can vary.
700mm- 1000mm high.*

11. CONSTRUCTION DETAILS AND MATERIALS

Preamble:

The original construction details and the materials play an important supporting role in establishing the historic character of the estate.

Principles:

The use of similar materials and details in the visible parts of new development and in restoration of the original features of dwellings is encouraged.

The introduction of visually intrusive new elements (eg exposed air conditioners) is discouraged.

Guidelines:

Construction details and materials should address the following guidelines:

11.1 Roofs

- Apart from the experimental dwellings on Howe Parade, all roofs in the Fishermans Bend area are of glazed terra cotta tiles. The same colour mix of terra cotta tiles should be used on new roofs, except where they cannot be seen from the street.
- Roofs are hipped on the concrete dwellings and gabled on the brick dwellings. There is an occasional example of a later hipped porch on the gabled dwellings and vice-versa. Additions should generally use the same roof forms as the existing dwellings, although there are exceptions, for example, where a hip roof is necessary when building to the boundary or where a flat roof with parapet walls is appropriate for a small single storey addition.

11.2 Chimneys

- Chimney stacks above the roofs are usually shared between adjacent dwellings. They form an important part of the roof-scapes and should preferably be retained if they are visible from the street.
- Chimneys in front of the transverse ridge are a priority. As they are normally on the joint wall between pairs, their retention should not be difficult.

11.3 Roof gutters, verges and downpipes

- The roof gutters of the concrete dwellings are of a special splayed type that are no longer commercially available. Their downpipes exit from the backs of the gutters with the horizontal arm concealed above the eaves lining. Sheet metal shops can fabricate replacement gutters and Council's heritage adviser has details of suppliers and costs. Retention and replacement of these elements is encouraged.
- The gutters on gable roofed dwellings appear to have originally been bull-nosed with exposed cranked circular downpipes. This is the pattern that should preferably be used in replacements.
- The eaves of the concrete dwellings are boxed with a splayed fascia, while the eaves of the gabled brick dwellings are raked and projecting with a vertical gutter board. This distinction should be maintained in new works.



The original details of a concrete dwelling. Note the splayed eaves verge and splayed gutter, the eaves boxing, the partially concealed downpipe and the sliding timber windows with horizontal glazing bars set directly in the projecting concrete architrave in which the tracks are located.

11.4 Walls and gable ends

- Types of brick used vary from over-burnt (clinker) to pink and cream. The gable ends of the single storey dwellings are in continuous brick, while those of the two storey dwellings are sheeted in vertical sawn boards. The gable ends of the later porches are sheeted in fibro-cement with a pattern of exposed battens. The pattern of wall treatments used in the original dwellings should be used in any new comparable works that are visible from the street.
- The walls of new development, where visible from the street, should generally match those of the existing dwelling, although slight variations in brickwork will be inevitable.
- The walls of additions to concrete dwellings, where visible from the street, should be similar in appearance. Suggestions include rendered brick or cement sheet, either plain or with an applied sand finished surface.

11.5 Porches

- The original porches to all of the dwellings were simple concrete canopies with a splayed edge supported on two slender steel pipe posts. The original porches should be retained, if possible.
- Later enclosed porches provided by the HCV, are hipped or gable projecting roofs supported on brick walls, with a large window and an open section supported either on a brick column or a timber post. They were not always added to both dwellings of a pair. These elements should be retained, if possible.
- Similar porches could be added to those dwellings lacking them.



Later rendered brick porches on concrete dwellings.

11.6 Windows and doors

- Many of the original timber windows of the dwellings have been replaced with aluminium by the HCV. This is presumably due to deterioration resulting from inadequate design. If original or later sliding windows are visible from the street and are to be replaced, they should be in a simple form with a pair of sashes reflecting the pattern of the original, either sliding or side hung casement, and with two horizontal glazing bars.
- Timber double hung windows, mostly paired, in many of the brick dwellings, should be retained if possible and/or replaced in similar form, if visible from the street.
- Front doors were simple framed vertical board doors without sidelights or highlights. This pattern should preferably be maintained.
- The conversion of windows to doors is discouraged, where visible from the street.
- New or enlarged windows and bay windows are discouraged, where visible from the street.
- Original windows and doors should be retained and, where possible, the reinstatement of windows and doors in the original form is encouraged.

11.7 Attachments

- Fixed awnings or other permanent attachments for sun control should be suitable to the architectural style of the dwelling and designed to be as unobtrusive as possible.
- Air conditioners should be as unobtrusive as possible and should not be placed in the front elevation of dwellings and the upper sides of two storey dwellings. Roof mounted condensers or packaged units should be placed on the rear planes of a roof. Such equipment should be located where it cannot be seen from the principal street frontage(s).

12. EXTERNAL DECORATION

Preamble:

In order to minimise maintenance, the only painted surfaces in the original dwellings were exposed timber and metal. All other surfaces were natural, including the precast concrete walls. All of the concrete walls have now been painted, presumably initially by the HCV, but most brickwork remains natural. This is important to the character of the area.

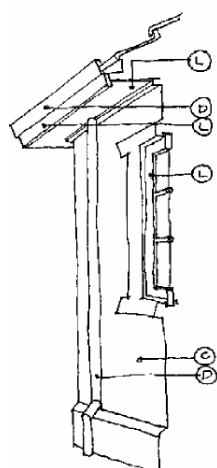
Principle:

That decoration of the visible external fabric of a dwelling is consistent with the original palette of materials and finishes used in Fishermans Bend.

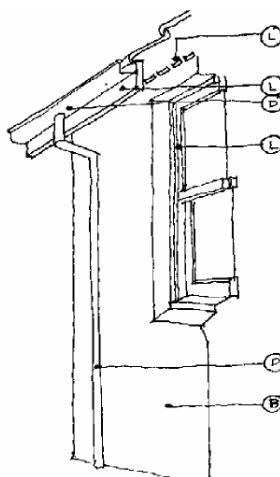
Guidelines:

External decoration of dwellings should address the following guidelines:

- Unpainted brickwork should not be painted.
- Neighbours in pairs are encouraged to agree on the same or similar colour schemes for painted surfaces.
- Concrete surfaces should preferably be repainted in pale grey or stone colours, using a matt or slightly textured acrylic paint.
- As a general guide, colours should be kept simple, using only two colours - a dark colour for metal trim including roof gutters and a light colour for timber trim, including windows.
- The gable end vertical boards of the two storey dwellings were probably originally creosote stained. A similar finish should be maintained. Suggestions include modern timber finishes such as fence-coat or deep brown matt acrylic paint.



Concrete dwelling



Brick dwelling

Location of colours.

L = Light trim

D = Dark trim

C = Concrete or render

B = Brick

Eg, Off white, cream, light green

eg, Deep red, deep green

Eg, Warm grey, stone, beige, etc

Not to be painted

13. LANDSCAPING

The following information is provided for interest only.

The typical inter war house garden designs that are represented in Fishermans Bend have the following characteristics:

- simple and formal;
- lawns contained in border planting;
- central features and specimen trees;
- no concreting of front setbacks;
- no garden structures (such as pergolas); and
- low, transparent front fences.

Residents are encouraged to select drought tolerant trees and plantings, and to utilise water sensitive urban design measures where possible.

Council's heritage adviser has further information on the popular styles of garden of the inter war and immediate post war period.

14. DEMOLITION

Preamble:

Section 5 of these Guidelines identifies the heritage provisions in the Port Phillip Planning Scheme that apply to Fishermans Bend. As noted, all of the buildings and the open spaces in Fishermans Bend are identified significant heritage places on the City of Port Phillip heritage policy map, December 2000.

Principle:

The Port Phillip heritage policy (Clause 22.04 of the Port Phillip Planning Scheme) applies to all heritage overlay areas in Port Phillip, including the Fishermans Bend heritage overlay area (HO2). It contains heritage objectives and policies in relation to the demolition of significant heritage places as follows:

- Require all applications for demolition of significant buildings to be accompanied by an application for new development.
- To allow the demolition of part of a significant building if it will not affect the significance of the building and the proposed addition is sympathetic to the scale and form of the place.
- Not support the demolition of a significant building unless and only to the extent that: -
 - The building is structurally unsound or cannot be feasibly reused.
 - The replacement building and/or works displays design excellence which clearly and positively supports the ongoing heritage significance of the area.

Guidelines:

In the Fishermans Bend area, it is recognised that:

- The rear section of some dwellings may be able to be demolished without affecting the significance of the building and subject to a rear addition being sympathetic to the scale and form of the original building and respecting the character of the area.
- Some dwellings may be structurally unsound or cannot be feasibly reused. The onus will be on the applicant to demonstrate that this is the case and also that the replacement building displays design excellence that does not detract from the heritage significance of the area. A replacement building should respond to the character and form of the original dwelling, particularly because any individual dwelling is usually one of a pair.

15. SUBDIVISION AND MULTI-UNIT DEVELOPMENT

Preamble:

The application of heritage provisions and other provisions in the Port Phillip Planning Scheme generally do not provide much scope for other dwellings on the small sites in Fishermans Bend. Therefore, multi unit development is generally not feasible.

Principle:

The repeating rhythm of pairs of attached dwellings is critical to the character of Fishermans Bend.

Guideline:

- Subdivision and multi-unit development is discouraged within Fishermans Bend.

16. CONSOLIDATION OF SITES

Preamble:

The consolidation of individual sites into larger lots may be proposed in order to achieve practicable multi-unit development, but this will threaten the significant rhythm of the subdivision pattern and site layouts.

Principle:

The original rhythm of the subdivision pattern and site layouts in Fishermans Bend should be retained.

Guideline:

- Consolidation of individual sites into larger areas for multi-unit development is discouraged.



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■ ABN 23 308 903 866

The Dunstan Estate

Griffin Avenue, Southward Crescent, part
Graham Street and part Williamstown Road,
Port Melbourne

Heritage Guidelines

Prepared for
City of Port Phillip
by
heritage ALLIANCE

November 2007



HERITAGE GUIDELINES FOR ALTERATIONS AND ADDITIONS TO DWELLINGS IN THE DUNSTAN ESTATE, PORT MELBOURNE

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PART ONE: BACKGROUND TO THE GUIDELINES

1.0 Introduction

History

The Dunstan Estate of dwellings was constructed in 1936 for the Premier's Department of the State Government of Victoria to designs prepared by the Victorian Public Works Department. The project occurred as a result of long-term concerns about housing conditions in Melbourne in the early 20th century and various efforts to address the issue, which had become focused on the vacant land of Fisherman's Bend following ongoing Port Melbourne Council pressure since 1906.

The State Bank Estate (Garden City) was constructed for deserving low-income families to purchase dwellings on favourable terms, but had not addressed the real problem of the need for rental housing. The Government was proceeding to appoint the Housing Investigation & Slum Abolition Board (HISAB) in 1936 when, in the attempted coup designed to relieve the political pressure, the Country Party Premier - Albert Dunstan, announced plans for 50 new dwellings to be built in the eastern corner of the intersection of Williamstown Road and Graham Street.

The minority Country Party Government was supported at the time by the Labour Party, hence the need for Dunstan to demonstrate some action to address working class concerns. Dunstan was also under pressure from social reformers, led by Oswald Barnett, who became chair of the HISAB and was responsible for the establishment of the Housing Commission of Victoria and one of its four foundation members.

The Estate was never named but for obvious historical reasons and for simplicity in these guidelines, it is referred to as the *Dunstan Estate*. The designs for the Estate by the Public Works Department under Chief Architect, Percy Everett, continued the theme of the garden city set by the State Bank Estate but reflected the popular eclectic tastes of the 1930's including English cottage, Tudor and neo-Georgian variations of basic types. Forty-six of the fifty proposed dwellings were built, four being omitted at the east end of the Estate where the Housing Commission "Quartets" now stands.

The use of distinctly varied styles and the diagonal arrangement of the sites and dwellings in Griffin Crescent and Southward Avenue are the major innovations of this project. To some extent it addressed concerns about the smallness of sites at Garden City, but otherwise it could be described as "window dressing" for political purposes. The addition of less than fifty dwellings to the housing stock of the metropolis was going to have little impact on the severe shortage of housing available for rental in the inner city – this problem had to wait until the Housing Commission of Victoria was formed in 1938 and began the business of serious dwelling construction at Fisherman's Bend in 1939.

Nonetheless, the Dunstan Estate was the first housing estate built for rental tenancies by the State Government of Victoria.

2.0 The Extent of the Dunstan Estate

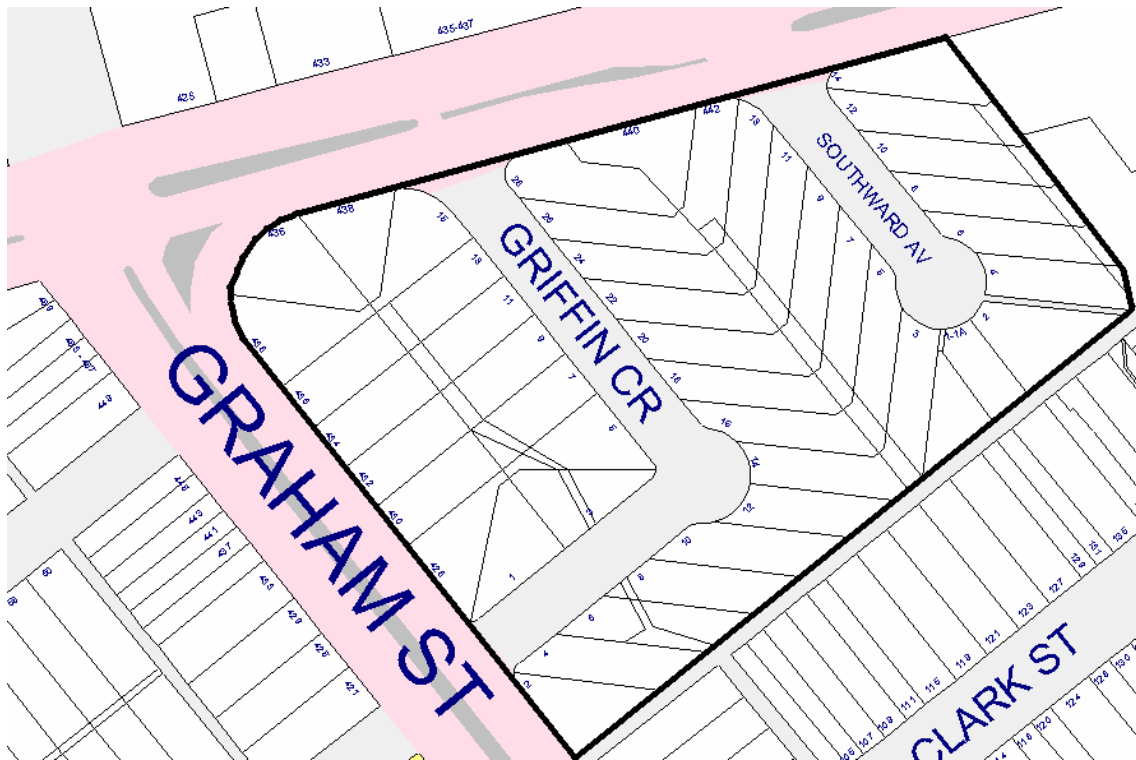


Figure 1: The Dunstan Estate

The properties covered by these guidelines are:

- ❑ 428 - 438 Graham Street
- ❑ 1 - 15 and 2 - 28 Griffin Crescent
- ❑ 1-1A - 13 and 2 - 14 Southward Avenue
- ❑ 436 - 442 Williamstown Road

3.0 The Heritage Significance of the Dunstan Estate

The Dunstan Estate in Port Melbourne, initiated by the then Premier of Victoria, Albert Dunstan, and constructed in 1936 to the designs of the Public Works Department, is of state significance as the first housing built by the State Government for rental rather than for sale.

The historical importance of the Estate lies in its contribution to a political progress towards the formation of a state-wide rental housing policy and its ultimate implementation by the Housing Commission of Victoria. The Dunstan Estate represents a small but important chapter in the attempts of politicians to provide working class housing.

The Estate's layout and collection of dwelling types adds some colour and interest to the essentially grim problem it confronted and its comfortable suburban image is softer than the more regimented designs of the earlier State Bank Estate and the Housing Commission Fisherman's Bend Estate which followed.

The Estate's heritage significance is physically embodied in the following;

- ❑ The layout of the estate and its continuation of the Garden City concepts first demonstrated at the adjacent State Bank Housing Estate in 1926.
- ❑ The unique layout of sites and buildings set diagonally to the street alignment.
- ❑ The integration of attached dwellings into the apparently much grander single composition that lent status to otherwise modest accommodation.
- ❑ The use of fashionable and pleasing designs for basic dwellings to support an image of a caring and capable State Premier who was prepared to tackle the housing crisis.
- ❑ The use of distinct Old English styles to create both ownership status and a variety of appearances for an otherwise standard dwelling type.

4.0 Important Features of the Dunstan Estate

The Estate is built on two streets which have asphalt finishes, a bluestone edge and concrete kerbing and in most places a wide nature strip with trees, concrete footpath, low height fencing, and a generous set back to the dwellings beyond.

The Estate comprises 23 pairs of semi-detached dwellings designed to four layouts:

- ❑ Type A of one bedroom (single storey);
- ❑ Type B of two bedrooms (double storey);
- ❑ Type C of three bedrooms (double storey) and
- ❑ Type D of four bedrooms (double storey).

Only two pairs of type A and two pairs of type D were ever built.

The variety in appearance of the dwellings was achieved by varying the façade treatment of each type and they are based on Old English styles popular at the time of construction.

The designs provided two treatments for Type B, three of Type C and two of Type D. In Type C (the most prolific), C1 is inspired by English cottages, C2 is neo-Georgian and C3 is Tudor influenced. The two pairs of dwellings omitted at the east corner of Griffin Crescent were intended to have been Type D.

The eclectic approach and the agreeably fashionable designs of the dwellings were probably a reaction to the much more strict approach to design employed at Garden City.

From the evidence of a number of early woven wire *Cyclone* side fences remaining, it would appear that this type was originally used consistently throughout the Estate for front fences. The existing street tree planting of Desert Ash reflects the popularity of the species at the time.

The most important feature of this small estate is its pleasing architectural and planning character as a close or contained group of dwellings, enhanced by the diagonal siting of buildings on relatively large sites.



Figure 2: Type A dwelling (single story)



Figure 3: Two storey Tudor style Type C dwelling



Figure 4: Two storey Georgian style Type C dwelling



Figure 5: Two storey Cottage style Type C dwelling

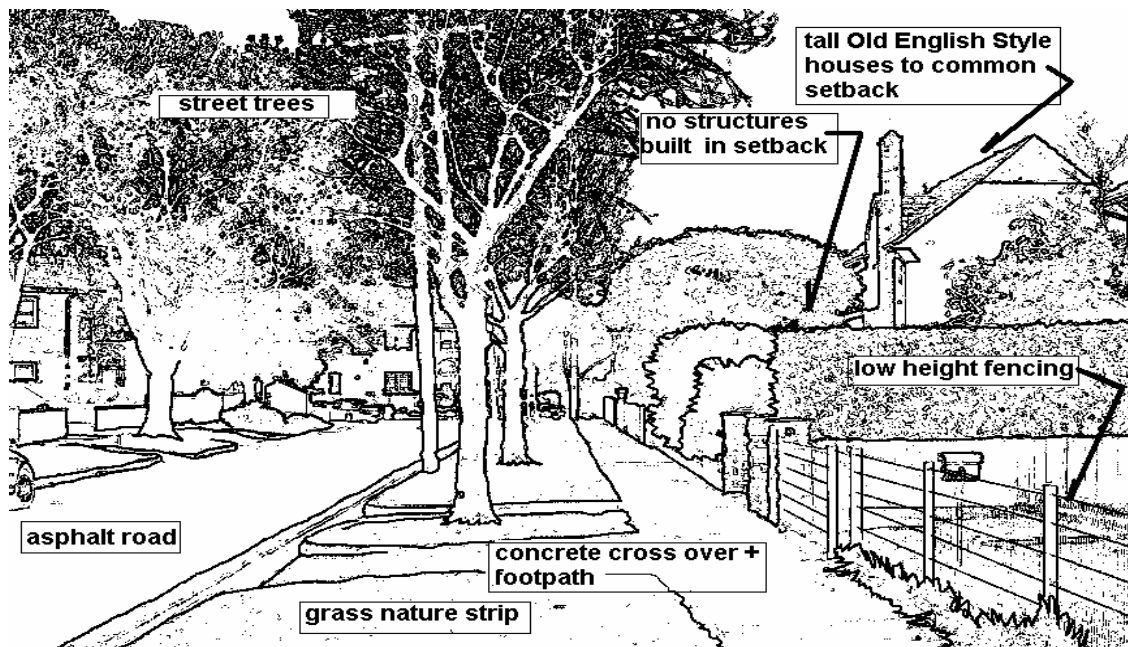


Figure 6: Essential features of the Estate

Individually, the buildings are significant in the way in which they each specifically contribute to the overall character of the Dunstan Estate. This is principally achieved by the recognisable variations in dwelling styles, both obvious and subtle. An example is the bricks used on one type of dwelling which vary from cream to clinker. The specific character of each dwelling therefore needs to be recognised and respected.

Each and every dwelling on the Estate contributes to the heritage significance and character of the Estate. Particular architectural features of note are:

- ❑ The scale of the Estate – the buildings, while being small dwellings, combine as pairs to appear more substantial than they are. The design of the single bedroom pairs with a large common gable roof belies the smallness of the individual dwellings.
- ❑ The differentiation between the types and their recognizable repetitions.

- ❑ The exposure of one side of pairs of dwellings on diagonal sites.
- ❑ The design of the dwellings as pairs both asymmetrical and symmetrical. The prominence of one entry and virtual concealment of the other in the asymmetrical types enhances this effect. In the neo-Georgian Type C2, the central porch with the symmetrical façade also reads as a single dwelling.
- ❑ The use of contrasting brick and rendered wall surfaces with occasional half timbering to dormer projections.
- ❑ A strong emphasis on chimneys.
- ❑ Consistent use of terracotta tiled roofs.
- ❑ Low front fences that do not obscure the public view of the dwellings.
- ❑ Concrete footpaths, kerb and channel and crossovers with grassed nature strips and asphalt roads.
- ❑ Wide side access to the rear of properties.
- ❑ Mature trees in private gardens, which enhance the English village atmosphere.
- ❑ Consistent street tree planting of Desert Ash in the nature strips, generally at the line of each side boundary (similar spacing to Garden City).



Figure 7: Desert Ash Tree, used for street plantings in Victoria, South Australia and parts of NSW.

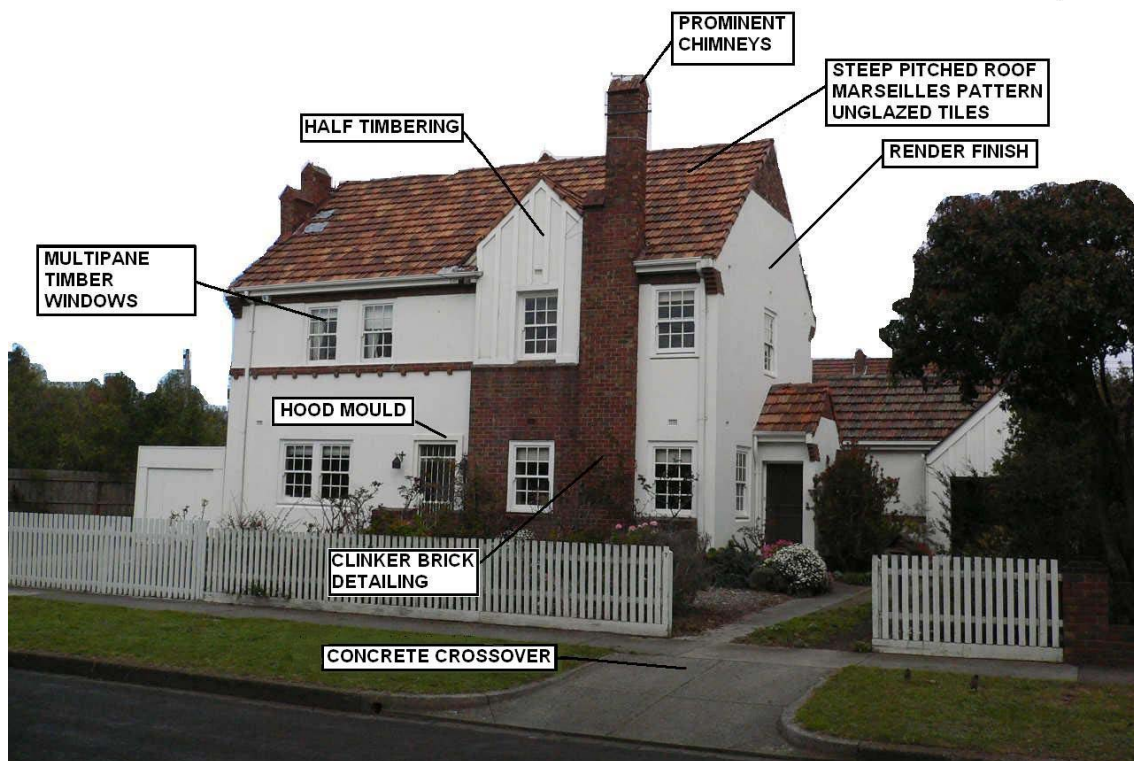


Figure 8: An intact Tudor influenced Old English style dwelling in the Estate, showing some of its important original features.

Interwar Old English style dwellings can be found throughout the suburbs of Melbourne. Those at the Dunstan Estate are a particularly fine collection of mostly two storey dwellings. The Tudor influenced dwellings like the one above have many distinguishing features such as:

- ❑ A high pitched roof accentuating gables and hipped forms with Marseille pattern tiles as the roof covering.
- ❑ Prominent chimneys, chimney backs and often individual chimney pots.
- ❑ Decorative render finishes combined with clinker bricks.
- ❑ Renderwork hood moulds over doors and windows.
- ❑ Tall multi-paned timber framed windows, sometimes using leaded diamond glazing.
- ❑ Half timbering, a feature that makes the style distinctive (half timbering is a combination of exposed timber framing elements with panels of rendering arranged as a decorative feature).
- ❑ Side porches.

5.0 Current Condition of the Dunstan Estate (2006)

The layout and architectural form of the Estate remains largely intact, although a number of planning permits have been issued for alterations and additions since 2000. The highly varied forms and complexity of the original buildings tend to reduce the impact of visible additions, most of which are sympathetic in form and detail and not immediately recognizable when viewed from the street. The wide side setbacks generally allow vehicles to be located towards the rear of the property and there is only one example of a car shelter intruding into the character of the Estate.

There are very few alterations to the fronts of buildings, except for changes to windows and the occasional use of roller shutters over windows. Fence designs tend to be varied. Apart from some properties fronting Williamstown Road, most fences are low and unobtrusive. A number of remnant woven wire fences can be seen at the sides of properties fronting Graham Street. There are only a small number of high impervious fences.

The condition of the dwellings and gardens is generally good. The condition of street trees is varied and some appear to have been removed along Graham Street and Williamstown Road.

Generally, the Estate has matured well and as yet there are no serious alterations to its character and heritage significance. There is an excellent opportunity for the continuing maintenance and further enhancement of its heritage values and the preservation of the Estate as an attractive place to live.

6.0 Application of the Guidelines

The Estate was once fully owned by the Office of Housing and maintenance works were organised through the Public Works Department. Today, most of the dwellings are privately owned and there is a need to set out standards for works and alterations so that changes do not affect the character of the individual dwellings or their contribution to the streetscape. The preservation of the streetscape character of the Estate and the visible individual forms of dwellings is essential for the retention of the Estate's architectural heritage significance.

In contrast with other estates developed for low-income occupants, the Dunstan Estate offers much more flexibility for development without diminishing its essential character. The guidelines therefore have an impact where, for example, an owner may seek to individualise one of a symmetrical pair of dwellings.

In the period since the dwellings were constructed there has been a substantially changed perception of accommodation needs and this has the potential to impact on the character of the Estate. The guidelines seek to direct change in order to support the objective of maintaining the Estate's special heritage values and character.

The Estate is included in a Heritage Overlay (HO2) in the Port Phillip Planning Scheme. As a consequence, a planning permit is required to:

- ❑ Subdivide or consolidate land.
- ❑ Construct a building.
- ❑ Construct, or carry out works (including fences).
- ❑ Demolish, remove or make external alterations to a building or works.

Whilst the Heritage Overlay offers general protection for the character of heritage places, it does not provide specific guidance on conservation management in individual areas such as the Dunstan Estate. These guidelines are prepared in order to define the important elements in the Estate, clarify development issues, and provide advice on conserving existing buildings and managing development in the area.

The guidelines should be used as the first step in the consideration of any development or works that may require a Planning Permit. It should be noted that works which are discouraged by the guidelines are not necessarily prohibited while works which are encouraged are not mandatory, however applications which comply with the guidelines have a greater chance of obtaining a permit.

The guidelines will be considered in the assessment of all buildings and works within the Estate that require a planning permit. Council planning staff should therefore be consulted early in the process to clarify how the guidelines will be applied to the assessment of individual applications.

The guidelines have been prepared by *heritage ALLIANCE* – Conservation Architects and Heritage Consultants, based on draft guidelines prepared by Trevor Westmore (former Heritage Adviser for the City of Port Phillip) in 2000.



PART TWO: THE GUIDELINES

7.0 Objectives of the Guidelines:

These objectives relate specifically to the Dunstan Estate;

- ❑ To protect the unique cultural significance and architectural character of the Dunstan Estate.
- ❑ To inform the community about the special qualities and significance of the Estate and how that significance is understood through the built fabric and layout of the Estate
- ❑ To recognize the changing needs of people living in the Estate and the potential to make alterations and additions to dwellings which do not affect the character of the area.
- ❑ To provide a clear and consistent basis for the assessment of planning permit applications, having regard to the heritage significance and important features of the Dunstan Estate.

8.0 Works in Public Areas

The Estate, when laid out, was to a common standard in terms of the detailing of roads (asphalt with bluestone edge and concrete gutters), footpaths (plain concrete pavements), pedestrian easements and the like. It is important to maintain that standard and to intrude on the original arrangement as little as possible.

Council is the majority owner of land in the municipality and has an equal responsibility that when carrying out infrastructure works, it plays a positive role in conserving, enhancing and maintaining the heritage character of the area.

Guidelines

- ❑ Works within the public areas of the Estate, including roads and the public reserves, should be consistent with the forms and materials of the original works with the minimum modification required to the original layout in order to meet current requirements and standards. This applies to roads and road surfaces, kerbs and channels, footpaths, crossovers and nature strips.
- ❑ Road surfaces should be maintained as asphalt with bluestone edging, with concrete gutters and kerbing.
- ❑ Any new works that are not part of the original (including repairs) should match the form and materials of the original parts.
- ❑ Road narrowing should be avoided as it impacts on the planned layout of the Estate.
- ❑ Tree planting conforming to the original layout should be enhanced (maximum of one *Fraxinus oxycarpa*, Desert Ash planted in the nature strip in front of each dwelling).

10.0 Additions to Single Storey Dwellings

Guidelines

Single storey additions to single and two-storey dwellings should be designed to preserve the original major elements of the visible facades of the dwelling. Contemporary additions will be considered where they are sympathetic to the Dunstan Estate and are recessive in their form.

Additions will generally be supported where:

- ❑ They are wholly behind the adjacent front wall of the dwelling by a minimum distance of 600mm or;
- ❑ They are set back behind the original dwelling entry porch or canopy, if it is located at the side of the building.
- ❑ Roof forms, pitches and materials are the same as the main structure.
- ❑ The original chimneys are retained.
- ❑ Window openings, if visible from the street, are of similar proportions to those of the existing dwelling (i.e. if windows are of a vertical arrangement, then new windows should follow that pattern where visible from the street).
- ❑ The original main gable remains dominant and unaltered. A new gable or hip roof to a single storey dwelling may be extended to the side but the height of the new roof should be significantly lower.
- ❑ Use of the roof space to create an additional level will be acceptable provided that the roof is not altered and no new windows, dormers or extensions to the roof are visible from the public domain.

Two storey additions to single storey dwellings will generally be supported where:

- ❑ The extensions are to the rear of the property and are designed to be difficult to see from the public domain.
- ❑ Any visible portions are minor in nature and are views of walling or roofing.
- ❑ Materials used are substantially similar to the existing dwelling.
- ❑ The overall footprint of the extension is less than the existing building.
- ❑ The development is contained within a 10 degree development line which extends from the front gutter of the single storey dwelling as per Figure 10 below.

Contemporary additions will be considered where they are sympathetic to the Dunstan Estate and recessive in their form.

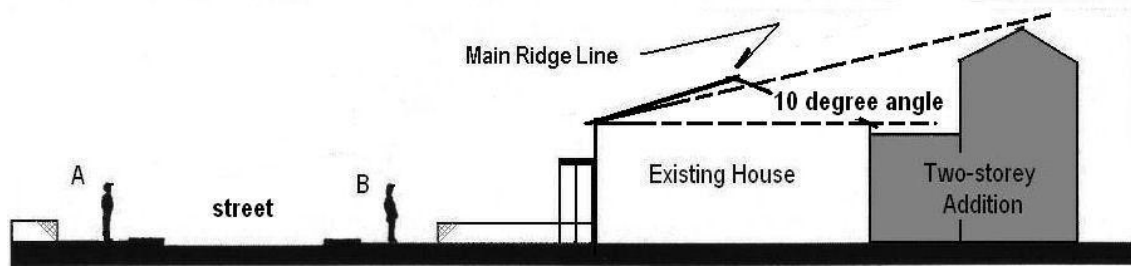


Figure 10: Side view of a two-storey addition to a single storey dwelling may be partially visible in distant views (eg to person A) but should be contained within a 10° building envelope established from the front of the dwelling and using the gutter line as the base line. The addition should be almost wholly invisible to person B.

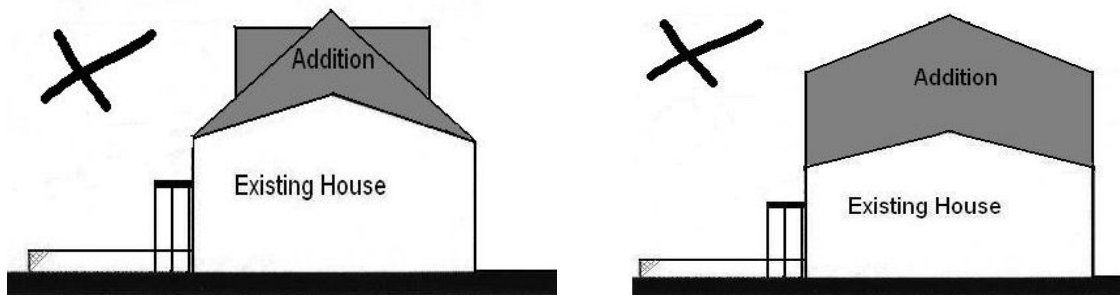


Figure 11: Side view of an unacceptable two-storey addition to a single storey dwelling. These forms change and dominate the roof of the original dwellings.

11.0 Two Storey Additions to Single and Two Storey Dwellings

Guidelines

Two storey additions to single and two storey dwellings may be permitted if:

- ❑ They are sited to the rear of the existing dwelling.
- ❑ They are set back behind the major original roof form. In some cases a building may be less exposed to the public view. In this case a two-storey addition may encroach to half the depth of the original dwelling (refer to Figure 12).
- ❑ The roof is of noticeably lesser height than that of the original dwelling.
- ❑ The walls of the addition do not exceed the height of those of the existing dwelling.
- ❑ Roof forms, pitches and materials are the same as the main structure.
- ❑ The original chimneys are retained and unaltered.
- ❑ Window openings visible from the street are of similar proportions to those of the existing dwelling.
- ❑ Any new dormers in the roof space are sited to the rear (refer to Figure 15).
- ❑ Contemporary additions will be considered where they are sympathetic to the Dunstan Estate and are recessive in their form.

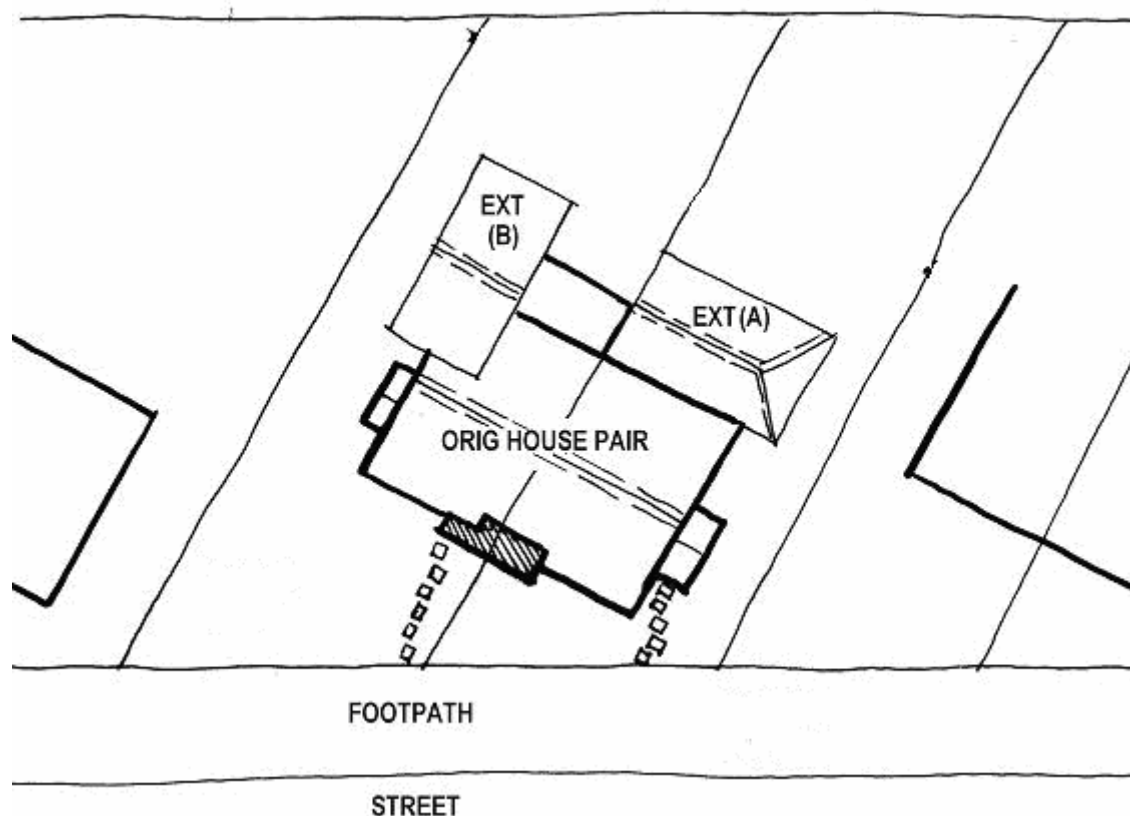


Figure 12: Plan view of extensions at the rear of diagonally placed dwelling pairs.

A two storey extension on a diagonally sited dwelling pair may be behind the main roof form of the dwelling (Ext A) or may encroach to half the depth of the original dwelling (Ext B).

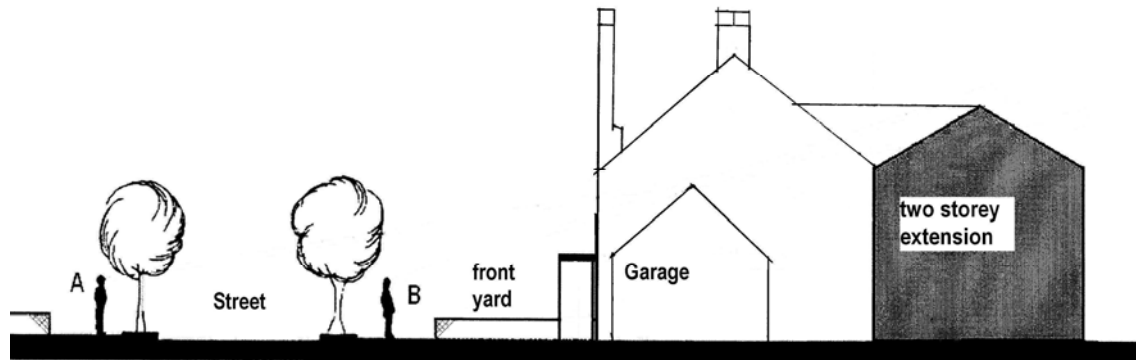


Figure 13: Side view of an acceptable two-storey addition to a two storey dwelling with the roof set lower than the main roof. If the new roof is visible it should be finished in Marseilles terracotta tiles to match the original roof.

12.0 Parking, Garages and Carports

The layout of the Estate has allowed for the parking of motor vehicles off the two main streets at some, but not all, of the dwellings. There is a laneway running parallel with Clark Street from which some off street parking can be introduced. No original garages appear to have been built in the Estate.

New garages and carports fall under the same policies as single storey additions. New built structures should not be built in the front yards of these dwellings.

Guidelines

Parking of cars, and construction of garages and carports should address the following guidelines:

- ❑ New driveways to uncovered parking, carports or garages may be permitted provided that the proposed crossover does not coincide with the location of street trees and a suitable distance is provided between the edge of the crossover and the street tree (in accordance with Council requirements).
- ❑ New driveways and crossovers should be constructed in uncoloured concrete and tracks should be used wherever possible to minimise their impact.

Garages and carports may be permitted if:

- ❑ They are sited behind the adjacent front wall of the dwelling by a minimum of 600mm.
- ❑ They are set back behind the original dwelling entry porch or canopy if this original feature is located at the side.
- ❑ Carports (an open structure) are designed to reflect a pergola type construction, or are built in similar materials and forms to that of the dwelling. Carports including their roof eaves should not be built forward of the front wall of the dwelling.
- ❑ Garage roofs at the frontage reflect the roof pitch of the main dwelling and are finished with terracotta Marseilles pattern tile. Garage doors should not be roller shutters but may be tilt up doors with vertical timber or metal cladding.

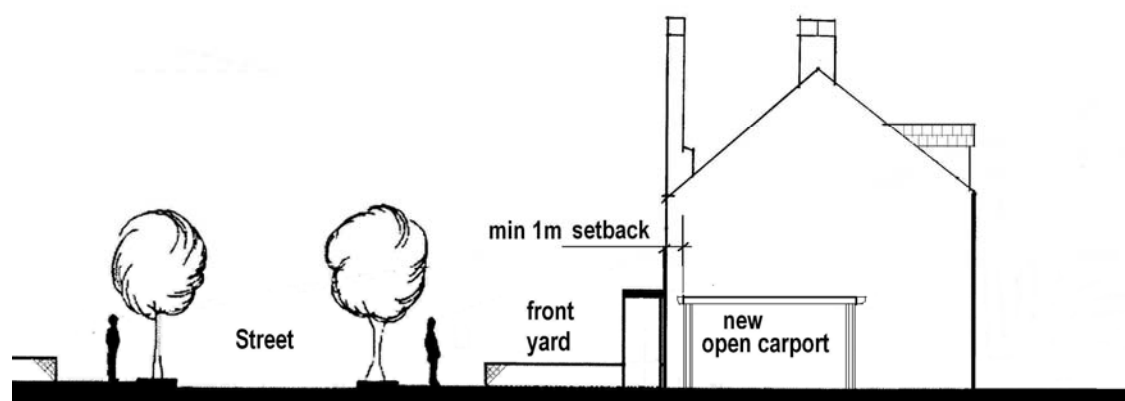


Figure 14: Side view – a new open carport must be set back at least 1 metre from the front façade of the dwelling.

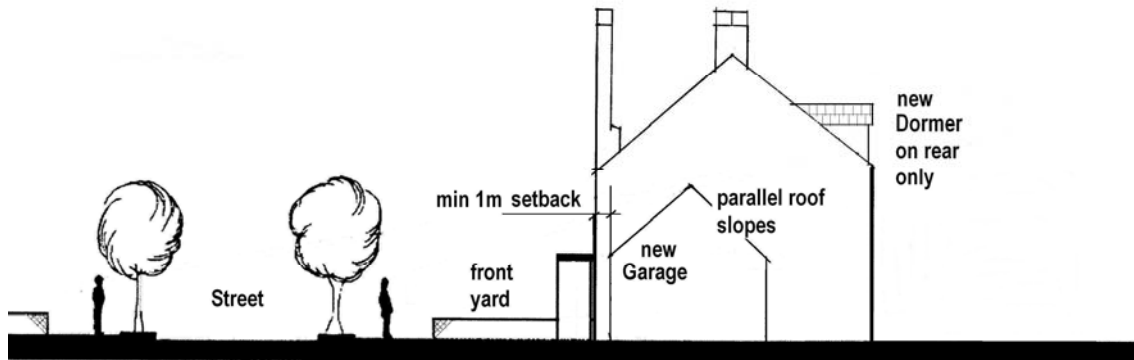


Figure 15: Side view – a new garage must be set back at least 600mm from the front façade of a dwelling. Roofs must be parallel with the slope of the existing roof and use the same roofing material. Any new dormers in roof spaces must be at the rear only.

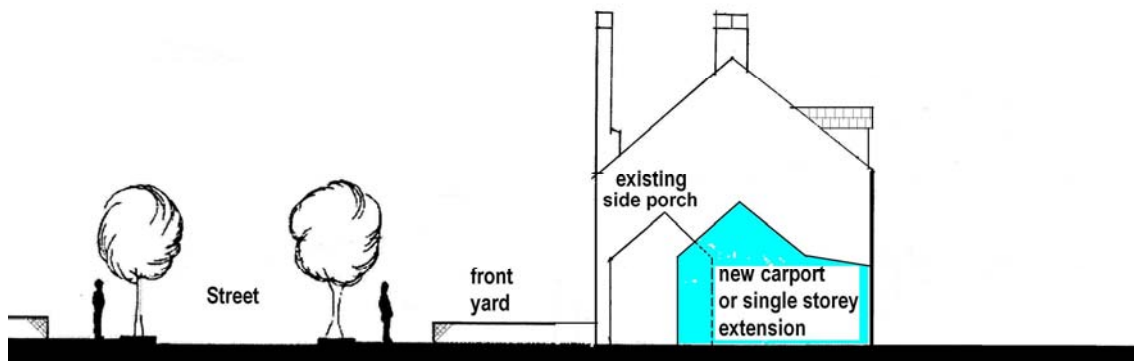


Figure 16: Side view – a new addition used as a carport, garage or dwelling space must be set behind an existing side porch. Roofs must be parallel with the slope of the existing roof or porch and use the same roofing material.

13.0 Fences

There is little evidence of the Estate's original fences and it may be that very few were constructed. The earliest identifiable type is a cyclone wire and pipe rail fence. It is known that in other housing estates very simple horizontal plank fences were constructed as an interim measure.

Many of the existing fences appear to have been constructed in the 1950's in brick as well as brick and wrought metal work. More recently paling fences of various designs have been introduced along with high brick walls.

Most replacement fences now found in the Estate are relatively low and use various combinations of brick and wrought iron. To maintain the character of the Estate, front fences should remain low and the materials should be in character with the materials of the Estate. Inter-war styles are highly appropriate.

Guidelines

New fences may be permitted if:

- ❑ At the front of the property (including the side fences in the dwelling frontage), they are no higher than 1.2 metres to the top of the fence panels for transparent/diamond wire fences and 1 metre to the top of the fence for brick fences. Posts may be higher.
- ❑ The style of fence is reasonably appropriate to the period of the Estate, i.e. Interwar styles using masonry or wire between steel tube posts or timber posts with steel tube rails. Decorative woven wire and diamond woven wire are preferred.
- ❑ Apart from posts or piers, fences have a straight top and are not scalloped.
- ❑ Side and rear fences behind the front wall of the dwelling and from the side boundary to the building do not exceed 2 metres in height and are preferably of palings.

Front timber fences, including pickets, are generally inappropriate. Side and rear fences may be timber. Some acceptable examples are shown in the illustrations overleaf.

It should be noted that fences and walls of any reasonable height are ineffective in providing useful acoustic protection on major traffic routes – double-glazing the windows facing the road will be more effective.

Residents seeking greater privacy should use landscaping rather than high solid fences or walls.



Figure 17: An early metal fence – Cyclone steel tube type (acceptable)



Figure 18: New woven wire fence on timber framing set to a low height (acceptable)



Figure 19: Timber fence of simple board pickets with a top rail (acceptable)

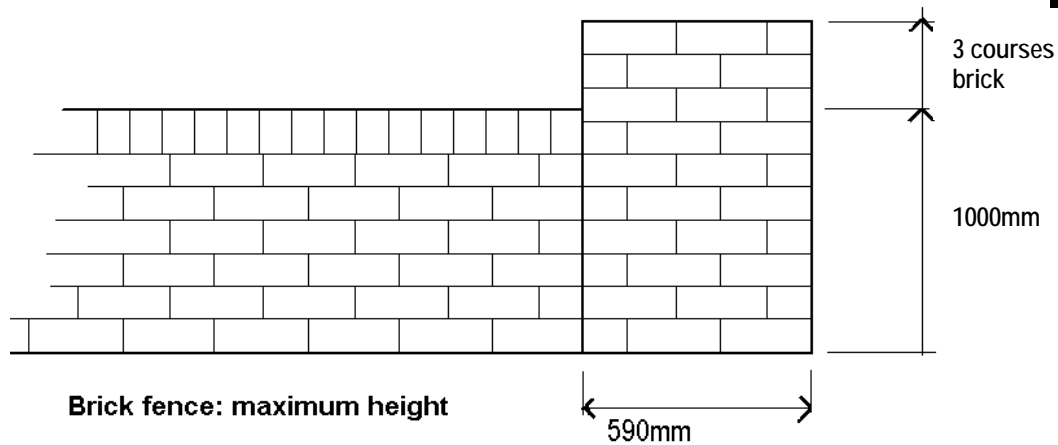


Figure 20: Simple brick front fence with bricks of similar colour and size to the clinkers of dwellings. Maximum height 1000mm.

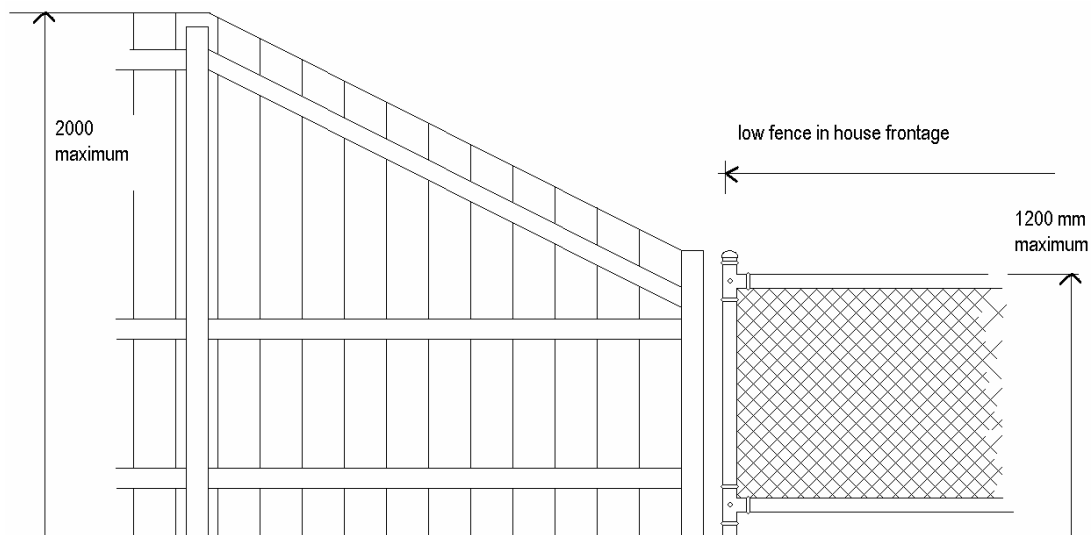


Figure 21: Mid fence between two dwellings with the solid timber fence starting behind the façade of the dwelling and the lower transparent fence in the front yard

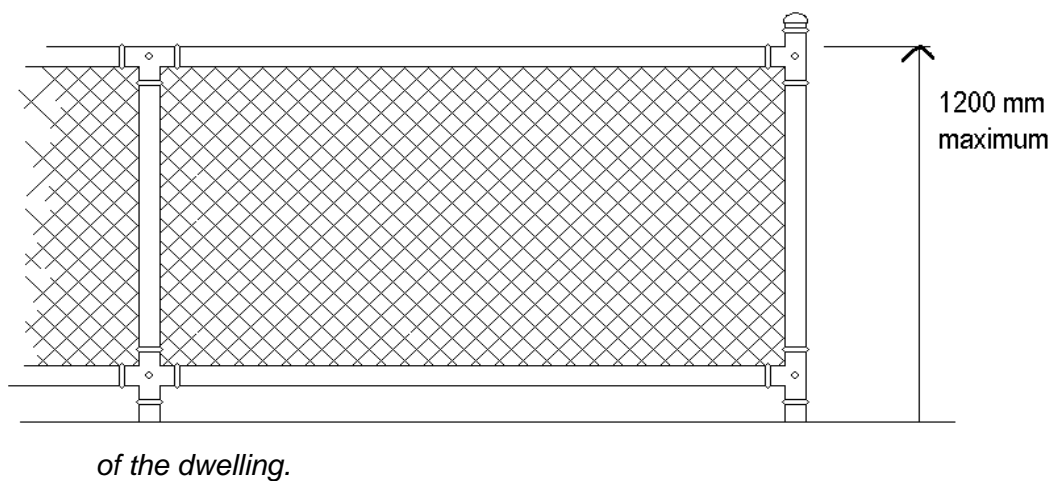


Figure 22: Front fence in diamond wire. Maximum height 1200mm.

14.0 The Dwellings: Construction Details and Materials

The original construction details and materials play an important supporting role in establishing the historic character of the Estate and it is important that the pattern of varied materials is not altered. The use of similar materials and details in the visible parts of new work and in restoration of the original features of dwellings is therefore encouraged.

The Estate uses materials common across all the dwellings. It is the placement of those materials which make the dwellings different from each other but maintains the visual cohesiveness of the Estate.

The restoration of missing original materials and details is encouraged. The use of compatible materials and detail is also encouraged on new additions which are partially visible in the public domain.

Guidelines

14.1 *Roofs*

Roofs of all dwellings are of Marseilles pattern terracotta tiles of a subtle colour mix. The same materials and colour mix should be used for repairs and for additions. Dark colour tiles, glazed tiles, concrete tiles and tiles not in a Marseilles pattern are inappropriate and are not encouraged.

14.2 *Chimneys*

Chimneys stacks above the roofs are an important part of the design of many of the dwellings and form important elements of their elevations. All externally expressed chimney features must be retained.

14.3 *Roof Gutters, Verges and Downpipes*

The roof verge details of dwellings vary. The existing pattern on the original building should be employed in new works. Bullnosed gutters are the standard for the Estate and other types should be avoided. Downpipes should be circular in section.

Plastic, aluminium and Colorbond squared gutters are inappropriate and are not encouraged.

14.4 *Walls*

Wall treatments vary between the dwelling types – bricks vary in colour from cream to over-burnt (clinker) while Type B1 is wholly rendered. Some dwellings combine both rendered and brick wall surfaces. These distinctions must be retained in all alterations and additions to dwellings.

- ❑ The pattern of wall treatments used in the original dwellings should be applied in any new works which create habitable space. This will mean that similar bricks (often second hand) may need to be purchased.
- ❑ The walls of new work should generally match those of the existing dwelling, although slight variations in brickwork will be inevitable.

14.5 Porches and Entries

The original porches and entries to the dwellings also vary considerably in form, from the coupled central porches of the neo-Georgian pairs (Type C3) to the basic side entry doors with a small canopy on the English cottage type (Type B1) and others.

The patterns of porches and entries are critical to the design of the pairs.

- ❑ The original porches should be retained in all cases.
- ❑ Modest alterations and additional elements such as extensions of canopies may be acceptable, but only to unprotected doors that are more or less concealed at the sides of dwellings.
- ❑ The use of transparent screen doors at porch entries is acceptable, however glazed or solid doors are inappropriate and are not encouraged. Screen doors should be of a dark colour.

14.6 Windows and Doors

Many, but not all, of the original timber double-hung multi-pane windows of the Estate have been replaced. While this does not significantly reduce the heritage values of the Estate, it does diminish its character. Retention of the existing multi-pane windows and reinstatement of those windows which are missing (where they are visible in the public domain) is encouraged.

- ❑ Where non-original windows are to be replaced, the preferred option is that they revert to their original form and detail. This is important if the windows are visible in the public domain.
- ❑ Alteration to the size of window openings in the front facades of dwellings will not be acceptable. New or enlarged windows and bay windows are not acceptable at the front of the dwelling.
- ❑ Alteration to windows in the side facades of dwellings may be permitted provided that the proportions of the new windows relate to existing windows.
- ❑ Front doors are generally not exposed. Retention of original doors is encouraged, but if the door is not visible from the street, it may vary in form.
- ❑ The conversion of windows to doors is discouraged on the front and side elevations.
- ❑ Fly or security screens, if added, should be of a simple pattern, preferably painted in a colour matching the window frames.

Where noise attenuation is an issue, double-glazing is encouraged. This does not require a planning permit if the pattern of the existing window glazing is not altered, and the double-glazing of windows (which may be in the form of inward opening casements) occurs on the inside of the existing windows.

14.7 External Finishes

The general character of the Estate relies on the use of contrasting materials. Some dwelling types have uniform wall finishes of brick or cement render, but it is the types with varied surfaces that lift the visual character of the Estate.

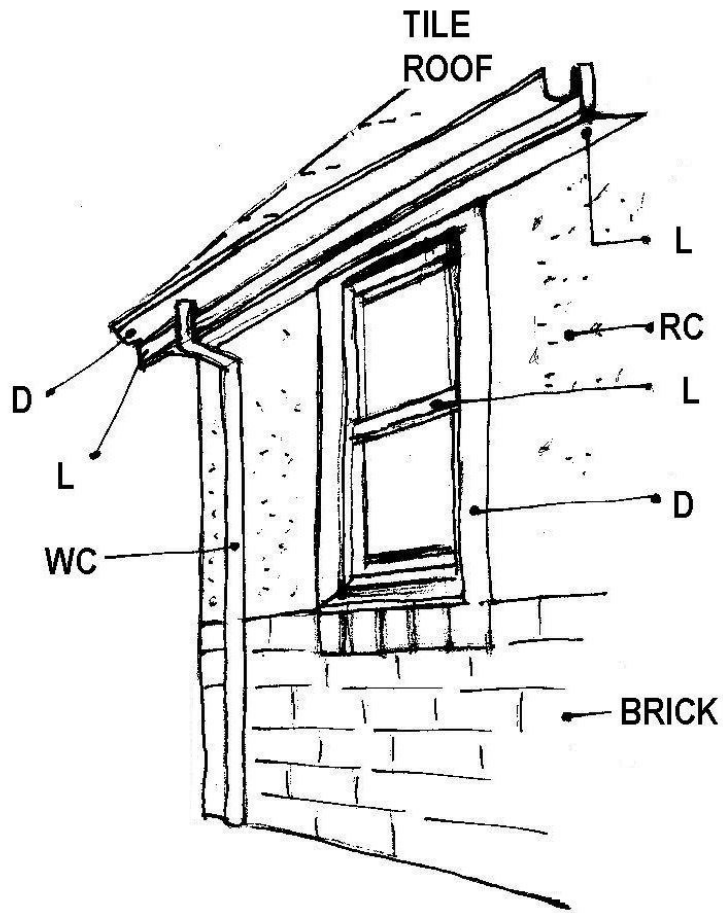
Given that the dwelling pairs were designed to appear as one building, it is important that adjacent owners do not attempt to individualise their side as this may result in an artificial differentiation between parts of the pair. Owners should attempt to achieve a common solution for the treatment of pairs, particularly in the use of finishing schemes. The following guidelines should be observed:

- ❑ In no circumstance should unpainted brickwork be painted. It is recommended that any previously painted brickwork have the paint removed (by high pressure water methods or chemical methods, not sandblasting).
- ❑ Rendered surfaces should ideally be repainted in pale grey or stone colours, using a matt or slightly textured acrylic paint.
- ❑ Neighbours in pairs are encouraged to agree on the same or similar colour schemes.

14.8 Colours

The following general approach should be used as a guide for colour selection.

1. **Roof Gutters** Should be painted in a dark colour. Deep reds or deep greens or red-browns would be most appropriate (refer to area 'D' in Fig. 23).
2. **Fascias and Eaves** Should be painted in a colour ranging from cream to light stone (refer to area 'L' in Fig. 23).
3. **Rendered Surfaces** Should be painted in characteristic render colour and should not be painted if in good condition (refer to area 'RC' in Fig. 23).
4. **Brickwork** Brickwork should never be painted. If brick walls have already been painted, the removal of paint by high pressure water or chemical means is recommended. Brickwork should never be sandblasted.
5. **Half Timbering** Half timbering was probably originally stained. The use of opaque timber coats to replicate natural stained timber is recommended. The timber should contrast with the rendered background.
6. **Windows** The original dwellings probably used two colours on windows. Use of the dark trim colour as for roof gutters is recommended for window frames. The moving parts (i.e. the sashes) should be painted in a contrasting light colour, as should fascias and eaves (refer to areas 'D' and 'L' in Fig. 23).
7. **Exposed Doors** Should be painted in the dark trim colour. Doors under porches should be painted a light colour.
8. **Screen doors** Should be painted in a dark colour.
9. **Downpipes** Should be painted to match the render colour on the wall (refer to area 'WC' in Fig. 23).



D – dark trim colours for gutters and outer window frames

L – lighter colours for barges, eaves and inner window frames

WC – render colour (wall) for downpipes

RC – render colour (wall)

BRICK – unpainted

Figure 23: Application of colours to dwellings in the Estate.

15.0 External Fittings

External elements should be located so they are substantially invisible in the public domain. This will mean that most new fittings will either be wholly to the rear of the property or in the case of pipes, cables and security screens, the works will be internal.

Guidelines

The following guidelines should be addressed in any proposal to add external elements:

15.1 *Solar Hot Water and Solar Cells*

Solar panels should either be wholly hidden on rear roof slopes or be on subsidiary roofs where they are not apparent in the streetscape.

They should not be on the front roof slope of the dwelling

15.2 *Satellite Dishes*

Satellite dishes should be on rear roof elements. Satellite dishes must not be on the front roof or chimneys or attached anywhere that makes them apparent in the streetscape.

15.3 *External Cabling, Ducting and Pipes*

External ducting, pipes and cabling should not be on the portions of the dwelling visible from the street. This applies to cabling for television, service pipes for water and sewerage, and air handling ducts. Such cables and pipes may be placed externally on the rear of the dwelling.

15.4 *Pergolas and Vergolas*

Pergolas and Vergolas® should not be attached to the front façade of the dwelling or be constructed in the front setback between the fence and the dwelling frontage.

Depending on the degree of visibility and size, they may be permitted to the rear or sides of the dwelling.

15.5 *Sunblinds*

Sunblinds and awnings (temporary and permanent) should compliment the character of the dwelling and should be designed to be as unobtrusive as possible.

External sunblinds can be simple pull-out striped canvas shades.

15.6 *Screens and Security Grilles*

Electric metal shutters should not be installed on the side or front windows of the dwelling. They may be installed on rear windows where not seen from the street.

Timber shutters may be permitted on windows provided they serve a functional purpose, i.e. the shutters should be fully operable. False shutters which clearly could not cover the window and serve no functional purpose are not acceptable. It should be noted that the neo-Georgian dwelling-pairs were originally intended to have timber casement window shutters on the upper floor and original shutter clips remain on some of these designs.

Security grilles should be installed inside windows that can be seen from the street. Grilles may be installed externally where the window is obscured in the view from the street.

15.7 Air Conditioners and Air Package Units, ducted heating furnaces

Air conditioners, air package units and heater furnaces should not be visible from the street. Air package units may be located on rear roof slopes and below the ridge lines.

Air conditioners, fan units and condensers and furnaces may be located:

- ☐ At the rear of the dwelling.
- ☐ At the side of the dwelling, as long as they are more than halfway back from the front of the dwelling. Fan units and furnaces must not be at the dwelling frontage.

16.0 Landscaping

A planning permit is only required for landscaping purposes where a tree is subject to specific controls, such as a Heritage Overlay, or where buildings and works are required, such as the construction of a pergola. The guidelines for Landscaping are included for those that wish to plant in the traditional manner.

The Estate was established with very simple features, primarily lawn and simple uncoloured concrete paths. Planting out (apart from street trees) was essentially a task of the owner. The gardens which were established reflected the simple tastes and sparsity of the interwar period.

Interwar period gardens are encouraged, as are front gardens that do not include built features such as walls and structures.

Guidelines

16.1 Gardens and Structures

A planning permit is not required for gardening. Very few gardens suffer from neglect or poor quality within the Estate. Residents are encouraged to maintain established gardens and trees to complement their homes. The existing well-maintained gardens provide a reasonable standard.

Garden structures such as pergolas do require a planning permit and can be integrated into garden designs, but they should be unobtrusive and should not screen the dwelling. In no case should they be located in front of the dwelling, integrate with the front fence or entry, or be attached to the dwelling.

16.2 Paths

Pathways should be maintained as simple uncoloured concrete tracks for cars.

Paths may also use Castlemaine slate finishes, Crazy paving or paving squares in concrete or Castlemaine slate.

Coloured concrete or bluestone sets should not be used where visible in the public domain.



Figure 24: Driveway and pedestrian pathways as plain uncoloured concrete tracks, causing minimal use of hard pavings.

17.0 Demolition

The Estate is highly intact with no loss of original buildings – while some parts of buildings may have been removed, the dwellings have been substantially retained. The Estate therefore has a high level of integrity and demolition of whole buildings or visible elements of dwellings will severely diminish the significance of the area as a whole.

The Port Phillip Heritage Policy in the Planning Scheme applies to all Heritage Overlay areas in Port Phillip, including the Dunstan Estate (Heritage Overlay area HO2). The Port Phillip Heritage Policy contains heritage objectives and policies in relation to demolition, and applications for demolition of buildings within the Estate must address the relevant provisions of this policy.

Guidelines

- ❑ Demolition of an entire dwelling will generally not be permitted. The rear section of some dwellings may be able to be demolished without affecting the heritage significance of the building, subject to any new addition being sympathetic to the scale and form of the original building and respecting the character of the area.
- ❑ Any decision regarding demolition will be assessed against the provisions of the Port Phillip Planning Scheme, particularly Clause 22.04 Heritage Policy.

18.0 Subdivision, Consolidation and Multi-Unit Development

The repeating rhythm of pairs of attached dwellings is critical to the character of the Estate and additional dwellings which disrupt this rhythm are discouraged.

Additional dwellings will only be considered where the original dwelling is retained and where the new dwelling is at the rear of the site and effectively out of view from the street. Given the angled exposure of most of the properties within the Estate, almost all of the sites are not suitable for multi-unit developments.

Guidelines

18.1 Subdivision and Multi-Unit Development:

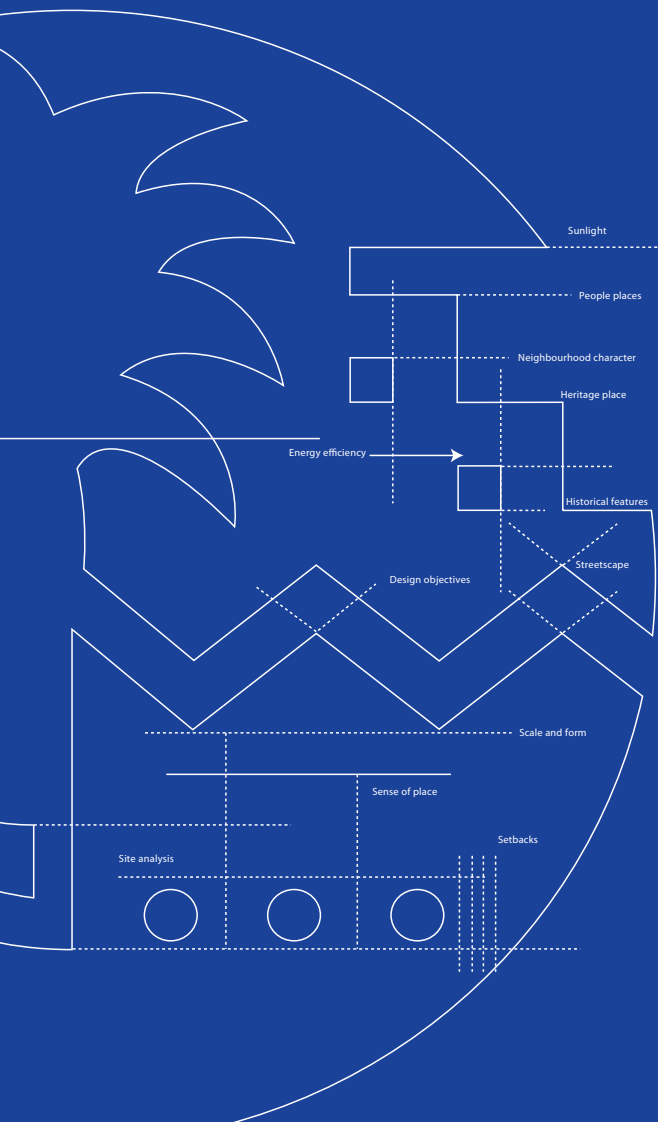
- ❑ Subdivision and multi-unit development is not encouraged within the boundaries of the Estate. Dual occupancy may be permitted if the criteria of these guidelines are wholly satisfied.
- ❑ “Granny flats”, or dependant persons units, may be permitted provided they do not involve subdivision and conform to the guidelines for alterations and additions.

18.2 Consolidation

Consolidation of individual sites into larger lots in order to achieve practicable multi-unit development at the rear of existing dwellings may be appealing, however this will threaten the significant rhythm of the subdivision pattern and site layouts.

- ❑ Consolidation of individual sites into larger areas for multi-unit development or any other purpose is not encouraged.
- ❑ Joining of two dwellings into one may be permitted provided that the criteria of these guidelines are observed.

Port Phillip Design Manual



Appendix 1

Neighbourhood Character Statements - Established Residential Precincts

Residential Precinct R1

Character Type: Main Road Diverse Character



Key Existing Characteristics

- A range of dwelling styles from different eras are present. There are some limited examples of Victorian dwellings that are generally in an average condition, as well as Edwardian, Interwar, 1960s-70s flats and contemporary dwellings and blocks of flats.
- Smaller single storey dwellings are often dominated by the 2-3 storey blocks of flats present in this precinct.
- These two sections of Inkerman Street and Chapel Street contain varied lot sizes, ranging from medium to large. Front and side setbacks are varied.
- Gardens are generally low maintenance. Where they exist, lawns, garden beds, small trees and shrubs help to soften the built form.
- Both roads are fairly heavily trafficked and some properties have high front fences.

Preferred Character Statement

Replacement of buildings in poor condition will occur in the form of new contemporary development that is of a high standard of design. New single dwellings and medium density housing will sit alongside restored and renovated original dwellings. While the building scale of the precinct will be varied, new development will provide a transitional scale to adjoining buildings. Front boundaries will continue to be either open or lined with fences that allow views through or over the fence into the front garden. Garden areas will be landscaped with vegetation suited to small or limited landscaping areas.

Residential Precinct R1

Design Guidelines

Character Element	Objective	Design Response	Avoid
Existing Buildings	To encourage the replacement, or adaptive reuse/renovation of buildings in poor condition with medium-density, contemporary infill development.	<ul style="list-style-type: none"> > New development should be distinguishable from original dwelling stock through high quality contemporary architecture. 	<i>Reproduction styles and detailing.</i>
Landscaping and Vegetation	To strengthen the garden settings of dwellings and to create outdoor spaces and landscaping areas wherever possible in new development.	<ul style="list-style-type: none"> > Retain established trees and provide for the planting of new trees and vegetation. > In new multi-dwelling developments, consider the use of more innovative landscaping options such as planter boxes, climbers, creepers and the establishment of roof-top gardens. 	<i>Lack of landscaping and vegetation.</i>
Siting	To minimise the loss of front garden space and the dominance of car parking structures and vehicle crossovers.	<ul style="list-style-type: none"> > Locate garages and carports behind the line of the dwelling. > Minimise paving in front garden areas including driveways and crossovers. > In new multi-dwelling developments consider the use of underground car parking or car parking that can be accessed from the rear of the site. > Where possible, provide vehicular access from a rear laneway. 	<p><i>Car parking structures that dominate the façade or view of the dwelling.</i></p> <p><i>Creation of new crossovers and driveways or wide crossovers.</i></p> <p><i>Front setbacks dominated by impervious surfaces.</i></p>
Height and Building Form	To ensure that new development is respectful of existing period buildings.	<ul style="list-style-type: none"> > New multi-storey developments adjoining a lower-scale period dwelling should respect the height of the existing dwelling through the use of design techniques such as transitional scaling (recessing upper levels). 	<i>Buildings that dominate period buildings by height, siting or massing.</i>
Materials and Design Detail	To encourage innovative architecture and cultural expression through design.	<ul style="list-style-type: none"> > New buildings should be individually designed to respond to the characteristics of the site and adjoining buildings. > Cultural expression through colour, building details and architectural reinterpretation should be employed, including the use of contemporary building materials and the modern interpretation of period features. . > Articulate building façades through the well considered composition of openings and variations in wall surfaces. 	<p><i>Large, bulky buildings with unarticulated wall surfaces.</i></p> <p><i>Reproduction heritage styles.</i></p>
Front Boundary Treatment	To increase the openness of streetscapes.	<ul style="list-style-type: none"> > Provide a low or transparent front fence that allows views to the front garden and dwelling. > Applications for new development along heavily trafficked roads should investigate other measures for mitigating noise and privacy impacts, such as vegetation screening and double glazing. 	<i>High, solid front fences.</i>

Residential Precinct R2

Character Type: Victorian / Edwardian / Interwar / Post-war with Modern Overbuilding



Key Existing Characteristics

- Pakington Street is quite wide and is lined with consistent native street trees and 90° or angle parking in some parts. These attributes form a strong feature of the precinct and tend to dominate the character of the built form.
- The precinct contains a range of building styles including Victorian, Edwardian, Interwar and Post-war dwellings with 1960s-70s flats, some of which have been more recently updated.
- Single dwellings are between one and two storeys in height, while blocks of flats range in height up to four storeys.
- Lot size is varied, while front and side setbacks are generally consistent and small.
- Gardens are limited by small front setbacks but are generally established with small shrubs and occasional trees. The front setbacks of some blocks of flats are dominated by car parking areas.
- Consistent Paperbark street trees are present along Pakington Street.
- Fences are of mixed heights and styles. In some cases fences are absent altogether.

Preferred Character Statement

This precinct will accommodate further contemporary redevelopment through the replacement or updating of existing blocks of flats and replacement of existing dwellings in poor condition. New development that is located next to a period building will respect the scale of the existing dwelling. Medium density housing will be surrounded by landscaping that combines with street trees to add to a leafier character. New buildings will be articulated with architectural features such as balconies and variations in wall surfaces. Where possible, on-site car parking will be sited beneath or to the rear of buildings to reduce its dominance and allow for visible landscaping space.

Residential Precinct R2

Design Guidelines

Character Element	Objective	Design Response	Avoid
Existing Buildings	To encourage retention of Victorian, Edwardian and Interwar dwellings in good condition, and replacement of buildings in poor condition with contemporary infill development.	<ul style="list-style-type: none"> > Retain Victorian, Edwardian and Interwar dwellings that contribute to the valued character of the area. > Alterations and extensions to Victorian, Edwardian and Interwar buildings should be appropriate to the building era and style. > New development should be distinguishable from original dwelling stock through high quality contemporary architecture. 	<p><i>Demolition of visible parts of intact Victorian, Edwardian and Interwar dwellings.</i></p> <p><i>Reproduction styles and detailing.</i></p>
Landscaping and Vegetation	To strengthen the garden settings of dwellings and to create outdoor spaces and landscaping areas wherever possible in new development.	<ul style="list-style-type: none"> > Retain established trees and provide for the planting of new canopy trees and vegetation. > 	<i>Lack of landscaping and vegetation.</i>
Siting	To provide space for front gardens.	<ul style="list-style-type: none"> > Buildings should be set back from the front boundary to accommodate a front garden. 	<i>Loss of front garden space.</i>
	To minimise the loss of front garden space and the dominance of car parking structures and vehicle crossovers.	<ul style="list-style-type: none"> > Locate garages and carports behind the line of the dwelling. > Minimise paving in front garden areas including driveways and crossovers. > In new multi-dwelling developments consider the use of underground car parking or car parking that can be accessed from the rear of the site. > Where possible, provide vehicular access from a rear laneway. 	<p><i>Car parking structures that dominate the façade or view of the dwelling.</i></p> <p><i>Creation of new crossovers and driveways or wide crossovers.</i></p> <p><i>Front setbacks dominated by impervious surfaces.</i></p>
Height and Building Form	To ensure that the height and scale of new development is respectful of existing period buildings.	<ul style="list-style-type: none"> > New multi-storey developments adjoining a lower-scale period dwelling should respect the height of the existing dwelling through the use of design techniques such as transitional scaling (recessing upper levels). 	<i>Buildings that dominate period buildings by height, siting or massing.</i>
Materials and Design Detail	To encourage innovative architecture and cultural expression through design.	<ul style="list-style-type: none"> > New buildings should be individually designed to respond to the characteristics of the site and adjoining buildings. > Cultural expression through colour, building details and architectural reinterpretation should be employed, including the use of contemporary building materials and the modern interpretation of period features. > Articulate building façades through the well considered composition of openings and variations in wall surfaces. > A restrained mix of materials should be used, especially for small sites. 	<i>On small sites, the use of several differing materials and colours in building facades.</i>
Front Boundary Treatment	To maintain and increase the openness of streetscapes.	<ul style="list-style-type: none"> > Provide a low or transparent front fence that allows views to the front garden and dwelling. > Provide a front fence that is compatible with the era of development. 	<i>High, solid front fences.</i>

Residential Precinct R3

Character Type: Victorian / Interwar / Post-war Garden Suburban



Key Existing Characteristics

- This precinct has a strong base of double fronted Victorian cottages, however significant overbuilding has occurred throughout the Interwar and Post-war eras. There are some limited examples of Edwardian terraces and contemporary buildings.
- Streetscapes generally have a small-scale, single storey atmosphere, however occasional larger buildings between two and three storeys interrupt this pattern.
- Dwellings are often grouped by era with consistent buildings in a row – e.g. Victorian cottages, blocks of flats and occasionally, Edwardian terraces.
- Lot sizes are generally small but some larger blocks occur throughout. Front and side setbacks are also generally small.
- Gardens are limited by small front setbacks, but are generally established with small shrubs and occasional trees.
- Consistent Paperbark street trees are present along Camden and Linton Streets.
- In Linton Street, Paperbark street trees are particularly large and bushy, forming a significant visual element in the streetscape that tends to dominate the built form.
- Front fences are low to medium height and of mixed styles.
- On-street car parking is a dominant feature of the area, with few properties having on-site car parking due to small front setbacks.

Preferred Character Statement

Existing Victorian and Interwar dwellings will be maintained and restored. Post-war development will be updated or replaced with buildings that respect the predominant scale of existing buildings. The consistent character elements of this precinct, such as front and side setbacks and height and building form will be reflected in new development. Gardens spaces will be enhanced with vegetation suited to small areas that combines with street trees to create a leafy character. Front gardens will be lined with low to medium height front fences that are suited to the era of development.

Residential Precinct R3

Design Guidelines

Character Element	Objective	Design Response	Avoid
Existing Buildings	To encourage retention of Victorian and Interwar dwellings in good condition.	<ul style="list-style-type: none"> > Retain Victorian and Interwar dwellings that contribute to the character of the area. > Alterations and extensions to Victorian and Interwar buildings should be appropriate to the building era and style. > New development should be distinguishable from original dwelling stock through the use of contemporary architecture. 	<p><i>Demolition of visible parts of intact Victorian and Interwar dwellings.</i></p> <p><i>Reproduction styles and detailing.</i></p>
Landscaping and Vegetation	To strengthen the landscape qualities of small garden settings and to create outdoor spaces and landscaping areas wherever possible in new development.	<ul style="list-style-type: none"> > Retain established trees and provide for the planting of new trees and vegetation. > In new multi-dwelling developments, consider the use of more innovative vertical landscaping options such as planter boxes, climbers, creepers and the establishment of roof-top gardens. 	<i>Lack of landscaping and vegetation.</i>
Siting	To provide space for front gardens.	<ul style="list-style-type: none"> > Buildings should be set back from the front boundary to accommodate a small front garden. 	<i>Loss of front garden space.</i>
	To minimise the loss of front garden space and the dominance of car parking structures and vehicular crossovers.	<ul style="list-style-type: none"> > Locate garages and carports behind the line of the dwelling. > Minimise paving in front garden areas including driveways and crossovers. > In new multi-dwelling developments consider the use of underground car parking or car parking that can be accessed from the rear of the site. > Where possible, provide vehicular access from a rear laneway. 	<p><i>Car parking structures that dominate the façade or view of the dwelling.</i></p> <p><i>Creation of new crossovers and driveways or wide crossovers.</i></p> <p><i>Front setbacks dominated by impervious surfaces.</i></p>
Height and Building Form	To ensure that the height and scale of new development is respectful of existing period buildings and the streetscape.	<ul style="list-style-type: none"> > Where a new development abuts existing single storey period dwellings on both sides, set back upper levels so that the higher elements are recessive in the streetscape. > Where a new multi-storey development abuts existing buildings of differing heights (e.g. single storey on one side and two or three storey on the other), a graduated transitional scale should be employed in the design of the new development. 	<i>Buildings that dominate period buildings by height, siting or massing.</i>
Materials and Design Detail	To encourage innovative architecture and cultural expression through design.	<ul style="list-style-type: none"> > New buildings should be individually designed to respond to the characteristics of the site and adjoining buildings. > Cultural expression through colour, building details and architectural reinterpretation should be employed, including the use of face brick, stucco render and weatherboard. > Articulate building façades through the well considered composition of openings and variations in wall surfaces. 	<i>Large, bulky buildings with unarticulated wall surfaces.</i>
Front Boundary Treatment	To maintain and increase the openness of streetscapes.	<ul style="list-style-type: none"> > Provide a low front fence that allows views to the front garden and dwelling. > Provide a front fence that is compatible with the era of development. 	<i>High, solid front fences.</i>

Residential Precinct R4

Character Type: Victorian / Edwardian / Interwar / Post-war with Modern Overbuilding



Key Existing Characteristics

- The precinct has a strong interface at the core area of the Carlisle Street Activity Centre, facing onto car parks and the rear of businesses located on Carlisle Street.
- Building style is mixed however the precinct has a strong base of Edwardian dwellings. There are also a very small number of Victorian dwellings towards the west of the precinct.
- Overbuilding has occurred largely in the form of 1960s-70s flats and single dwellings. There are also some examples of Interwar dwellings and, particularly towards the east of the precinct, newer contemporary medium-density dwellings.
- Dwellings are predominantly single storey, while the small number of blocks of flats are two storeys in height.
- Lot sizes are varied. Front and side setbacks are generally small.
- Gardens are limited by small front setbacks and street trees are irregular. Native street trees lining the edge of the car parks south of the precinct help to soften the interface between the residential and commercial area.
- Front fences are of mixed heights and styles.

Preferred Character Statement

This precinct will continue to be comprised of a mix of building styles. Building replacement may occur along Alfred Street in the form of medium density housing, and will provide a transitional scale to the existing dwellings to the north. Any new development along Alfred Street will respond to the design detail of existing Edwardian dwellings, and will provide the perception of a ground floor dwelling every six to eight metres. The interface between the residential and commercial area will appear seamless, with transitional building scales and retention of existing landscaping space. Existing Edwardian and Interwar dwellings in good condition along Camden and Nelson Streets will be retained and enhanced through restoration. Any redevelopment along these streets will reflect the existing scale and built form.

Residential Precinct R4

Design Guidelines

Character Element	Objective	Design Response	Avoid
Existing Buildings	<p>Along Camden and Nelson Streets, to encourage retention of Edwardian and Interwar dwellings in good condition.</p> <p>Along Alfred Street, to encourage the replacement of dwellings in poor condition with medium-density, contemporary infill development.</p>	<ul style="list-style-type: none"> > Retain Edwardian and Interwar dwellings that contribute to the valued character of Camden and Nelson Streets. > Alterations and extensions to Edwardian and Interwar buildings should be appropriate to the building era and style. > New development should be distinguishable from original dwelling stock through the use of contemporary architecture. 	<p><i>Demolition of visible parts of intact Edwardian and Interwar dwellings.</i></p> <p><i>Reproduction styles and detailing.</i></p>
Landscaping and Vegetation	To strengthen the garden settings of dwellings and to create outdoor spaces and landscaping areas wherever possible in new development.	<ul style="list-style-type: none"> > Retain established trees and provide for the planting of new trees and vegetation. > In new multi-dwelling developments, consider the use of more innovative landscaping options such as planter boxes, climbers, creepers and the establishment of roof-top gardens. 	<i>Lack of landscaping and vegetation.</i>
Siting	To minimise the dominance of car parking structures and vehicular crossovers.	<ul style="list-style-type: none"> > Minimise paving in front garden areas including driveways and crossovers. > In new multi-dwelling developments consider the use of underground car parking or car parking that can be accessed from the rear of the site. > Where possible, provide vehicular access from a rear laneway. 	<p><i>Car parking structures that dominate the façade or view of the dwelling.</i></p> <p><i>Creation of new crossovers and driveways or wide crossovers.</i></p> <p><i>Front setbacks dominated by impervious surfaces.</i></p>
	To provide space for front gardens	> Buildings should be set back from the front boundary to accommodate a small front garden.	<i>Loss of front garden space.</i>
Height and Building Form	Along Alfred Street, to provide a graduated transition from the established single storey development to the north, and the more intense building forms anticipated in the commercial area to the south.	> New development along Alfred Street should incorporate a higher built form at the street boundary, scaling down to abutting single storey dwellings to the rear.	<p><i>Buildings that dominate the single storey scale of dwellings to the rear of Alfred Street properties.</i></p> <p><i>Underdevelopment of sites at the interface with the commercial precinct (Carlisle Street Activity Centre).</i></p>
Materials and Design Detail	<p>Along Alfred Street, to reflect the design detail of existing Edwardian dwellings.</p> <p>To encourage innovative architecture and cultural expression through design.</p>	<ul style="list-style-type: none"> > New buildings should be individually designed to respond to the characteristics of the site and adjoining buildings. > Cultural expression through colour, building details and architectural reinterpretation should be employed. > Articulate building façades through the well 	<p><i>Large bulky buildings with unarticulated facades.</i></p> <p><i>Poor composition of openings and setbacks.</i></p>

		considered composition of openings and variations in wall surfaces.	
Front Boundary Treatment	To maintain the strong relationship between buildings and the street environment.	<ul style="list-style-type: none"> > Provide a low front fence that allows views to the dwelling and any open space at the front of the dwelling. > Provide a front fence that is compatible with the era of development. 	<i>High, solid front fences.</i>

Residential Precinct R5

Character Type: Victorian / Interwar / Post-war Garden Suburban



Key Existing Characteristics

- Originally this precinct would have been dominated by larger Victorian homes, however significant overbuilding occurred throughout the Interwar and Post-war eras, particularly in the form of blocks of flats, duplexes and larger single brick dwellings
- Inkerman Street is dominated by blocks of flats from the post-war era, generally between two and three storeys.
- There are some examples of terraces from different eras, in groups of 2-5 dwellings.
- Recent years have seen the renovation and external rendering of 1960s-70s flats as well as the development of some contemporary buildings.
- Freestanding dwellings are generally one or two storeys in height and blocks of flats are generally between two and three storeys in height. The small number of terraces in the precinct are, in contrast, smaller scale and single storey.
- Buildings are generally grouped with other buildings of similar height and built form.
- Larger block sizes are present throughout Blenheim and Westbury Streets.
- Front and side setbacks are varied. Gardens are established, but small in Westbury Street. Streets are lined with bluestone kerbing and asphalt footpaths.
- In Blenheim Street, regular Paperbark trees are planted on the footpath whereas Westbury Street contains irregular plantings of mixed species.
- Front boundary treatment is mixed, however most front fences are low to medium height.

Preferred Character Statement

The mixed character of this precinct will continue to attract different styles of development that sit alongside dwellings from various eras. The large lot sizes will attract medium density housing developments of a high architectural standard. New development will interpret features of any adjoining period buildings in a contemporary manner, and will reflect the existing pattern of setbacks. Existing post-war flats will be redeveloped or renovated in order to revitalise streetscapes. Limited garden spaces will be innovatively landscaped to soften the existing hard-edged built form.

Residential Precinct R5

Design Guidelines

Character Element	Objective	Design Response	Avoid
Existing Buildings	To encourage retention of Victorian and Interwar dwellings in good condition.	<ul style="list-style-type: none"> > Retain Victorian dwellings that contribute to the character of the area. > Alterations and extensions to Victorian and Interwar buildings should be appropriate to the building era and style. > New development should be distinguishable from original dwelling stock through the use of contemporary architecture. 	<p><i>Demolition of visible parts of intact Victorian and Interwar dwellings.</i></p> <p><i>Reproduction styles and detailing.</i></p>
Landscaping and Vegetation	To strengthen the garden settings of dwellings and to create outdoor spaces and landscaping areas wherever possible in new development.	<ul style="list-style-type: none"> > Retain established trees and provide for the planting of new trees and vegetation. > In new multi-dwelling developments, consider the use of more innovative landscaping options such as planter boxes, climbers, creepers and the establishment of roof-top gardens. 	<i>Lack of landscaping and vegetation.</i>
Siting	To provide space for front gardens.	<ul style="list-style-type: none"> > Buildings should be set back from the front boundary to accommodate a front garden. 	<i>Loss of front garden space.</i>
	To minimise the loss of front garden space and the dominance of car parking structures and vehicular crossovers.	<ul style="list-style-type: none"> > Locate garages and carports behind the line of the dwelling. > Minimise paving in front garden areas including driveways and crossovers. > Where required, provide only one vehicular crossover per frontage. > In new multi-dwelling developments consider the use of underground car parking or car parking that can be accessed from the rear of the site. 	<p><i>Car parking structures that dominate the façade or view of the dwelling.</i></p> <p><i>Creation of new crossovers and driveways or wide crossovers.</i></p> <p><i>Front setbacks dominated by impervious surfaces.</i></p>
Height and Building Form	To ensure that the height and scale of new development is respectful of existing period buildings and the streetscape.	<ul style="list-style-type: none"> > Where a new development abuts existing single storey period dwellings on both sides, set back upper levels so that the higher elements are recessive in the streetscape. > Where a new multi-storey development abuts existing buildings of differing heights (e.g. single storey on one side and two or three storey on the other), a graduated transitional scale should be employed in the design of the new development. 	<i>Buildings that dominate period buildings by height, siting or massing.</i>
Materials and Design Detail	To encourage innovative architecture and cultural expression through design.	<ul style="list-style-type: none"> > New buildings should be individually designed to respond to the characteristics of the site and adjoining buildings. > Cultural expression through colour, building details and architectural reinterpretation should be employed. > Use of feature materials reflective of Interwar development, including brickwork and polychromatic bricks. > Articulate building façades through the well considered composition of openings and 	<i>Large, bulky buildings with unarticulated wall surfaces.</i>

variations in wall surfaces.

Front Boundary Treatment	To maintain and increase the openness of streetscapes.	<ul style="list-style-type: none">> Provide a front fence that allows views to the front garden and dwelling.> Provide a front fence that is compatible with the era of development.	<i>High, solid front fences.</i>
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Residential Precinct R6

Character Type: Victorian / Edwardian / Interwar Garden Suburban



Key Existing Characteristics

- Building style is dominated by Victorian and Edwardian single dwellings. Some Interwar dwellings are also present. Very little infill has occurred throughout the precinct and is generally in the form of 1960s-70s and contemporary flats at the edge of the precinct.
- Buildings are predominantly single storey however blocks of flats at the edge of the precinct are between two and three storeys in height.
- Lot sizes are small to medium (generally less than 500sqm), however there are some larger lots that are occupied by flats.
- Front setbacks are generally small to medium while side setbacks are varied. Buildings are generally set back from one or both side boundaries.
- Gardens are low to medium scale with occasional canopy trees that combine with irregular plantings of street trees in Balston Street to create a leafy character.
- Along Inkerman Street, the lack of street trees and nature strips means that the built form is the dominant characteristic of the street.
- Front boundary treatment is mixed.

Preferred Character Statement

Victorian, Edwardian and Interwar dwellings will be maintained and enhanced through careful restoration, and where applicable, appropriate adaptation. Some buildings in poor condition will provide an opportunity for new development that respects the existing scale of the precinct and provides a contemporary interpretation of existing period dwellings. New development will also continue to reflect the existing pattern of front setbacks. Gardens will become a more established feature of streetscapes, with increased plantings of medium sized canopy trees and shrubs. Front fences will be low or transparent to allow views through or over the fence into the front garden.

Residential Precinct R6

Design Guidelines

Character Element	Objective	Design Response	Avoid
Existing Buildings	To encourage retention of intact dwellings that contribute to the valued character of the precinct.	<ul style="list-style-type: none"> > Retain Victorian, Edwardian and Interwar dwellings that contribute to the character of the area. > Alterations and extensions to buildings should be appropriate to the building era and style. > New development should be distinguishable from original dwelling stock through the use of contemporary architecture. 	<p><i>Demolition of visible parts of intact Victorian, Edwardian and Interwar dwellings.</i></p> <p><i>Reproduction styles and detailing.</i></p>
Landscaping and Vegetation	To maintain and increase the garden settings of streetscapes.	<ul style="list-style-type: none"> > Buildings should be sited and designed to retain large, established trees where present and to incorporate space for the planting of substantial vegetation, including canopy trees, on larger sites. 	<p><i>Lack of landscaping and substantial vegetation.</i></p> <p><i>Removal of large, established, healthy trees.</i></p>
Siting	To provide space for front gardens.	<ul style="list-style-type: none"> > Buildings should be set back from the front boundary to accommodate a front garden. 	<i>Loss of front garden space.</i>
	To maintain and reinforce the existing rhythm of spacing between dwellings.	<ul style="list-style-type: none"> > In Balston Street, where dwellings are predominantly detached, new development should reflect the existing pattern of side setbacks. > Where a new development is proposed to be sited on one side boundary, the section of the building on the boundary should be set back further than the facade of the adjoining building to create the appearance of space between buildings. 	<i>Boundary to boundary development where a detached style predominates.</i>
	To minimise the loss of front garden space and the dominance of car parking structures and vehicular crossovers.	<ul style="list-style-type: none"> > Locate garages and carports behind the line of the dwelling. > Minimise paving in front garden areas including driveways and crossovers. > In Balston Street, provide vehicular access from a rear laneway. 	<p><i>Car parking structures that dominate the façade or view of the dwelling.</i></p> <p><i>Creation of new crossovers and driveways or wide crossovers.</i></p> <p><i>Front setbacks dominated by impervious surfaces.</i></p>
Height and Building Form	To ensure that new development is respectful of the dominant height and building form in the streetscape.	<ul style="list-style-type: none"> > The height of the front of new development should match adjoining wall heights. Where adjoining wall heights differ (e.g. single storey on one side and two or three storey on the other), a graduated transitional scale should be employed. 	<i>Buildings that exceed by more than one storey the predominant building height in the street and nearby properties.</i>
	To ensure that the height of new development does not overwhelm adjoining single storey period buildings.	<ul style="list-style-type: none"> > Upper levels that exceed the predominant building height in the street should be accommodated in the roof space or recessed from the front façade. > Where a new development abuts single storey dwellings on both sides, setback upper levels so that the higher elements are recessive in the streetscape. > In Balston Street, provide a roof consistent with the predominant roof form in the street. 	<i>New development that dominates existing single storey dwellings.</i>

Materials and Design Detail	To encourage buildings that contribute positively to the streetscape by presenting visually interesting facades to the street.	<ul style="list-style-type: none"> > Articulate the form and facades of buildings through the use of different colours and materials, through the composition of openings and setbacks and through variations in wall surfaces. > Use materials and finishes that complement those evident in the streetscape, including a modern interpretation of period detailing. 	<i>Large, bulky buildings with unarticulated facades.</i>
Front Boundary Treatment	To maintain the openness of streetscapes.	<ul style="list-style-type: none"> > Provide a front fence that is compatible with the era of development and allows views to the front garden and dwelling. 	<i>High, solid front fences.</i>

Residential Precinct R7

Character Type: Victorian / Interwar / Post-war Garden Suburban



Key Existing Characteristics

- Hotham Street is a busy main road. Many buildings are sheltered from the main road through the use of vegetation or high front fences.
- Whilst the character of the area is mixed, dwellings that are present from earlier eras are particularly intact. Some individual buildings may be of potential heritage value.
- There is a strong base of dwellings from the Victorian era mixed with some examples of Interwar and immediate post war dwellings from the late 1940s and early 1950s. The larger lot sizes have also attracted 1960s-70s blocks of flats to the precinct.
- Building height ranges between one and three storeys.
- Lot sizes range from medium to very large. Building setbacks are varied.
- Paperbark street trees help to soften the built form, while gardens are generally low-maintenance.
- The wide road and relatively low buildings mean that the precinct has a strong horizontal emphasis.

Preferred Character Statement

Existing Victorian dwellings in good condition will be retained. Larger lot sizes will attract further redevelopment including medium density housing. New development will be well articulated with variations in wall surfaces and through the composition of openings and setbacks. Where a new development adjoins an existing Victorian dwelling, it will provide an innovative design response that respects the scale, form and setbacks of the existing building. The built form and dominance of hard surfaces will be lessened through landscaping suited to the size of garden spaces. Where possible, higher front fences will be avoided in favour of other measures, such as vegetation screening, to enhance privacy and reduce noise impacts.

Residential Precinct R7

Design Guidelines

Character Element	Objective	Design Response	Avoid
Existing Buildings	To encourage retention of Victorian and Interwar dwellings in good condition.	<ul style="list-style-type: none"> > Retain Victorian and Interwar dwellings that contribute to the character of the area. > Alterations and extensions to Victorian and Interwar buildings should be appropriate to the building era and style. > New development should be distinguishable from original dwelling stock through the use of contemporary architecture. 	<p><i>Demolition of visible parts of intact Victorian and Interwar dwellings.</i></p> <p><i>Reproduction styles and detailing.</i></p>
Landscaping and Vegetation	To strengthen the garden settings of dwellings and to create outdoor spaces and landscaping areas wherever possible in new development.	<ul style="list-style-type: none"> > Retain established trees and provide for the planting of new canopy trees and vegetation. > In new multi-dwelling developments, consider the use of more innovative landscaping options such as planter boxes, climbers, creepers and the establishment of roof-top gardens. 	<i>Lack of landscaping and vegetation.</i>
Siting	To minimise the loss of front garden space and the dominance of car parking structures and vehicular crossovers.	<ul style="list-style-type: none"> > Locate garages and carports behind the line of the dwelling. > Minimise paving in front garden areas including driveways and crossovers. > In new multi-dwelling developments consider the use of underground car parking or car parking that can be accessed from the rear of the site. > Where possible, provide vehicular access from a rear laneway. 	<p><i>Car parking structures that dominate the façade or view of the dwelling.</i></p> <p><i>Creation of new crossovers and driveways or wide crossovers.</i></p> <p><i>Front setbacks dominated by impervious surfaces.</i></p>
Height and Building Form	To ensure that the height and scale of new development is respectful of existing period buildings.	<ul style="list-style-type: none"> > New multi-storey developments adjoining a lower-scale period dwelling should respect the height of the existing dwelling through the use of design techniques such as transitional scaling (recessing upper levels). 	<i>Buildings that dominate period buildings by height, siting or massing.</i>
Materials and Design Detail	To encourage innovative architecture and cultural expression through design.	<ul style="list-style-type: none"> > New buildings should be individually designed to respond to the characteristics of the site and adjoining buildings. > Cultural expression through colour, building details and architectural reinterpretation should be employed. > Articulate building façades through the well considered composition of openings and variations in wall surfaces. 	<i>Large, bulky buildings with unarticulated wall surfaces.</i>
Front Boundary Treatment	To increase the openness of streetscapes.	<ul style="list-style-type: none"> > Provide a front fence that allows views to the front garden and dwelling. > Where high front fences are proposed, the fence should be set back, or should incorporate recesses for planting. > Applications for new development along heavily trafficked roads should investigate other measures for mitigating noise and privacy impacts, such as vegetation screening and double glazed windows. 	<i>High, solid front fences.</i>

Residential Precinct R8

Character Type: Victorian / Interwar / Post-war Garden Suburban



Key Existing Characteristics

- The precinct is predominated by Victorian dwellings and post-war 1960s-70s buildings. There are also some limited examples of Interwar and Edwardian dwellings and some overbuilding has occurred in the form of contemporary units and duplexes.
- 1960s-70s buildings tend to dominate other building styles within the precinct.
- Some Victorian era dwellings are particularly intact and may be of potential heritage value, while others are in relatively poor condition.
- Both the Avenue and Gourlay Street have relatively wide roadways, however characteristics such as on-street car-parking and a high proportion of blocks of flats results in streetscapes that appear to be in proportion.
- Building height ranges between one and three storeys.
- Buildings are generally set back from one or both side boundaries.
- Lot sizes range from medium to large. Front and side setbacks are generally small, however some original Victorian homes have larger setbacks.
- Front gardens are generally limited due to small front setbacks. Street trees are inconsistent.
- Front boundary treatment is mixed.
- Although technically not in the Carlisle Street Activity Centre, the southern side of Gourlay Street is comprised of almost all 1960s-70s flats which are particularly consistent in building scale, form and setbacks. This has a strong impact on the character of the streetscape.

Preferred Character Statement

Walk-up flats and contemporary development in the form of apartments and duplexes will become the most dominant building styles throughout this precinct. 1960s-70s walk-up flats will be updated to revitalise streetscapes, and Victorian dwellings will be maintained and carefully restored where necessary. Where located next to an existing Victorian dwelling in good condition, new development will provide an innovative design response that respects the scale, form and setbacks of the existing dwelling. Buildings will be articulated with features such as variations in wall surfaces and through the composition of openings and setbacks. Landscaping will play a greater role in new development, with carefully considered garden spaces and vegetation that reduces the impact of the built form on the streetscape.

Residential Precinct R8

Design Guidelines

Character Element	Objective	Design Response	Avoid
Existing Buildings	To encourage retention of Victorian and Interwar dwellings in good condition.	<ul style="list-style-type: none"> > Retain Victorian and Interwar dwellings that contribute to the character of the area. > Alterations and extensions to Victorian and Interwar buildings should be appropriate to the building era and style. > New development should be distinguishable from original dwelling stock through the use of contemporary architecture. 	<p><i>Demolition of visible parts of intact Victorian and Interwar dwellings.</i></p> <p><i>Reproduction styles and detailing.</i></p>
Landscaping and Vegetation	To strengthen the garden settings of dwellings and to create outdoor spaces and landscaping areas wherever possible in new development.	<ul style="list-style-type: none"> > Retain established trees and provide for the planting of new trees and vegetation. > In new multi-dwelling developments, consider the use of more innovative landscaping options such as planter boxes, climbers, creepers and the establishment of roof-top gardens. 	<i>Lack of landscaping and vegetation.</i>
Siting	To minimise the loss of front garden space and the dominance of car parking structures and vehicular crossovers.	<ul style="list-style-type: none"> > Locate garages and carports behind the line of the dwelling. > Minimise paving in front garden areas including driveways and crossovers. > In new multi-dwelling developments consider the use of underground car parking or car parking that can be accessed from the rear of the site. > Where possible, provide vehicular access from a rear laneway. 	<p><i>Car parking structures that dominate the façade or view of the dwelling.</i></p> <p><i>Creation of new crossovers and driveways or wide crossovers.</i></p> <p><i>Front setbacks dominated by impervious surfaces.</i></p>
	To maintain and reinforce the existing rhythm of spacing between buildings	<ul style="list-style-type: none"> > New development should reflect the existing pattern of side setbacks. 	<i>Boundary-to-boundary development, unless characteristic of a site or adjoining sites.</i>
Height and Building Form	To ensure that the height and scale of new development is respectful of existing period buildings.	<ul style="list-style-type: none"> > New multi-storey developments adjoining a lower-scale period dwelling should respect the height of the existing dwelling through the use of design techniques such as transitional scaling (recessing upper levels). 	<i>Buildings that dominate period buildings by height, siting or massing.</i>
Materials and Design Detail	To encourage innovative architecture and cultural expression through design.	<ul style="list-style-type: none"> > New buildings should be individually designed to respond to the characteristics of the site and adjoining buildings. > Cultural expression through colour, building details and architectural reinterpretation should be employed, including the use of contemporary materials and detailing. > Articulate building façades through the well considered composition of openings and variations in wall surfaces. 	<i>Large, bulky buildings with unarticulated wall surfaces.</i>
Front Boundary Treatment	To maintain and increase the openness of streetscapes.	<ul style="list-style-type: none"> > Provide a low or transparent front fence that allows views to the front garden and dwelling. > Provide a front fence that is compatible with the era of development. 	<i>High, solid front fences.</i>

Residential Precinct R9

Character Type: Victorian / Edwardian / Interwar / Post-war with Modern Overbuilding



Key Existing Characteristics

- This precinct is dominated by Edwardian dwellings. There are also some examples of Interwar and post-war era buildings, including Californian Bungalows in Hawsleigh Avenue and 1960s-70s flats throughout.
- Buildings are generally set back from both side boundaries.
- The southern end of Carlisle Avenue is dominated by updated flats from the post-war era.
- Building height is predominantly between one and two storeys.
- Lot sizes are small-medium sized in Hawsleigh Avenue and medium sized in Carlisle Avenue. Building setbacks are varied. Older dwellings tend to have more generous front and side setbacks.
- In Carlisle Avenue, large avenue street trees combine with nature strips and bluestone kerbing to create a special character that may be of potential neighbourhood character significance.
- Hawsleigh Avenue is lined with consistent native street trees which also enhance its character.
- Front gardens are small, but well established, and combine with street trees and nature strips to create a leafy character.
- Front boundary treatment is mixed, however, most front boundaries are lined with medium to high fences.

Preferred Character Statement

Edwardian dwellings will continue to provide a strong base for this precinct, and will be well maintained and where necessary, appropriately restored. Post-war flat development will be updated with contemporary materials and finishes that complement those of period homes. Buildings in poor condition will provide opportunities for new replacement development that is respectful of the scale and form of period dwellings. New development will provide adequate space for landscaping that contributes to the leafy atmosphere of the precinct. Car-parking and associated structures will be avoided in the front setback of all properties in order to maximise space for landscaping and enhance the spacious feel of the streetscape. Front fences will be low or absent altogether in order to retain views to buildings.

Residential Precinct R9

Design Guidelines

Character Element	Objective	Design Response	Avoid
Existing Buildings	To encourage retention of Edwardian and Interwar dwellings in good condition.	<ul style="list-style-type: none"> > Retain Edwardian and Interwar dwellings that contribute to the valued character of the area. > Alterations and extensions to Edwardian and Interwar buildings should be appropriate to the building era and style. > New development should be distinguishable from original dwelling stock through the use of contemporary architecture. 	<p><i>Demolition of visible parts of intact Edwardian and Interwar dwellings.</i></p> <p><i>Reproduction styles and detailing.</i></p>
Landscaping and Vegetation	To strengthen the garden settings of dwellings and to create outdoor spaces and landscaping areas wherever possible in new development.	<ul style="list-style-type: none"> > Retain established trees and provide for the planting of new trees and vegetation. > In new multi-dwelling developments, consider the use of more innovative landscaping options such as planter boxes, climbers, creepers and the establishment of roof-top gardens. 	<i>Lack of landscaping and vegetation.</i>
Siting	To provide space for front gardens and maintain established setbacks.	<ul style="list-style-type: none"> > Buildings should be set back from the front boundary to accommodate a front garden. > Buildings should be set back from both side boundaries, reflective of existing built form. 	<i>Loss of front garden space.</i>
	To minimise the loss of front garden space and the dominance of car parking structures and vehicular crossovers.	<ul style="list-style-type: none"> > Locate garages and carports behind the line of the dwelling. > Minimise paving in front garden areas including driveways and crossovers. > In new multi-dwelling developments consider the use of underground car parking or car parking that can be accessed from the rear of the site. > Where possible, provide vehicular access from a rear laneway. 	<p><i>Car parking structures that dominate the façade or view of the dwelling.</i></p> <p><i>Creation of new crossovers and driveways or wide crossovers.</i></p> <p><i>Front setbacks dominated by impervious surfaces.</i></p>
Height and Building Form	To ensure that the height and scale of new development is respectful of existing period buildings.	<ul style="list-style-type: none"> > The height of the front of new development should match adjoining wall heights. Where adjoining wall heights differ (e.g. single storey on one side and two or three storey on the other), a graduated transitional scale should be employed. > Upper levels that exceed the predominant building height in the street should be accommodated in the roof space or recessed from the front façade. > Where a new development abuts single dwellings on both sides, setback upper levels so that the higher elements are recessive in the streetscape. > In Hawsleigh Avenue, provide a roof consistent with the predominant roof form in the street. 	<i>New development that overwhelms the single storey scale of the street.</i>
Materials and Design Detail	To encourage buildings that contribute positively to the streetscape by presenting visually interesting facades	<ul style="list-style-type: none"> > Articulate the form and facades of buildings through the use of different colours and materials, through the composition of openings and setbacks and through variations in wall surfaces. 	<i>Large, bulky buildings with unarticulated facades.</i>

to the street.

- > Use materials and finishes that complement those evident in the streetscape, including a modern interpretation of period detailing.
- > A restrained mix of materials should be used, especially for small sites.

On small sites, the use of several differing materials and colours in building facades.

**Front
Boundary
Treatment**

To maintain and increase the openness of streetscapes.

- > Provide a low or transparent front fence that allows views to the front garden and dwelling.
- > Provide a front fence that is compatible with the era of development.

High, solid front fences.

Residential Precinct R10

Character Type: Victorian Edwardian Inner Urban



Key Existing Characteristics

- This precinct is a particularly consistent neighbourhood character area, which has been previously investigated for heritage significance (Heritage Alliance, 2004).
- The precinct is dominated by single-fronted Victorian dwellings. There are also limited examples of buildings from other eras, including those from the Edwardian, Interwar, post-war and contemporary periods.
- Materials are a mix of timber, brick and render. Most streets are dominated by lightly-coloured timber.
- Most streetscapes have a small-scale atmosphere due to narrow roadways, small-scale buildings and limited front setbacks and properties with narrow frontages to the street.
- Buildings are predominantly single storey, although there are some examples of double storey buildings.
- Bothwell Street is noticeably different from the other streets in this precinct. It cuts across the southern half of the precinct in a north-west to south-west direction and the properties that front onto it are of an irregular shape and size.
- The northern side of Bothwell Street is characterised by contemporary infill which is generally two storeys in height, while the southern side is characterised by a single-storey scale and rear fences of properties. A landscaped median strip runs down the centre of the street, creating a more spacious, leafy atmosphere.
- The fine-grain subdivision of this precinct has a strong influence on its overall character. Lot sizes are generally small, with only occasional larger lot sizes. Dwellings have minimal front and side setbacks, and there are some examples of terraced buildings throughout.
- Front gardens are typical of inner urban areas, with landscaping suited to limited planting areas due to small front setbacks. Gardens generally comprise small trees, shrubs and occasional garden beds.
- Streets are lined with bluestone kerbing and narrow asphalt footpaths, with only occasional street trees planted along the footpath. These characteristics add to the small-scale, built form dominated atmosphere of most streetscapes throughout the precinct.
- Rear laneways also influence the character of this precinct and are evidence of the fact that it is an older subdivision dating from the Victorian era.
- On-street car parking is a dominant feature of the area, with few properties having on-site car parking due to small front setbacks.
- Most properties are lined with low picket front fences.

- The car-parks and larger-scale buildings located on the northern side of Marlborough Street have an impact on the character of the northern edge of the precinct and result in a highly varied scale at this interface.

Preferred Character Statement

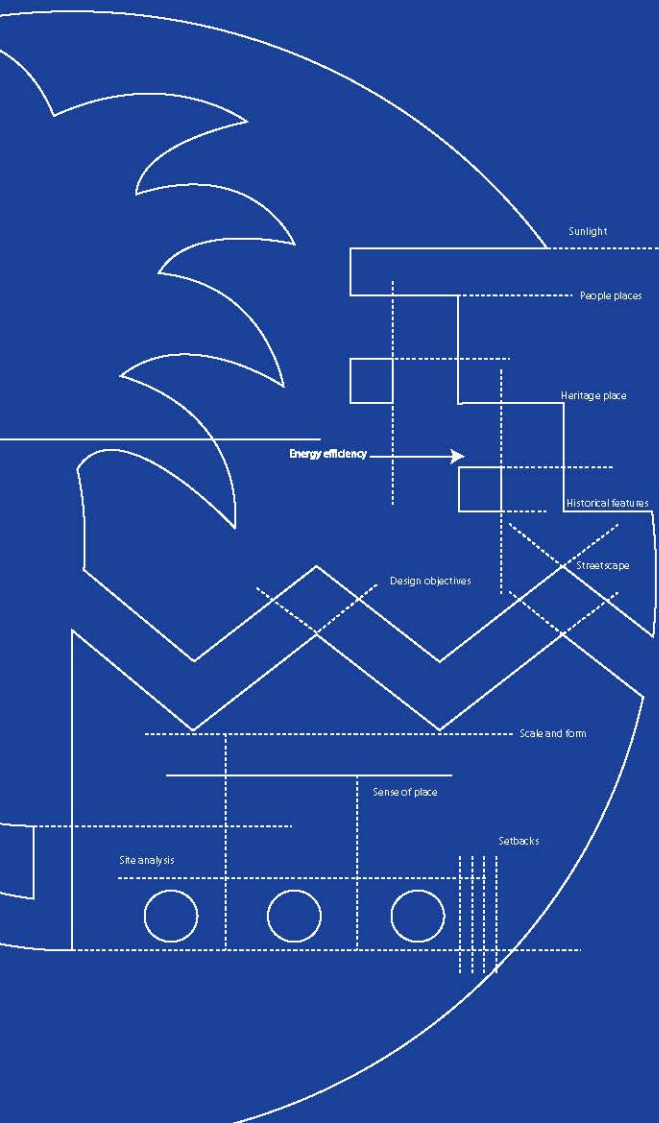
This precinct will retain the key character elements that contribute to its high level of consistency. Existing Victorian, Edwardian and Interwar dwellings will be well maintained and carefully restored where necessary. The small-scale inner urban atmosphere will be upheld in new development, with new buildings respecting the height, form and setbacks of existing buildings. New development will adopt simple design details that respect but do not replicate period detailing. Building materials and finishes will continue to contribute to the lightness of streetscapes created by lightly coloured timber. Front setbacks will be innovatively landscaped and front boundaries lined with low fences that suit the era of development and maintains the relationship between the street and buildings. New development on the northern side of Marlborough Street may be of a higher scale and more contemporary style, but should respond to the valued character elements of the southern side of the street through careful selection of building materials and modern interpretation of period design elements such as pitched roofs and verandas.

Residential Precinct R10

Design Guidelines

Character Element	Objective	Design Response	Avoid
Existing Buildings	To encourage retention of Victorian and Edwardian dwellings that contribute to the valued character of the area.	<ul style="list-style-type: none"> > In streets that contain intact groups of Victorian and Edwardian dwellings, consider retaining and/or adapting those buildings that are in good condition wherever possible in new development. > Alterations and extensions to Victorian and Edwardian dwellings should be appropriate to the building era and style. > New development should be distinguishable from original dwelling stock through the use of contemporary architecture. 	<p><i>Demolition of visible parts of intact Victorian and Edwardian dwellings.</i></p> <p><i>Reproduction of historic building styles.</i></p>
Landscaping and Vegetation	To strengthen the small scale pattern of garden settings.	<ul style="list-style-type: none"> > New development should provide for the planting of vegetation suited to small garden areas. > 	<p><i>Lack of landscaping and vegetation.</i></p> <p><i>Large areas of impervious surfaces.</i></p>
Siting	To provide space for front gardens.	<ul style="list-style-type: none"> > Buildings should be set back from the front boundary to accommodate a front garden. 	<i>Loss of front garden space.</i>
	To maintain and reinforce the existing rhythm of spacing between dwellings.	<ul style="list-style-type: none"> > In streets with predominantly terrace style housing, new dwellings should reflect the lack of side setbacks. > Where side setbacks are of a consistent size in the street, new development should reflect the existing pattern. 	<i>Large side setbacks in streets where a terraced style predominates.</i>
Height and Building Form	<p>To ensure that new development is respectful of the dominant height and building form in the streetscape.</p> <p>To ensure that the height of new development does not overwhelm adjoining single storey period buildings</p>	<ul style="list-style-type: none"> > The height of the front of new development should match adjoining wall heights. Where adjoining wall heights differ (e.g. single storey on one side and two or three storey on the other), a graduated transitional scale should be employed. > Upper levels that exceed the predominant building height in the street should be accommodated in the roof space or recessed from the front façade. > Where a new development abuts single storey dwellings on both sides, set back upper levels so that the higher elements are recessive in the streetscape. > Provide a pitched roof. 	<p><i>Buildings that exceed by more than one storey the predominant building height in the street and nearby properties.</i></p> <p><i>New development that dominates existing single storey dwellings.</i></p>
Materials and Design Detail	To ensure that the use of materials and design detail in new development complements that of the predominant building styles in the street.	<ul style="list-style-type: none"> > New development should reflect the use of materials and reinterpret the design details of Edwardian and Victorian dwellings in a contemporary manner. > A restrained mix of materials should be used, especially for small sites. > Articulate the form and façades of buildings through the use of different colours and materials, through the composition of openings and setbacks and through variations in wall surfaces. > New dwellings should incorporate timber (weatherboards) or other non-masonry materials and use subdued colours on external finishes. 	<p><i>Reproduction styles and detailing.</i></p> <p><i>Large buildings with poorly articulated wall surfaces.</i></p> <p><i>On small sites, the use of several differing materials and colours in building facades.</i></p>
Front Boundary Treatment	To maintain the openness of streetscapes.	<ul style="list-style-type: none"> > Provide a low front fence that allows views to the front garden and dwelling. > Provide a front fence that is compatible with the era of development and makes use of timber or other non-masonry materials. 	<i>High, solid front fences.</i>

Port Phillip Design Manual



ESTABLISHED RESIDENTIAL PRECINCTS

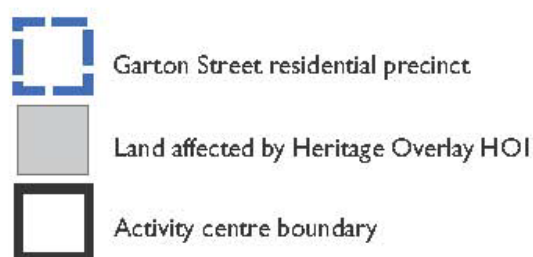
Introduction

The residential areas to the south-east and north-west of the *Activity Centre* have a fine grain and low scale character, and are substantially affected by *Heritage Overlay 1 (HO1 – Port Melbourne)*. The structure plan aims to protect these areas, whilst providing for incremental housing growth in residential areas proximate to the activity centre that has existing diverse neighbourhood character.

There are four residential areas (*Residential Precincts 1 to 4*) that have capacity for incremental residential growth, based on a diverse neighbourhood character, which are identified on the *Established Residential Areas Housing Framework Plan*.

The following section comprises *Preferred Neighbourhood Character Statements* for these areas. Development must be in accordance with the 'Preferred Character Statement' and 'Design Response' contained in each statement.

Residential Precinct I: Garton Street



Above: north side of Garton Street.



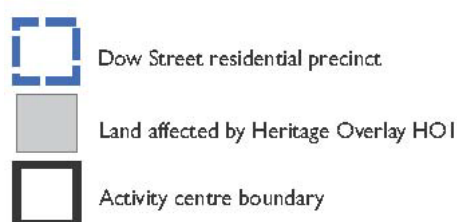
Above: south side of Garton Street.



Existing character / Future category	Future Development Statement	Preferred Character Statement
Existing character type Victorian / Modern overbuilding	<ul style="list-style-type: none"> The two sides of Garton Street present differing residential development opportunities adjacent to the Crockford Street renewal precinct. Future development will make a transition in height down from Crockford Street (which is anticipated to be up to 5 storeys) towards the low scale established streetscapes in Bay Street (1 and 2 storeys). Lots on the southern side of the Garton Street are larger and have an interface with bulky buildings (fronting Crockford Street and also in Garton Street) and have redevelopment potential. New development on the southern side will be constructed up to 3 storeys. Townhouse type development will reflect the existing grain and setback in Garton Street. The remaining warehouse building is also a development opportunity. The small lot size and consistent low building scale on the northern side of Garton Street will limit future development to single lot development, including single dwellings and additions to existing dwellings. Development on the northern side will respond to the existing low scale and intimate residential environment made up of 'contributory' properties outside of the Heritage Overlay (as shown on the City of Port Phillip Neighbourhood Character Policy Map). 	<ul style="list-style-type: none"> Existing Victorian dwellings in good condition will be encouraged to be retained particularly on the northern side. New buildings will reflect the existing streetscape grain and provide small front setbacks. Contemporary design responds to consistent existing streetscape elements, including verandahs, fascia lines and fence heights. Development on the southern side will be transitional in scale of 3 storeys (with scope for a 4th level if recessed from view). Townhouse style buildings will be well articulated with punctuated openings and balconies to the street. Redevelopment of the existing warehouse building will reinstate a residential land use and generally match the height of the existing building on the same site. Development on the northern side will respond to the existing low building scale and front setbacks and be limited to two storeys. Development adjacent to heritage places will be transitional in height and setbacks. Garages, carports, car parking spaces and vehicle access within the front setback are strongly discouraged.
Future residential change category <i>Incremental change</i>		

Design Response	
Frontage widths:	Match existing widths (range of 4 - 6 metres.)
Siting and setbacks:	<u>North side</u> : Match existing front setbacks (3 metres minimum).
	<u>South side</u> : 2 metres.
Building height:	<u>South side</u> : 3 storeys (any 4th level must be recessed from view).
	<u>North side</u> : 2 storeys.
Design detail:	<p>Reflect key horizontal lines within the street.</p> <p>Discourage vehicle access within front setback.</p> <p>Surveillance of street from all levels.</p> <p>Low scale fencing (0.8 – 1.2 metres).</p>
Landscaping / planting:	Low scale planting within front setback.

Residential Precinct 2: Dow Street (north)



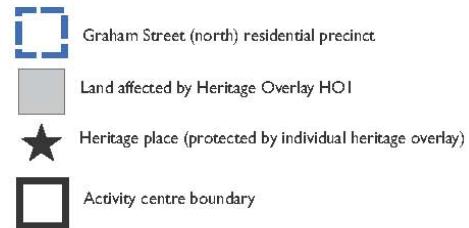
Above: Dwellings in Dow Street (west and east).

Above: Esplanade West.

Existing character / Future change category	Future Development Statement	Preferred Character Statement
Existing character type Victorian / Edwardian / Modern overbuilding	<ul style="list-style-type: none"> The varied character in the precinct provides scope for development which responds to existing identified character elements, and adjacent heritage places. New development will predominantly be in the form of new dwellings and extensions to existing dwellings. Multi-unit development will be limited and occur on the few larger remaining lots, or consolidated lots. Development will be 2 storeys in height, reference key streetscape elements, and continue predominant front setbacks. Surveillance of the street is incorporated at ground and first floor level. 	<ul style="list-style-type: none"> Further residential development to a high architectural standard responds to the existing pattern of development and nearby building styles. 'Contributory' properties outside of the Heritage Overlay (as shown on the City of Port Phillip Neighbourhood Character Policy Map) are encouraged to be retained. Development maintains consistent streetscape elements, including low scale buildings, small and medium sized front setbacks, and low fencing.
Future residential change category <i>Incremental change</i>	<ul style="list-style-type: none"> In Dow Street, the existing weatherboard dwellings will contribute to a visually interesting precinct, with new building forms emerging. In Esplanade West, continued modern development sits alongside characteristic original weatherboard dwellings. Dual front setbacks in response to the diagonal alignment of the street are continued, and dwellings overlook Lagoon Reserve to the east. 	<ul style="list-style-type: none"> The character of the precinct will be maintained and enhanced through respecting existing setback arrangements and tree planting. In Esplanade West, the small to medium dual setbacks are continued, and verandah and roof elements are referenced. Low fencing and planting is provided. In Dow Street, the strongly prevalent hipped roofs and horizontal lines created through verandah and fascia elements are reflected in new buildings. 3rd floor levels will be discouraged, unless substantially hidden from view. Development adjacent to heritage places will be transitional in height and setbacks.

Design Response	
Frontage widths: Match existing, as follows:	<u>Esplanade West</u> : minimum 6 metres.
	<u>Dow Street</u> : range of 5 – 7 metres.
Siting and setbacks: Match existing, as follows:	<u>Esplanade West</u> : 4 metres; or 2 metre and 5 metre dual setbacks for properties fronting the diagonally aligned section of Esplanade West.
	<u>Dow Street</u> : approximately 3 metres.
Building height:	Two storeys.
Design detail:	<u>Esplanade West</u> : Incorporate verandahs and street surveillance, brick and weatherboard materials, low fences with low planting behind.
	<u>Dow Street</u> : Reflect horizontal elements (eg: verandahs and fascia lines) and hipped roofs, and incorporate brick and weatherboard materials and low / medium fences of open design.
Landscaping / planting:	Low / medium scale planting in the front setback.

Residential Precinct 3: Graham Street (north)



Above: Princes Street



Stokes Street



Graham Street

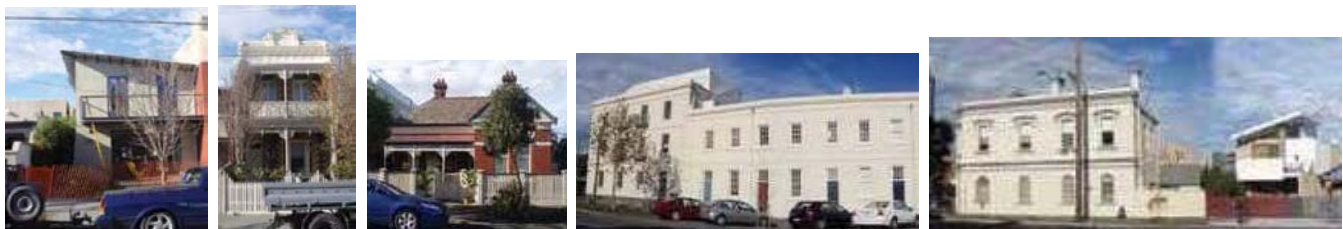
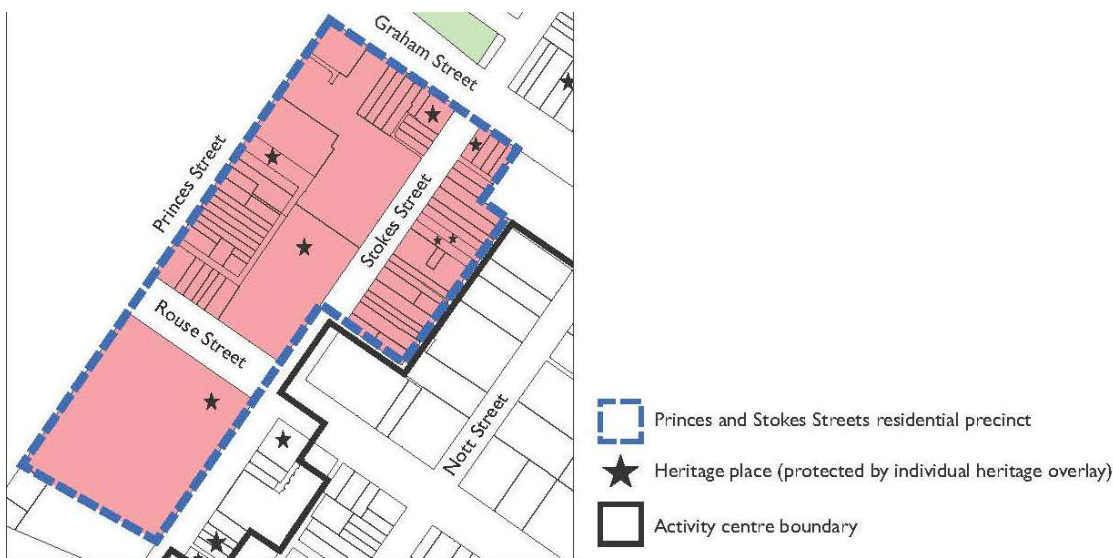


Nott Street.

Existing type / Future change category	Future Development Statement	Preferred Character Statement
Existing character type Victorian / Edwardian / Modern overbuilding	<ul style="list-style-type: none"> Each street in this precinct has differing established and emerging character attributes which will set the context for future development. Development will respond to the identified character elements of each street, as well as heritage places. The precinct is well located in proximity to the light rail and also the core retail area, which will support incremental intensification. There is scope for further well designed infill development of up to two storeys on remaining larger lots, particularly in Princes Street where townhouse style development is predominant, and also in Nott Street. Properties in Graham Street have generous proportions and can accommodate some development that respects heritage and 'contributory' properties outside of the Heritage Overlay (as shown on the City of Port Phillip Neighbourhood Character Policy Map). Streets with fine grain subdivision elements and small lots, such as Stokes Street, will experience limited development unless lot consolidation occurs. There are atypical taller built form elements of 3 and 4 storeys which act as poor references for future development. 	<ul style="list-style-type: none"> Contemporary development will continue to occur which is respectful of the scale, form and siting of period dwellings. Period dwellings will be encouraged to be retained. New development will respond to the established scale and setbacks of the precinct, and incorporate front setbacks to allow for planting that contributes to the streetscape and rear yard environs. Princes Street will accommodate further townhouse development, with small front setbacks and windows and balconies that overlook the street. Graham Street will accommodate sensitive new development, such as at the rear of period dwellings. Development in Stokes Street will reflect existing character elements, including front and side setbacks, verandahs and low fences. Turville Place and Nott Street will continue to accommodate further low scale multi-unit residential development. New residential development will appear as 2 storeys. A recessed 3rd level can be accommodated in new development fronting Princes and Turville Streets. The housing estate will be updated with contemporary materials and finishes. Any redevelopment will not result in additional height beyond that of the existing buildings.
Future residential change category <i>Incremental change</i>		

Design Response	
Frontage widths:	<u>Princes and Stokes Streets:</u> 5 – 7 metres.
	<u>Nott Street:</u> 6 – 9 metres.
	<u>Graham Street:</u> 6 metres.
Siting and setbacks:	<u>Princes Street:</u> 1 metre front setback.
	<u>Stokes and Nott Streets:</u> 1 - 4 metre front setback.
	<u>Stokes Street:</u> A 0.5 metre side setback.
	<u>Graham Street:</u> 3 – 4 metre front setback.
Building height:	2 storeys (unless specified).
	<u>Graham Street:</u> 3 storeys
Design detail:	<u>Princes Street:</u> Modern design; punctuated facades with openings to both floor levels.
	<u>Stokes Street:</u> Reflect existing elements including gable roof ends, verandahs, low fences.
	<u>Nott Street:</u> Low – medium height fencing.
Landscaping / planting:	<u>Princes Street:</u> Minor decorative planting.
	<u>Stokes & Nott Streets:</u> Low –medium height planting.
	<u>Graham Street:</u> Medium height trees.

Residential Precinct 4: Princes and Stokes Streets



Above: (from L to R) dwellings in Stokes Street, Princes Street, Rouse Street, Graham Street.

Existing type / Future change category	Future Development Statement	Preferred Character Statement
Existing character type Victorian / Edwardian / Interwar Modern overbuilding	<ul style="list-style-type: none"> The precinct is on the edge of a former industrial area that has made a transition to high density residential development. Some medium density development has occurred within the precinct. The precinct has little capacity for further medium density development. Residential lots are typically small in size and will limit further development in the precinct. Stokes and Princes Streets will retain a low scale and fine grain of development, supported by low fences and low planting that contributes to an intimate residential environment. 	<ul style="list-style-type: none"> Period dwellings, including 'contributory' properties outside of the Heritage Overlay (as shown on the City of Port Phillip Neighbourhood Character Policy Map), are encouraged to be retained, to protect the low scale, fine grain character of the precinct. Significant and contributory heritage places are retained. The existing rhythm of development is reinforced through new development. A consistent building scale is achieved for each street section, and changes in scale part or mid-way are strongly discouraged.
Future residential change category <i>Incremental change</i>	<ul style="list-style-type: none"> Future development will predominantly be in the form of sensitive additions to existing dwellings, new dwellings on individual lots as they become available, and well-designed low scale medium density development on larger single or consolidated lots. Rouse and Graham Street have some further propensity for change, comprising larger lots and short street sections that have disparate character elements. Heritage places will continue to play a key role in underpinning the unique fabric of the precinct. Intensive development of small individual lots is not envisaged as part of the future character of the precinct. Any further development of the Swallow and Ariel warehouse is set well back from the façade, is limited in height, and visually recessive. 	<ul style="list-style-type: none"> New development responds to the existing characteristics of the precinct, and responds to low scale adjoining properties. New development responds to the setbacks of adjoining properties, and responds to key character elements including the fine grain and characteristic street interface of development, such as setbacks, verandahs, and low fencing. Crossovers, car parking and associated structures will be avoided in the front setback. First and second floor levels are setback from the frontage to limit their visibility. Building scale does not exceed two storeys in Princes Street, and does not exceed three storeys in Stokes Street.

Design Response	
Frontage widths: Match existing, being:	<u>Princes & Stokes Streets</u> : 5 – 9 metres.
	<u>Rouse & Graham Streets</u> : 6 – 7 metres.
Siting and setbacks:	<u>Princes Street</u> : 3 – 6 metres.
	<u>Stokes Street</u> : 1 – 4 metres.
	<u>Graham Street</u> : 6 metres.
Building height:	<u>Princes Street</u> : 2 storeys.
	<u>Stokes Street</u> : 3 storeys.
	<u>Graham Street</u> : 3 storeys
Design detail:	<u>Precinct wide</u> : Respond to fine grain streetscape rhythm, low building scale, planted front setback areas.
	<u>Princes Street</u> : Single storey elements, verandahs, low – medium fencing.
	<u>Stokes Street</u> : Verandahs and balconies, low fencing, brick and weatherboard materials.
Landscaping / planting:	Low to medium size planted front setbacks.