



# Carlisle Street Urban Design Framework

Adopted November 2009  
(Updated December 2011)

# Carlisle Street





Carlisle Street UDF - November 2009

# Contents

<b>1 INTRODUCTION .....</b>	<b>2</b>	<b>3 URBAN DESIGN PRINCIPLES &amp; OBJECTIVES.....</b>	<b>25</b>
1.1 BACKGROUND.....	2	3.1 PRINCIPLES .....	26
1.2 PURPOSE AND SCOPE .....	2	3.2 ACTIVE EDGES .....	31
1.3 PROJECT AREA .....	2	<b>4 PRECINCT GUIDELINES .....</b>	<b>33</b>
1.4 CONSULTATION .....	2	4.1 CARLISLE STREET WEST PRECINCT .....	34
1.5 STRATEGIC PLANNING CONTEXT .....	4	4.2 CARLISLE STREET CORE RETAIL AND EASTERN PRECINCT.....	38
1.6 PRESCRIPTIVE AND PERFORMANCE CONTROLS.....	5	4.3 ST KILDA ROAD BUSINESS PRECINCT.....	42
<b>2 ANALYSIS .....</b>	<b>7</b>	4.4 INKERMAN/PAKINGTON STREETS MIXED-USE PRECINCT.....	46
2.1 INTRODUCTION .....	7	4.5 CHAPEL STREET MIXED USE PRECINCT.....	50
2.2 EXISTING LAND USE .....	8	4.6 CHAPEL STREET NORTH PRECINCT .....	54
2.3 HERITAGE.....	10	4.7 WILLIAM STREET INDUSTRIAL PRECINCT .....	58
2.4 BUILDING HEIGHTS.....	12	4.8 SUPERMARKETS PRECINCT.....	62
2.5 ICONS AND LANDMARK BUILDINGS .....	14	<b>STRATEGIC SITE GUIDELINES.....</b>	<b>67</b>
2.6 TOPOGRAPHY, VIEWS AND VISTAS.....	16	4.9 COLES SUPERMARKET SITE .....	68
2.7 MOVEMENT NETWORK.....	18	4.10 SAFEWAY SUPERMARKET SITE .....	70
2.8 PUBLIC TRANSPORT.....	20	4.11 MARLBOROUGH STREET CAR PARK SITE.....	72
2.9 OPEN SPACE .....	22	4.12 BALACLAVA STATION CAR PARK SITE.....	74
2.10 SOLAR ACCESS TO PRIORITY STREETS .....	22	4.13 ST KILDA PARISH MISSION SITE .....	76
		4.14 AUSTRALIA POST DELIVERY CENTRE SITE.....	78

# 1 Introduction

---

## 1.1 BACKGROUND

Carlisle Street, Balaclava is designated as a Major Activity Centre in the State Government of Victoria's planning blueprint for Melbourne, Melbourne 2030.

A Structure Plan for the Activity Centre and surrounding residential area is being prepared however a more detailed urban design study is needed to rigorously define and outline the preferred built form outcomes of the Structure Plan.

The Carlisle Street Activity Centre Structure Plan (referred to as the "Structure Plan") incorporates the retail strip of Carlisle Street as well as the surrounding mixed-use, commercial, industrial and established residential areas.

The Carlisle Street Activity Centre boundary has been defined through the structure planning process as incorporating all precincts with potential for new development. It is this area that the Carlisle Street Urban Design Framework focuses on.

## 1.2 PURPOSE AND SCOPE

This document builds upon the strategic directions of the Structure Plan and sets out an urban design framework which comprises urban design principles, objectives and guidelines that will together ensure that the future built form in the activity centre helps to realise the vision for the centre while respecting the existing and valued character of the area.

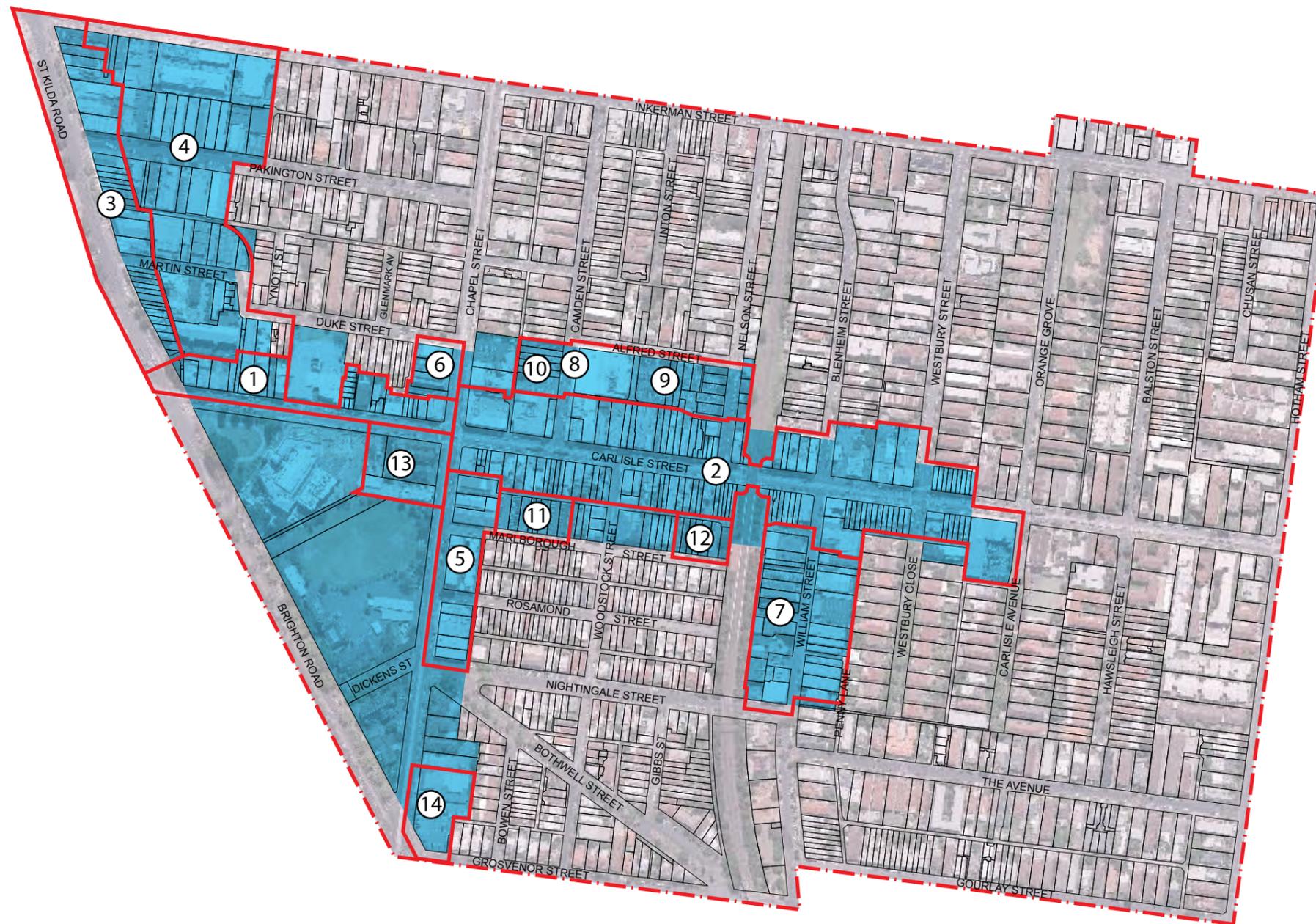
## 1.3 PROJECT AREA

The project study area was defined via the structure planning process and has remained the same for the Urban Design Framework. This study area is bounded by Inkerman Street to the north, Hotham Street to the east, St Kilda and Brighton Roads to the west and Grosvenor and Gurlay Streets to the south.

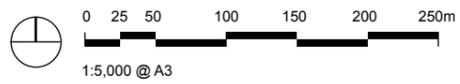
The Urban Design Framework itself concentrates on the precincts identified through the structure planning process as having potential for new development, with their borders defined mainly according to their existing and desired future land uses as well as their propensity for change. The boundaries of the precincts generally follow existing land use zoning boundaries, as identified in the City of Port Phillip Planning Scheme.

## 1.4 CONSULTATION

The Structure Plan has undergone an extensive consultation process in its preparation. The vision and strategic objectives of the Structure Plan have been developed in close consultation with the community and reflect the shared aspirations for the Carlisle Street area. It is the intention of this framework to facilitate and guide the development of the urban environment over the next 20 years to reflect these aspirations.



- ① Carlisle Street West Precinct
- ② Carlisle Street Core Retail and Eastern Precinct
- ③ St Kilda Road Business Precinct
- ④ Inkerman/Pakington Streets Mixed-use Precinct
- ⑤ Chapel Street Mixed-use Precinct
- ⑥ Chapel Street North Precinct
- ⑦ William Street Industrial Precinct
- ⑧ Supermarkets Precinct
- ⑨ Coles Supermarket Site
- ⑩ Safeway Supermarket Site
- ⑪ Marlborough Street Car Park Site
- ⑫ Balaclava Station Car Park Site
- ⑬ St Kilda Parish Mission Site
- ⑭ Australia Post Delivery Centre Site
- - - Structure Plan Study Area
- Precincts/Strategic Sites
- Activity Centre Boundary



## 1.5 STRATEGIC PLANNING CONTEXT

Strategic Planning in the City of Port Phillip is undertaken within the Victorian State planning policy and legislative context, both of which are implemented at the local level by the Council.

The State Planning Policy Framework (SPPF) outlines the strategic directions for metropolitan growth in Melbourne as developed in the “planning blueprint” Melbourne 2030, Planning for Sustainable Growth (M2030). The 8 directions for sustainable growth and their supporting objectives and strategies are outlined in Clause 12 of the planning scheme. Of particular relevance to the Carlisle Street Major Activity Centre are:

- Direction 1 - A compact city. Identified as a Major Activity Centre under M2030, this direction aims to ensure that Carlisle Street accommodates ongoing investment and change in retail, office, service and residential markets to ensure a mixture of uses within the centre. The clustering of uses, in particular new housing, around existing centres aims to provide compact mixed-use centres that will go some way to alleviating the pressure for more residential growth on the fringe of Melbourne.
- Direction 5 - A great place to be. With the pressure for more residential development in existing centres this direction aims to ensure that the urban environment of places such as Carlisle Street are not compromised but improved. It aims to ensure that new development responds to the existing neighbourhood character, cultural identity and sense of place using high quality urban design.

- Direction 8 - Better transport links. Of particular relevance to the Carlisle Street Activity Centre due to its location well served by multiple tram and train services, this direction aims to support the use of the principal public transport network (PPTN) by increasing residential densities around it. Walking and cycling are also to be encouraged within centres.

The Local Planning Policy Framework (LPPF) sets out the City of Port Phillip’s vision for the municipality in the form of the Municipal Strategic Statement (MSS) and Local Policies. Specific recommendations for the East St Kilda Area relate to:

- Maintaining and enhancing the diversity of housing stock while respecting the heritage, character and amenity of the area.
- Ensuring the mixed use zone in Chapel Street transitions to residential and office uses avoiding industrial uses or a retail function that may result in competition with Carlisle Street.
- Supporting the retention of the William Street Industrial precinct as the only industrial area in Port Phillip outside of South Melbourne and Port Melbourne.



## 1.6 PRESCRIPTIVE AND PERFORMANCE CONTROLS

Prescriptive controls provide fixed directions for planners for decision-making. These controls can set building heights, setbacks, floor areas etc. Prescriptive controls are sometime criticised for their inflexible and “blunt” approach and can sometimes result in uniform outcomes that diminish the diversity of places.

Performance-based controls have the ability to create better places by increasing the flexibility of decision-making. They set standards that describe the end result and acceptable limits of impact. These controls offer the decision-maker the discretion to respond to the site and application on its merits and also encourage greater dialogue between the stakeholders.

This Urban Design Framework will consist of a mixture of both prescriptive and performance based controls. It will aim to outline minimum standards to be met while also allowing some flexibility for potential developers to encourage high standards of design.



## 2 *Analysis*

---

### 2.1 INTRODUCTION

The analysis of the UDF study area was carried out under the themes of:

- Existing land uses
- Heritage
- Building heights
- Icons and landmark buildings
- Topography, views and vistas
- Movement network
- Public transport and
- Open space
- Solar access to priority streets

This analysis has informed the overarching urban design principles and objectives in Section 3 and the more precinct and key site specific objectives and guidelines in Section 4.

## 2.2 EXISTING LAND USES

The Carlisle Street Major Activity Centre is a traditional Melbourne Strip Shopping Centre. The existing Carlisle Street strip is zoned Business 1 and extends from the west, at the St Kilda Public Library, to the east, at Carlisle Avenue.

To the south/west of the strip is an area zoned for Public Use (either PUZ2 or PUZ6) which is occupied by the St Kilda Town Hall and the St Kilda Primary School.

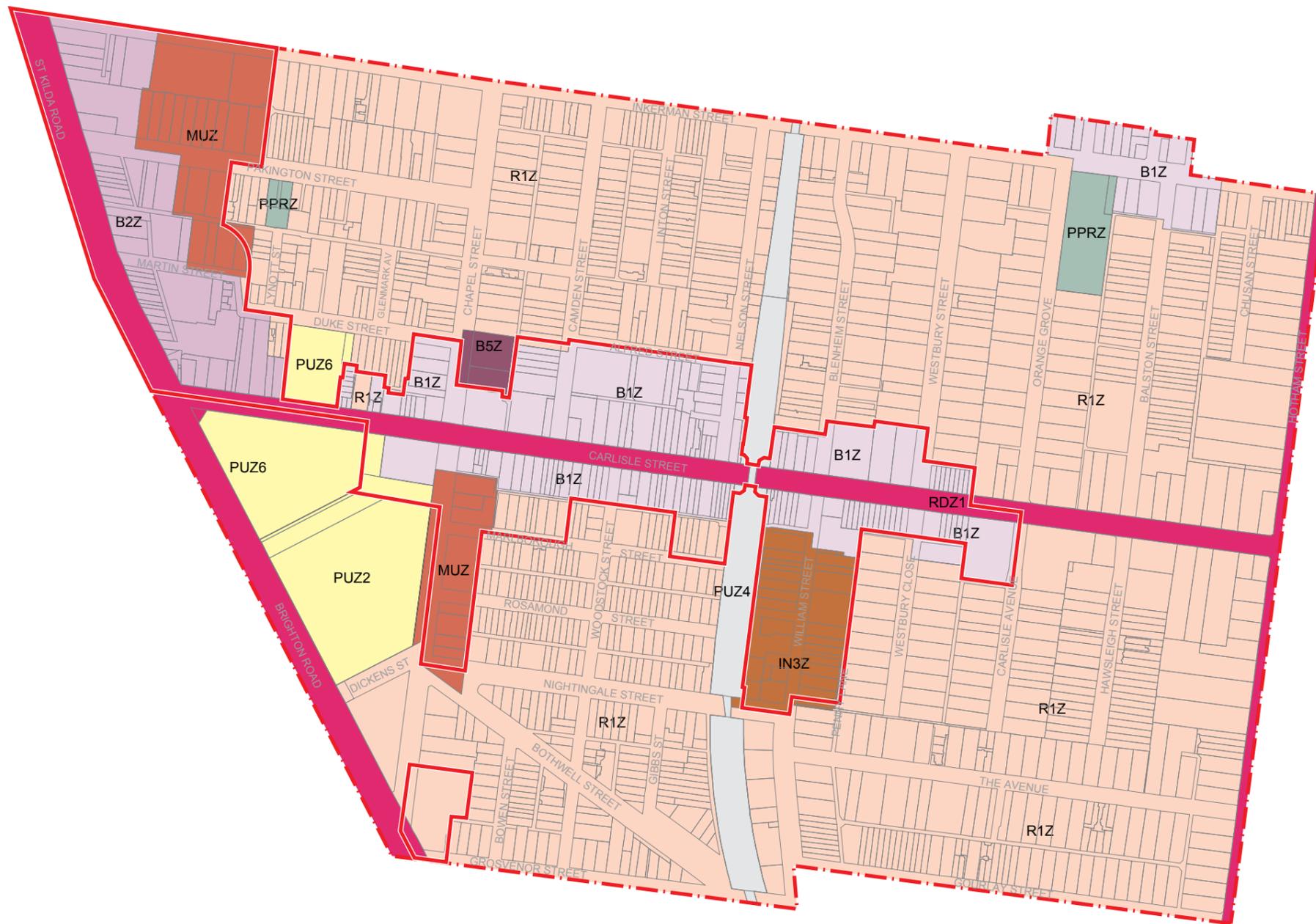
Also to the south of Carlisle Street is a mixed use zone along Chapel Street dominated by a new multi unit residential development and older factory buildings.

An Industrial 3 zone can be found to the south of Carlisle Street along William Street. This is the only remaining area of Industrial zoned land in St Kilda.

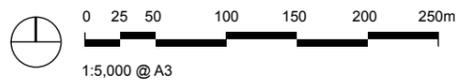
To the north west of Carlisle Street is a Business 2 zone that fronts St Kilda Road, this strip contains office uses as well as highway based retail uses.

Behind the Business 2 zone, on Inkerman, Pakington and Martin Streets, is a mixed use zone dominated by new commercial/residential developments such as the Aldi development with some existing factories still remaining.

The remainder of the study area is largely zoned Residential 1 with a small area of Business 1 zone on Inkerman Street that accommodates a neighbourhood centre and an area of Business 5 zone north of Carlisle Street occupied by the Police Station.



- R1Z Residential 1 Zone
- B1Z Business 1 Zone
- B2Z Business 2 Zone
- B5Z Business 5 Zone
- MUZ Mixed Use Zone
- IN3Z Industrial 3 Zone
- PUZ2 Public Use Zone 2
- PUZ6 Public Use Zone 6
- PPRZ Public Park and Recreation Zone
- Structure Plan Study Area
- Activity Centre Areas included in the Urban Design Framework



### 2.3 HERITAGE

A heritage overlay (H07) covers the greater portion of the Carlisle Street retail strip with the St Kilda Town Hall, St Kilda Post Office, St Kilda Primary School and Holy Trinity Church Complex protected under site specific heritage overlays and in some cases by a listing on the Victorian Heritage Register.

Additional buildings are currently being evaluated for addition to H07. They will potentially include some more contemporary (20th Century) buildings along Carlisle Street. An area of residential buildings south of Carlisle Street, generally between Marlborough and Rosamond Streets, is also currently subject of a proposed planning scheme amendment (C68) to expand the Heritage Overlay.

While the heritage overlays do not preclude development in all cases they may constrain the extent to which a property either covered by an overlay or adjacent an overlay may be developed.



## 2.4 BUILDING HEIGHTS

Building heights throughout the UDF study area are generally one and two storeys with greater building heights generally found along St Kilda Road, Chapel Street and Inkerman Street.

Carlisle Street has three different characters within the centre. West of Chapel Street building heights along the northern edge are almost exclusively one storey with any new higher development set back around ten metres. On the southern edge of this section stands the St Kilda Town Hall with the original building and tower setback from the street in a garden setting and a newer four storey section fronting Carlisle Street. East of the railway viaduct building heights are either one or two storeys with whole blocks being typically either one or the other. It is between Chapel Street and the railway viaduct where the heights along the street are the most varied with a mixture of one and two storey buildings alternating with each other.

Building heights along St Kilda Road are predominantly two storeys with some one and three storey buildings in the mix. Due to the 60 metre width of St Kilda Road at this point the two storey building heights do little to enclose the street.

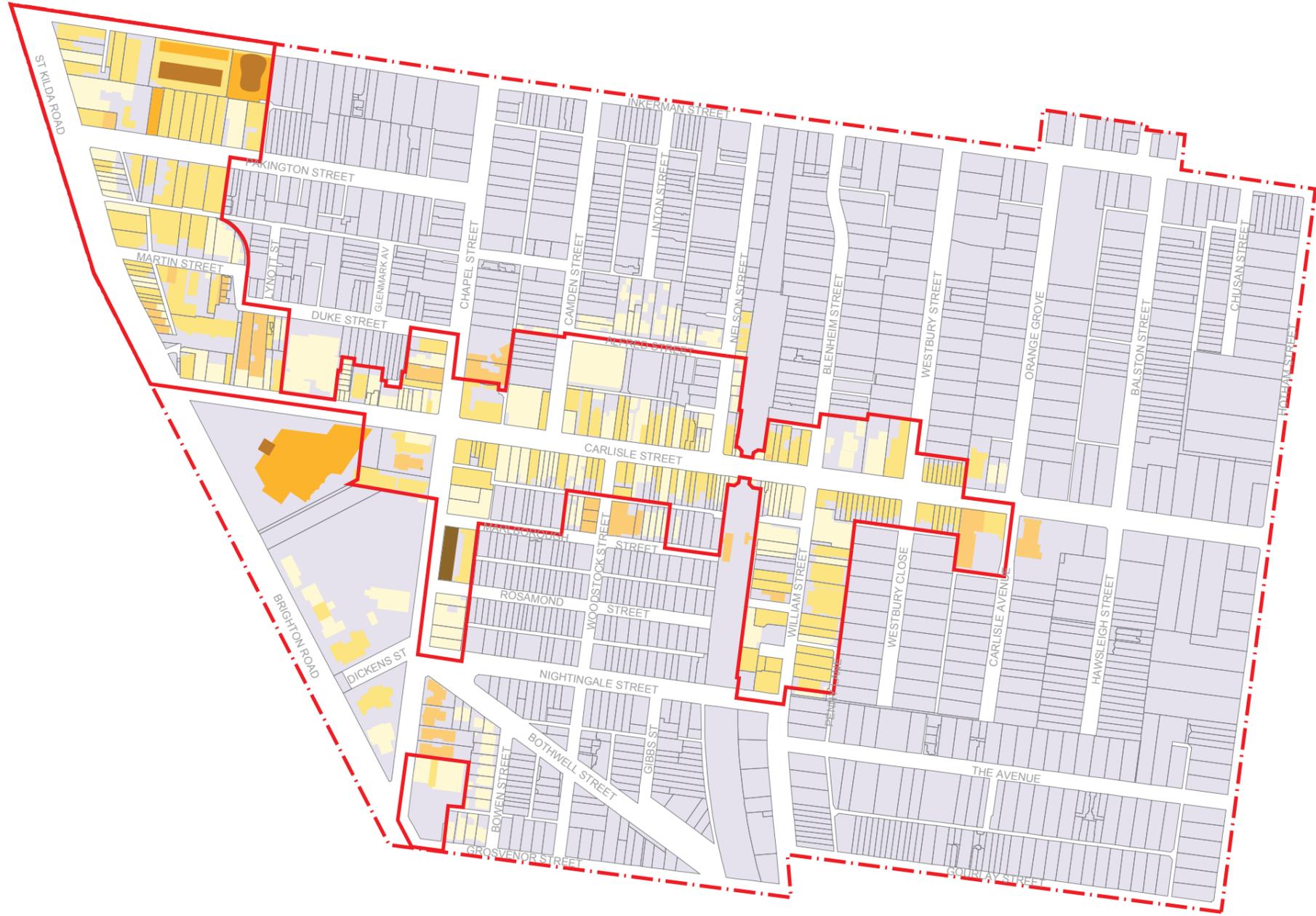
The eastern side of Chapel Street south of Carlisle Street is dominated by a six storey development between Marlborough and Rosamond Streets with the remainder of the street made up of single storey factory buildings and three storey walk-up flats.

The mixed use section of Inkerman Street is dominated by two new developments which have maximum heights of five storeys.

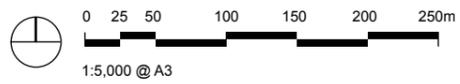
The mixed use precinct of Pakington, Martin and Duke Streets is dominated by two storey factory buildings with some buildings of up to four storeys, where residential conversion has occurred.

The William Street Industrial Precinct contains factory buildings which are mainly two storeys in height, although recent development is at three levels.

The surrounding residential areas within the structure plan study area are predominantly developed with a mix of traditional single storey detached housing, 2 and 3 level 'walk-up' flats and some contemporary medium density development.



- 1 Storey
- 2 Storey
- 3 Storey
- 4 Storey
- 5 Storey
- 6 Storey
- Structure Plan Study Area
- Activity Centre Areas included in the Urban Design Framework



## 2.5 ICONS AND LANDMARK BUILDINGS

A number of iconic buildings and landmarks exist throughout the study area. Some of these are significant because of their historic nature however others are significant either because of their height and bulk or their cultural significance to the local population.

There are a number of significant civic, institutional and religious buildings, most commonly with towers and spires that announce their importance, that act as significant landmarks. These are:

- St Kilda Town Hall
- St Kilda Parish Mission Uniting Church
- St Kilda Primary School
- Holy Trinity Anglican Church
- St Colman's School Hall and Church
- Former St Kilda Post Office
- St Kilda Public Library

The railway viaduct that runs north-south through the study area creates landmarks wherever it crosses the major east-west streets in the form of the large bluestone embankments supporting the railway bridges. The railway crossing of Carlisle Street has been accentuated by the installation of a public artwork entitled the "Lady of St Kilda".

Grand private buildings, including shops along Carlisle Street also act as landmarks with the shop at 292 Carlisle Street being a good example with its pitched roof like tower and viewing deck.

A number of landmark buildings exist that stand out purely because of their bulk and greater height. These include:

- 13 storey housing commission tower on Inkerman Street (outside the Structure Plan study area)
- 135 Inkerman Street (Aldi Development)
- 163-169 Inkerman Street
- 128-140 Chapel Street

Other iconic buildings are those such as the Hole in the Wall cafe which have attained their iconic status due to their land uses and importance to the local community, rather than their built form.



## 2.6 TOPOGRAPHY, VIEWS AND VISTAS

The Carlisle Street Study Area lies in a shallow basin with bluestone drainage channels running between Pakington and Martin Streets ensuring that rainwater is carried away from the low lying land to the north of Carlisle Street. The most substantial and noticeable topography is artificial in the form of the Railway embankment which carries the train lines from north to south across the basin. This embankment is most noticeable at the points where it crosses Inkerman, Carlisle, Nightingale and Grosvenor Streets as bridges with sturdy retaining walls. Otherwise it is hidden behind residential and industrial buildings.

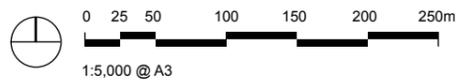
The gentle sloping of the land ensures that prominent views and vistas focus on man-made topography, such as the railway embankments, and iconic and landmark buildings rather than on natural features of the topography or sweeping views from a natural highpoint.

Prominent views within the centre are from the public realm, generally the street, towards prominent buildings such as the St Kilda Town Hall, numerous Churches, railway bridges or iconic buildings. Upon arrival at Balaclava Station views can also be had across the rooftops towards the spires of the St Kilda Town Hall and St Kilda Primary School.

Visible from along the entire length of Carlisle Street from Chapel Street to Orange Grove/Carlisle Avenue are views of the railway bridge and the "Lady of St Kilda" public artwork. These views down the length of Carlisle Street terminating at the railway bridge are some of the most dominant in the centre.



- 4m
- 5m
- 6m
- 7m
- 8m
- 9m
- 10m
- 11m
- 12m
- 13m
- 14m
- 15m
- Waterway/drainage line
- Icon/Landmark
- Railway Embankment
- View
- Vista
- Structure Plan Study Area
- Activity Centre Areas included in the Urban Design Framework



## 2.7 MOVEMENT NETWORK

The road hierarchy is made up of a grid of local streets with the diagonal of St Kilda and Brighton Roads and the north-south Hotham Street denoted as primary main roads. Carlisle Street is denoted as a secondary main road while Chapel and Inkerman Streets are collector roads. Westbury Street is a local collector.

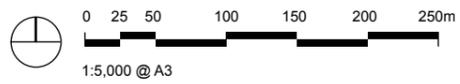
The nature of St Kilda Road, Brighton Road, Inkerman Street and Hotham Street as a focus of vehicle traffic has caused them to become barriers to pedestrian movement. This was one reason why they were chosen to delineate the edge of the study area.

The pedestrian movement network is based around and supported by the grid of local streets spaced at distances of between 100 and 250 metres. The regular nature of this pattern aids the legibility of the network for pedestrians. In places where large areas of open car parking dominate, this legibility can be compromised.

A finer grain of pedestrian only movement exists in parts where arcades, laneways and shared use paths augment the road network and give pedestrians a greater choice in the route they choose to take. These links take the form of the shared use paths either side of the rail line or the mid block link between Inkerman and Pakington Streets.



-  Gateway corner locations
-  Pedestrian route - footpath
-  Pedestrian route - arcade/path
-  Inactive edge
-  At grade car parking
-  Significant barrier to pedestrians
-  Structure Plan Study Area



## 2.8 PUBLIC TRANSPORT

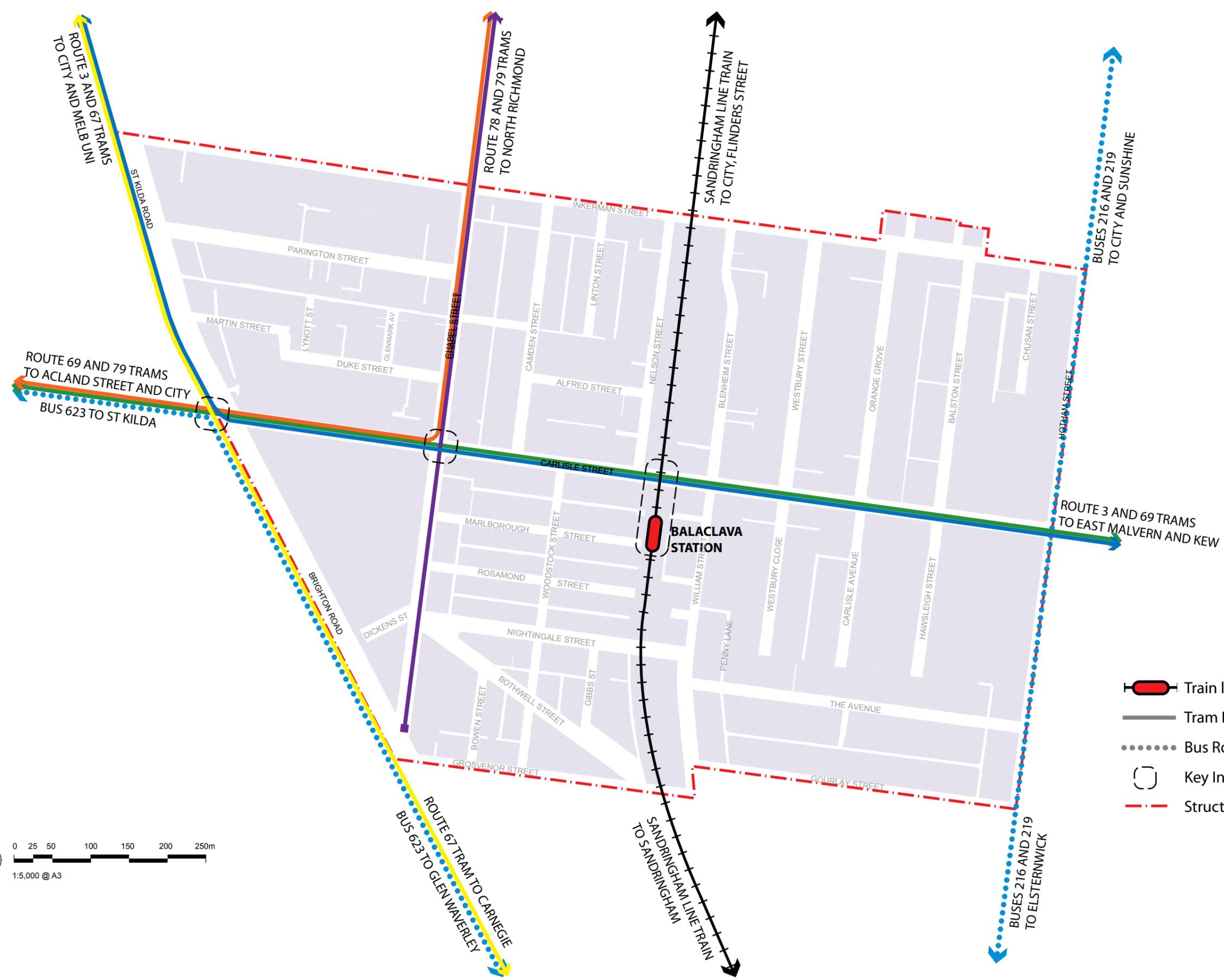
A key concept of Melbourne 2030's Activity Centre Policy is to concentrate development in centres well served by public transport in order to reduce the use of and reliance on private vehicles.

The Carlisle Street Major Activity Centre is well served by public transport with almost the entire study area being within a ten minute walk of Balaclava Station on the Sandringham line. From Balaclava Station it is a 13 min ride to Flinders Street Station and 17 min to the beach at Sandringham.

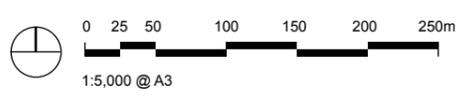
The centre is also served by 5 tram lines, two of which have a direct interchange with rail services at Balaclava Station. The trams run along Carlisle Street, Chapel Street, St Kilda Road and Brighton Road and connect the centre to St Kilda, the Melbourne CBD, Melbourne University, North Richmond, East Malvern, Kew and Carnegie.

Buses run along the eastern and western boundaries of the study area and connect to St Kilda, Elsternwick, Glen Waverley and Sunshine via the Melbourne CBD.

With such a rich supply of public transport infrastructure the Carlisle Street Activity Centre is well positioned to support a lifestyle with a reduced reliance on the private vehicle.



-  Train line and Station
-  Tram Route
-  Bus Route
-  Key Interchange Points
-  Structure Plan Study Area



## 2.9 OPEN SPACE

The study area contains a limited supply of open space although significant open space assets are located within walking distance (ie St Kilda Botanic Gardens and Alma Park). The St Kilda Town Hall Gardens and the small reserve behind the St Kilda Library are the only public green spaces within the retail centre of Carlisle Street. They are supplemented by the Pakington Street Reserve, the Hewison Reserve and the William Street Reserve, located well outside the retail heart of the centre.

The lack of open space within the centre reinforces the importance of the public realm, the streets, footpaths and spaces between buildings and the need for them to be of a high quality and pedestrian friendly nature.

Carlisle Street, as the principal movement spine and retail heart of the activity centre, is the most important open space in the study area.

## 2.10 SOLAR ACCESS TO PRIORITY STREETS

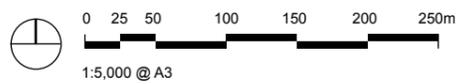
The southern footpath of Carlisle Street currently receives a minimum of approximately 2 hours of direct sunlight at the winter solstice. In some parts this period is even greater due to the sections of single storey buildings on the northern side of the street allowing the lower winter sun to penetrate to the southern footpath for even longer periods.

As the retail heart of the activity centre and a priority street, it is important that solar access to the southern footpath of Carlisle Street is maintained.

While solar access to the southern footpaths of other east-west streets is beneficial, development potential of the sites to the north of these streets should not be restricted to protect this access alone as they are not priority streets.



- Public Open Space
- Priority Street
- Structure Plan Study Area
- Activity Centre Areas included in the Urban Design Framework





# 3 *Urban Design Principles and Objectives*

---

Urban design principles for the Carlisle Street Activity Centre have been drawn from the key findings of the background analysis in this study and informed by the strategic intent of the Structure Plan.

They are supported with objectives and guidelines which set out in detail how these principles can be upheld in the design of new development and the public realm.

General objectives can be found on the following pages with precinct and site specific design objectives and guidelines to be found in Chapter 4.

### 3.1 PRINCIPLES

1. The design of buildings and spaces within the Carlisle Street Activity Centre will support the realisation of the vision, strategic directions and desired outcomes of the Structure Plan.
2. The footpaths, laneways and public spaces of the activity centre will be optimised as vibrant, safe and attractive places for pedestrian activity.
3. Reinforce the fundamental urban structure and built form elements which underpin the identity and legibility of the activity centre.
4. The unique built form and cultural heritage of the activity centre will be protected and enhanced by new development that responds to the established character of the centre and retains the diversity of its individual precincts.
5. New development across the activity centre will achieve architectural and urban design excellence, and will demonstrate best practice in sustainable design.
6. The design of new buildings will contribute to a high standard of amenity for residents across the activity centre and will protect the amenity of surrounding residential areas, especially at the 'interface' with the activity centre.
7. The Carlisle Street Activity Centre will be a sustainable transport hub, with pedestrian movement as the priority form of transport, followed by cycling and public transport. Cars will be accommodated in such a way that they will not dominate streets and will not detract significantly from the quality of the urban environment or the available sustainable transport options.
8. Development of a number of larger key sites will be facilitated to capitalise on their potential to achieve the desired change across the activity centre.

1. **The design of buildings and spaces within the Carlisle Street Activity Centre will support the realisation of the vision, strategic directions and desired outcomes of the Structure Plan.**

#### OBJECTIVES

- 1.1 To facilitate diversification of the activity mix of the centre, including additional retail activity, office space and community uses, and significant new residential development,.
- 1.2 To prevent the 'underdevelopment' of identified strategic sites within the activity centre, recognising their importance to ensuring the centre has capacity for growth to maintain its retail viability and the diversification of its activity mix.

**2. The footpaths, laneways and public spaces of the activity centre will be optimised as vibrant, safe and attractive places for pedestrian activity.**

**OBJECTIVES**

- 2.1 To ensure that new development is of a scale and form that does not overwhelm the public realm.
- 2.2 To ensure that buildings provide a clear definition between the public and private realm, to assist in defining the space of the street and to provide street enclosure.
- 2.3 To ensure year round solar access to key pedestrian spaces, in particular the southern footpath along Carlisle Street.
- 2.4 To ensure that new development presents an active edge to the public realm.
- 2.5 To encourage balconies or terraces at upper levels that overlook the street.
- 2.6 To ensure pedestrian entries are clearly visible from the public domain.
- 2.7 To ensure that new development provides passive surveillance of the public realm.
- 2.8 To avoid wide building frontages with single uses and blank walls, and encourage smaller scale uses with active frontages to 'wrap' the edges of larger developments.
- 2.9 To limit the visibility of car parking from the public realm.
- 2.10 To ensure the design of new development incorporates awnings, verandas, vegetation etc. to create a micro-climate that promotes pedestrian activity.
- 2.11 To employ well-considered and creative lighting to encourage night time activity, create a safe environment and emphasise key buildings.
- 2.12 To incorporate Crime Prevention through Environmental Design (CPTED) Principles in the design of new development and the public realm.

**3. Reinforce the fundamental urban structure and built form elements which underpin the identity and legibility of the activity centre.**

**OBJECTIVES**

- 3.1 To maintain the 'linear' heritage streetscape and 'human scale' of the traditional shopping strip along Carlisle Street.
- 3.2 To reinforce, reinstate and extend the fine grain 'grid' pattern of streets and laneways, especially within the retail precinct.
- 3.3 To encourage innovative, contemporary architecture in laneways.
- 3.4 To ensure that the frontage design of larger or consolidated sites (over 10 metres) reflects the predominant scale of the wider streetscape.
- 3.5 Identify important 'corner' and 'entry' sites and ensure development contributes to a sense of arrival to the activity centre.
- 3.6 To protect and maintain views of landmark buildings, urban icons and 19th century railway infrastructure within the activity centre.
- 3.7 To maintain the characteristic hard edged alignment of built form to the street.
- 3.8 To promote a consistent street-wall height as a unifying element within precincts across the activity centre.
- 3.9 To reconnect the surrounding residential areas to the activity centre where currently segregated by large 'at grade' car parks.

**4. The unique built form and cultural heritage of the activity centre will be protected and enhanced by new development that responds to the established character of the centre and retains the diversity of its individual precincts.**

**OBJECTIVES**

- 4.1 To ensure new development, including public realm treatments, builds upon the diverse and distinct built form character, as defined by setbacks, heights, building rhythm and materials, of individual precincts.
- 4.2 To ensure new development in a heritage overlay area complies with the Port Phillip Heritage Policy at Clause 22.04 of the Planning Scheme.
- 4.3 To protect and/or encourage the adaptive reuse of identified heritage buildings and urban icons (as identified in the City of Port Phillip Urban Iconography Study, 2004), and buildings which contribute to the character of a precinct (as identified in the City of Port Phillip Neighbourhood Character Policy Map, 2007).
- 4.4 To encourage new development to creatively reinterpret existing architectural themes.
- 4.5 To encourage new contemporary development that continues the eclectic mix of building styles characteristic of the activity centre.
- 4.6 To define streetscapes with planting themes and public realm treatments that reflect the desired urban character of each precinct.
- 4.7 To encourage public and urban art that expresses the identity and cultural heritage of the community.
- 4.8 To ensure that advertising signage makes a positive and innovative contribution to the urban environment.

**5. New development across the activity centre will achieve architectural and urban design excellence, and will demonstrate best practice in sustainable design.**

**OBJECTIVES**

- 5.1 To ensure that new development contributes positively to the character of the activity centre through contemporary best practice architecture.
- 5.2 To ensure that new development is of a high architectural standard in terms of its form, scale, massing, articulation, roof profile and materials, and that these design elements respond appropriately to the streetscape context.
- 5.3 To ensure that new development achieves a range of sustainable design outcomes through the use of Ecologically Sustainable Design (ESD) and Water Sensitive Urban Design (WSUD) principles.
- 5.4 To ensure buildings are flexibly designed to enable adaptation to a variety of future uses.

**6. The design of new buildings will contribute to a high standard of amenity for residents across the activity centre and will protect the amenity of surrounding residential areas, especially at the 'interface' with the activity centre.**

**OBJECTIVES**

- 6.1 To ensure the amenity of adjoining residential properties is not unreasonably adversely impacted by new development.
- 6.2 To ensure new development abutting Residential 1 Zone properties complies with the side and rear setback, overlooking and overshadowing objectives of Clause 55 (ResCode standards)
- 6.3 To ensure that new development at increased densities respects the scale of and provides a sensitive transition to any adjoining low scale residential buildings.
- 6.4 To ensure that new residential developments within and proximate to the activity centre are provided with adequate visual and acoustic privacy.
- 6.5 To ensure that new residential developments have safe, convenient and identifiable residential addresses.
- 6.6 To ensure that new residential developments are sited, oriented and configured to receive adequate solar access, natural light and natural ventilation.

- 7. The Carlisle Street Activity Centre will be a sustainable transport hub, with pedestrian movement as the priority form of transport, followed by cycling and public transport. Cars shall be accommodated in such a way that they will not detract significantly from the quality of the urban environment or the available sustainable transport options.**

**OBJECTIVES**

- 7.1 To improve access to, the transfer between transport modes, and the overall amenity of the Balaclava Railway Station and environs.
- 7.2 To enhance the amenity, safety and access of tram stops.
- 7.3 To design the road network in such a way that gives pedestrians highest priority.
- 7.4 To maintain the laneways and smaller streets of the activity centre as significant pedestrian connections.
- 7.5 To promote more intense levels of development in close proximity to public transport infrastructure.
- 7.6 To ensure new development on key pedestrian routes make a positive contribution to the amenity of the street for pedestrians.

- 8. Development of a number of larger key sites will be facilitated to capitalise on their potential to achieve the desired change across the activity centre.**

**OBJECTIVES**

- 8.1 To encourage an optimum level of development on key sites.
- 8.2 To encourage an appropriate mix of land uses, relevant to the location of the sites within the activity centre.
- 8.3 To ensure development on key sites sets a high standard of design and acts as a benchmark for future development in the activity centre.
- 8.4 To ensure that larger sites express the 'fine urban grain' of their surroundings.

## ACTIVE EDGES

Active frontages allow a high degree of connection between the public realm or street and the interior of the building, providing a vibrant and interesting public realm. Active frontages also contribute to casual surveillance and perceptions of improved safety through more eyes on the street.

Across the activity centre, there will be three different degrees of 'activeness' depending on the ground floor uses. These active edges are defined as:

**Retail Active Edge** - Characterised by a predominantly clear-glazed facade from footpath level to a height of 2 metres with pedestrian entries at least every 15 metres.

**Commercial Active Edge** - at least 50% clear-glazed between a height of 1 metre and 2 metres above footpath level with pedestrian entries at least every 30 metres.

**Residential Active Edge** - many entries to individual dwellings from the public realm and clear glazing. Entries should be set back from the street and may be within private open space or elevated above street level to provide privacy whilst still allowing surveillance.

A building should also provide an active edge at upper levels, using windows and balconies to facilitate a connection between building occupants and persons in the public realm.



## 4 Precinct Guidelines

---



Northern Elevation



Southern Elevation (not part of Carlisle Street West Precinct)

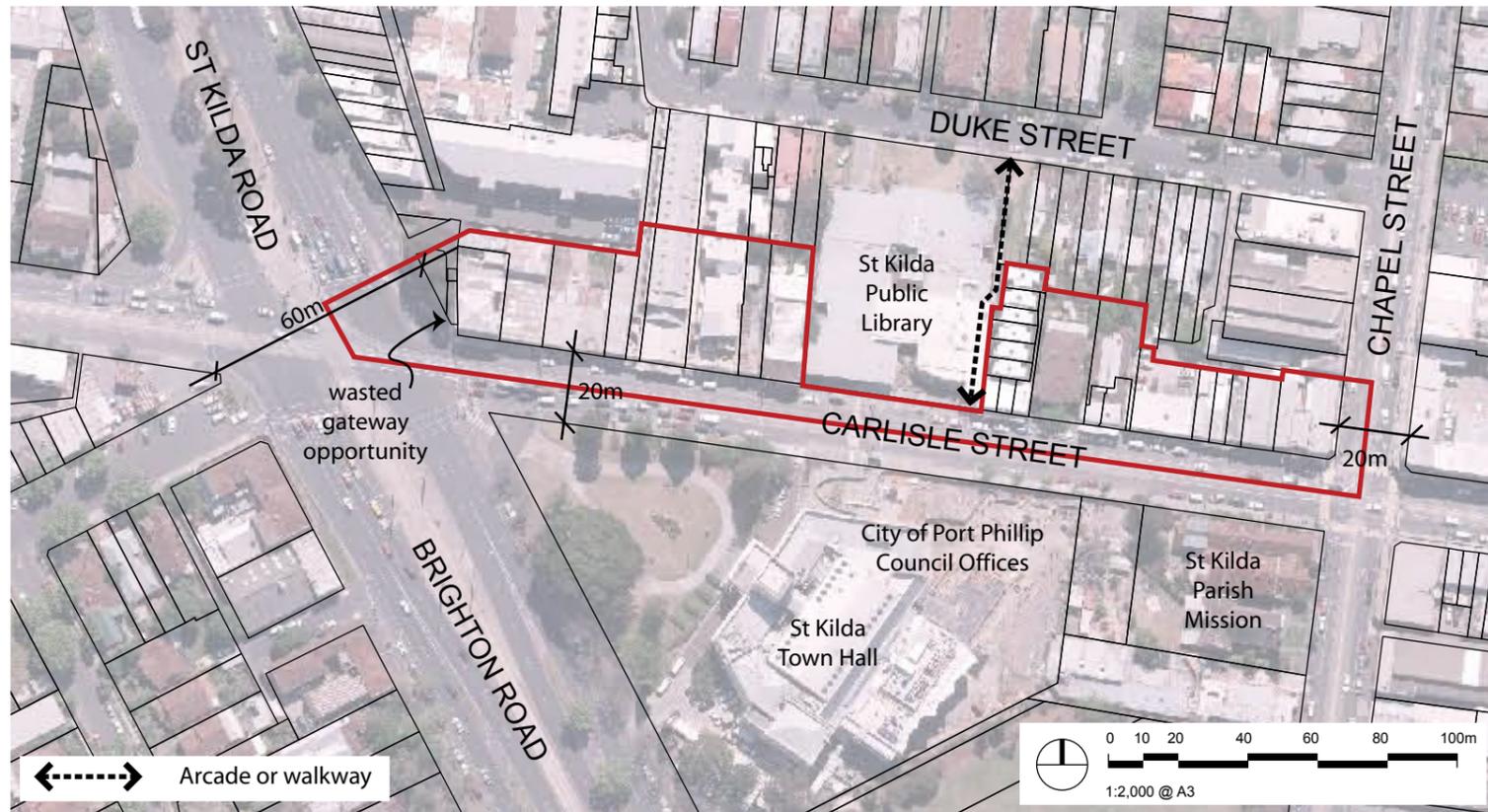
## 4.1 CARLISLE STREET WEST PRECINCT

### EXISTING URBAN CHARACTER

The Carlisle Street West Precinct is an extension of the retail spine of the Activity Centre but also contains a mixture of housing and community facilities.

- Dominated by the St Kilda Town Hall on the southern side of Carlisle Street. A 4 storey extension is currently being completed parallel with the Carlisle Street frontage.
- Distinctive single storey library building located on the northern side of Carlisle Street opposite the St Kilda Town Hall.
- Predominantly single storey development. Some additional height, associated with new development, set back well behind the existing parapets.
- Mixture of original Victorian and Edwardian styles to inter-war and post war styles.

- Abutting the mixed use and business precincts to the north west.
- Parallel parking along both kerbs with footpath extensions at the existing pedestrian crossing point between the Library and Town Hall.
- Predominantly retail uses on the ground floor with some apartments and residences either above or on the ground floor.
- Building frontages typically greater than 10 metres west of the library and between 5 and 10 metres wide to the east. All buildings have a zero setback from Carlisle Street other than the library and the apartment building at number 162.
- Fewer verandas along the northern edge of this section of Carlisle Street than to the east of Chapel Street.
- The western edge of the centre is defined by the intersection of Carlisle Street and St Kilda Road. This section of St Kilda Road was widened in the 1970's and is a major and divisive road that restricts pedestrian flows.



- As a result of the 1970's road widening a building and park now stand on the corner of Carlisle Street and St Kilda Road that were not originally on a corner. This building and park do not create a strong corner treatment.
- Access from Duke Street to Carlisle Street via an internal link through the library.
- No street trees or any vegetation within the road reserve.
- Substantial tree plantings within the property line are associated with the St Kilda Town Hall, the St Kilda Parish Mission and a residential flat building at 162 Carlisle Street.
- Less footpath activity than the eastern section of Carlisle Street.

#### 4.2 PREFERRED CHARACTER STATEMENT

This precinct will be characterised by:

A pedestrian environment with activated frontages that supports this precinct as an extension of the vibrant and active shopping strip to the east of Chapel Street.

A high quality public realm and enhanced public spaces which support its role as the centre's 'civic node' and community services destination.

The dominance of civic / community buildings, in particular the St Kilda Town Hall and Library.

A legible single storey built form on the northern side of Carlisle Street.

### DESIGN OBJECTIVES

*These design objectives are additional to the general objectives found in Section 3. They must be read and applied in conjunction with the general objectives and principles found in section 3.*

To ensure the visual prominence of the St Kilda Town Hall is maintained.

To ensure that future development on the north-east corner of St Kilda Road and Carlisle Street reflects its important location at a key point of entry into the activity centre.

To protect and enhance the open space fronting the St Kilda Town Hall as the primary green space of the activity centre.

To develop the St Kilda Town Hall public plaza (facing Carlisle Street) and the Library forecourt as key open space areas in the activity centre.

To enhance the pedestrian experience of this part of Carlisle Street, as an extension of the centre's principal pedestrian spine.

To ensure new development maintains the predominantly low rise character on the northern side of Carlisle Street.

To ensure that new development above the street wall is legible as a separate and visually recessive built form element.

To ensure that larger sites express the 'fine urban grain' of their surroundings.

### DESIGN REQUIREMENTS

Buildings must have a zero setback to the street frontage.

The streetwall parapet height must be no greater than 6 metres; this is equivalent to the prevailing height of existing facades along the northern side Carlisle Street.

Buildings should not exceed a preferred maximum height of 10 metres (3 storeys).

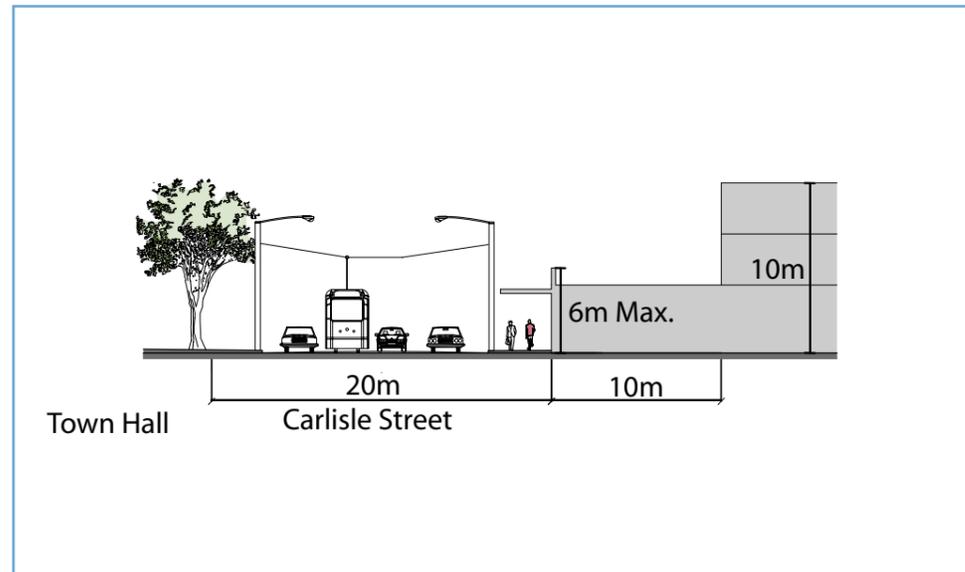
Above the streetwall additional storeys must be set back 10 metres so as to be viewed as separate forms.

The streetwall must have a veranda for the full width of the frontage. This veranda should match those of neighbouring properties in terms of height above and coverage of the footpath.

The streetwall should be built to the side boundaries.

The streetwall should have vertical articulation and window proportions and locations that reflect those of the neighbouring buildings both above and below the veranda line.

The streetwall must have a parapet. The parapet should be well articulated and contain detail consistent with the character of Carlisle Street.



On corner sites, buildings must address both street frontages with either doors or street level windows.

Architectural features above the existing parapet are encouraged on the corner of Chapel and Carlisle Streets and Carlisle Street and St Kilda Road, provided they are integrated with the design of the building.

Side walls visible above adjoining buildings should be visually recessive through variations in forms, materials, openings or colours.

The ground floor must be level with the footpath.

The ground floor facade should be designed to facilitate contact between building occupants and people in the street. It should include pedestrian entries and clear glazing.

The building facade on sites with a frontage over 10 metres must be well articulated through variations in forms, materials, openings or colours or the inclusion of vertical design elements. This will ensure that the finer grain of the precinct is continued.

Vehicular access must be provided from the rear or side of lots wherever possible. Where it is not possible new garage doors must be integrated with the design of the facade.

Side and rear setbacks at boundaries with properties zoned Residential 1 should comply with the objectives of Clause 55.04-1 (side and rear setbacks objective) of the Port Phillip Planning Scheme.

Development at boundaries with properties zoned Residential 1 should comply with the objectives of Clause 55.04-5 (overshadowing open space objective) of the Port Phillip Planning Scheme.

Solar access must be maintained to the southern footpath of Carlisle Street between 11am and 2pm on the 21st of June.



Northern Elevation



Southern Elevation

## 4.2 CARLISLE STREET CORE RETAIL AND EASTERN PRECINCT

### EXISTING URBAN CHARACTER

The Carlisle Street East Precinct is the retail heart of the Activity Centre. It stretches along Carlisle Street from Carlisle Avenue in the east to Chapel Street in the west. It is a traditional strip shopping street with an eclectic mixture of single and double storey shops in a mixture of styles.

- Mixture of original Victorian and Edwardian styles to inter-war and newer modernist styles.
- Predominantly separated from the surrounding residential areas by rear laneways and 'at grade' car parks.
- Parallel parking along both kerbs with footpath extensions and street tree plantings at side streets and at key intersections and pedestrian crossing points.
- Retail uses on the ground floor with some offices, apartments and residences above.
- Building frontages typically between 5 and 10 metres wide with a zero setback from Carlisle Street.
- Some wider building frontages due to lot consolidation by the arcades and supermarkets during their redevelopment.

- Relatively few street trees when compared with surrounding residential streets however shelter from the elements is provided by almost continuous awnings and verandas.
- Footpaths packed with pedestrians arriving in the centre by tram and train or walking from the car parks and shoppers walking the length of the centre to purchase goods.
- Balaclava Station entry hidden from view behind trees, shops and retaining walls.
- Balaclava Station accessed via non DDA compliant ramps located in narrow gaps in the street wall.
- Eastern part of the precinct dominated by specialty stores providing Jewish goods and services.
- The western section displays an eclectic juxtaposition of tall against short with blank brick side walls visible through gaps and large parapets on single storey shops filling some of the gaps. This mixture of single and double storey buildings interspersed at random is due to the original construction of separate single shopfronts.



**PREFERRED CHARACTER STATEMENT**

This precinct will be characterised by:

- To the east a more uniform streetscape has resulted from the shops being predominantly developed in sets of between two and nine.
- The southern block between William Street and the station is comprised of single storey shops with the remaining eastern streetscape dominated by two storey buildings apart from the BP service station on the corner of Blenheim Street and the neighbouring Balaclava Fresh Centre.
- Footpaths filled with displays of wares for sale in the shops and tables for outdoor dining, resulting in some congestion.
- Pedestrian environment severely compromised by the BP Service Station which creates a break in the zero setback of the retail strip and also results in a significant break in the continuous verandas of the street.
- The Safeway Supermarket, with its blank facade onto Carlisle Street and loading bay access across the footpath, does little to promote a pedestrian friendly environment.

A vibrant, traditional shopping strip which includes a diverse mix of uses, with a focus on core retailing at the street frontage.  
 A consistent built form that retains the heritage scale of Carlisle Street.  
 A key pedestrian thoroughfare that is safe, comfortable and attractive.  
 Traditional heritage shop-fronts contrasted by contemporary design.  
 Unique building features, public art and signage in new development and public realm improvements which strengthen the identity of the strip.



### DESIGN OBJECTIVES

These design objectives are additional to the general objectives found in Section 3. They must be read and applied in conjunction with the general objectives and principles found in section 3.

To protect and enhance the built form character typical of a 19th century strip shopping centre.

To maintain visual consistency and a sense of streetscape enclosure.

To ensure that new development is designed to complement the heritage scale of Carlisle Street and is not dominant in the streetscape.

To ensure new development contributes to the eclectic mix of building types within Carlisle Street.

To enhance the pedestrian experience of Carlisle Street as the principal pedestrian spine of the activity centre.

To emphasise corner sites through built form and street level activity.

To ensure that larger sites express the 'fine urban grain' of their surroundings.

To ensure new development maintains year round solar access to the southern footpaths of Carlisle Street.

To minimise the impact of new development on the solar access of residential properties to the south.

### DESIGN REQUIREMENTS

Buildings must have a zero setback to the street frontage.

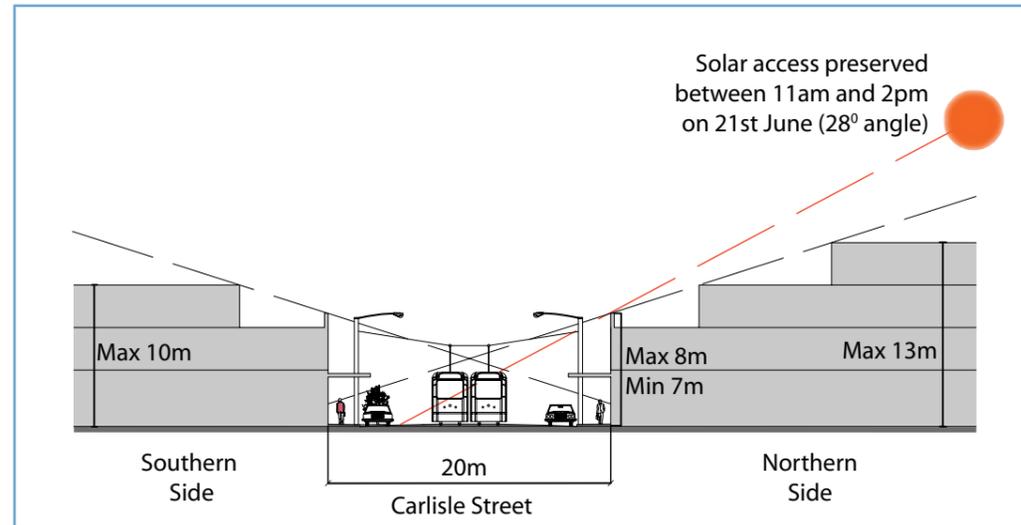
The streetwall parapet must be a minimum of 7 metres to a maximum of 8 metres in height; this is equivalent to the height of existing two storey facades in Carlisle Street.

Buildings should not exceed a preferred maximum height of 13 metres (4 storeys) on the northern side of Carlisle Street and 10 metres (3 storeys) on the southern side, except for the property on the corner of Carlisle Street and Carlisle Avenue which should not exceed a preferred maximum height of 13 metres.

Above the streetwall, additional storeys must be set back so as not to be visible when viewed from standing eye level (1.6m) at the street frontage directly across the street.

The above setback also applies to the side street boundary on corner lots, except in Camden Street where the third storey may come to the streetwall and a fourth storey must be set back 5 metres so as to be visually recessive. This setback may be reduced by up to 2 metres if the architecture of the upper level renders it distinctly different and visually recessive through variations in form, material, openings or colour.

The streetwall must have a veranda for the full width of the frontage. This veranda should match those of neighbouring properties in terms of height above and coverage of the footpath.



The streetwall should be built to the side boundaries.

The streetwall should have vertical articulation and window proportions and locations that reflect those of the neighbouring buildings both above and below the veranda line.

The streetwall must have a parapet. The parapet should be well articulated and contain detail consistent with the character of Carlisle Street.

On corner sites, buildings must address both street frontages with either doors or street level windows.

Architectural features above 13 metres in height are encouraged on the corner of Chapel and Carlisle Streets and Carlisle Street and Carlisle Avenue, provided they are integrated with the design of the building.

Side walls visible above adjoining buildings should be visually recessive through variations in forms, materials, openings or colours.

The ground floor must be level with the footpath.

The ground floor facade should be designed to facilitate contact between building occupants and people in the street. It should include pedestrian entries and clear glazing.

The building facade on sites with a frontage over 10 metres must be well articulated through variations in forms, materials, openings or colours or the inclusion of vertical design elements. This will ensure that the finer grain of the precinct is continued.

Vehicular access must be provided from the rear or side of lots wherever possible. Where it is not possible new garage doors must be integrated with the design of the facade.

Side and rear setbacks at boundaries with properties zoned Residential 1 should comply with the objectives of Clause 55.04-1 (side and rear setbacks objective) of the Port Phillip Planning Scheme.

Development at boundaries with properties zoned Residential 1 should comply with the objectives of Clause 55.04-5 (overshadowing open space objective) of the Port Phillip Planning Scheme.

Solar access must be maintained to the southern footpath of Carlisle Street between 11am and 2pm on the 21st of June.



St Kilda Road Elevation

### 4.3 ST KILDA ROAD BUSINESS PRECINCT

#### EXISTING URBAN CHARACTER

The St Kilda Road Business Precinct stretches along the eastern side of St Kilda Road between Inkerman Street in the North and Carlisle Street in the south. It currently contains a mixture of narrow older shops and newer wider shops and offices.

- 4 lanes of vehicle traffic each way plus turning lanes and bike lanes.
- Central median with tram lines and native tree plantings.
- Mixture of building styles from Victorian through to modern.
- Separated from the mixed-use area behind by rear laneways.
- A lack of consistent character in the development opposite on St Kilda Road is due to the disjointed lot structure caused by the 1970's road widening, it is a mixture of residential and business uses between one and three storeys in height.
- Parallel parking along the kerb with left-in, left-out turning movements only from Pakington and Martin Streets.
- Pedestrian crossing points to residential areas to the west of St Kilda Road are restricted to Inkerman and Carlisle Streets.
- St Kilda drain runs through the precinct parallel and between Pakington and Martin Streets.
- A mixture of commercial/office uses on the ground floor with few offices above.

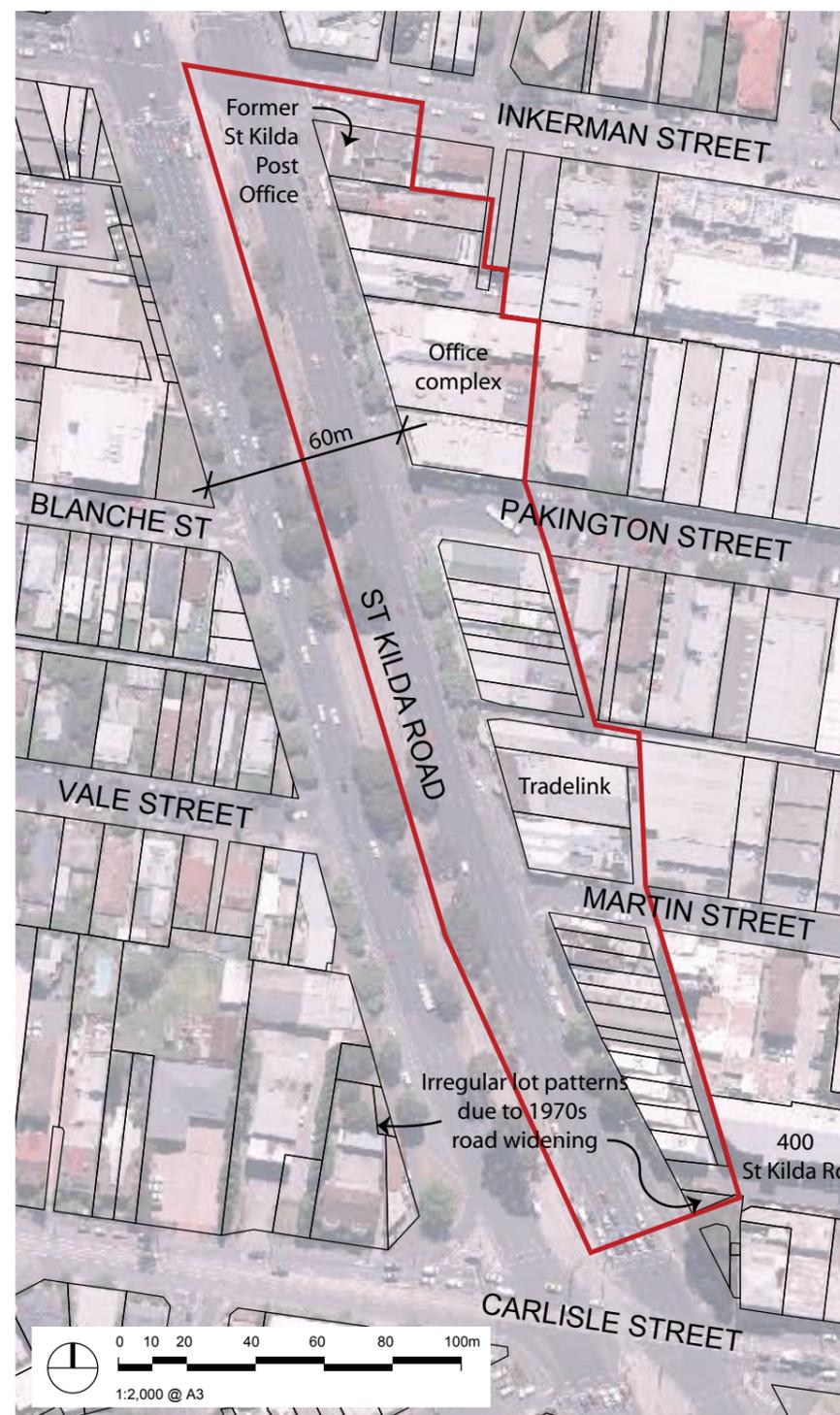
- All buildings in the precinct built to the front boundary with most also built to both side boundaries.
- Original finer grained subdivision pattern evident in places with frontages typically between 5 and 10 metres wide.
- Some wider frontages to St Kilda Road as a result of consolidation for use as office complex or trade supplies.
- Mixture of predominantly single and double storey buildings.
- Prominent corner feature on the old hotel on the northern corner of Pakington Street.
- The modern three storey buildings in the southern block are a result of the road widening of the 1970s.
- A significant number of native street trees when compared with surrounding residential streets
- Footpaths are wide but devoid of activity due to lower levels of amenity and the nature of commercial and specialised retail uses
- Some properties provide awnings and verandas however the inconsistent nature of this provision is a deterrent to pedestrian movement.
- A property in Duke Street, part of the mixed use precinct behind, uses a break in the built form of St Kilda Road to claim the 400 St Kilda Road address.

### PREFERRED CHARACTER STATEMENT

This precinct will be characterised by:

A built form that supports a mixture of uses, including offices and display based commercial uses at street level, with potential for residential apartments at upper levels.

A high standard of design in new and refurbished buildings and the public realm, with emphasis on built form that provides a sense of street enclosure and protection from the elements for pedestrians.



### DESIGN OBJECTIVES

*These design objectives are additional to the general objectives found in Section 3. They must be read and applied in conjunction with the general objectives and principles found in section 3.*

To promote design excellence in new development, appropriate to this high profile edge to the Carlisle Street Activity Centre.

To ensure new development contributes to a more coherent edge to St Kilda Road.

To ensure new development respects the heritage character of the former St Kilda Post Office and the heritage streetscape of St Kilda Road between Inkerman and Pakington Streets.

To enhance the amenity, human scale and experience of St Kilda Road as a pedestrian movement corridor.

To encourage ground floor design that contributes to interaction with streetscape activity and balconies or terraces at the upper levels that overlook the street.

To ensure residential development is designed to minimise the impacts of noise from other uses on residents.

### DESIGN REQUIREMENTS

Buildings must have a zero setback to the street frontage.

The streetwall parapet height must be no more than 10 metres in height; this will allow three storeys of development.

Buildings should not exceed a preferred maximum height of 13 metres (4 storeys).

Above the streetwall an additional storey must be set back 5 metres so as to be visually recessive. This setback may be reduced by up to 2 metres if the architecture of the upper level renders it distinctly different and visually recessive through variations in form, material, openings or colour.

The above setback also applies to the side street boundary on corner lots.

The streetwall must have a veranda for the full width of the frontage. This veranda should match those of neighbouring properties in terms of height above and coverage of the footpath.

The streetwall should be built to the side boundaries.

The streetwall must have a parapet. The parapet should be well articulated and contain detail consistent with the character of St Kilda Road.



On corner sites, buildings must address both street frontages with either doors or street level windows.

Architectural features above 13 metres in height are appropriate on corner sites provided they are integrated with the design of the building.

Side walls visible above adjoining buildings should be visually recessive through variations in forms, materials, openings or colours.

The ground floor facade should be designed to facilitate contact between building occupants and people in the street. It should include pedestrian entries and clear glazing.

The facade of the ground floor of sites with a frontage over 10 metres must be well articulated through variations in forms, materials, openings or colours or the inclusion of vertical design elements. This will ensure an interesting and finer grained form more conducive to pedestrian activity.

Vehicular access must be provided from the rear or side of lots.



#### 4.4 INKERMAN/PAKINGTON STREETS MIXED-USE PRECINCT

##### EXISTING URBAN CHARACTER

The Inkerman/Pakington mixed-use precinct is a former light industrial area which has been rezoned to mixed-use and has started on a transition towards a mixture of office, commercial and residential uses.

- Mixture of post war warehouses, contemporary warehouse conversions and new mixed use developments.
- Inkerman Street has a consistent treatment of Bottlebrushes (Callistemon) planted as street trees. Street tree plantings in Pakington and Martin Streets are smaller native species providing little shelter from the elements.
- Separated from the neighbouring St Kilda Road Business Precinct to the west by lanes.
- Residential areas to the east hard-up against existing light industrial and mixed use development.
- Building frontages typically greater than 10 metres wide.
- Development typically has a zero front setback and is fully built out for the entire street frontage.
- Buildings on the northern side of Martin Street are set back behind at grade car parks.
- The ground floors of buildings on the northern side of Pakington Street are set back behind at grade car parking with upper storeys flush with the lot boundary creating an undercroft situation.
- Wider street frontages due to lot consolidation by the mixed use developments along Inkerman Street.
- Predominantly double storey building heights.
- A through block link between Inkerman Street and the pedestrian crossing point on Pakington Street that connects through, via the park, to Duke Street and the library.
- New 5 storey mixed-use developments have been constructed along Inkerman Street and contain some retail and office uses including an Aldi Supermarket.
- The block south of Martin Street has been almost entirely converted to warehouse apartments with some commercial uses on the ground floors.
- Ancillary office uses occupy either the ground floor or the floor above the undercroft parking of industrial uses.
- New development in the north and south of the precinct reaches 4 and 5 storeys.
- Lack of encouragement for pedestrian movement as footpaths are dominated by vehicle crossovers and parked cars.

### PREFERRED CHARACTER STATEMENT

This precinct will be characterised by:

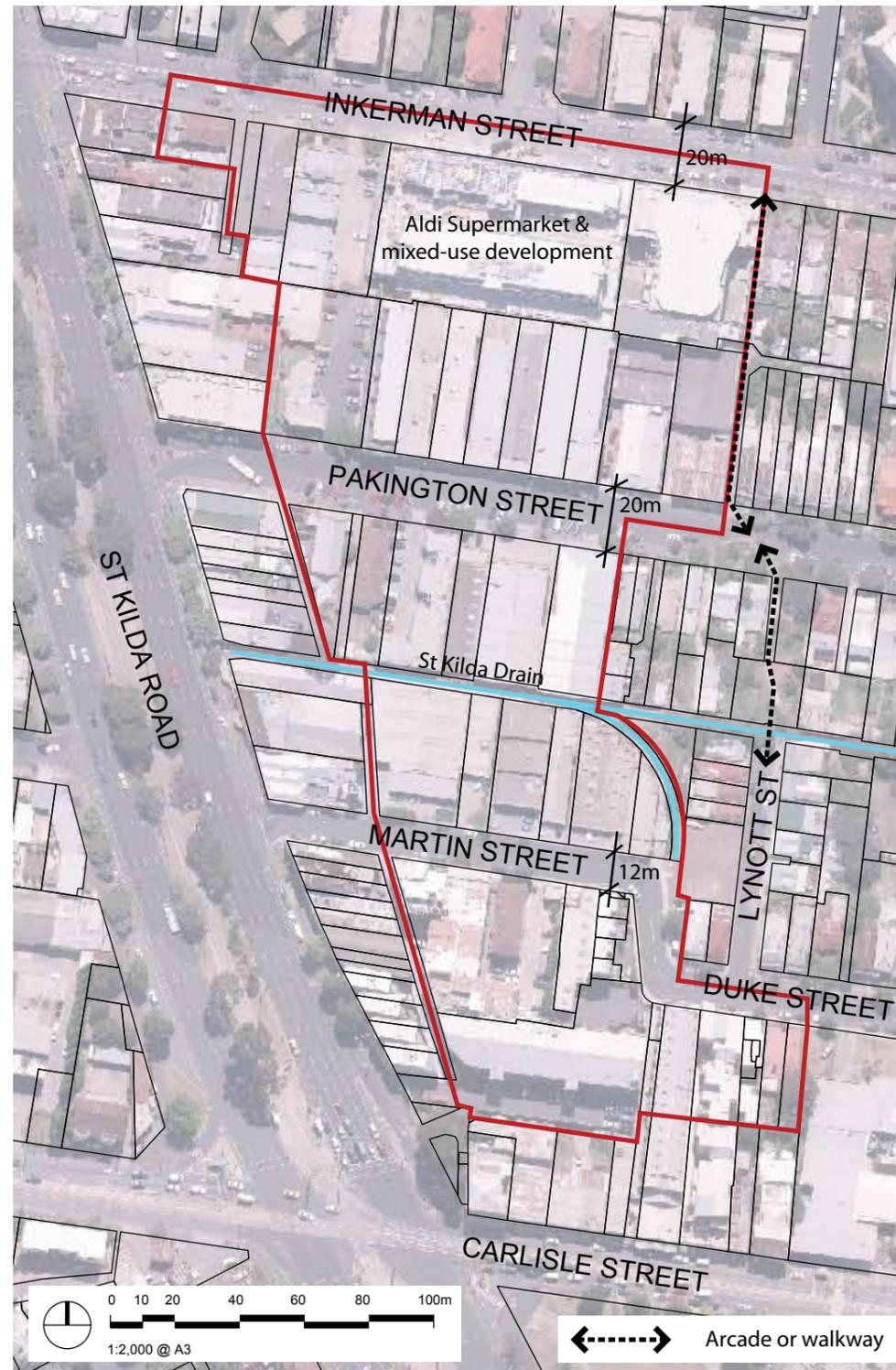
A more intensive built form that supports a mixture of uses, including an increasing proportion of residential apartments and the potential for commercial uses at street level.

A vibrant, safe and attractive pedestrian environment with buildings actively fronting the streets.

Retention of the open bluestone canal (main drain) parallel to Pakington and Martin Streets.

New development that draws on its industrial past to develop a unique contemporary style that maintains strands of the existing 'hard edged' industrial character.

Built form of a height and bulk that maintains a human scale and responds to the confined street spaces of Martin and Duke Streets, and the more open street spaces of Pakington and Inkerman Streets.



### DESIGN OBJECTIVES

*These design objectives are additional to the general objectives found in Section 3. They must be read and applied in conjunction with the general objectives and principles found in section 3.*

To foster a new and distinct contemporary urban character that complements and reflects the traditional industrial feel of this precinct.

To ensure that development enhances the prominent corner of Inkerman Street and St Kilda Road as a gateway to the activity centre.

To encourage the re-use of existing buildings / facades where these contribute to the preferred character of the area.

To ensure that the scale of new development reinforces the specific character of individual streets.

To ensure that new development maintains adequate solar access to footpaths and a sense of openness to the sky.

To reduce the visual dominance of levels above the streetwall.

To encourage the ground floor of new development to address both east-west streets and north-south pedestrian links with active frontages.

To minimise the visual dominance of vehicular access and on-site parking.

To ensure that large or consolidated sites reduce the impact of their visual bulk on neighbouring properties.

To create a transition down in the intensity of built form where sites adjoin established low-rise 'fine grain' residential areas to the east.

To ensure development minimises the impact of visual bulk when viewed from Lynott Street, the Pakington Street open space reserve, and residential properties to the east.

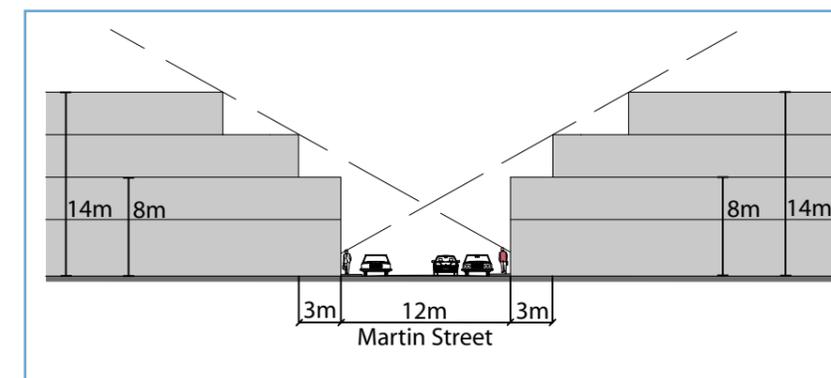
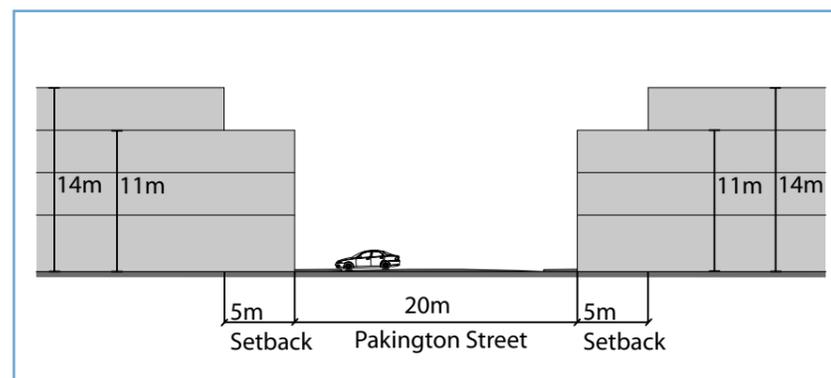
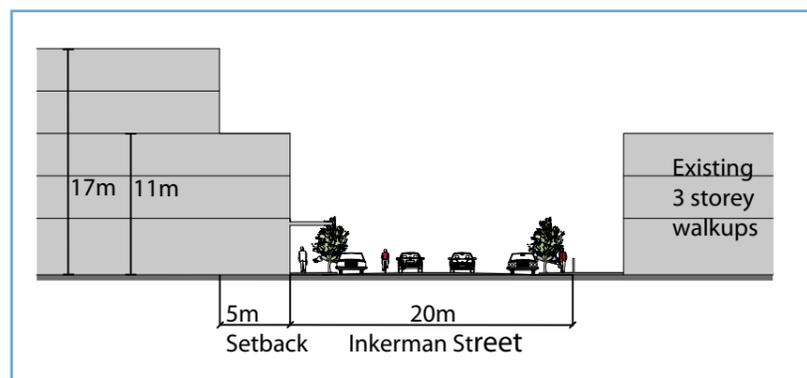
### DESIGN REQUIREMENTS

Buildings may have a zero setback to the street frontage or be stepped back up to 5 metres provided that the resultant space is not used for car parking or loading.

The streetwall height along Inkerman and Pakington Streets must be no more than 11 metres in height; this will allow up to three storeys of development.

The streetwall height along Martin and Duke Streets must be no more than 8 metres in height; this will allow two storeys of development.

Buildings fronting Inkerman Street should not exceed a preferred maximum height of 17 metres (5 storeys).



Above the streetwall in Inkerman Street any additional storeys must be set back 5 metres so as to be visually recessive. This setback may be reduced by up to 2 metres if the architecture of the upper level renders it distinctly different and visually recessive through variations in form, material, openings or colour.

Buildings fronting Pakington, Martin and Duke Streets should not exceed a preferred maximum height of 14 metres (4 storeys).

Above the streetwall in Pakington Street any additional storey must be set back 5 metres so as to be visually recessive. This setback may be reduced by up to 2 metres if the architecture of the upper level renders it distinctly different and visually recessive through variations in form, material, openings or colour.

Above the streetwall in Martin and Duke Streets, one storey may be visible provided that it is set back at least 3 metres. Any additional storey must be set back so as not to be visible when viewed from standing eye level (1.6m) at the street frontage directly across the street.

The streetwall should be built to the side boundaries.

Any development of No. 44 Pakington Street must address both its street and laneway frontage with doors and street level windows.

Side walls visible above adjoining buildings should be articulated through variations in forms, materials, openings and colours.

The ground floor facade should be designed to facilitate contact between building occupants and people in the street. It should include pedestrian entries and clear glazing.

The facade of the ground floor on sites with a frontage over 10 metres must be well articulated through variations in forms, materials, openings or colours or the inclusion of vertical design elements.

The architecture of the building should draw on the precinct's industrial past to develop a unique and contemporary style.

Vehicular access must be provided from the rear or side of lots wherever possible. Where it is not possible new garage doors must not dominate the street frontage and must be integrated with the design of the facade.

On site parking should be provided behind, below or above uses on the ground floor that front the street.

Side and rear setbacks at boundaries with properties zoned Residential 1 should comply with the objectives of Clause 55.04-1 (side and rear setbacks objective) of the Port Phillip Planning Scheme.

Development at boundaries with properties zoned Residential 1 should comply with the objectives of Clause 55.04-5 (overshadowing open space objective) of the Port Phillip Planning Scheme.



## 4.5 CHAPEL STREET MIXED USE PRECINCT

### EXISTING URBAN CHARACTER

The Chapel Street mixed-use precinct is an area of peripheral retail, commercial and residual industrial activity stretching south from Chapel Street's intersection with Carlisle Street. Its proximity to the retail centre and public transport has seen some substantial residential developments approved recently, with more proposed.

- Mixture of inter-war, post 1950s and contemporary building styles.
- Separated from the residential area to the east by rear laneways and at grade car parks.
- Footpath and parallel parking along both kerbs of Chapel Street with a pedestrian/school crossing point between Rosamond and Nightingale Streets.
- Mixture of single and double storey shops, offices, warehouses and larger apartment buildings.
- Modern, 6 storey mixed-use, apartment building situated between Marlborough and Rosamond Streets (128-140 Chapel Street) out of scale and character with the rest of the precinct and lacks articulation.
- All buildings have zero lot line setbacks and are built across the entire lot frontage.

- Lots are 10 metres wide or greater.
- Some peripheral retail uses on ground floors with offices and apartments above.
- The western side of Chapel Street is occupied by the buildings and grounds of the St Kilda Parish Mission and Uniting Church, Maternal and Child Health Centre, St Kilda Primary School and Holy Trinity Anglican Church.
- A few street trees within the road reserve adjacent the new development at 128-140 Chapel Street.
- Awnings and verandas only present on shop fronts in the southern most block of the precinct.
- 300 metre walk to city bound trains through the fine grained residential neighbourhood to the east.
- Development at 128-140 Chapel Street has a poor street interface with no passive surveillance from the apartments located in an elevated position.
- General lack of activity along the street interface due to warehouse and commercial uses.

#### PREFERRED CHARACTER STATEMENT

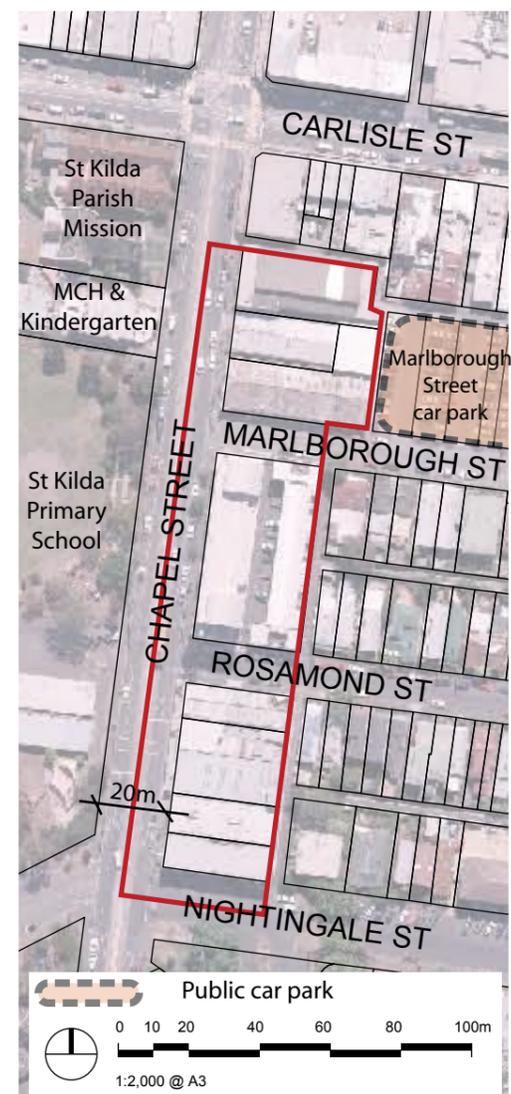
This precinct will be characterised by:

A more intensive built form that supports an increase in residential activity and the potential for commercial uses at street level.

A vibrant, safe and attractive pedestrian environment with buildings actively fronting the street.

New contemporary development that maintains strands of the existing 'hard edged' industrial character.

Built form that responds, and provides an appropriate transition, to the residential properties to the east and Carlisle Street to the north.



### DESIGN OBJECTIVES

*These design objectives are additional to the general objectives found in Section 3. They must be read and applied in conjunction with the general objectives and principles found in section 3.*

To promote design excellence in new development, appropriate to this high profile location within the Carlisle Street Activity Centre.

To establish a new and distinct built form character that reflects the former industrial nature of this precinct.

To realise the development potential of larger sites, whilst maintaining a relationship with the existing building scale within the activity centre.

To capitalise on the outlook to open space on the western side of Chapel Street.

To ensure new development contributes to a more coherent edge to Chapel Street.

To enhance the pedestrian experience along Chapel Street as a principal walking route.

To encourage the re-use of existing buildings / facades where these contribute to the preferred character of the area.

To emphasise corner sites through built form and street level activity.

To create a transition down in the intensity of built form to adjoining established low-rise 'fine grain' residential areas to the east and Carlisle Street to the north.

### DESIGN REQUIREMENTS

Buildings must have a zero setback to the street frontage.

The streetwall parapet height along Chapel Street must be no more than 11 metres in height; this will allow three storeys of development.

Buildings should not exceed a preferred maximum height of 16 metres (5 storeys).

Above the streetwall any additional storeys must be set back 5 metres so as to be visually recessive. This setback may be reduced by up to 2 metres if the architecture of the upper levels renders them distinctly different and visually recessive through variations in forms, materials, openings and colours.

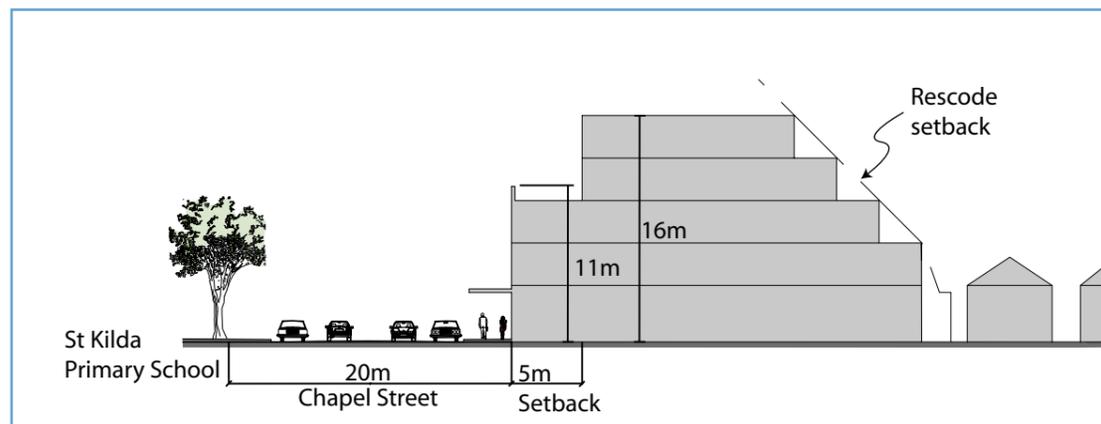
The streetwall should have a veranda for the full width of the frontage unless this is inconsistent with an original facade that is being retained.

The streetwall should be built to the side boundaries.

On corner sites, buildings must address both street frontages with either doors or street level windows.

Side walls visible above adjoining buildings must be articulated through variations in forms, materials, openings and colours.

The ground floor facade should be designed to facilitate contact between building occupants and people in the street. It should include pedestrian entries and clear glazing.



The facade of the ground floor on sites with a frontage over 10 metres should be well articulated through variations in forms, materials, openings and colours and the inclusion of vertical design elements to enhance the pedestrian experience.

Vehicular access must be provided from the rear or side of lots wherever possible. Where it is not possible new garage doors must be integrated with the design of the facade.

Side and rear setbacks at boundaries with properties zoned Residential 1 should comply with the objectives of Clause 55.04-1 (side and rear setbacks objective) of the Port Phillip Planning Scheme.

Development at boundaries with properties zoned Residential 1 should comply with the objectives of Clause 55.04-5 (overshadowing open space objective) of the Port Phillip Planning Scheme.



## 4.6 CHAPEL STREET NORTH PRECINCT

### EXISTING URBAN CHARACTER

The Chapel Street North Precinct comprises four (4) properties, located between Duke Street and the rear of retail properties fronting Carlisle Street. The precinct is dominated by commercial / office uses, with residential properties directly to its north and west. It marks a clear transition in built form between the residential areas north along Chapel Street and the retail strip along Carlisle Street to the south. This transition emphasises this location as a gateway to the activity centre when approaching from the north.

- Mixture of modern single and double storey commercial buildings, and an older 3 storey factory-warehouse with a distinctive saw tooth roof.
- The majority of buildings have zero lot line setbacks and are all built across the entire lot frontage.
- Inconsistent street-wall height.

- Separated from the weatherboard cottages to the immediate west by a lane which runs along the rear of the precinct.
- 3 storey factory-warehouse has a zero setback to the lane.
- Most lots 10 metres wide or greater.
- Buildings at 147-149 and 149A-151 Chapel Street are identified to be of heritage value.
- The eastern side of Chapel Street is occupied by the 3 storey St Kilda Police Station and older shops with parapet heights of around 10-12 metres.
- Single storey residential dwellings on the northern side of Duke Street have a side on relationship with the street, excepting one Edwardian era duplex which is directly opposite the rear of 141 Chapel Street.

### PREFERRED CHARACTER STATEMENT

This precinct will be characterised by:

A more intensive built form that supports an increase in commercial and residential activity, with the continuation of commercial or retail uses at street level.

Retention of heritage buildings and a high standard of design in new contemporary development appropriate to mark the entry point to the activity centre.

Built form of a height and bulk that maintains a human scale and responds to the predominant scale of existing development.

A vibrant, safe and attractive pedestrian environment with buildings actively fronting the street.

Built form that responds, and provides an appropriate transition, to the residential properties situated to the north and west.



### DESIGN OBJECTIVES

*These design objectives are additional to the general objectives found in Section 3. They must be read and applied in conjunction with the general objectives and principles found in Section 3.*

To promote design excellence in new development, appropriate to this high profile location within the Carlisle Street Activity Centre.

To ensure new development contributes to a more coherent edge to Chapel Street.

To realise the development potential within the precinct, whilst respecting heritage buildings and the existing building scale along Carlisle Street.

To enhance the pedestrian experience along Chapel Street as a principal walking route.

To emphasise the corner site through built form and street level activity.

To create a transition down in the intensity of built form to the adjoining low-rise 'fine grain' residential areas to the west and north, and Carlisle Street to the south.

### DESIGN REQUIREMENTS

Buildings must have a zero setback to the street frontage.

The street-wall parapet height must be no more than 11 metres in height; this will allow three storeys of development.

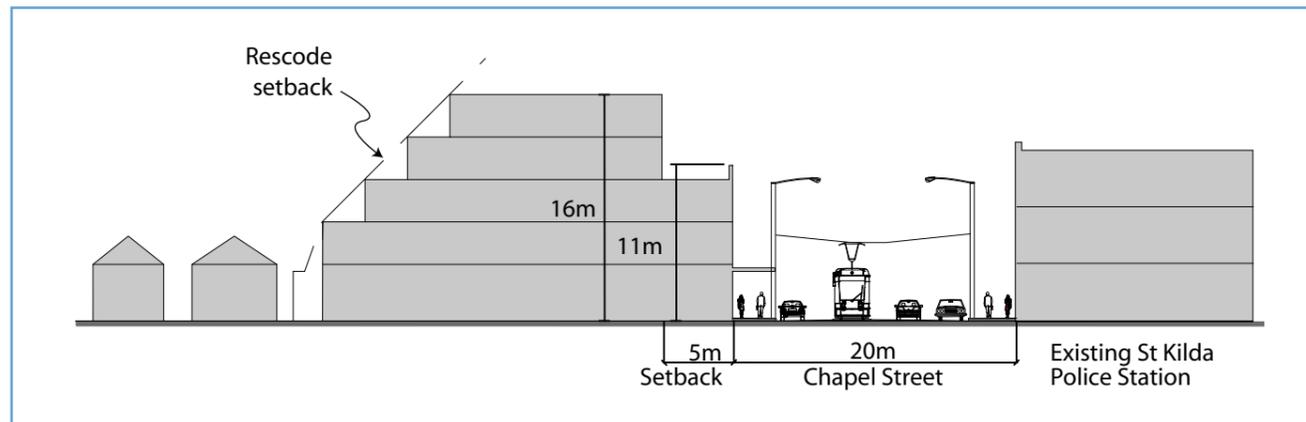
Buildings should not exceed a preferred maximum height of 16 metres (5 storeys).

Above the street-wall any additional storeys must be set back 5 metres so as to be visually recessive. This setback may be reduced by up to 2 metres along Chapel Street, if the architecture of the upper levels renders them distinctly different and visually recessive through variations in forms, materials, openings and colours.

The above setback also applies to the side street boundary on the corner lot.

Side walls visible above adjoining buildings should be visually recessive through variations in forms, materials, openings or colours.

The street-wall must be built to the side boundaries.



The street-wall on Chapel Street should have a veranda for the full width of the frontage, unless inconsistent with an original façade that is being retained.

The ground floor facade should be designed to facilitate contact between building occupants and people in the street. It should include pedestrian entries and clear glazing.

On the corner site, the building must address both street frontages with either doors or street level windows.

The facade of the ground floor of sites with a frontage over 10 metres must be well articulated through variations in form, materials, openings or colours, or the inclusion of vertical design elements to enhance the pedestrian experience.

Vehicular access must be provided from the rear or side of lots.

Side and rear setbacks at boundaries with properties zoned Residential 1 should comply with the objectives of Clause 55.04-1 (side and rear setbacks objective) of the Port Phillip Planning Scheme.

Development at boundaries with properties zoned Residential 1 should comply with the objectives of Clause 55.04-5 (overshadowing of open objective) of the Port Phillip Planning Scheme.



## 4.7 WILLIAM STREET INDUSTRIAL PRECINCT

### EXISTING URBAN CHARACTER

The William Street Industrial Precinct is the only remaining piece of industrial zoned land within the Activity Centre. While it is of regional significance as an industrial pocket, it has a high degree of potential for change to residential and/or office uses with few constraining factors, particularly given its proximity to Balaclava Station.

- Mixture of post 1950s and contemporary industrial buildings constructed mainly of brick and reinforced concrete.
- Directly abutting the rear of residential properties to the east and the side of residential properties to the south.
- Elevated rail line and Balaclava Station to the west.
- Pedestrian walkway running parallel with the rail line from Carlisle Street to Nightingale Street along the blank rear walls of William Street properties.
- William Street closed to the south to avoid trucks using residential streets.

- Parallel parking along the eastern kerb of William Street and in the side streets of Charles Street, Stuart Street and William Place.
- Light industrial, warehousing and commercial uses.
- Building frontages typically 10 metres or wider.
- Most buildings with a zero setback from William Street though some set back behind areas of car parking or loading.
- Some lots comprising at grade parking only.
- Predominantly double storey buildings.
- No street trees.
- No awnings or verandas.
- Pedestrian movements restricted due to footpaths dominated by crossovers, parked cars and truck movements.
- Poor direct connectivity to Balaclava Station.
- Strong presence of Jewish focussed goods and services.

**PREFERRED CHARACTER STATEMENT**

This precinct will be characterised by:  
High quality light industrial, warehousing and commercial buildings.  
High quality, safe pedestrian environments along William Street and Balaclava Walk East.



### DESIGN OBJECTIVES

*These design objectives are additional to the general objectives found in Section 3. They must be read and applied in conjunction with the general objectives and principles found in section 3.*

To promote high quality design and opportunities for additional development that supports a range of light industrial and commercial uses.

To create overall visual consistency and a greater sense of streetscape enclosure.

To ensure that development retains the sense of openness to the sky.

To realise the development potential of larger sites, particularly those adjacent to Balaclava Station on the western side of William Street.

To ensure development on the eastern side of William Street maintains the privacy and amenity of adjoining residential properties.

To ensure development facilitates safe and convenient pedestrian movement through the precinct, including through the minimisation of vehicle crossovers.

### DESIGN REQUIREMENTS

Buildings may have a zero setback to the street frontage or be stepped back up to 5 metres provided that the resultant space is used for active pedestrian uses.

The streetwall must be no more than 7 metres in height; this will allow two storeys of development.

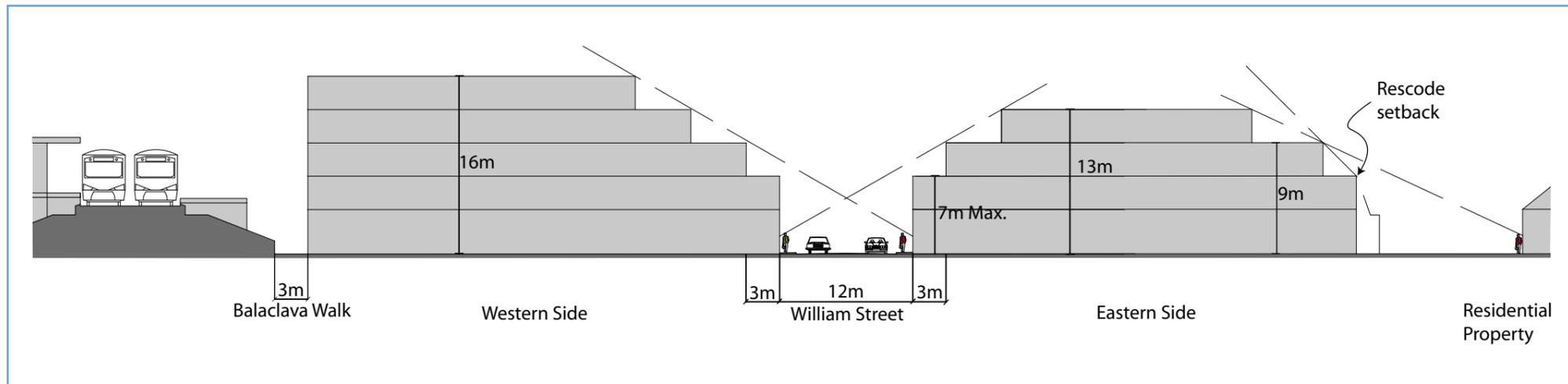
Buildings on the eastern side of William Street must not exceed a maximum height of 13 metres (4 storeys).

Buildings on the western side of William Street should not exceed a maximum height of 16 metres (5 storeys).

Above the streetwall one additional storey may be visible from the street provided that it is set back at least 3 metres so as to ensure the visual predominance of the streetwall.

One further storey may be built on the eastern side of William Street and a further two storeys on the western side, provided they are set back so as not to be visible when viewed from standing eye level (1.6m) at the street frontage directly across the street.

The above setback also applies to the side street boundary on Nightingale Street.



Vehicular access should not dominate the street frontage and must be integrated with the design of the facade.

Buildings should respond to the existing industrial character of the precinct by using materials characteristic of the precinct on the streetwall with minimal detail.

Side and rear setbacks at boundaries with properties zoned Residential 1 should comply with the objectives of Clause 55.04-1 (side and rear setbacks objective) of the City of Port Phillip Planning Scheme.

Development above 9 metres in height must be set back so that it is not visible from standing eye level (1.6m above ground level) at the rear building line of any adjoining residential property.

Development at boundaries with properties zoned Residential 1 should comply with the objectives of Clause 55.04-5 (overshadowing open space objective) of the Port Phillip Planning Scheme.



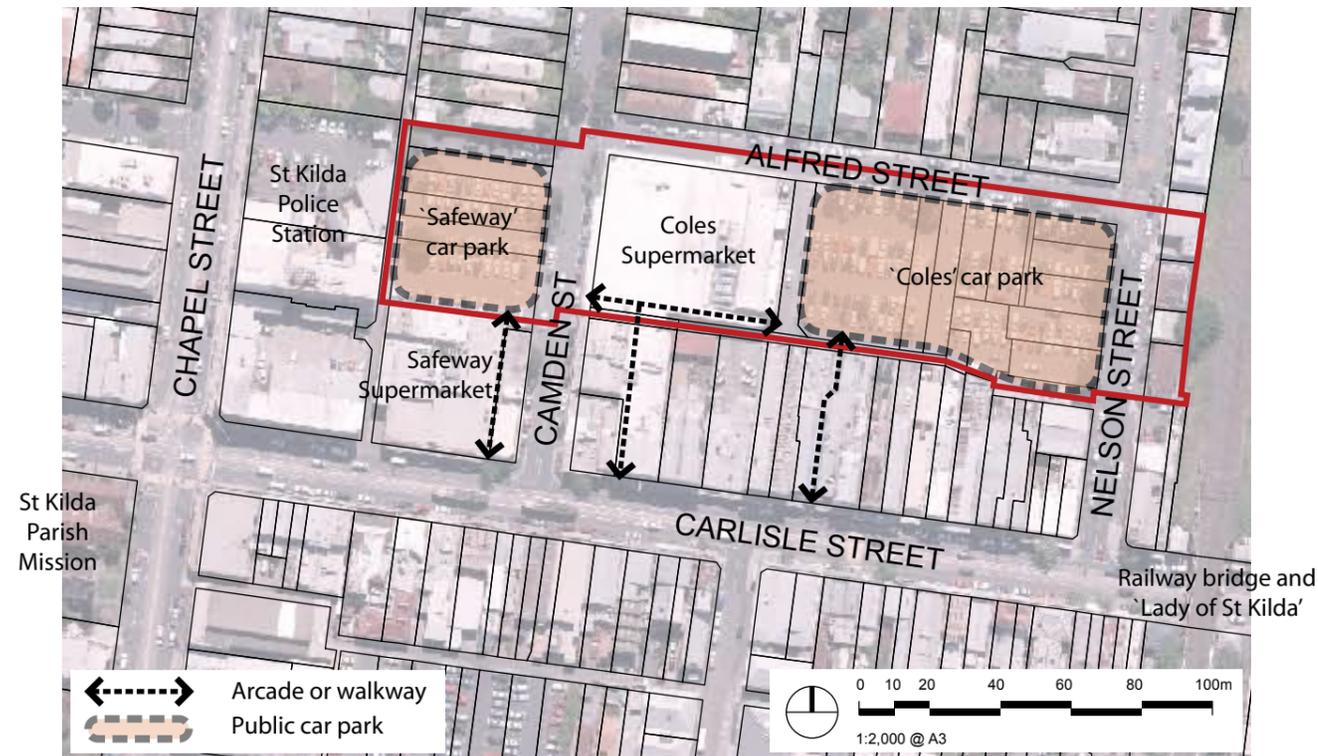
## 4.8 SUPERMARKETS PRECINCT

### EXISTING URBAN CHARACTER

The Supermarkets Precinct is located immediately behind and to the north of the Carlisle Street retail strip and presents open areas of at grade car parking and blank facades to any pedestrians entering the centre from the north.

- Post 1950s buildings of rendered or painted brickwork.
- Commonwealth Bank and associated staff car park.
- Nelson Street 14 metre wide road reserve with two way traffic south of Alfred Street but only south-bound traffic allowed north of Alfred Street
- Alfred Street 12 metre wide road reserve with minimal footpaths along the southern edge.
- Arcades link the Coles Supermarket, Coles car park and Safeway car park to Carlisle Street.
- Blank facades of Coles and Safeway Supermarkets onto their car parking areas.
- Zero setbacks and blank facades of Coles and Safeway Supermarkets on their Carlisle Street, Camden Street and Alfred Street frontages.
- The rear of Carlisle Street shops fronting the Coles Car Park.

- 3 storey St Kilda Police Station separated from Safeway Supermarket car park by laneway.
- Parallel parking along both kerbs of Nelson, Alfred and Camden Streets.
- Significant mature native vegetation, gum trees, located on the edges of the Coles car park.
- Some native street tree plantings in Camden Street.
- Public toilets located in the centre of Coles car park.
- Rubbish and crates stored within the Coles car park.
- Significant levels of council ownership of car park areas.
- Nelson Street predominantly fronted by former residential buildings now converted to retail uses.
- Northern side of Alfred Street contains Victorian, Edwardian and modern residential buildings set back between 2 and 4 metres.
- Poor pedestrian environment due to inactive frontages and undefined street edges.



### PREFERRED CHARACTER STATEMENT

This precinct will be characterised by:

A vibrant retail hub dominated by the two renewed supermarkets, which support the retail role of the activity centre and act as key 'anchors' attracting visitors to the centre.

A built form that supports a mixture of uses, including retailing at street level and commercial / office uses and residential apartments at upper levels.

A built form that responds to the opportunities offered by the large supermarket sites whilst respecting the adjoining 'fine grain' residential areas and heritage scale of the Carlisle Street retail strip.

Well designed new buildings and a public realm which create a safe and attractive pedestrian environment.

### DESIGN OBJECTIVES

*These design objectives are additional to the general objectives found in Section 3. They must be read and applied in conjunction with the general objectives and principles found in section 3.*

To realise the development potential of the key retail sites (supermarkets and car parks).

To ensure that development retains the sense of openness to the sky and preserves the lower rise character of the centre as viewed from the public realm.

To ensure that the scale and form of new development respects the heritage elements on Carlisle Street.

To create overall visual consistency and a greater sense of streetscape enclosure in Camden and Nelson Streets.

To improve the interface with the residential properties on the north side of Alfred Street.

To ensure that new development contributes to improved pedestrian permeability within the precinct, and integration of the precinct through to Carlisle Street.

To enhance the pedestrian experience of Camden Street as an important pedestrian thoroughfare with continuous retail frontages and a clearly defined public realm.

To ensure that development does not unreasonably impact on solar access to the footpaths of Camden Street.

To encourage the design of new development to incorporate public open space with good solar access and adjoining active land uses.

To encourage loading bays not to face public spaces and streets.

To reduce the dominance of vehicular movement throughout the precinct.

### DESIGN REQUIREMENTS

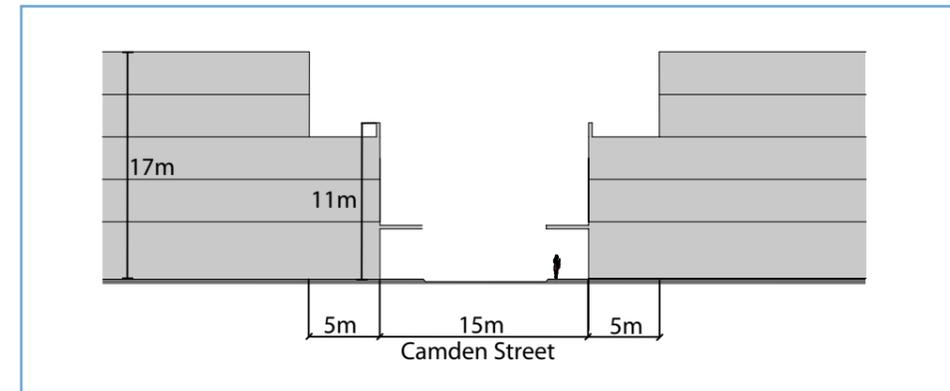
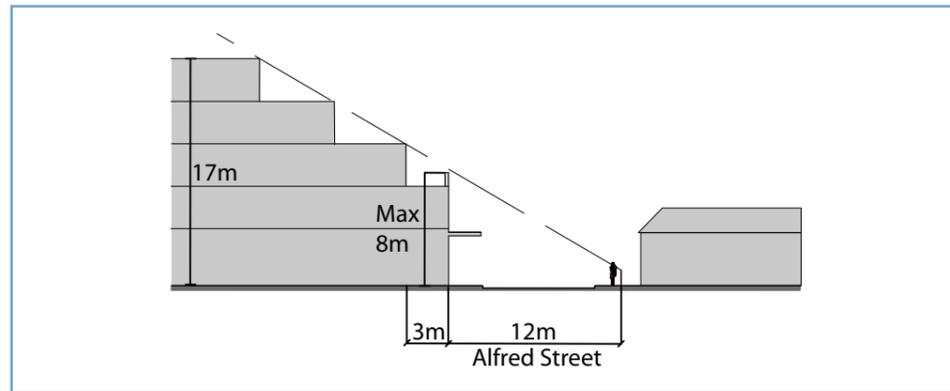
#### Alfred Street frontage:

Buildings may have a zero setback to the street frontage or allow a proportion of the frontage to be stepped back so long as the resultant space is used for active pedestrian uses and/or to preserve existing vegetation.

The streetwall parapet height must be a maximum of no more than 8 metres; this will allow two stories of development with the ground floor accommodating a supermarket function.

Buildings should not exceed a height of 17 metres (5 storeys).

Above the streetwall one additional storey must be set back 3 metres with additional storeys set back so as not to be visible when viewed from standing eye level (1.6m) at the street frontage directly across the street. The streetwall must have a veranda for the full width of the frontage.



#### Camden and Nelson Street frontages:

Buildings may have a zero setback to the street frontage or allow a proportion of the frontage to be stepped back so long as the resultant space is used for active pedestrian uses and/or to preserve existing vegetation.

The streetwall parapet height may be a maximum of 11 metres; this will allow three stories of development with the ground floor accommodating a supermarket function.

Buildings should not exceed a preferred maximum height of 17 metres (5 storeys).

Above the streetwall additional storeys must be set back 5 metres so as to be visually recessive. This setback may be reduced by up to 2 metres if the architecture of the upper level renders it distinctly different and visually recessive through variations in form, material, openings or colour.

#### Overall guidelines:

The building fronting Alfred Street should be well modulated and articulated to minimise its visual bulk when viewed from Linton Street to the north.

The streetwall should have a veranda for the full width of the frontage. This veranda should match those of neighbouring properties in terms of height above and coverage of the footpath.

The streetwall should be built to the side boundaries.

The streetwall should have vertical articulation and window proportions and locations that reflect those of the neighbouring buildings both above and below the veranda line.

The streetwall must have a parapet. The parapet should be well articulated and contain detailed consistent with the character of Carlisle Street.

On corner sites, buildings must address both street frontages with either doors or street level windows.

Side walls visible above adjoining buildings should be visually recessive through variations in forms, materials, openings or colours.

The ground floor must be level with the footpath.

The ground floor facade should be designed to facilitate contact between building occupants and people in the street. It should include pedestrian entries and clear glazing.

The building facade on sites with a frontage over 10 metres must be well articulated through variations in forms, materials, openings or colours or the inclusion of vertical design elements. This will ensure that the finer grain of the precinct is continued.

Vehicular access to the car park areas must be provided from Nelson and Camden Streets, south of Alfred Street.

Side and rear setbacks at boundaries with properties zoned Residential 1 should comply with the objectives of Clause 55.04-1 (side and rear setbacks objective) of the Port Phillip Planning Scheme.

Development at boundaries with properties zoned Residential 1 should comply with the objectives of Clause 55.04-5 (overshadowing open space objective) of the Port Phillip Planning Scheme.

Solar access must be maintained to the southern footpath of Carlisle Street between 11am and 2pm on the 21st of June.



## Strategic Site Guidelines

---



## 4.9 COLES SUPERMARKET SITE

### DESIGN OBJECTIVES

Refer to Supermarket Precinct Objectives

### DESIGN REQUIREMENTS

#### Carlisle Street frontage:

Refer Carlisle Street East Precinct Guidelines.

#### Alfred Street frontage:

Refer to Supermarket Precinct Guidelines

#### Camden and Nelson Street frontages:

Refer to Supermarket Precinct Guidelines

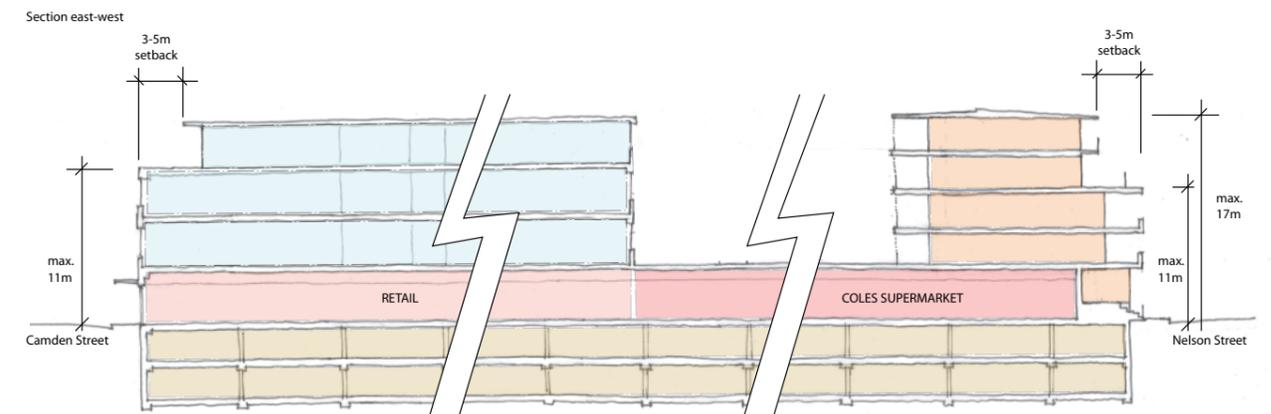
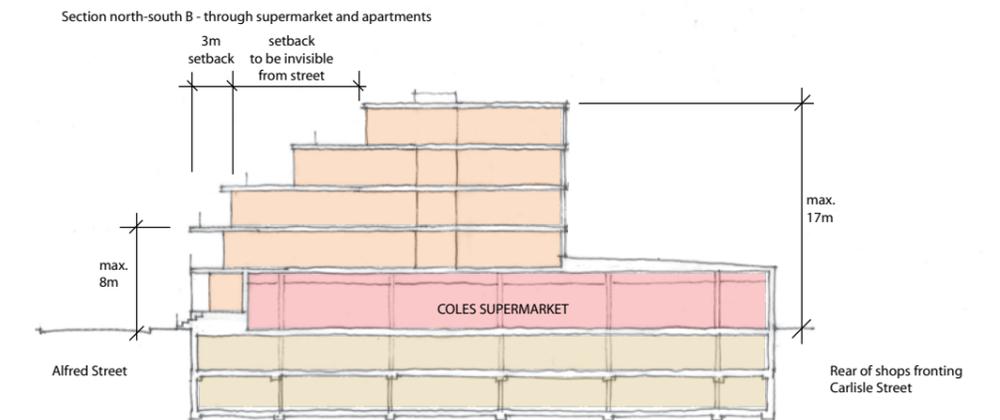
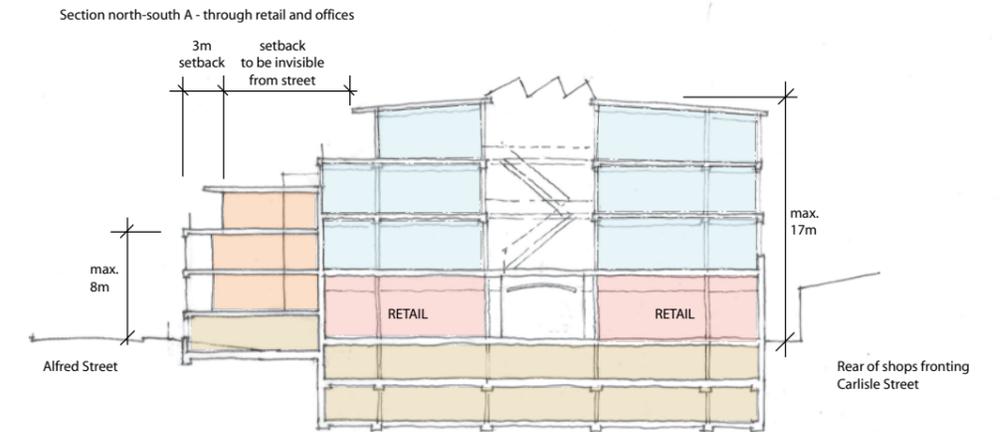
#### Overall guidelines:

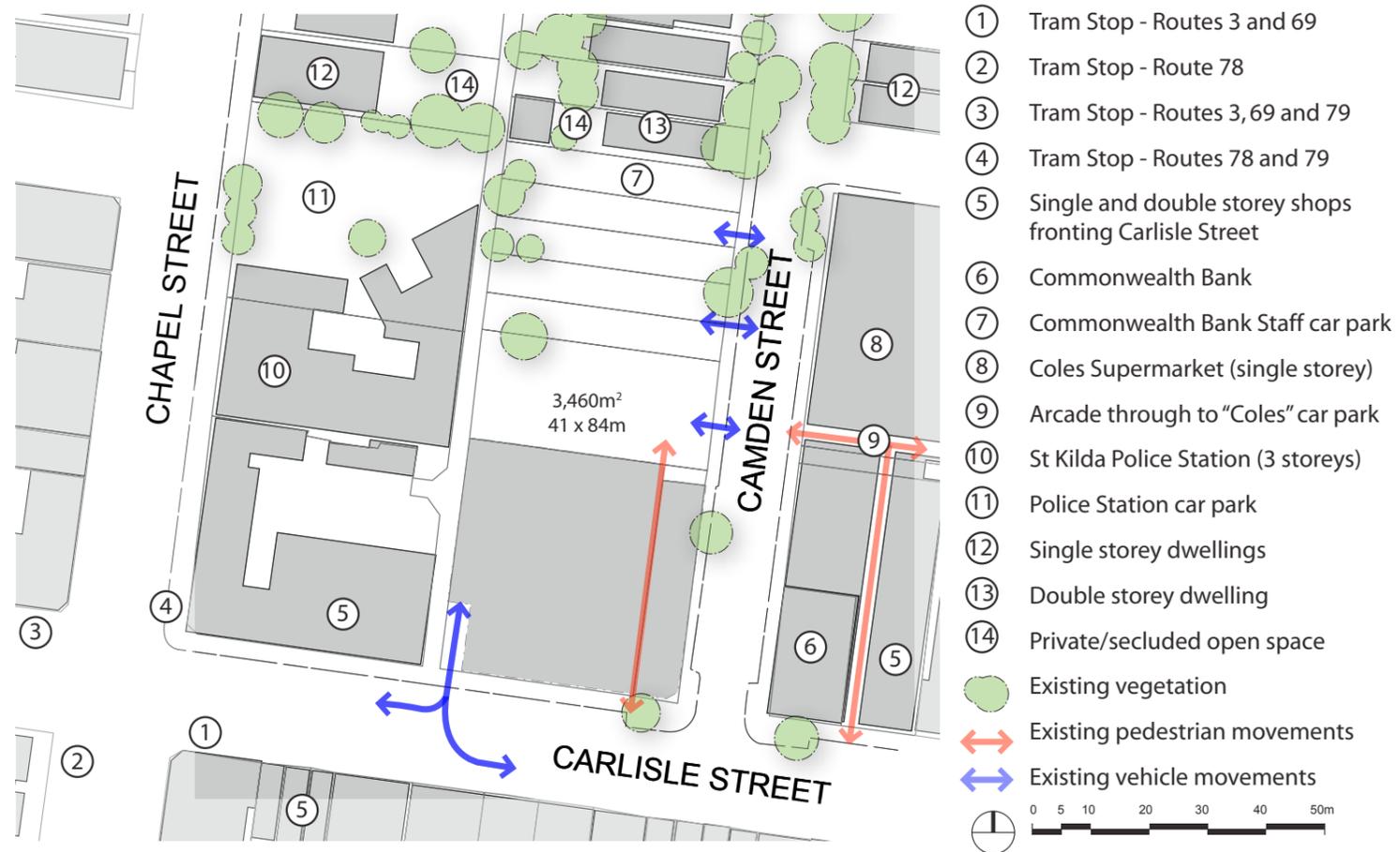
Refer Supermarkets Precinct Guidelines.

**POSSIBLE DESIGN SOLUTION**



This is a potential design response only. More options exist to develop this site.





#### 4.10 SAFEWAY SUPERMARKET SITE

##### DESIGN OBJECTIVES

Refer to Supermarket Precinct Objectives

##### DESIGN REQUIREMENTS

###### Carlisle Street frontage:

Refer Carlisle Street East and Supermarkets Precinct Guidelines.

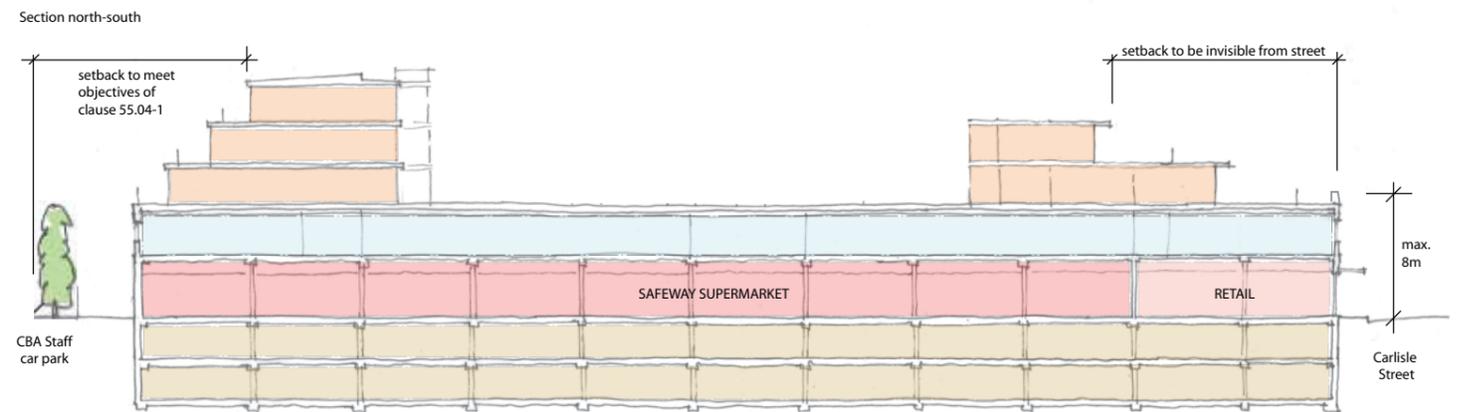
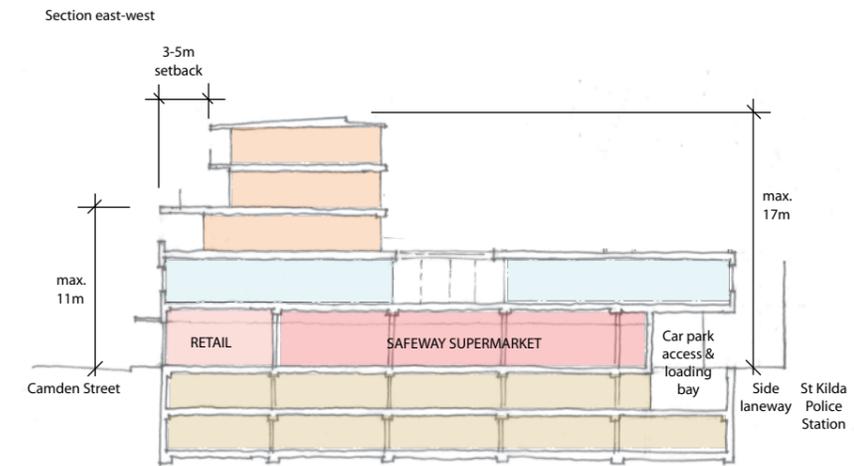
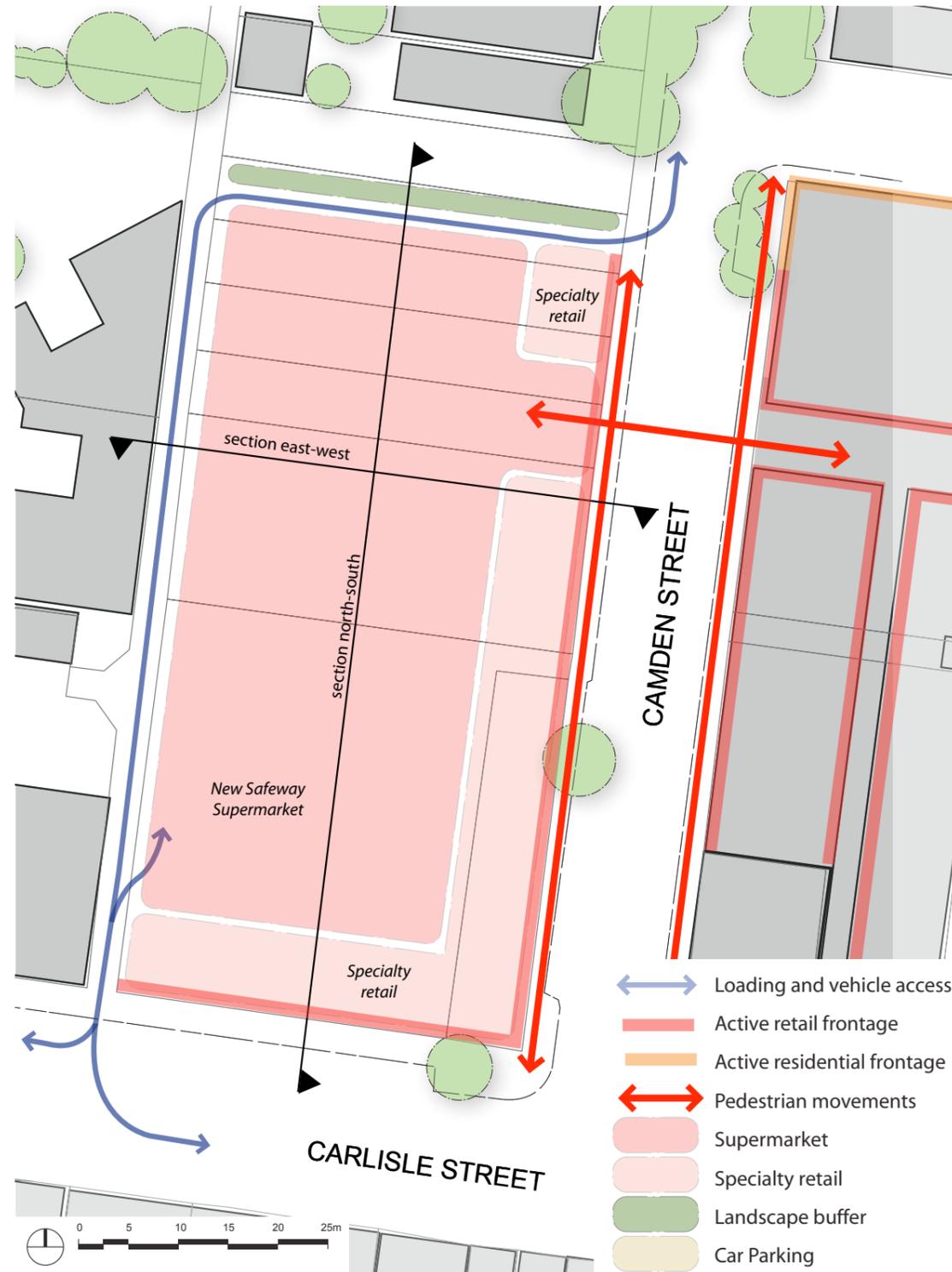
###### Camden and Nelson Street frontages:

Refer Supermarkets Precinct Guidelines.

###### Overall guidelines:

Refer Supermarkets Precinct Guidelines.

**POSSIBLE DESIGN SOLUTION**



This is a potential design response only.  
More options exist to develop this site.



#### 4.11 MARLBOROUGH STREET CAR PARK SITE

##### DESIGN OBJECTIVES

To provide for additional dwellings on the edge of the Carlisle Street activity centre.

To ensure development reflects the surrounding residential character of Marlborough Street.

To encourage a higher built form adjacent to the rear laneway and the Chapel Street mixed use precinct, recognising that these interface areas have the potential to accommodate a more intensive built form.

To provide a transition down in the intensity of built form to the low-rise 'fine grain' residential areas to the south and east.

To improve the interface with the rear laneway.

##### DESIGN REQUIREMENTS

New development on the Marlborough Street frontage should have a maximum setback of between 3 to 4 metres configured as useable private open space.

Buildings should not exceed a preferred maximum height of 13 metres (4 storeys).

Buildings must be no more than 8 metres high within 5 metres of the boundary.

Above the Marlborough Street facade one additional storey may be visible provided that it is set back at least 5 metres so as to be visually recessive. This setback may be reduced by up to 2 metres if the architecture of the upper levels renders them distinctly different and visually recessive through variations in forms, materials, openings or colours.

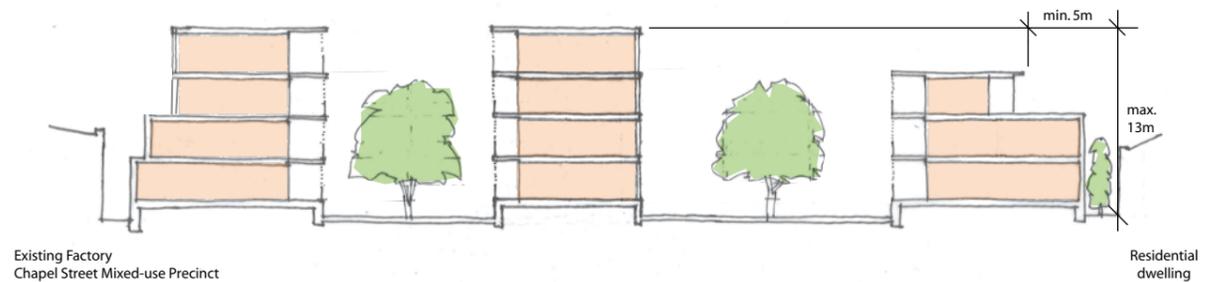
Any further storeys must be set back so as not to be visible when viewed from standing eye level (1.6m) at the street frontage directly across the street.

The facade should have vertical articulation and window proportions and locations that reflect those of the neighbouring buildings.

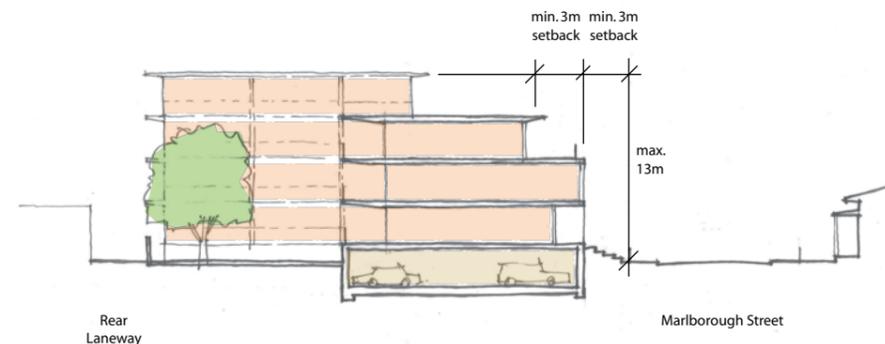
**POSSIBLE DESIGN SOLUTION**



Section east-west



Section north-south



This is a potential design response only.  
More options exist to develop this site.

Side walls visible above adjoining buildings should be visually recessive through variations in forms, materials, openings or colours.

The ground floor facade should be designed to facilitate contact between building occupants and people in the street. It should include pedestrian entries and clear glazing.

Vehicular access should not dominate the Marlborough Street frontage and must be integrated with the design of the facade.

Side and rear setbacks at boundaries with properties zoned Residential 1 should comply with the objectives of Clause 55.04-1 (side and rear setbacks objective) of the Port Phillip Planning Scheme.

Development at boundaries with properties zoned Residential 1 should comply with the objectives of Clause 55.04-5 (overshadowing open space objective) of the Port Phillip Planning Scheme.



#### 4.12 BALACLAVA STATION CAR PARK SITE

##### DESIGN OBJECTIVES

- To provide for additional dwellings on the edge of the Carlisle Street activity centre.
- To integrate the design of new development with the upgrade of Balaclava Station to create a safe and attractive pedestrian environment.
- To create a public space adjacent to Balaclava Station that encourages social interaction and contributes to an improved connection along the western walkway to the station.
- To ensure development fronting Marlborough Street responds to the residential character and scale of Marlborough Street.
- To encourage a higher built form adjacent to Balaclava Station and the rear laneway, recognising these interface areas have the potential to accommodate a more intensive built form.
- To provide a transition down in the intensity of built form to the low-rise 'fine grain' residential areas to the south and west.
- To encourage ground floor design that contributes to interaction with streetscape activity and balconies or terraces at the upper levels that overlook the street, rear laneway and Balaclava Station / Balaclava Walk.
- To improve the interface with the rear laneway.

##### DESIGN REQUIREMENTS

- New development on the Marlborough Street frontage must have a setback consistent with neighbouring properties (minimum 3 metres).
- New development must be set back a minimum of 4 metres from the eastern boundary to allow the creation of an active pedestrian space and pedestrian link adjacent to Balaclava Station.
- The facade should have vertical articulation and window proportions and locations that reflect those of the neighbouring buildings.
- Buildings should not exceed a preferred maximum height of 13 metres (4 storeys).
- Buildings must be no more than 8 metres high within 5 metres of the boundary.
- Above the Marlborough Street facade one additional storey may be visible provided that it is set back at least 5 metres so as to be visually recessive. This setback may be reduced by up to 2 metres if the architecture of the upper levels renders them distinctly different and visually recessive through variations in forms, materials, openings or colours.
- Any further storeys must be set back so as not to be visible when viewed from standing eye level (1.6m) at the street frontage directly across the street.

**POSSIBLE DESIGN SOLUTION**



The development must address both Marlborough Street and Balaclava Walk frontages with doors and street level windows.

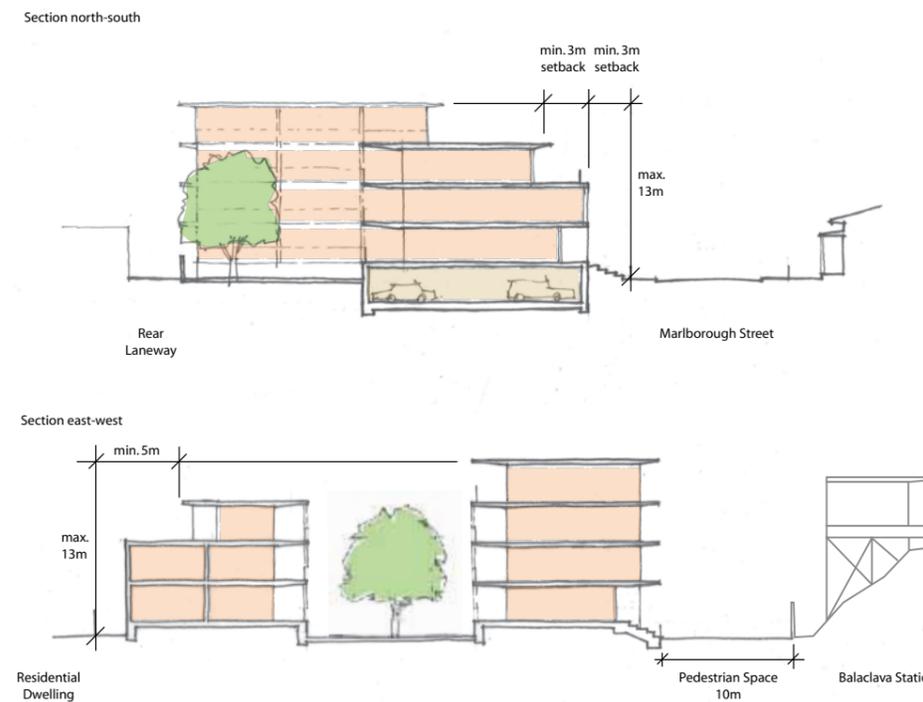
Side walls visible above adjoining buildings should be visually recessive through variations in forms, materials, openings or colours.

The ground floor facade should be designed to facilitate contact between building occupants and people in the street. It should include pedestrian entries and clear glazing.

Vehicular access should be provided from the rear laneway to the north if possible.

Side and rear setbacks at boundaries with properties zoned Residential 1 should comply with the objectives of Clause 55.04-1 (side and rear setbacks objective) of the Port Phillip Planning Scheme.

Development at boundaries with properties zoned Residential 1 should comply with the objectives of Clause 55.04-5 (overshadowing open space objective) of the Port Phillip Planning Scheme.



This is a potential design response only. More options exist to develop this site.

## SITE ANALYSIS PLAN



- ① Tram Stop - Routes 3 and 69
- ② Tram Stop - Route 78
- ③ Tram Stop - Routes 3, 69 and 79
- ④ Tram Stop - Routes 78 and 79
- ⑤ Single and double storey shops fronting Carlisle Street
- ⑥ St Kilda Town Hall
- ⑦ St Kilda Council Offices
- ⑧ St Kilda Public Library
- ⑨ St Kilda Primary School
- ⑩ 5 storey apartment complex
- ⑪ 2 storey walk-up flats
- ⑫ Single storey brick warehouses
- ⑬ St Kilda Parish Mission Uniting Church
- ⑭ Kindergarten and Maternal and Child Health Centre
- Existing vegetation
- Existing pedestrian movements

## 4.13 ST KILDA PARISH MISSION SITE

### DESIGN OBJECTIVES

To maintain, and extend the opportunity for, the accommodation of a range of community services and facilities.

To ensure the built form and massing of new development reinforces the site as a street corner and as an important entry point into the retail core of the activity centre.

To ensure the siting and scale of new development reflects the future character of the Carlisle Street retail strip, through a prominent streetwall and recessed upper level.

To ensure new development respects the heritage values, including siting (front and side setbacks), of the adjacent Uniting Church building.

### DESIGN REQUIREMENTS

Buildings must have a zero setback to Carlisle and Chapel Streets.

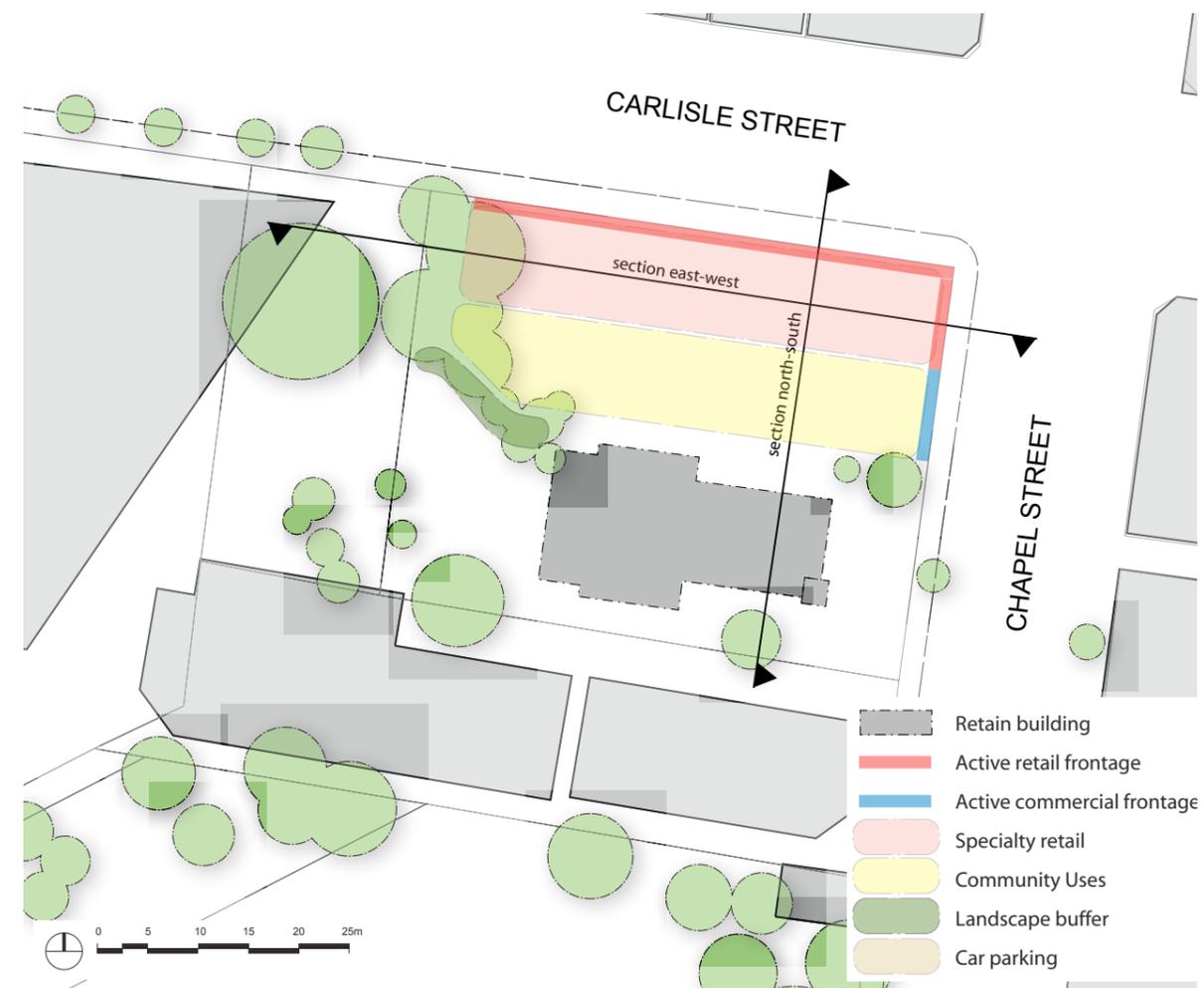
The street-wall parapet height must be a minimum of 7 metres and a maximum of 8 metres, which is equivalent to the height of existing two storey facades in Carlisle Street.

Buildings should not exceed a preferred maximum height of 10 metres (3 storeys).

Above the street-wall, any additional storey(s) must be setback so as not to be visible when viewed from standing eye level (1.6 metres) at the street frontage directly opposite.

The facade should be well articulated in form, material, opening, colour and have vertical articulation, window proportions and locations, and awnings that reflect those of the other buildings on the corners of Carlisle and Chapel

**POSSIBLE DESIGN SOLUTION**



Streets and reinforce the fine grain character of Carlisle Street

Buildings should not be sited any closer to the Uniting Church than the existing southern wall of the hall at 161 Chapel Street.

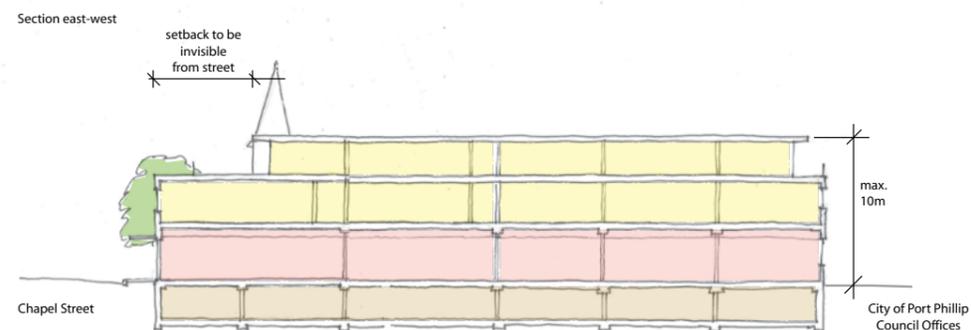
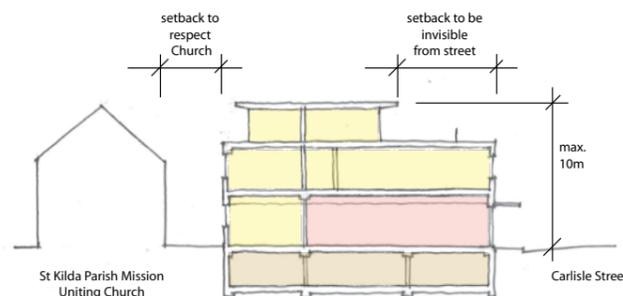
The facade should be designed to facilitate contact between building occupants and the public realm and should exhibit transparency to support passive surveillance.

The facade on Carlisle Street must be designed to provide an active retail frontage to the street.

Side walls visible above adjoining buildings should be visually recessive through variations in form, material, opening or colour.

The ground floor must be level with the footpath.

Vehicular access should be provided from Carlisle Street.



This is a potential design response only. More options exist to develop this site.



#### 4.14 AUSTRALIA POST DELIVERY CENTRE SITE

##### DESIGN OBJECTIVES

To ensure that new development reflects and emphasises the importance of the site as a key point of entry into the activity centre.

To emphasise the corner site through a hard-edged built form to Brighton Road.

To reduce the visual dominance of levels above the street-wall.

To create a transition down in the intensity of built form to the 'fine-grain' residential areas to the east.

##### DESIGN REQUIREMENTS

Development should have a zero setback to the street frontage.

The streetwall parapet height along Chapel Street, Brighton Road and Grosvenor Street must be no more than 11 metres in height; this will allow three storeys of development.

The streetwall should have a veranda for the full width of the frontage.

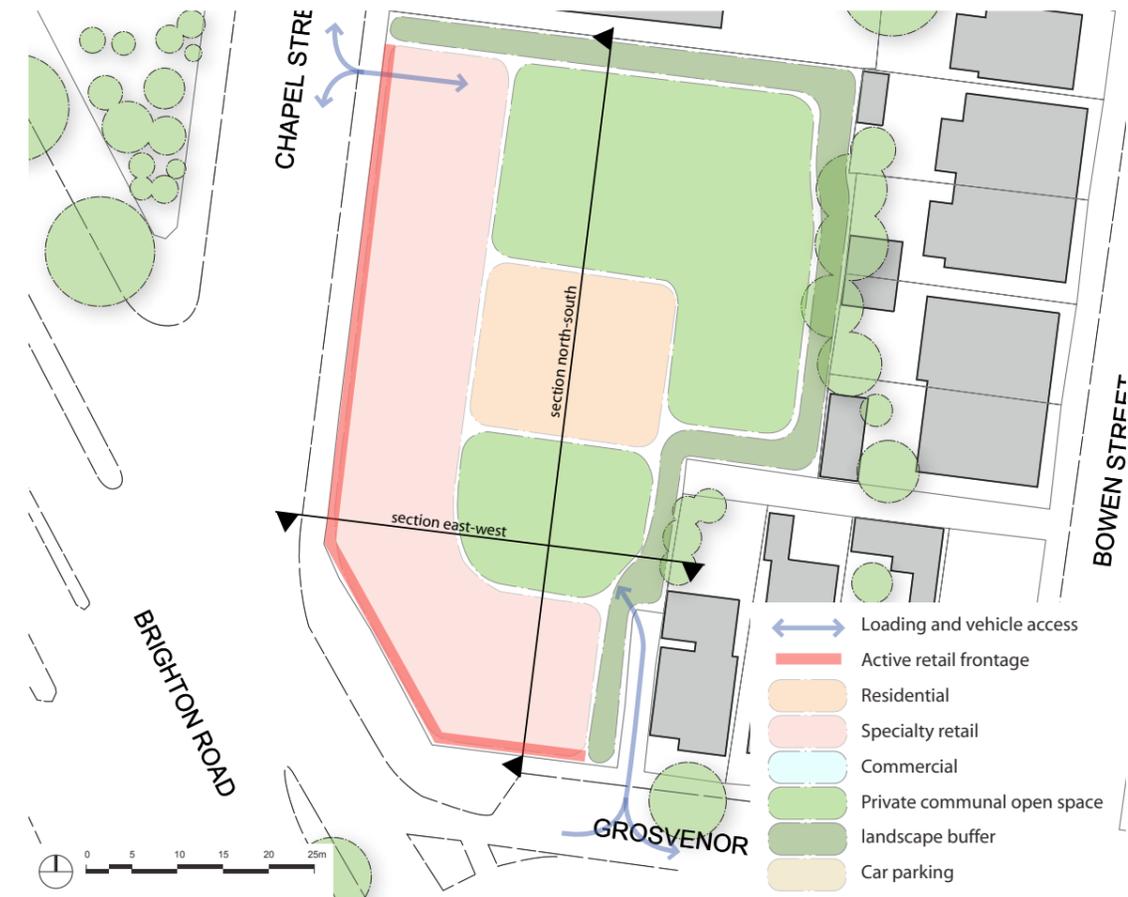
Development should not exceed a preferred maximum height of 16 metres (5 storeys).

Above the streetwall any additional storeys must be set back 5 metres from the property line so as to be visually recessive. This setback may be reduced by up to 2 metres if the architecture of the upper levels renders them distinctly different and visually recessive through variations in forms, materials, openings and colours.

This setback may be further reduced to zero on the Brighton Road frontage if it contributes to the identification of the corner as an important entry point to the centre.

Additional height/architectural feature (maximum one storey) above 16 metres is encouraged on the Brighton Road corner of the site provided it is integrated with the design of the building and contributes to the identification of the corner as an important entry point to the centre. The floor area of this feature should not exceed 10% of the gross floor area of the top building level.

**POSSIBLE DESIGN SOLUTION**



The building must address all street frontages with doors and street level windows.

Side walls visible above adjoining buildings should be articulated through variations in forms, materials, openings and colours.

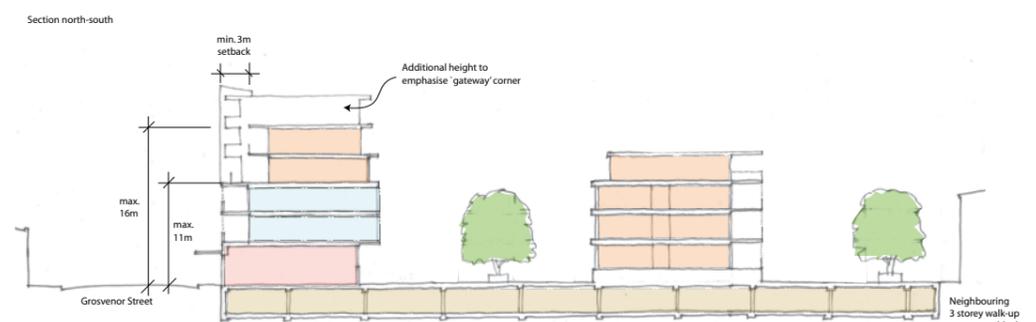
The ground floor facade should be designed to facilitate contact between building occupants and people in the street. It should include pedestrian entries and clear glazing.

The facade of the ground floor should be well articulated through variations in forms, materials, openings and colours and the inclusion of vertical design elements to enhance the pedestrian experience.

Vehicular access should be provided from the rear of the site. If it is not possible, new garage doors must be integrated with the design of the facade.

Side and rear setbacks at boundaries with properties zoned Residential 1 should comply with the objectives of Clause 55.04-1 (side and rear setbacks objective) of the Port Phillip Planning Scheme.

Development at boundaries with properties zoned Residential 1 should comply with the objectives of Clause 55.04-5 (overshadowing open space objective) of the Port Phillip Planning Scheme.



This is a potential design response only. More options exist to develop this site.

# Carlisle Street

---

