

PRECINCTS
AND PROJECTS

Adopted
26th August 2014

BAY STREET

ACTIVITY CENTRE



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OPPORTUNITY PRECINCTS

There are four precincts within the Activity Centre that have particular capacity to implement the *Strategic Directions* set out in Part 4. Each precinct has differing opportunities to contribute to the vibrancy and character of the Centre, through improved land use and built form outcomes, public realm works or access improvements.

This section sets out the strategic role, preferred outcomes, and an implementation framework for each opportunity precinct. The location and extent of each precinct in the context of the overall Activity Centre is shown below.

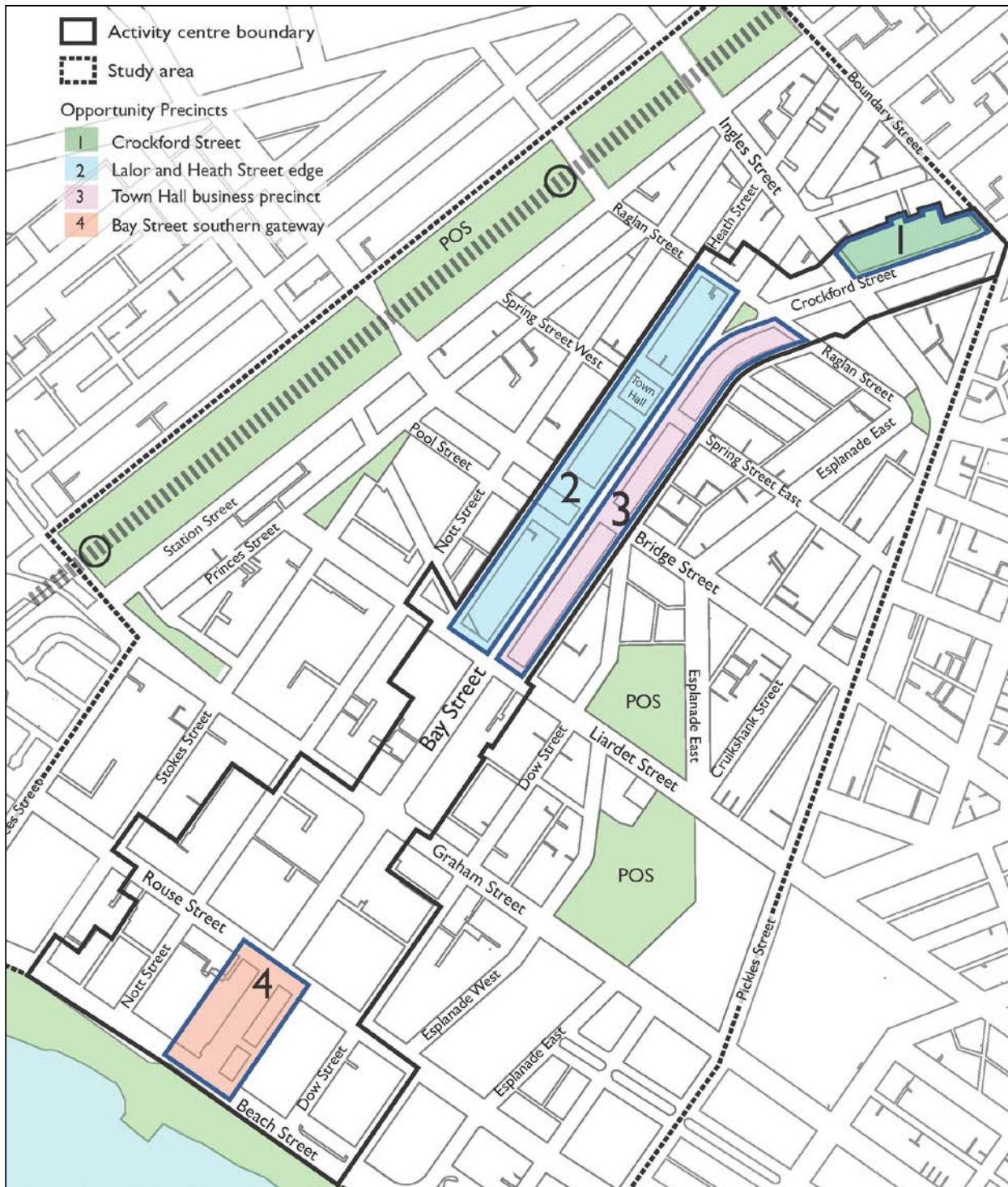


Figure 1: Opportunity Precincts Plan

Opportunity Precinct I: Crockford Street Precinct

I. Precinct Description

Crockford Street is the northern gateway to the activity centre, located beyond the core retail area in Bay Street and immediately south of the South Melbourne industrial area (Montague Precinct).

The precinct focus is on the northern side of Crockford Street where there is currently a mixture of office, service (eg car wash) and warehouse buildings, some with car parking areas fronting the street, contributing to its service industrial character. The precinct abuts residential properties to the north in Garton Street.

Crockford Street is a 30 metre wide main road which has an important access role between the Bay Street Activity Centre and the CBD via City Road, as well as being an important section of the local bus network.

The opposite (southern) side of Crockford Street has a residential focus, comprising a mix of single dwellings and medium density multi-unit development.

Residential properties surrounding the precinct create sensitive interfaces, but there is potential to improve these through future land use change and redevelopment.



Figure 2: Precinct location plan

2. Strategic Role

The strategic potential of the precinct is to provide new opportunities for office and residential activity at the northern end of the activity centre, through comprehensive redevelopment of the precinct over time. Built form renewal of the precinct will create an improved 'gateway' to the activity centre through a moderate scale of high quality development, which will also respond to the surrounding lower scale residential context to create an improved interface between the activity centre and residential properties.

3. Precinct outcomes sought:

- Renewal of the precinct will be achieved by providing opportunities for higher order uses (commercial and residential).
- A vertical mix of land uses is sought, to achieve active commercial frontages at ground level that contribute to daytime activity on the street. Primary retail and industrial land uses will be discouraged.

- Residential uses will be encouraged on upper levels to contribute to housing growth objectives and ensure an ‘after hours’ presence in the precinct.
- New development will create a clear sense of entry to the activity centre through new buildings at an overall height of 5 storeys, with a 3 storey street wall height and recessed upper floor levels.
- Development will be required to step down to the low scale established residential context to create an improved interface between the activity centre and residential properties.

4. Implementation

Land use

The desired outcomes for the precinct will be supported through rezoning from the current Industrial 3 Zone to a Mixed Use Zone. The purpose of the Mixed Use Zone is “To provide for a range of residential, commercial, industrial and other uses which complement the mixed-use function of the locality.”. Rezoning will facilitate comprehensive renewal and achieve a more appropriate interface with adjoining dwellings.

Local Planning Policy will guide discretion under the Mixed Use zone to:

- Encourage residential uses on upper floors.
- Discourage industrial activity.
- Discourage primary retail (shop) uses, excepting a convenience shop and restricted retail (ie larger specialised display based retailing / showrooms).

The Environmental Audit Overlay must also be applied, given the rezoning will allow for residential and other sensitive uses to occur on former industrial land. This will ensure that any contamination is addressed prior to any residential use commencing.

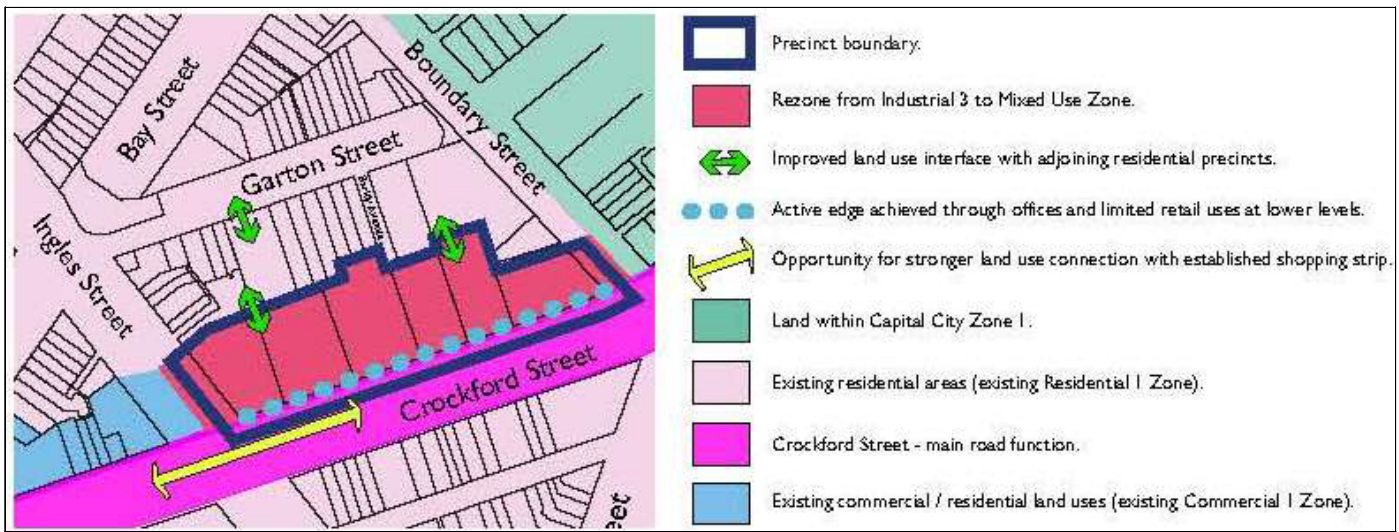


Figure 3: Proposed land use outcome

Built Form

Preferred Character

The built form character of the precinct will develop through a distinct and moderate scale of development up to 5 storeys, providing a connection between City Road and Bay Street. This will act as a ‘book-end’ to the southern end of Bay Street.

New development will reflect the hard edge character of Crockford Street and Bay Street, with street wall heights of 3 storeys and recessed upper floors.

Well designed new buildings will connect to the public realm to support an improved pedestrian environment, though active ground floor levels, entrances and openings in the principal façade.

Development will achieve high quality design outcomes, which recognises the precincts important gateway role.

New buildings will create a sensitive interface with residential properties, maximise northern orientation, and be designed to a high standard of ecological sustainable design.

Design Objectives

- To ensure new development contributes to a more coherent edge to Crockford Street.
- To ensure that new development reflects and emphasises the importance of this precinct as a gateway to the activity centre.
- To create a transition down in scale to the adjoining low-rise 'fine grain' residential area to the north.
- To minimise overshadowing of adjoining properties, and properties on the opposite side of Crockford Street.
- To recess upper level development in order to minimise the visual impact of levels above the street wall and protect solar access to the southern footpath.
- To maximise the northern orientation of potential development sites and achieve high quality design outcomes including ESD.
- To enhance the pedestrian experience along Crockford Street, through encouraging street life and activity, and passive surveillance of the public realm.
- To discourage vehicle access from Crockford Street.

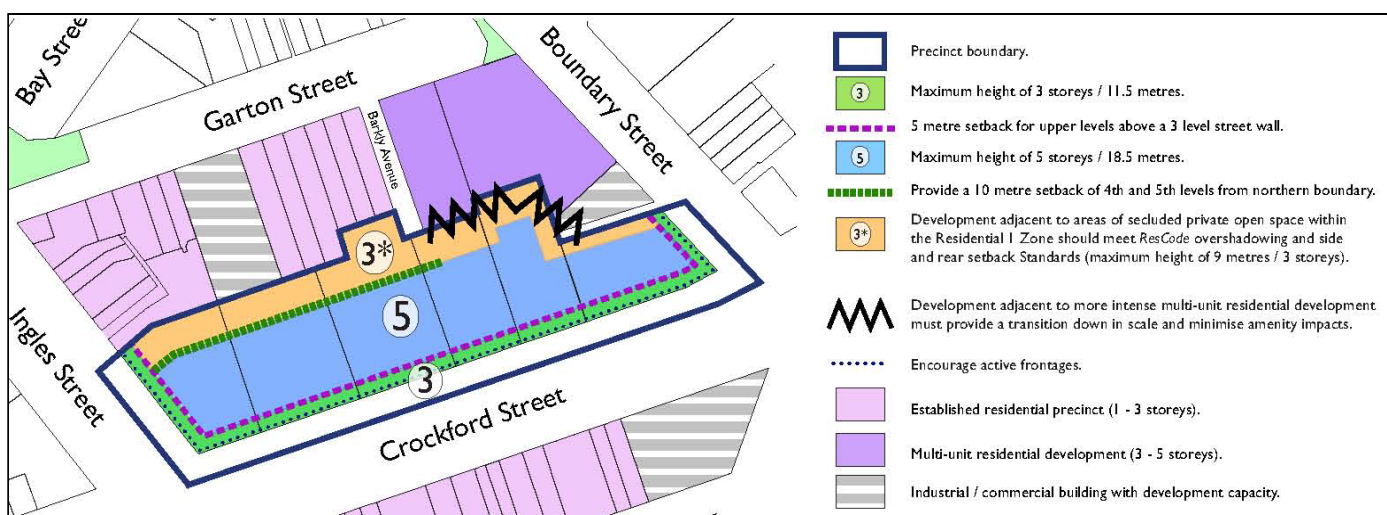


Figure 4: Proposed built form outcome

Requirements

- Buildings must have a zero setback to Ingles, Crockford and Boundary Streets.
- The street wall parapet heights must be no more than 11 metres, which will allow for 3 storey development.
- The street wall must be built to the side boundaries.
- Corner sites may have a have a maximum street wall parapet height of 14.5 metres, for a maximum length of 5 metres along any frontage. Buildings must address both street frontages with either doors or street level windows.

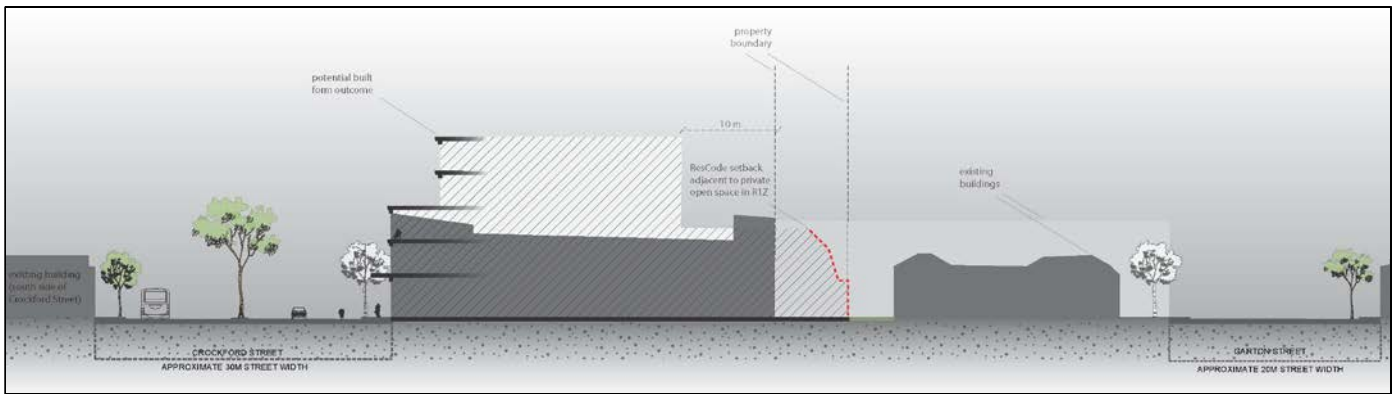


Figure 5: Cross section through Crockford and Garton Street properties.

- Buildings should not exceed a maximum height of 18 metres (5 storeys).
- Above the street wall, any additional storey must be setback a minimum of 5 metres from the property line.
- Development adjacent to multi-unit residential development must provide a transition down in scale and minimise off-site amenity impacts.
- Ground, 1st and 2nd levels:
 - Development adjacent to areas of secluded private open space within the Residential 1 Zone should meet the overshadowing and side and rear setback objectives and standards (Standards B17 and B21) of Clause 55.04, and must not exceed 9 metres / 3 storeys in height.
 - Existing boundary walls that exceed these standards may be retained, however any increase in height should meet the ResCode Standards.
- 4th and 5th levels:
 - Development adjacent to areas of secluded private open space within the Residential 1 Zone must be setback 10m from the northern boundary.
- New buildings must meet Standard B22 of Clause 55.04-6 (Overlooking objective) of the Port Phillip Planning Scheme.

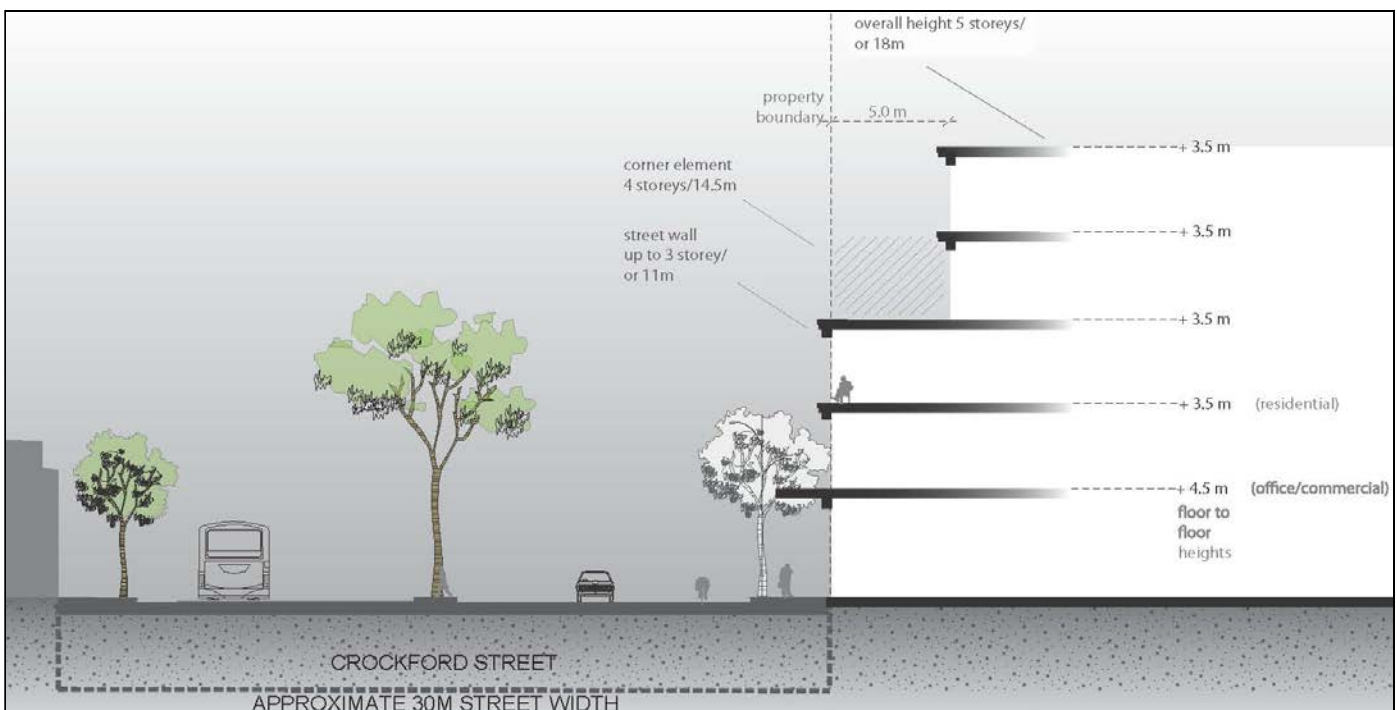


Figure 6: Section diagram showing proposed street wall, setbacks and overall building height.

- Ground floor facades should be designed to facilitate contact between occupants and people in the street, through pedestrian entries and clear glazing. Upper floors windows and balconies should be orientated to overlook the street.
- Building facades on sites with a frontage over 10 metres must be well articulated through variations in forms, materials, openings and the inclusion of vertical design elements.
- Floor-to-floor levels at ground floor should be 4.5 metres to accommodate a range of potential land uses.
- The ground floor should be level with the footpath.
- Additional crossovers are not encouraged. Vehicle access is to be provided from the side or rear of lots.

Opportunity Precinct 2: Lalor and Heath Street Edge

1. Precinct Description

This precinct comprises properties fronting the western side of Bay Street between Liardet and Raglan Streets, which typically have rear access to Lalor and Heath Streets. The Port Melbourne Town Hall is a focal point of the precinct.

Single and double storey retail buildings front Bay Street and have a fine grain shopfront design, contributing to the significant heritage character of the precinct. Lots are approximately 40 metres in depth with variable lot widths.

Development at two and three storeys, primarily for residential purposes, has occurred at the rear of some sites, while other properties have lower scale buildings or open car parking areas at the rear. Some recent developments are dominated by car parking and roller doors at street level, which has created a poor design outcome that must be addressed in future development.



Figure 7: Precinct location plan

2. Strategic Role

The precinct has capacity to accommodate further residential development at a scale that respects the sensitive residential interfaces at this edge of the activity centre, and to protect the heritage streetscape along Bay Street. New buildings must reflect the prevailing fine grain streetscape rhythm.

3. Precinct outcomes sought:

- 'Rear of strip' residential development will occur at the rear of Bay Street properties, fronting Heath and Lalor Streets. This will create an improved interface to existing residential properties on the western side of Heath and Lalor Streets through creating a residential edge on both sides of the street.

- Shopfront buildings fronting Bay Street are retained, and new development is recessed so that it is not visible from the opposite side (property line) of Bay Street. This will maintain the integrity of the heritage fabric.
- Commercial land uses are discouraged from occupying premises with a frontage to Heath and Lalor Streets, to reinforce the residential use and character of this location.
- Heritage elements are retained, including those in Heath and Lalor Streets and on corner sites. The fine grain building character is reflected in new development.

4. Implementation

Land use

Preferred land use outcomes can be achieved by retaining the existing Commercial I Zone.

Local Planning Policy will guide discretion under this zone to:

- Encourage new residential ‘infill’ development along the eastern side of Heath and Lalor Streets.
- Discourage development forms that would introduce commercial land use activity in this location, including retail premises, tavern, industry, and nightclub.

Built Form

Built form guidance will be established through Local Planning Policy, and the existing Heritage Overlay.

Planning policy will require that:

- The street-wall parapet height to Heath and Lalor Streets be a maximum of 7 metres, equivalent to a two storey street wall.
- Buildings should not exceed a maximum overall height of 14 metres, the equivalent of a four storeys.
- Development above the street wall height must be setback so that any third level is recessed by a minimum of 3 metres from the property line, and any fourth level is not visible from the opposite side of Heath / Lalor Streets, and from Bay Street when viewed from the opposite side of the street at standing eye level (1.6 metres).
- The fine grain building character of the precinct must be reflected in new development through vertical design elements.
- Vehicle access and parking entrances must not dominate the design of ground floor level façades.
- (New development fronting Bay Street will be subject to heritage considerations, and also the strategies contained in *Strategic Direction 4: Reinforcing Urban Character*).
- On corner sites with a principal frontage to Bridge, Spring, or Raglan Streets, development should:
 - Be setback behind the principal frontage so it is not visible when viewed from the opposite street frontage at standing eye level (1.6 metres) above the footpath level.
 - Have a maximum overall height: two storeys / 7 metres.
 - Address all street frontages.

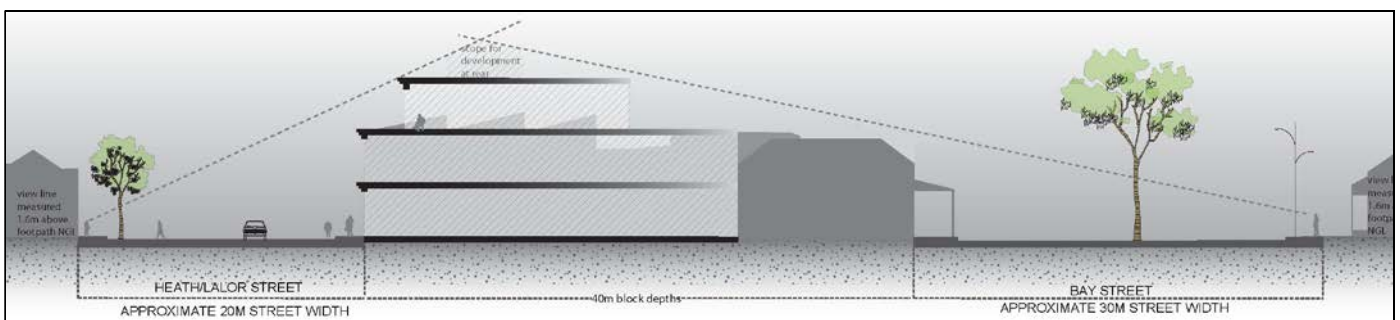


Figure 8: Cross section through Heath / Lalor Street and Bay Street showing potential built form outcome.

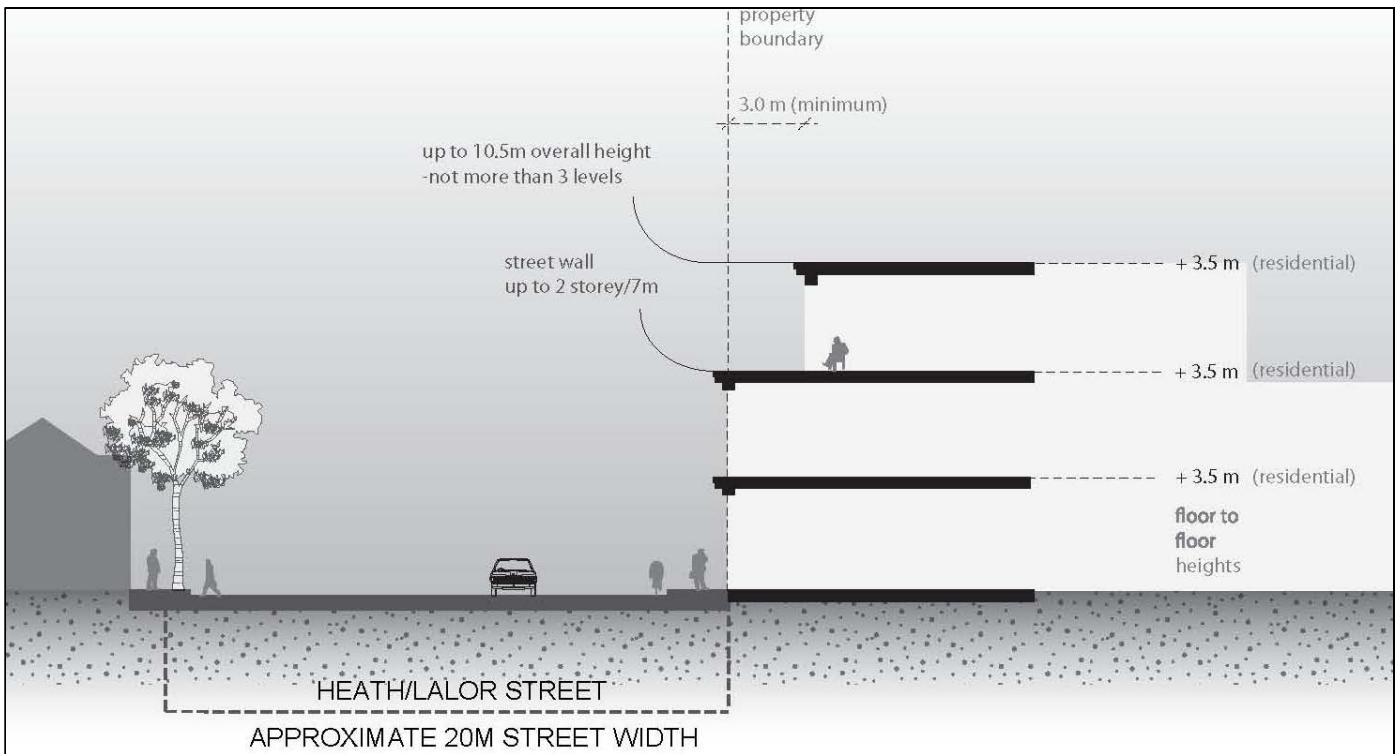


Figure 9: Section diagram showing street wall, setback beyond, and overall height from Heath / Lalor Streets.

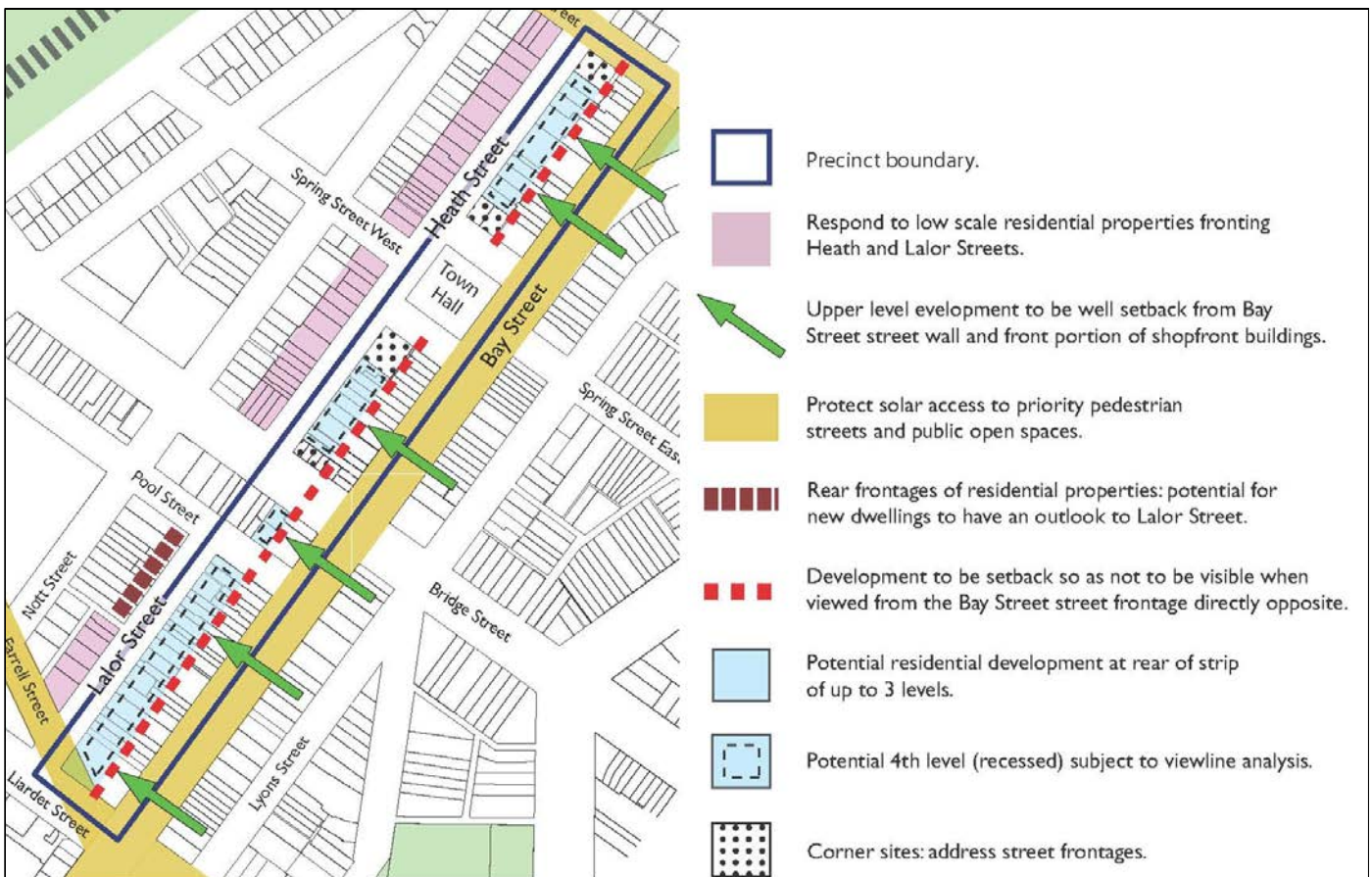


Figure 10: Proposed built form outcome

Opportunity Precinct 3: Town Hall Business Precinct

1. Precinct Description

The Town Hall Business Precinct has a strategic location opposite the Port Melbourne Town Hall and is part of the entrance to the traditional retail strip of Bay Street. It comprises properties on the south-eastern side of Bay Street between Raglan and Lardet Streets.

The Precinct currently has a diversity of land uses and building types in contrast with the more consistent western side of Bay Street. The 'patchwork' combination of residential and business zonings has resulted in a mix of retail, office and residential land uses. The precinct is characterised by clusters of low scale commercial and residential buildings with some taller individual buildings.



Figure 11: Precinct location plan.

2. Strategic Role

The future potential of the precinct is to strengthen the existing north-south connection of active land uses along Bay Street, and to create a business services niche as part of a complementary land use cluster around the Town Hall.

The precinct can accommodate some additional development to reinforce the sense of arrival at the northern end of the activity centre, and deliver consistent street wall heights as a unifying streetscape element, all within a heritage streetscape context.

3. Precinct outcomes sought:

- A stronger north-south connection of active land uses will be achieved by providing opportunities for a broader range of commercial uses, including office-based activity, through rezoning of sites currently zoned residential.

- Preferred land uses include office-based business services (particularly at street level) and residential uses north of Bridge Street. Retail land uses will be discouraged north of Spring Street, whilst a mix of retail and office activity is sought south of Bridge Street.
- New development must respect existing heritage and streetscape character attributes, and the amenity of residential properties on the edge of the activity centre (Lyons Street).

4. Implementation

Land use

The precinct is currently zoned part Commercial I and part Residential I Zone. The desired outcomes for the precinct will be supported through retaining the existing Commercial I Zone, whilst rezoning the current Residential I properties south of Spring Street to Commercial I Zone, and north of Spring Street to Mixed Use Zone.

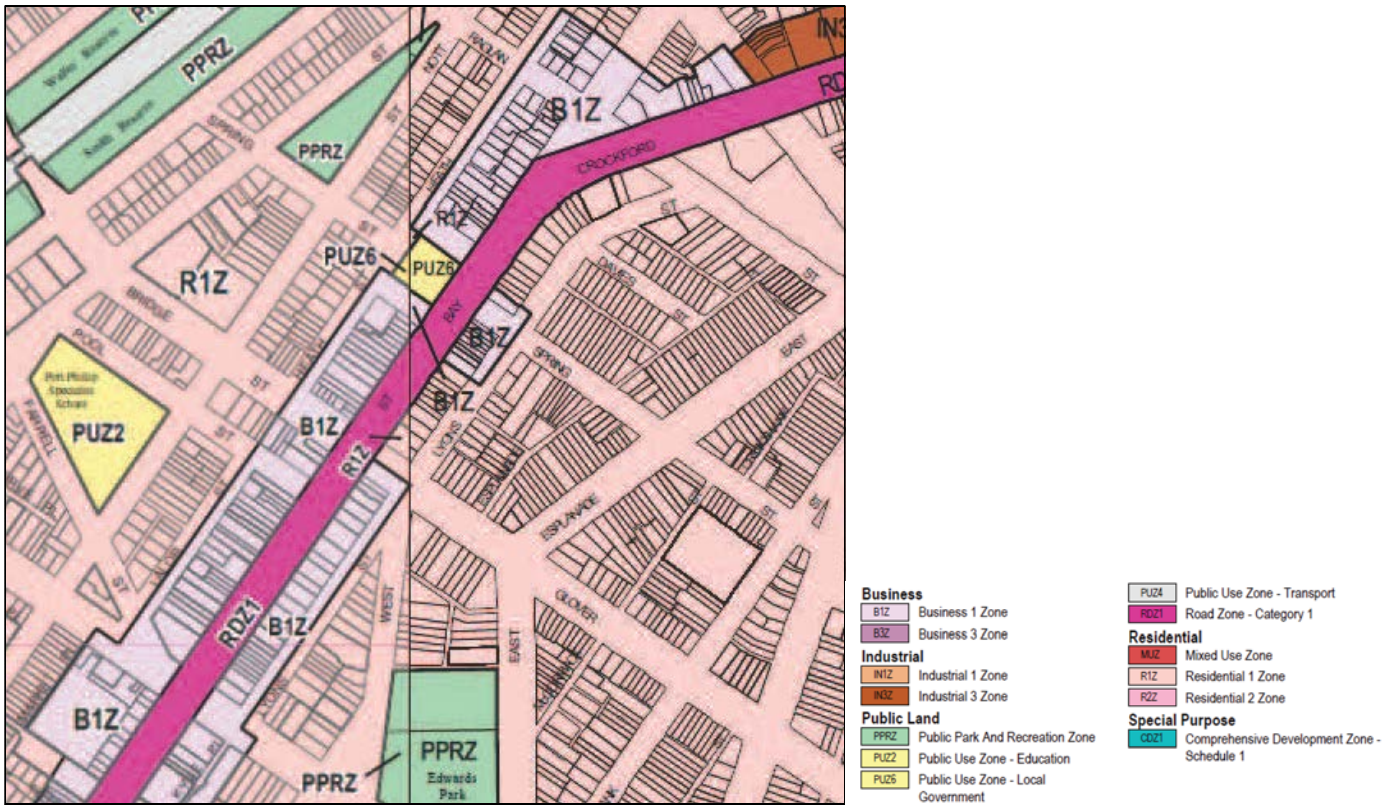


Figure 12: Existing Planning Scheme zone map

The Commercial I zone allows retail, residential and office uses ‘as-of-right’. The Mixed Use zone allows dwellings and limited-scale office, food and drink premises (restaurants etc), and shop uses ‘as-of-right’, whilst retail premises and larger scale commercial (office, food and drink premises, and shop) uses require a planning permit.

Local Planning Policy will guide discretion under the Commercial I zone to:

- Support office uses at street level.
- Encourage residential and office uses at upper levels.

Local Planning Policy will guide discretion under the Mixed Use zone to:

- Discourage retail (shop) activity north of Spring Street.
- Encourage residential above offices uses at street level.



Figure 13: Proposed land use outcome and sub-precinct rezoning

Built Form

The precinct contains clusters of heritage places that will set the context for built form change in the precinct.

Local Planning Policy will be used (together with the Heritage Overlay) to achieve:

- Development that respects and responds to the height and setback of adjacent heritage places. New development must be no more than one storey above the prevailing heritage streetscape of Bay Street.
- New buildings that have a street wall height of two storeys in Bay Street and 3 storeys in Lyons Street, and a maximum overall height of 4 storeys.
- Street walls along Bay Street that have parapet heights of a minimum of 8 metres and maximum of 9.5 metres.
- Greater utilisation of the rear of heritage places. Development at the rear of 'significant' and 'contributory' heritage places must not be visible within a viewline taken from the opposite side of Bay Street (at 1.6 metres).
- South of Bridge Street, floor levels above a two storey street wall to Bay Street must not be visible within a viewline taken from the opposite side of Bay Street (at 1.6 metres above street level).
- Provision of active frontages to Bay Street at ground floor level in new buildings.

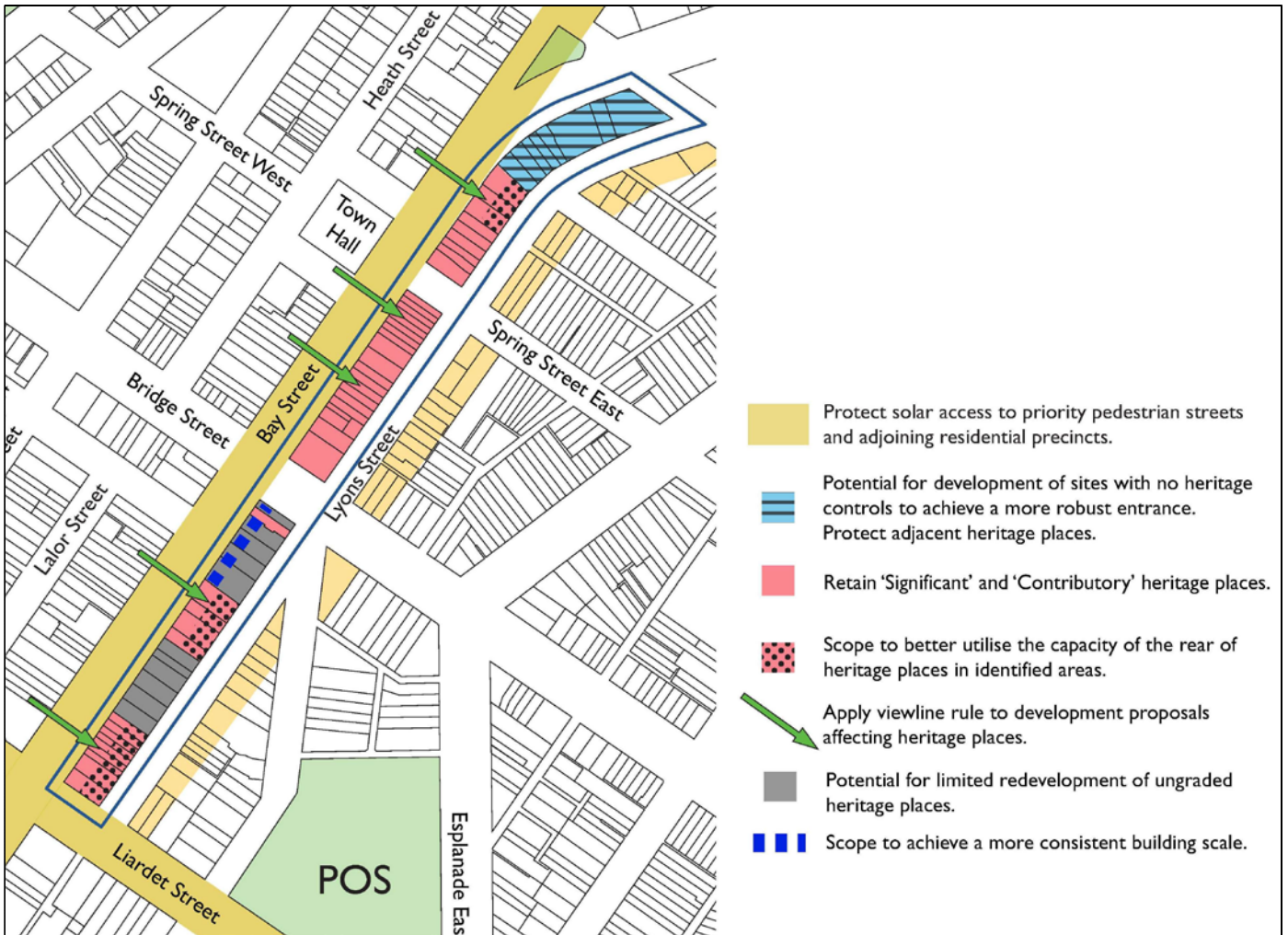


Figure 14: Proposed built form outcome



Figure 15: Potential built form outcome in Bay Street, south of Bridge Street.



Figure 16: Proposed built form outcome in Lyons Street, viewed from Bridge Street

Opportunity Precinct 4: Bay Street Southern Gateway

1. Precinct Description

This precinct is the southern gateway to the activity centre and includes the public realm of Bay Street, from Rouse Street to Bay Street.

The Precinct sits in the context of the substantial mixed use development which has occurred south of Graham Street. The building scale within the Precinct itself is low and sees a concentration of iconic heritage places, especially on the eastern side of Bay Street. A public housing estate dominates the streetscape on the western side.



Figure 17: Precinct location plan

2. Future Strategic Role

The future potential of the precinct is to support street activity and connections, which leverages off its location adjacent to the foreshore area, through land use change and public realm improvements. The land use focus will be on cultural, tourism leisure and retail activity that supports visitation and encourages pedestrian movement between the foreshore and the core retail area. Heritage places will be protected and play a role in activating and creating the character of the precinct.

3. Precinct outcomes sought:

- The precinct evolves into a vibrant gateway and pedestrian environment, supported by cultural, tourism, leisure and retail based (including cafes and restaurants) land uses.
- Active frontages will be encouraged in new buildings, and through sensitive alterations to heritage places (such as window or door openings).
- The retention, adaptation and reuse of significant Heritage Places convey the important industrial waterfront history of Port Melbourne to visitors and locals.
- Any future development will be of a high quality contemporary design that enhances the gateway environs and is respectful of adjacent heritage places.
- Bay and Beach Streets will be enhanced as priority pedestrian spines, including incorporating historical themes into the design of the public realm.
- Traffic speeds will be reduced and through traffic, including freight, will be discouraged.

4. Implementation

Land use

Land use outcomes can be achieved through retention of the existing Mixed Use Zone.

Local Planning Policy will be applied to:

- Encourage active uses which have a leisure / tourism focus; including indoor leisure and recreation, tourism (hotel or function centre), cultural uses (such as exhibition or gallery space), and retail (such as cafes / restaurants and shops).
- In the medium to longer term, redevelopment of the social housing site (western side of Bay Street) has potential to create an active retail edge at ground level to address Bay Street, whilst retaining social housing above and behind.
- Working with key stakeholders and landowners to secure preferred outcomes and renewal of sites will also occur.

Built Form

- Local Planning Policy, and the existing Heritage and Design and Development Overlays, will be used to provide built form guidance in this precinct.
- The preferred built form outcome is a reinforcement of the existing low building scale through any new development. There are limited opportunities for further development in the short term.
- Policy will encourage active frontages at ground level, sensitive alterations to heritage places to improve activation, and new development that is respectful of adjacent heritage places.
- The existing Heritage Overlay (HO1) will be replaced with site-specific Heritage Overlay controls, to reinforce the important of remaining individual heritage buildings.
- A revised Design and Development Overlay (DDO1) will maintain building height controls, and strengthen design objectives and requirements that prioritise heritage considerations.

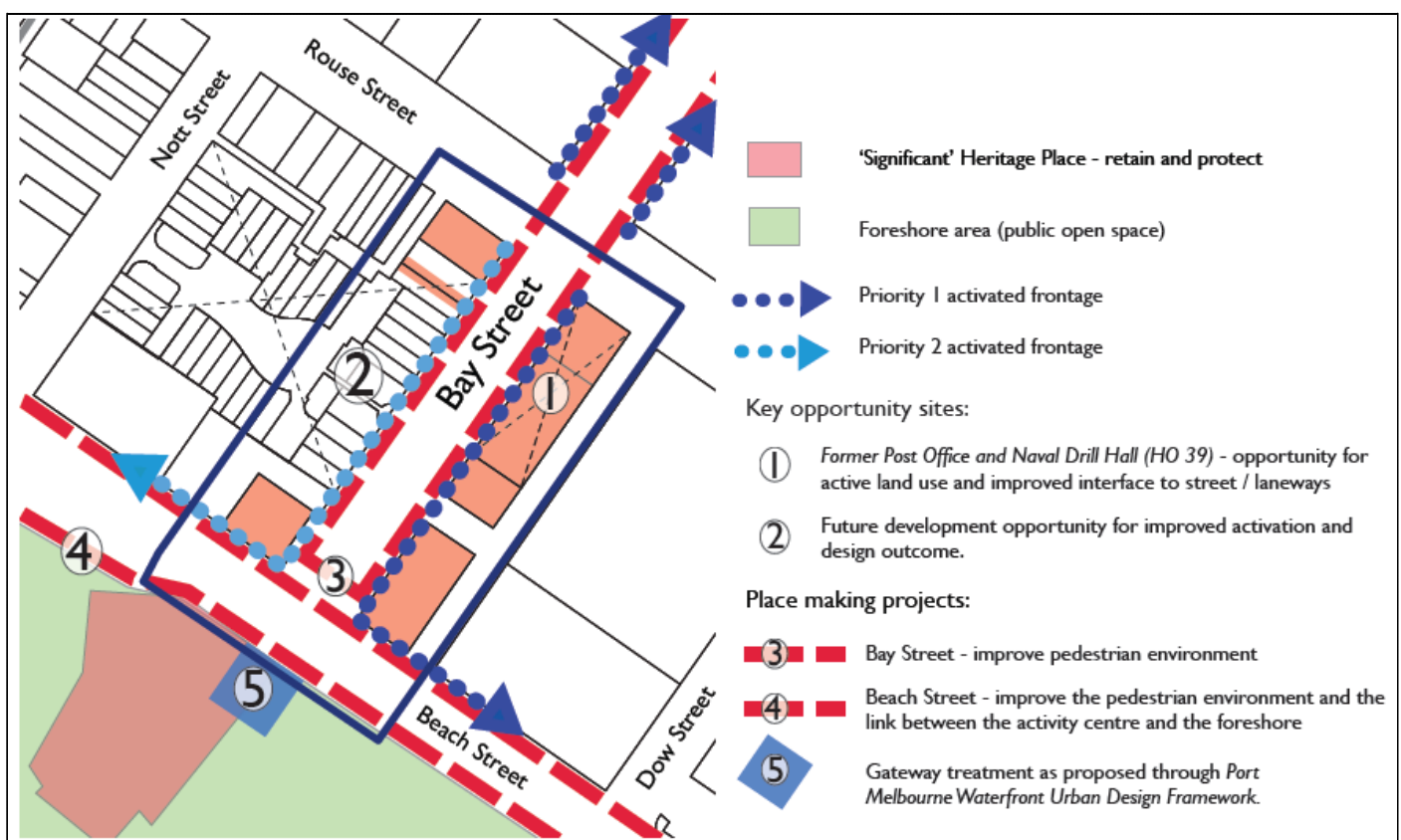


Figure 18: Proposed land use and public realm outcomes

Key opportunity sites

Former Post Office and Naval Drill Hall (40 Bay Street)

After a long tenancy by Circus Oz, these two heritage buildings will soon be vacated providing an opportunity for a strongly active land use to establish and assist in realising the broad activation of the gateway precinct. There is also potential to improve the contribution that the buildings make to the street through the removal of unoriginal elements and creation of sensitive openings at ground level. Potential land uses could include café/restaurant, education or community purposes.

Public Housing Estate (7 – 33 Bay Street)

The estate has medium to long-term redevelopment potential. Future development of the site should incorporate an active frontage to Bay Street. Land uses should include active uses at ground level, and also retention of the existing supply of social housing. This well-located site has then potential to accommodate larger premises such as a supermarket (subject to demand).

ESTABLISHED RESIDENTIAL PRECINCTS

Introduction

The residential areas to the south-east and north-west of the *Activity Centre* have a fine grain and low scale character, and are substantially affected by *Heritage Overlay 1 (HO1 – Port Melbourne)*. The structure plan aims to protect these areas, whilst providing for incremental housing growth in residential areas proximate to the activity centre that has existing diverse neighbourhood character.

There are four residential areas (*Residential Precincts 1 to 4*) that have capacity for incremental residential growth, based on a diverse neighbourhood character, which are identified on the *Established Residential Areas Housing Framework Plan*.

The following section comprises *Preferred Neighbourhood Character Statements* for these areas. Development must be in accordance with the 'Preferred Character Statement' and 'Design Response' contained in each statement.

Residential Precinct I: Garton Street



Above: north side of Garton Street.

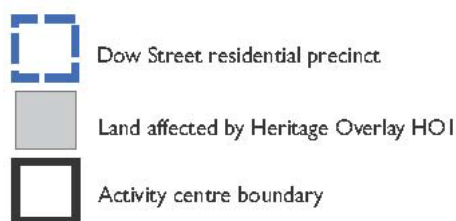


Above: south side of Garton Street.

Existing character / Future category	Future Development Statement	Preferred Character Statement
<p>Existing character type</p> <p>Victorian / Modern overbuilding</p>	<ul style="list-style-type: none"> The two sides of Garton Street present differing residential development opportunities adjacent to the Crockford Street renewal precinct. Future development will make a transition in height down from Crockford Street (which is anticipated to be up to 5 storeys) towards the low scale established streetscapes in Bay Street (1 and 2 storeys). Lots on the southern side of the Garton Street are larger and have an interface with bulky buildings (fronting Crockford Street and also in Garton Street) and have redevelopment potential. New development on the southern side will be constructed up to 3 storeys. Townhouse type development will reflect the existing grain and setback in Garton Street. The remaining warehouse building is also a development opportunity. The small lot size and consistent low building scale on the northern side of Garton Street will limit future development to single lot development, including single dwellings and additions to existing dwellings. Development on the northern side will respond to the existing low scale and intimate residential environment made up of 'contributory' properties outside of the Heritage Overlay (as shown on the City of Port Phillip Neighbourhood Character Policy Map). 	<ul style="list-style-type: none"> Existing Victorian dwellings in good condition will be encouraged to be retained particularly on the northern side. New buildings will reflect the existing streetscape grain and provide small front setbacks. Contemporary design responds to consistent existing streetscape elements, including verandahs, fascia lines and fence heights. Development on the southern side will be transitional in scale of 3 storeys (with scope for a 4th level if recessed from view). Townhouse style buildings will be well articulated with punctuated openings and balconies to the street. Redevelopment of the existing warehouse building will reinstate a residential land use and generally match the height of the existing building on the same site. Development on the northern side will respond to the existing low building scale and front setbacks and be limited to two storeys. Development adjacent to heritage places will be transitional in height and setbacks. Garages, carports, car parking spaces and vehicle access within the front setback are strongly discouraged.
<p>Future residential change category</p> <p>Incremental change</p>		

Design Response	
<i>Frontage widths:</i>	Match existing widths (range of 4 - 6 metres.)
<i>Siting and setbacks:</i>	<u>North side:</u> Match existing front setbacks (3 metres minimum).
	<u>South side:</u> 2 metres.
<i>Building height:</i>	<u>South side:</u> 3 storeys (any 4th level must be recessed from view).
	<u>North side:</u> 2 storeys.
<i>Design detail:</i>	<p>Reflect key horizontal lines within the street.</p> <p>Discourage vehicle access within front setback.</p> <p>Surveillance of street from all levels.</p> <p>Low scale fencing (0.8 – 1.2 metres).</p>
<i>Landscaping / planting:</i>	Low scale planting within front setback.

Residential Precinct 2: Dow Street (north)



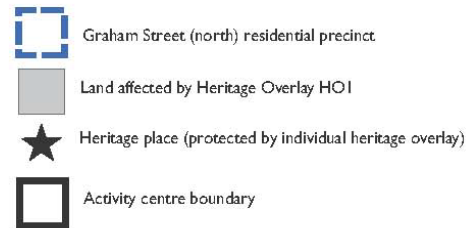
Above: Dwellings in Dow Street (west and east).

Above: Esplanade West.

Existing character / Future change category	Future Development Statement	Preferred Character Statement
Existing character type Victorian / Edwardian / Modern overbuilding	<ul style="list-style-type: none"> The varied character in the precinct provides scope for development which responds to existing identified character elements, and adjacent heritage places. New development will predominantly be in the form of new dwellings and extensions to existing dwellings. Multi-unit development will be limited and occur on the few larger remaining lots, or consolidated lots. Development will be 2 storeys in height, reference key streetscape elements, and continue predominant front setbacks. Surveillance of the street is incorporated at ground and first floor level. 	<ul style="list-style-type: none"> Further residential development to a high architectural standard responds to the existing pattern of development and nearby building styles. 'Contributory' properties outside of the Heritage Overlay (as shown on the City of Port Phillip Neighbourhood Character Policy Map) are encouraged to be retained. Development maintains consistent streetscape elements, including low scale buildings, small and medium sized front setbacks, and low fencing.
Future residential change category Incremental change	<ul style="list-style-type: none"> In Dow Street, the existing weatherboard dwellings will contribute to a visually interesting precinct, with new building forms emerging. In Esplanade West, continued modern development sits alongside characteristic original weatherboard dwellings. Dual front setbacks in response to the diagonal alignment of the street are continued, and dwellings overlook Lagoon Reserve to the east. 	<ul style="list-style-type: none"> The character of the precinct will be maintained and enhanced through respecting existing setback arrangements and tree planting. In Esplanade West, the small to medium dual setbacks are continued, and verandah and roof elements are referenced. Low fencing and planting is provided. In Dow Street, the strongly prevalent hipped roofs and horizontal lines created through verandah and fascia elements are reflected in new buildings. 3rd floor levels will be discouraged, unless substantially hidden from view. Development adjacent to heritage places will be transitional in height and setbacks.

Design Response	
<i>Frontage widths:</i> Match existing, as follows:	<u>Esplanade West:</u> minimum 6 metres.
	<u>Dow Street:</u> range of 5 – 7 metres.
<i>Siting and setbacks:</i> Match existing, as follows:	<u>Esplanade West:</u> 4 metres; or 2 metre and 5 metre dual setbacks for properties fronting the diagonally aligned section of Esplanade West.
	<u>Dow Street:</u> approximately 3 metres.
<i>Building height:</i>	Two storeys.
<i>Design detail:</i>	<u>Esplanade West:</u> Incorporate verandahs and street surveillance, brick and weatherboard materials, low fences with low planting behind.
	<u>Dow Street:</u> Reflect horizontal elements (eg: verandahs and fascia lines) and hipped roofs, and incorporate brick and weatherboard materials and low / medium fences of open design.
<i>Landscaping / planting:</i>	Low / medium scale planting in the front setback.

Residential Precinct 3: Graham Street (north)



Above: Princes Street



Stokes Street



Graham Street

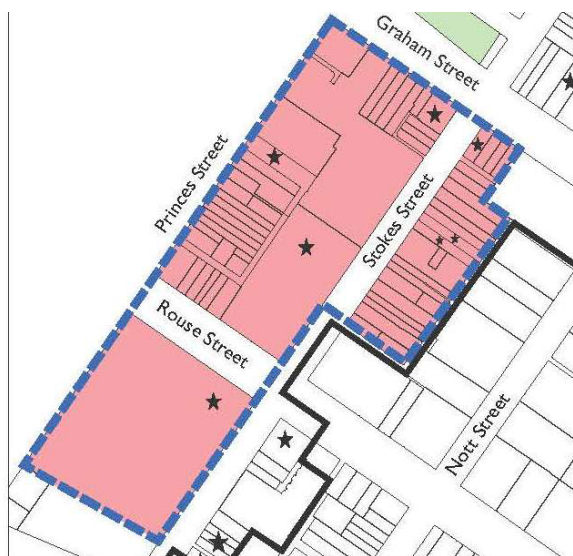





Nott Street.

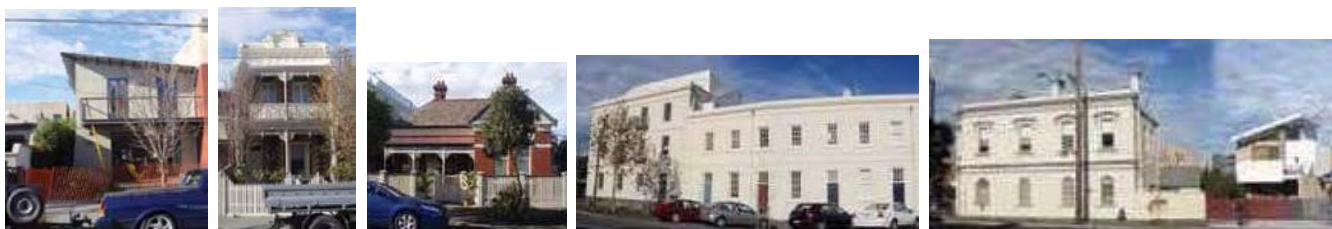
Existing type / Future change category	Future Development Statement	Preferred Character Statement
<p>Existing character type</p> <p>Victorian / Edwardian / Modern overbuilding</p>	<ul style="list-style-type: none"> Each street in this precinct has differing established and emerging character attributes which will set the context for future development. Development will respond to the identified character elements of each street, as well as heritage places. The precinct is well located in proximity to the light rail and also the core retail area, which will support incremental intensification. There is scope for further well designed infill development of up to two storeys on remaining larger lots, particularly in Princes Street where townhouse style development is predominant, and also in Nott Street. Properties in Graham Street have generous proportions and can accommodate some development that respects heritage and 'contributory' properties outside of the Heritage Overlay (as shown on the City of Port Phillip Neighbourhood Character Policy Map). Streets with fine grain subdivision elements and small lots, such as Stokes Street, will experience limited development unless lot consolidation occurs. There are atypical taller built form elements of 3 and 4 storeys which act as poor references for future development. 	<ul style="list-style-type: none"> Contemporary development will continue to occur which is respectful of the scale, form and siting of period dwellings. Period dwellings will be encouraged to be retained. New development will respond to the established scale and setbacks of the precinct, and incorporate front setbacks to allow for planting that contributes to the streetscape and rear yard environs. Princes Street will accommodate further townhouse development, with small front setbacks and windows and balconies that overlook the street. Graham Street will accommodate sensitive new development, such as at the rear of period dwellings. Development in Stokes Street will reflect existing character elements, including front and side setbacks, verandahs and low fences. Turville Place and Nott Street will continue to accommodate further low scale multi-unit residential development. New residential development will appear as 2 storeys. A recessed 3rd level can be accommodated in new development fronting Princes and Turville Streets. The housing estate will be updated with contemporary materials and finishes. Any redevelopment will not result in additional height beyond that of the existing buildings.
<p>Future residential change category</p> <p>Incremental change</p>		

Design Response	
<i>Frontage widths:</i>	<u>Princes and Stokes Streets:</u> 5 – 7 metres.
	<u>Nott Street:</u> 6 – 9 metres.
	<u>Graham Street:</u> 6 metres.
<i>Siting and setbacks:</i>	<u>Princes Street:</u> 1 metre front setback.
	<u>Stokes and Nott Streets:</u> 1 - 4 metre front setback.
	<u>Stokes Street:</u> A 0.5 metre side setback.
	<u>Graham Street:</u> 3 – 4 metre front setback.
<i>Building height:</i>	2 storeys (unless specified).
	<u>Graham Street:</u> 3 storeys
<i>Design detail:</i>	<u>Princes Street:</u> Modern design; punctuated facades with openings to both floor levels.
	<u>Stokes Street:</u> Reflect existing elements including gable roof ends, verandahs, low fences.
	<u>Nott Street:</u> Low – medium height fencing.
<i>Landscaping / planting:</i>	<u>Princes Street:</u> Minor decorative planting.
	<u>Stokes & Nott Streets:</u> Low –medium height planting.
	<u>Graham Street:</u> Medium height trees.

Residential Precinct 4: Princes and Stokes Streets



-  Princes and Stokes Streets residential precinct
-  Heritage place (protected by individual heritage overlay)
-  Activity centre boundary



Above: (from L to R) dwellings in Stokes Street, Princes Street, Rouse Street, Graham Street.

Existing type / Future change category	Future Development Statement	Preferred Character Statement
<p>Existing character type</p> <p>Victorian / Edwardian / Interwar Modern overbuilding</p>	<ul style="list-style-type: none"> • The precinct is on the edge of a former industrial area that has made a transition to high density residential development. Some medium density development has occurred within the precinct. • The precinct has little capacity for further medium density development. Residential lots are typically small in size and will limit further development in the precinct. • Stokes and Princes Streets will retain a low scale and fine grain of development, supported by low fences and low planting that contributes to an intimate residential environment. 	<ul style="list-style-type: none"> • Period dwellings, including ‘contributory’ properties outside of the Heritage Overlay (as shown on the City of Port Phillip Neighbourhood Character Policy Map), are encouraged to be retained, to protect the low scale, fine grain character of the precinct. • Significant and contributory heritage places are retained. • The existing rhythm of development is reinforced through new development. • A consistent building scale is achieved for each street section, and changes in scale part or mid-way are strongly discouraged.
<p>Future residential change category</p> <p>Incremental change</p>	<ul style="list-style-type: none"> • Future development will predominantly be in the form of sensitive additions to existing dwellings, new dwellings on individual lots as they become available, and well-designed low scale medium density development on larger single or consolidated lots. • Rouse and Graham Street have some further propensity for change, comprising larger lots and short street sections that have disparate character elements. • Heritage places will continue to play a key role in underpinning the unique fabric of the precinct. • Intensive development of small individual lots is not envisaged as part of the future character of the precinct. • Any further development of the Swallow and Ariel warehouse is set well back from the façade, is limited in height, and visually recessive. 	<ul style="list-style-type: none"> • New development responds to the existing characteristics of the precinct, and responds to low scale adjoining properties. • New development responds to the setbacks of adjoining properties, and responds to key character elements including the fine grain and characteristic street interface of development, such as setbacks, verandahs, and low fencing. • Crossovers, car parking and associated structures will be avoided in the front setback. • First and second floor levels are setback from the frontage to limit their visibility. • Building scale does not exceed two storeys in Princes Street, and does not exceed three storeys in Stokes Street.

Design Response	
<i>Frontage widths: Match existing, being:</i>	<u>Princes & Stokes Streets:</u> 5 – 9 metres.
	<u>Rouse & Graham Streets:</u> 6 – 7 metres.
<i>Siting and setbacks:</i>	<u>Princes Street:</u> 3 – 6 metres.
	<u>Stokes Street:</u> 1 – 4 metres.
	<u>Graham Street:</u> 6 metres.
<i>Building height:</i>	<u>Princes Street:</u> 2 storeys.
	<u>Stokes Street:</u> 3 storeys.
	<u>Graham Street:</u> 3 storeys
<i>Design detail:</i>	<u>Precinct wide:</u> Respond to fine grain streetscape rhythm, low building scale, planted front setback areas.
	<u>Princes Street:</u> Single storey elements, verandahs, low – medium fencing.
	<u>Stokes Street:</u> Verandahs and balconies, low fencing, brick and weatherboard materials.
<i>Landscaping / planting:</i>	Low to medium size planted front setbacks.

PLACE MAKING PROJECTS

There are five precincts that contain a collection of ten site specific place making projects that should be implemented to partially achieve the objectives of *Strategic Direction 6: An Integrated Public Realm*. The projects are designed to improve walking links, or to improve key public spaces within the Activity Centre.

This section sets out the purpose and design approach for each project. The location of each project is shown below.

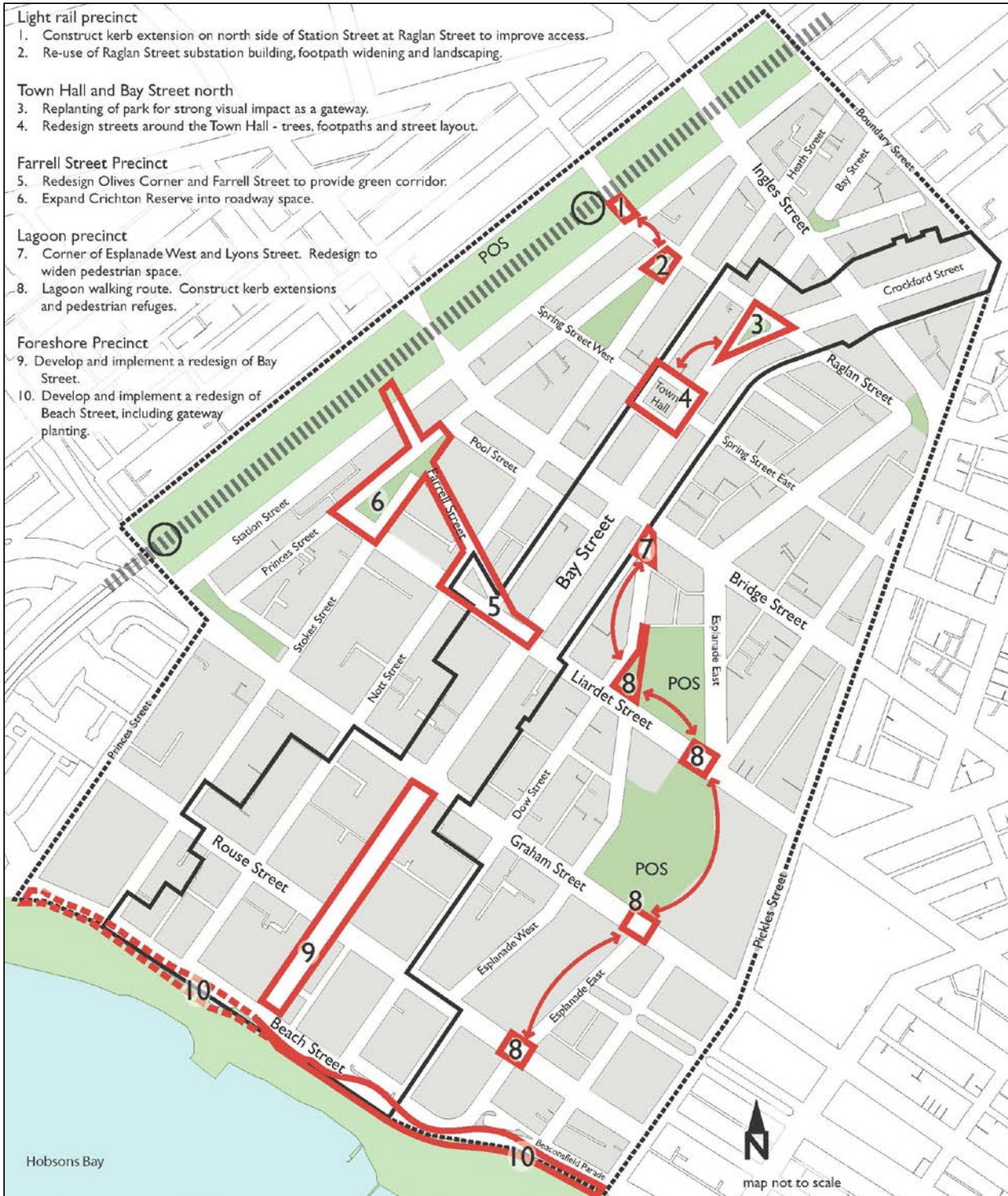


Figure 19: Place Making Projects Framework Plan

Light Rail Precinct

A hub of local activity is based around the light rail stop near Raglan Street. The historical role of this area is visible in buildings near the corner of Raglan and Station Streets, including the Railway Club Hotel and a former corner grocer. Also nearby are the Port Melbourne Bowling Club and a small playground in Sangster Reserve.

Important for its continuing role as a public transport node, key issues to address include safe access to the Raglan Street stop, and supporting its role as a focal point for the neighbourhood.

Works are recommended for:

- The pedestrian crossing of Station Street at the end of Raglan Street.
- The defunct Sangster Reserve substation.

I. Light rail access at Raglan Street

The Raglan Street road reserve extends to the light rail line and until recently the section between Station Street and the light rail was used as a car park. The car park south of the light rail was removed recently and the area grassed over. Low stone walls flanking the roadway (matching walls at Ingles Street and at the intersection of Bay and Raglan Streets) were retained after this work, and the ground levels still define the former roadway as a distinct space in the light rail reserve.

The integration of the reserve and access to the light rail can be improved by addressing the pedestrian crossing of Station Street at Raglan Street. With closure of the car park, the parking lane on the north side of Station Street was extended across Raglan Street. Pedestrians crossing Station Street now step out from between parked cars. Although traffic on Station Street is generally light, this is an unsafe arrangement.

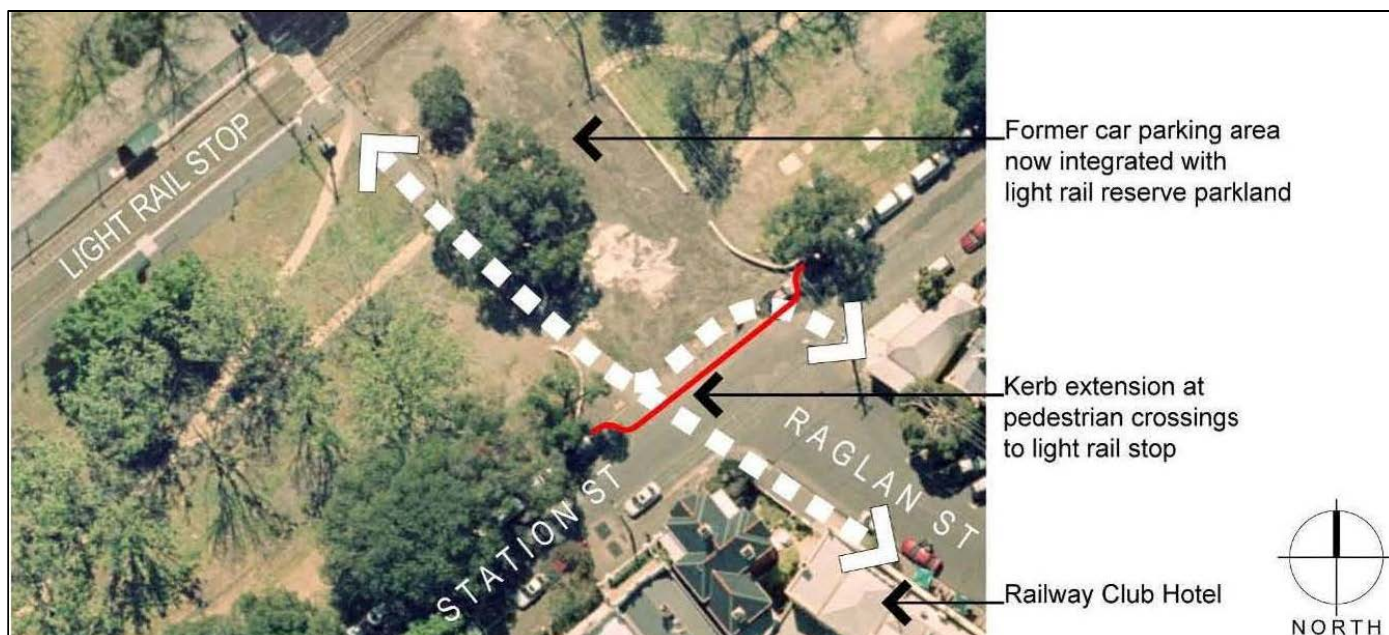
Strategic direction

Works should be undertaken to improve the safety and convenience of pedestrian access to and from the light rail reserve.

Actions

Construct a kerb extension across the parking lane on the north side of Station Street opposite Raglan Street.

Review street lighting and ensure that the crossing is lit to a high standard.



Above: Proposed kerb extension.

2. Raglan Street Substation

Sangster Reserve, at the north end of the Port Melbourne Bowling Club, is separated from Raglan Street by a defunct substation. The substation is a simple design with Art Deco detailing and appears to be in good condition, offering potential for adaptation and re-use.

Nott and Princes Streets feature angle parking on one side only (next to the bowling club), leaving large areas of surplus road space near the substation and Raglan Street. Footpaths around the substation could be widened with no substantial loss of parking spaces or traffic capacity.

Strategic direction

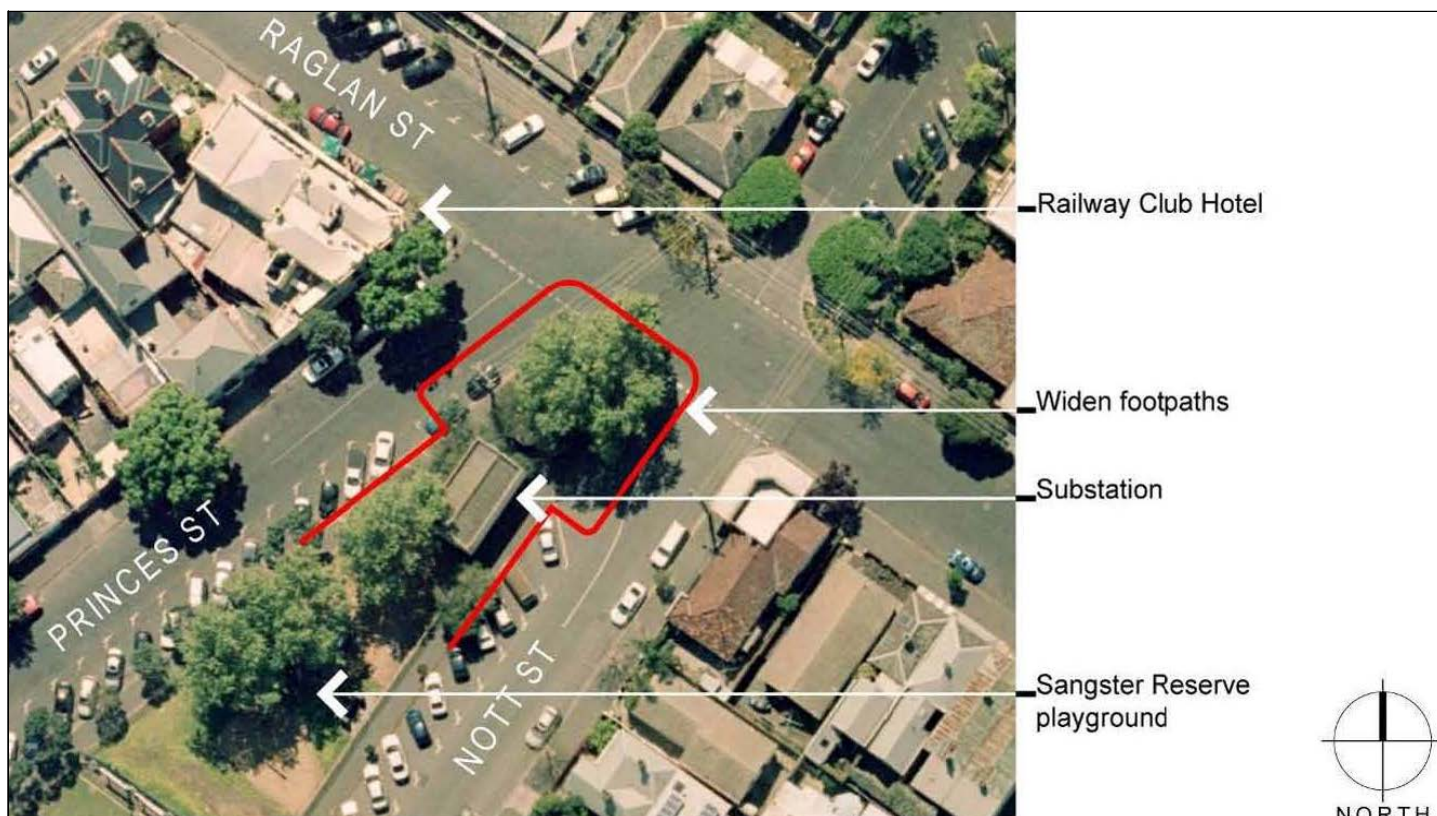
The substation and surrounds should be used to enhance pedestrian safety and convenience and to create an attractive and active small public plaza linked to the substation and Sangster Reserve.

Actions

Explore potential uses of the building, including community uses, and the scope for altering and adapting it to support new uses. Uses could relate to the adjoining playground as an indoor/outdoor play space, or it could serve as a kiosk catering to parents while they supervise children, or it could be used separately to provide greater diversity of activity in this area such as an artist's studio.

Prepare a detailed scheme for building conversion when a use is determined.

Widen footpaths around the substation to reduce crossing distances and define angle parking along Nott and Princes Streets, and prepare a landscape plan that integrates the treatment of the space around the substation with the widened footpaths. Thinning of the shrubby plantings is recommended to improve visibility and safety, and public space lighting should be addressed as part of the scheme.



Above: Substation and proposed streetscape works

Town Hall and Bay Street North

The northern end of Bay Street is quieter than the central retail area near Liardet Street, as its south-west side north of Bridge Street is predominantly a low rise residential area. However, this area is especially important to the identity of the activity centre and Port Melbourne. The Town Hall and Library are civic and community focal points, and are central to the character of the northern entrance to the Bay Street retail strip travelling from the direction of the CBD.

Two projects in this area are:

- Improvements of the Crockford Street entry to Bay Street at its intersection with Raglan Street.
- Redesign of streetscapes around the Town Hall to improve views of the building and to support street activity adjacent to retail premises around it (and also the Town Hall).

3. Gateway planting at Crockford Street entry

The existing treatment at this intersection includes a raised planter in the large traffic island, which appears to date from the 1940s or 50s. The stone walls match walls at street crossings of the light rail reserve. The raised bed provides an effective but unobtrusive visual anchor for the island. The planting is mixed, with various small trees (including Chinese elms and Gleditsias), roses and some low shrubs.



Above: The Fountain Inn Hotel in 1959, showing the raised planter at the intersection of Bay and Raglan Streets. (Photo by Colin Caldwell, 1962, La Trobe Picture Collection, State Library of Victoria).

A more cohesive and high-impact planting theme using a single tree species linked to the Bay Street streetscape would provide a more effective gateway to the activity centre, for example replacement of all trees in and around the island with *Washingtonia* palms. Continued planting of roses in the raised island would be appropriate but with simplification of other plantings to a single hardy low shrub or ground cover species.

No changes to the raised bed are recommended, although reinstatement of the former pipe railings originally set atop the walls may be desirable to provide additional detail.

Strategic direction

The Crockford Street entry to Bay Street is an important 'entrance' to the activity centre and should be improved with a design that has a strong impact at this scale and provides for shaded spaces. There may be scope to integrate works to improve the pedestrian environment near the bus stop and improve safety of the crossing of Crockford Street.

Action

Prepare and implement a detailed plan for replanting the intersection including selective removal of existing trees, new trees, and renovation of understorey plantings.

4. The Town Hall Precinct

The Town Hall is sited to dominate views where Crockford Street turns into Bay Street, as well as providing a focal point along the axis of Spring Street. This is a subtle and very fine example of townscape composition within a grid street pattern. It is effective at knitting Port Melbourne’s civic focal point into the activity centre. It is an iconic building that merits careful treatment, but parking bays and street trees now clutter the Town Hall’s facades – and in places almost hide them.

The existing design also provides the widest footpaths next to the Town Hall. A more sensitive plan would place parking bays, widened footpaths and trees on the opposite sides of Spring and Heath Streets, where the space could be used by adjoining businesses to help animate the street. It could be possible to create larger pedestrian spaces with a full road closures beside the Town Hall, but this would result in more extensive loss of parking and the advantage of a larger pedestrian space in this area is uncertain.

Strategic direction

The streets around the Town Hall should be redesigned to respect its important visual role in relation to the surrounding area, and to support animation of the streetscape based on activities in nearby buildings. Existing vegetation should be pruned to improve the prominence of the Town Hall building.

Action

Undertake a comprehensive redesign of streets surrounding the Town Hall.



Above: Concept for streetscape works.

Farrell Street Precinct

The intersection of Liardet and Bay Streets has long been a focal point of Port Melbourne. The adjacent Coles supermarket occupies the internal hall of the former Port Melbourne Market Buildings, and the Liardet Street Community Centre occupies the former Temperance Hall.

Farrell Street is as central to this area as Liardet Street. Like Esplanade East and West, the angle Farrell Street strikes across the otherwise rectilinear street grid reflects the underlying landscape. In effect a shallow branch of the former lagoon south-east of Bay Street, the block between Farrell Street and Pool Street was sufficiently low-lying to set it apart during early development but was eventually used for the Nott Street State School (now Port Phillip Specialist School). Farrell Street is also significant as a link to and across the light rail reserve, whereas Liardet Street terminates at Princes Street.

Still regarded by many as the centre of the activity centre, this area is important in providing access to major facilities and services in the centre and as a focus for community activity within the school, community centre and public open spaces including Olives Corner.

Two potential projects that would improve the area are:

- A redesign of Olives Corner – including reconfiguration of the intersection of Liardet and Farrell Streets, and the parking in Farrell Street between Lalor and Nott Streets.
- Extension of a bicycle and walking route along Farrell Street across Crichton Reserve to the light rail reserve together with expansion of Crichton Reserve to take up superfluous road space around it.

5. Olives Corner and Farrell Street

Olives Corner is a public plaza at the five-way intersection of Liardet, Farrell and Lalor Streets that acts as a local meeting place near the community centre.

The plaza was created by closing the end of Farrell Street while keeping Liardet and Lalor Streets open. This makes sense in response to vehicular traffic, as the major destination is the Coles car park entry. However, from a wider perspective Farrell Street is important for access to the Port Phillip Specialist School, light rail reserve and areas beyond that. The design of the plaza and the treatment of the section of Farrell Street to the west as a car park now obscure this link.

The condition of pavements and construction in the plaza is poor and creates trip hazards, and the plantings are of no particular significance. There are no active frontages onto it and it is hidden around the corner from Bay Street, reducing opportunities for people-watching and passive surveillance. It may be possible to close Lalor Street at Liardet Street, and to use this space in the redesign of Olives Corner.

Strategic direction

Olives Corner is an important meeting space and part of a pedestrian link between Bay Street, the community centre and light rail reserve, which merits a thoughtful upgrade and more responsive development on adjoining sites.

Actions

Undertake a comprehensive redesign of Olives Corner and Farrell Street extending through to the light rail reserve, considering the potential to increase pedestrian space and crossing facilities by road narrowings or partial closures near the plaza.

Encourage adjoining redevelopment to address Olives Corner.

6. Crichton Reserve

Crichton Reserve has been expanded at its north-east end with the closure of Farrell Street and Stokes Street near the Port Phillip Specialist School. However, the added green space is too small to be of use for recreation, and there is still considerably more area within the surrounding roadways than required to meet local traffic requirements. The Liardet Street median to the south-west of the reserve similarly provides greening but no useful recreation space.

The overall appearance of the public realm in this area is fragmented and dominated by road paving, which is not well used due to inefficient parking layouts around the reserve. To the south, the parking in Farrell Street along the school is laid out efficiently in right angle bays, but the trees between the parking bays are in poor condition.

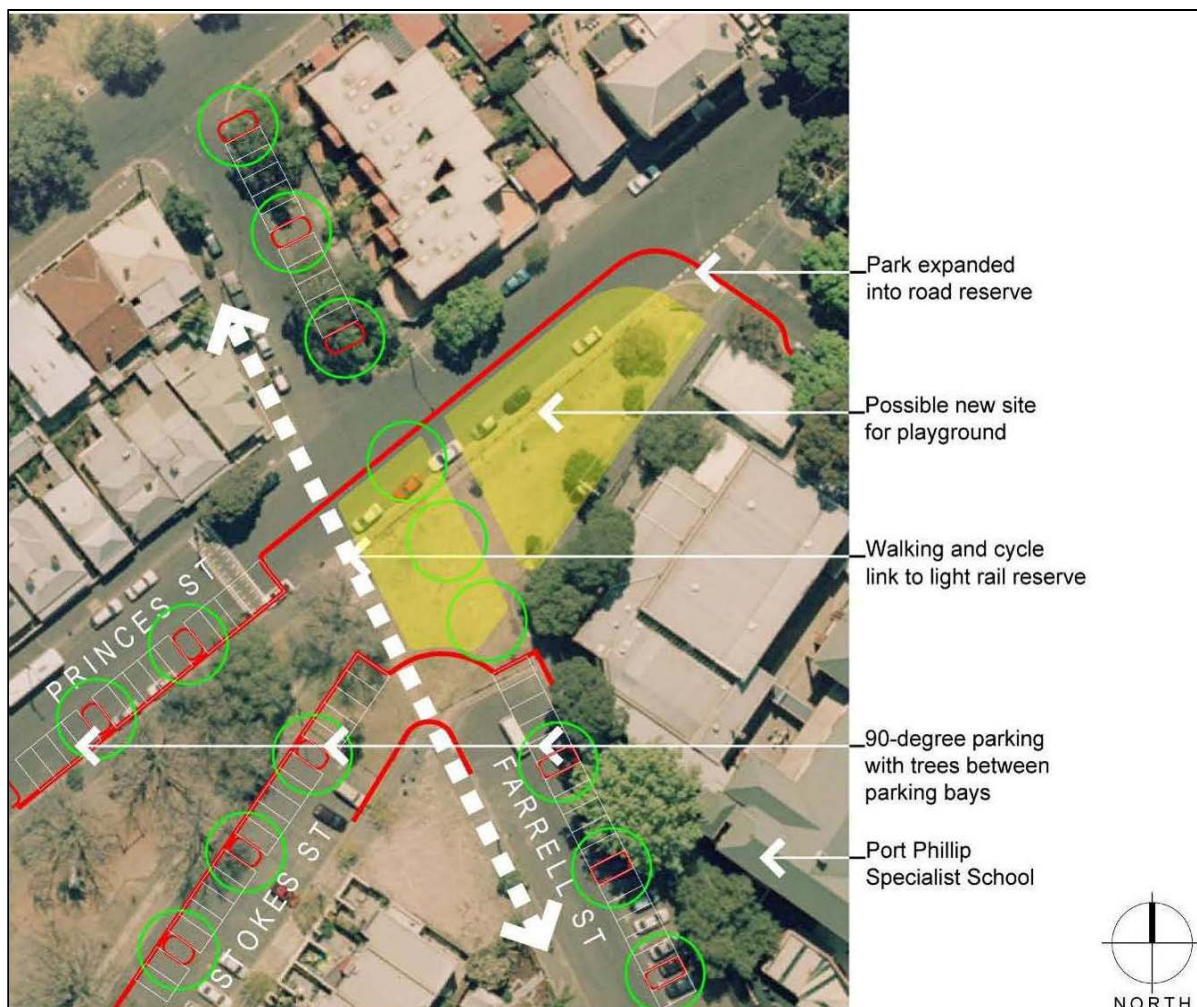
Strategic direction

Farrell Street and surplus road space around Crichton Reserve have potential to provide a better local open space area, high quality pedestrian and cycle access, and to support urban forest objectives to increase canopy cover and create more permeable surfaces.

Actions

Develop a design for Crichton Reserve that expands the open space using surrounding surplus road space, provides a walking and cycle link along the axis of Farrell Street, and rationalises surrounding parking layouts. Include consideration of possible reorganisation of features within the reserve as well as other potential uses.

Replace the trees in Farrell Street along the school, using an improved planting pit that incorporates WSUD principles.



Above: Concept plan for Crichton Reserve and Farrell Street.

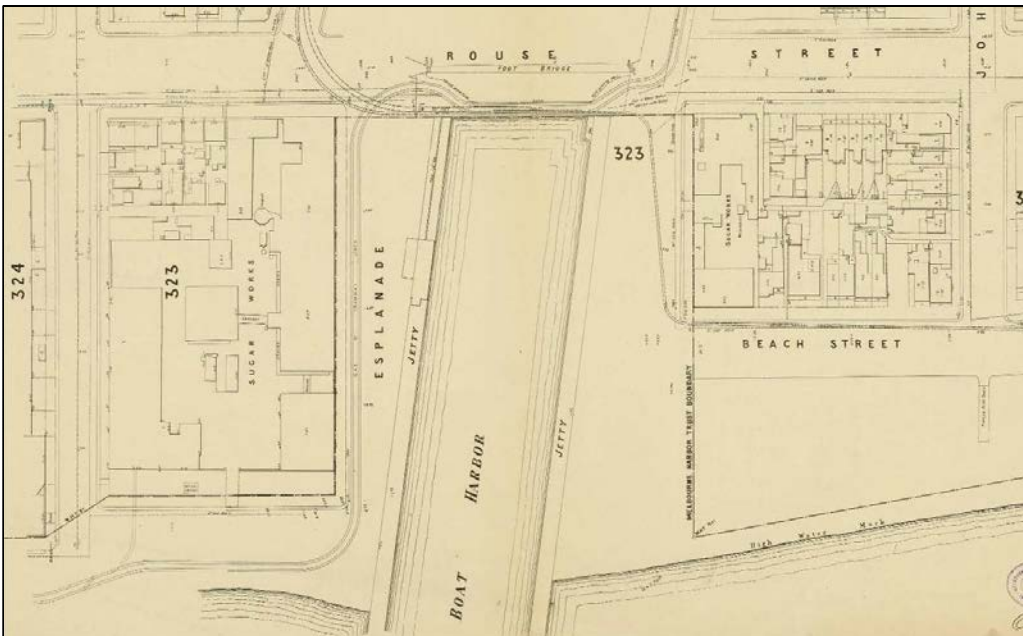
Lagoon Precinct

Port Melbourne once had two waterfronts. The 'Esplanade' followed both, running along the beach and then turning inland along the lagoon, which provided a protected small boat harbour. This second waterfront – one of the most significant landscape features of the area – is now gone but its footprint is marked by Esplanade East and West, Lagoon Oval and Edwards Park, and its entry from the bay is still marked by Lagoon Pier.

These provide a number of important local recreational spaces and, together, still provide continuous walking routes that connect through the activity centre to the foreshore along the route of the former lagoon.

Key projects in this area include:

- Development of a coordinated planting scheme for the lagoon area to reinforce the sense of a single landscape feature extending through the activity centre – including the major parks as well as the Esplanade East medians.
- Redesign of the Esplanade West / Lyons Street intersection to improve pedestrian safety and provide a greening opportunity.
- Improvement of pedestrian crossings at Esplanade East / Liardet Street and Esplanade East / Graham Street, and Esplanade West / Liardet Street.



Above: Detail from 1895 MMBW map showing the entry to the lagoon from the bay.

7. Esplanade West and Lyons Street

The intersection of Lyons and Esplanade West near Bridge Street is a very broad hard surfaced area that presents overly long pedestrian crossings from point-to-point, and little relief through roadside planting. It would be possible to take up superfluous road space at various corners, potentially through a series of kerb outstands, to improve this environment.

Strategic direction

The intersection Lyons Street and Esplanade West should be redesigned to reduce superfluous roadway areas to provide improved pedestrian spaces and introduce planting areas. Consideration should be given in the design process to referencing the original alignment of the former lagoon.

Action

Undertake a redesign of the Esplanade West and Lyons Street intersection.

8. Lagoon walking route

Recent streetscape works in Graham and Liardet Streets are inconsistent in provisions for pedestrian crossings, with kerb outstands in places but not others.

Strategic direction

A consistently high standard of pedestrian crossings should be provided throughout the lagoon precinct, especially along Esplanade East and West.

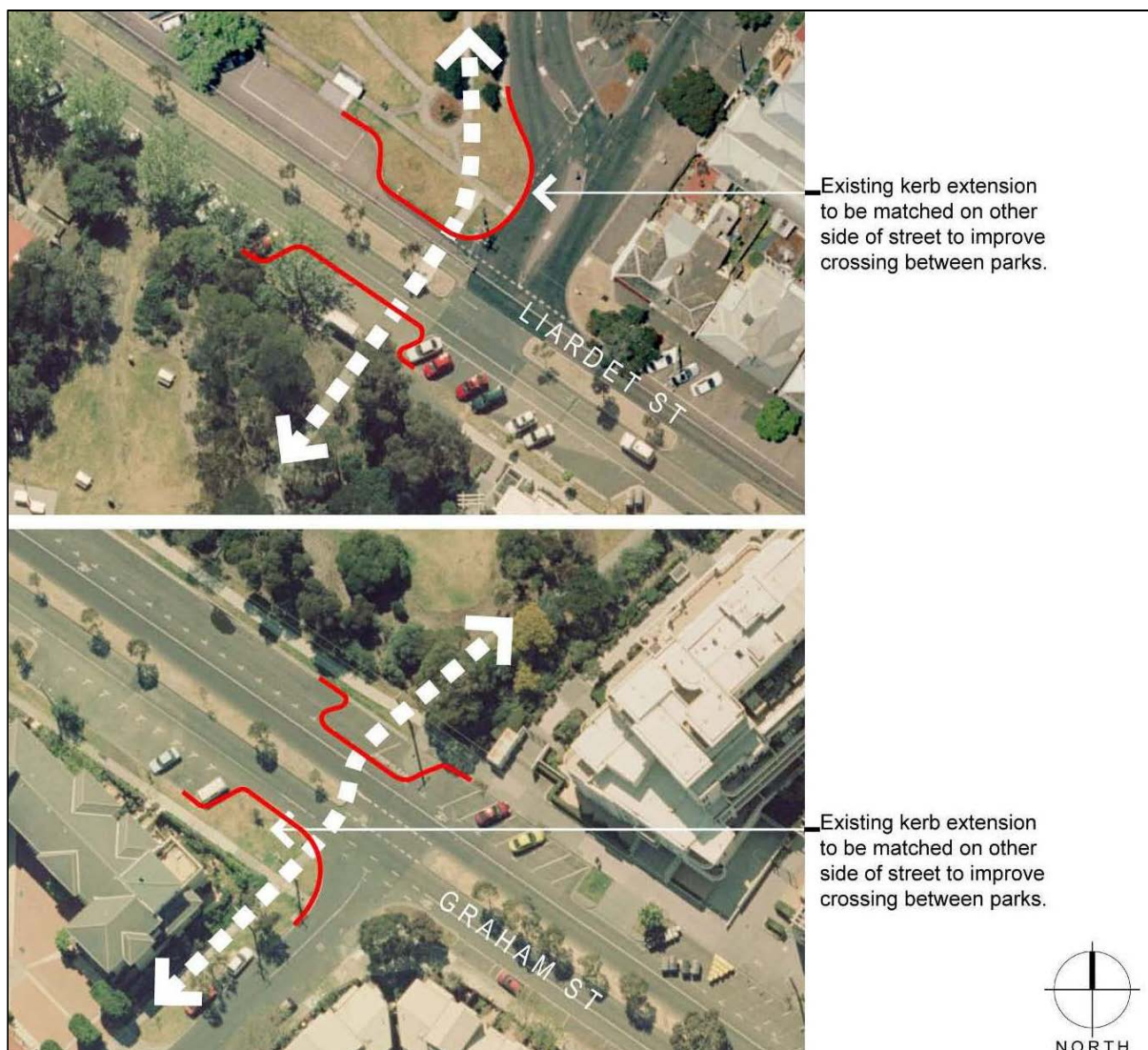
Actions

Construct a kerb extension into Liardet Street in line with the footpath along Esplanade West. Reorganise parking in Dow Street at this corner to provide right angle parking along the side towards Edwards Park to replace parking bays in Liardet Street eliminated by the kerb extension.

Construct kerb extensions on the alignment of Esplanade East at:

- The south side of Liardet Street
- The north side of Graham Street.

Construct kerb extensions and centre median pedestrian refuges in Rouse Street at Esplanade East.



Above: proposed kerb extensions at Liardet and Graham Streets.

Foreshore Precinct

Somewhat removed from Bay Street's core retail area, but at the heart of Port Melbourne in a historic and economic sense, the foreshore area brings great prominence to the activity centre. Whilst activity in the precinct has changed in character over time, it still contains a working port which is now focussed on interstate ferries and international cruises that draw tourists and travellers through the area, rather than being linked to warehousing and industries that once crowded around the piers.

Closure of local industries including a gasworks, a sugar mill, and a biscuit factory left several large underutilised brownfield sites. Many of these have now been redeveloped with apartment buildings and this former industrial area has contributed to significant residential growth, again making it a high-density focus of activities – although very different from those of the past. With change in land uses, complementary changes in the public realm are now appropriate.

Projects recommended for the area include:

- Redesign of Bay Street between Graham Street and Beach Street to significantly improve pedestrian capacity and amenity.
- Redesign of Beach Street between Esplanade East and Waterfront Place as one of the premier urban waterfronts in the Melbourne metropolitan area.

9. Bay Street between Graham Street and Beach Street

The works proposed in the *Development Contributions Plan* are generally appropriate but do not address Beach Street or Bay Street, which is a significant omission. Both streets are declared main roads and part of an over-dimensional truck route, which provide a passage for an obvious volume of heavy vehicle traffic; but in the context of the current traffic management regime severely constrains the immediate potential for public realm improvement.

However, in the context Bay Street being designated a major activity centre with a policy direction to improve the amenity of the centre, and the emergence of a high density local residential area and an active retail environment south of Graham Street, the presence of truck traffic and high levels of through traffic is undesirable, particularly at street level. The ability of Bay Street to connect to the foreshore is also undermined in this regard.

It is important the quality of the public realm along this section Bay Street is of a high standard to support its role as a priority pedestrian spine within the centre. In that context, options for Bay Street, in declining order of their potential to meet objectives for the area, may include:

- Option 1: Construct tree pits and localised kerb outstands in the existing parking lane at intersections and bus stops, to allow for relocation of obstacles such as bus shelters and other street furniture out of the existing circulation space, and to allow for street tree planting without eroding the available pedestrian space. This work should be designed to allow for future comprehensive footpath widening. (*Short term - assumes no change in traffic capacity is currently acceptable.*)
- Option 2: As per Option 3 but introduce peak hour clearways as required to maintain two traffic lanes mid-block only when needed. (*Short – medium term - assumes that a change in traffic capacity will be acceptable in off-peak traffic hours.*)
- Option 3: Widen footpaths consistently along the length of the street, relocate the parking lane and maintain a single through traffic lane mid-block with provision for turn lanes at intersections using the parking lane and indents into the median. (*Long term - subject to a SmartRoads network fit assessment, including considering the viability of alternate freight and traffic routes and consultation with all stakeholders.*)
- Option 4: No change. (*Assumes no change in traffic or parking capacity is acceptable.*)

Strategic direction

Beach Street and Bay Street are important spines within the mixed use redevelopment area as well as critical links between the Station Pier precinct and the established area of Bay Street to the north-east. The aim should be for both to be managed and improved as streets where the highest priority is assigned to pedestrian capacity, amenity and safety.

Action

Undertake a detailed study of design options for Bay Street between Graham and Beach Streets, including assessments of impacts and benefits and associated works necessary to deal with any re-allocation of through traffic functions.

10. The Beach Street esplanade

The Beacon Cove Precinct Committee was previously established to research the use, improvement and management of spaces along the waterfront, and published the *Port Melbourne Waterfront Revitalisation Report (2005)* which scoped a number of design options for Beach Street. The aim here was to increase space and amenity for walking, allow for a possible tramway, increase activated edges along the promenade, and strengthen Beach Street's identity.

The '*Port Melbourne Waterfront and Station Pier Precinct Vision Planning: 2006- 2010*' also explored opportunities to improve the area and proposed the aim of establishing 'a scale of landscape and active building frontages reminiscent of the place setting of a grand boulevard typology' for Beach Street. However, that aim is problematic without a definition of 'grand boulevard', and also since boulevards can tend to function as major traffic thoroughfares, which may be an undesirable objective for this foreshore area.

Both studies only deal with a segment of Beach Street rather than Port Melbourne's waterfront as a whole, and do not acknowledge differences between it and Beaconsfield Parade, both of which have differing and individually characteristics. Beaconsfield Parade is a corridor running parallel to the beach along Albert Park and Middle Park, but rather than flowing uninterrupted past Port Melbourne and St Kilda, its alignment and width change at each end. There is also substantial variation in built form between Port Melbourne and Albert Park.

Calling Port Melbourne's waterfront a 'grand boulevard' is not appropriate. Instead, the Port Melbourne and St Kilda esplanades form bookends to Beaconsfield Parade where the direction of movement turns ninety degrees and urban activity thrusts out to and even over the water.

It is more appropriate to address the waterfront area as a bookend 'esplanade' to Beaconsfield Parade that comprises all of Beach Street east of Princes Street, and the section of Beaconsfield Parade west of Pickles Street.

Strategic direction

The link between city and water is the essence of Port Melbourne's identity. Anything that cuts across and reduces the intimacy of this link – whether architectural gestures or through traffic – should be minimised and the space should be developed as an extension of Port Melbourne's public realm to the water.

Actions

Establish a vision for the esplanade that defines outcomes in terms of priorities for its use, relationships with its surroundings and the quality of experiences it provides to its users.

Examine circulation options to establish the minimal possible role for vehicular traffic on the esplanade, enabling traffic in Beach Street to be managed to suit local objectives rather than to support broader network links.

Establish a program to design and implement works that will enable the local traffic circulation network to support desired outcomes for the esplanade.

Develop and implement a design for the esplanade. The adopted *Port Melbourne Waterfront Urban Design Framework* contains specific initiatives for Beach Street and foreshore spaces west of Bay Street and these should be incorporated.

CITY OF PORT PHILLIP



BAY ST

City of Port Phillip

Private Bag 3

St Kilda VIC 3182

Tel: 9209 6777

web: www.portphillip.vic.gov.au

Email: assist@portphillip.vic.gov.au

